Agenda
Planning Commission
City Of Edina, Minnesota
City Council Chambers

Thursday, April 11, 2024
7:00 PM

Watch the meeting on cable TV or at EdinaMN.gov/LiveMeetings or Facebook.com/EdinaMN.

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Call 312-535-8110
Enter access code 2633 139 0260
Password is 5454
Press *3 on your telephone keypad when you would like to get in the queue to speak
A staff member will unmute you when it is your turn

I. Call To Order

II. Roll Call

III. Approval Of Meeting Agenda

IV. Approval Of Meeting Minutes

A. Regular Meeting Minutes and Special Work Session Minutes from March 27, 2024

V. Community Comment

During "Community Comment," the Board/Commission will invite residents to share relevant issues or concerns. Individuals must limit their comments to three minutes. The Chair may limit the number of speakers on the same issue in the interest of time and topic. Generally speaking, items that are elsewhere on tonight's agenda may not be addressed during Community Comment. Individuals should not expect the Chair or Board/Commission Members to respond to their comments tonight. Instead, the Board/Commission might refer the matter to staff for consideration at a future meeting.

VI. Public Hearings

A. Comprehensive Plan Amendment, Rezoning and Subdivision – 5120 & 5124 Hankerson Avenue

B. Site Plan with Variances and Subdivision – Edina Endodontics, 7300 Metro Boulevard

VII. Reports/Recommendations

VIII. Chair And Member Comments

IX. Staff Comments

X. Adjournment

The City of Edina wants all residents to be comfortable being part of the public process. If you need assistance in the way of hearing amplification, an interpreter, large-print documents or something else, please call 952-927-8861 72 hours in advance of the meeting.

CITY OF EDINA

4801 West 50th Street
Edina, MN 55424
www.edinamn.gov

Date: April 11, 2024

Agenda Item #: IV.A.

To: Planning Commission

Item Type: Minutes

From: Liz Olson, Planning Administrative Support Specialist

Item Activity: Action

Subject: Regular Meeting Minutes and Special Work Session Minutes from March 27, 2024

ACTION REQUESTED:
Approve Regular Meeting Minutes and Special Work Session Minutes from March 27, 2024.

INTRODUCTION:

ATTACHMENTS:

Regular Meeting Minutes 3-27-24

Special Work Session Minutes 3-27-24

Draft Minutes☒
Approved Minutes☐
Approved Date: ___, 2024

Minutes
City Of Edina, Minnesota
Planning Commission
Edina City Hall Council Chambers
March 27, 2024

I. Call To Order

Chair Bennett called the meeting to order at 7:00 PM.

II. Roll Call

Answering the roll call were: Commissioners Alkire, Miranda, Daye, Padilla, Smith, Hahneman, Felt, Hu, Schultze and Chair Bennett. Staff Present: Cary Teague, Community Development Director, and Liz Olson, Administrative Support Specialist.

Absent from the roll call: Bornstein.

III. Approval Of Meeting Agenda

Commissioner Miranda moved to approve the March 27, 2024, agenda. Commissioner Felt seconded the motion. Motion carried unanimously.

IV. Approval Of Meeting Minutes

A. Minutes: Planning Commission, March 13, 2024

Commissioner Padilla moved to approve the March 13, 2024, meeting minutes. Commissioner Daye seconded the motion. Motion carried unanimously.

V. Special Recognitions and Presentations

A. Welcome Commissioner Alkire

Chair Bennett introduced Commissioner Alkire back to the Planning Commission.

Commissioner Alkire introduced himself to the Planning Commission and public.

VI. Community Comment

None.

VII. Reports/Recommendations

A. Lincoln and Londonderry Small Area Plan – Working Group Member Confirmation

Commissioner Miranda explained there were 39 applicants, and the Co-Chairs picked the working group members. He noted there was discussion to pick six members plus 2 alternates and the Co-Chairs were

not all in agreement. Commissioner Daye felt that the six members could be announced and then the Commission could discuss if there should be two alternates or not.

Commissioner Miranda introduced Nancy Spannaus, Steve Brown, Jignasha Pandya, Coryn Griffeth, Jim Diley, and Russ Rubin as the six working group members agreed upon.

The Commission discussed the possibility of alternate members on the working group.

The Co-Chairs answered Commission questions and explained the criteria used for applicant selection.

After discussion, the Commission agreed at this time there should not be alternates for the working group and if needed revisit the pool of applicants.

Motion

Commissioner Felt moved that the Planning Commission approve the working group members, as recommended by the co-chairs (Commissioners Miranda, Felt, and Daye). Commissioner Daye seconded the motion. Motion carried unanimously.

VIII. Chair and Member Comments

Received.

IX. Staff Comments

Received.

X. Adjournment

Commissioner Alkire moved to adjourn the March 27, 2024, Meeting of the Edina Planning Commission at 7:59 PM. Commissioner Hahneman seconded the motion. Motion carried unanimously.

Draft Minutes☒
Approved Minutes☐
Approved Date: ___, 2024

Minutes
City Of Edina, Minnesota
Planning Commission
Work Session
March 27, 2024

I. Call To Order

Chair Bennett called the meeting to order at 5:45 PM.

II. Roll Call

Answering the roll call were: Commissioners Alkire, Padilla, Smith, Felt, Hahneman, and Chair Bennett. Staff Present: Cary Teague, Community Development Director, and Liz Olson, Administrative Support Specialist.

Absent from the roll call: Commissioners Hu and Schultze.

III. Zoning Ordinance Amendment – First Floor Ceiling Height & Building Transparency on Front Facing Facades

Director Teague presented the zoning ordinance amendment for first floor ceiling height & building transparency on front facing facades.

The Commission discussed the proposed amendment.

- Commissioners Miranda and Daye joined the work session at 5:57 PM.

The Commission asked questions and provided feedback which can be reviewed in the official City meeting audio.

The Commission will continue discussion on the draft amendment at a future workshop meeting.

IV. Adjournment

The meeting adjourned at 6:51 p.m. to go into the Planning Commission meeting.

CITY OF EDINA

4801 West 50th Street
Edina, MN 55424
www.edinamn.gov

Date: April 11, 2024

Agenda Item #: VI.A.

To: Planning Commission

Item Type: Report and Recommendation

From: Cary Teague, Community Development Director

Item Activity: Action

Subject: Comprehensive Plan Amendment, Rezoning and Subdivision – 5120 & 5124 Hankerson Avenue

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ACTION REQUESTED:
Recommend the City Council approve the requests.

INTRODUCTION:
Donnay Homes Inc. is proposing to tear down two existing single-dwelling units to build two double dwelling units/townhomes at 5120 and 5124 Hankerson Avenue. The development would gain access off the Alley on the west side of the properties similar to the townhomes to the south. The subject properties are zoned R-1, Single Dwelling unit District. The site is guided low density residential, which allows between 1-5 units per acre. The properties are 19,468 square feet in size total (9,769 and 9,699 s.f. each). The proposed density of this project is 9 units per acre. (See attached applicant narrative and plans.)

To accommodate the proposal, the applicant is requesting the following applications:

- A Comprehensive Plan Amendment to re-guide the site from Low Density Residential to Medium Density Residential, which would allow 5-12 units per acre.
- A Rezoning from R-1, Single-Dwelling Unit District to PUD-20, Planned Unit Development, to be part of the adjacent Grandview Townhome PUD. Flexibility through the PUD would be for structure setback, building coverage and lot area per dwelling, to match the existing PUD to the south.
- Subdivision/Preliminary Plat.

ATTACHMENTS:

Staff Report

Engineering Memo

Report from Affordable Housing Development Manager

Proposed Plans

Applicant Narrative

Site Location, Zoning and Comp. Plan

Sketch Plan

Bike and Pedestrian Plan

Pyramid of Discretion in the Land Use Process

Draft Ordinance Amendment

Staff Presentation

STAFF REPORT

Date: April 11, 2024

To: Planning Commission

From: Cary Teague, Community Development Director

Subject: Comprehensive Plan Amendment, Rezoning and Subdivision – 5120 & 5124 Hankerson Avenue

Information / Background:

Donnay Homes Inc. is proposing to tear down two existing single-dwelling units to build two double dwelling units/townhomes at 5120 and 5124 Hankerson Avenue. The development would gain access off the Alley on the west side of the properties similar to the townhomes to the south. The subject properties are zoned R-1, Single Dwelling unit District. The site is guided low density residential, which allows between 1-5 units per acre. The properties are 19,468 square feet in size total (9,769 and 9,699 s.f. each). The proposed density of this project is 9 units per acre. (See attached applicant narrative and plans.)

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- A Rezoning from R-1, Single-Dwelling Unit District to PUD-20, Planned Unit Development, to be part of the adjacent Grandview Townhome PUD. Flexibility through the PUD would be for structure setback, building coverage and lot area per dwelling, to match the existing PUD to the south.
- Subdivision/Preliminary Plat.

Attached is the "pyramid of discretion." This project is within the "green" zone, meaning this is a legislative decision in which the City has <u>significant discretion</u> when reviewing this application.

SUPPORTING INFORMATION

Surrounding Land Uses

Northerly: Single-family homes; zoned R-1, Single Dwelling Unit District and guided low density residential.
Easterly: Wells Fargo Bank; zoned PCD-2, Planned Commercial District and guided MXC, Mixed Use Center.
Southerly: High Density Residential Apartment; zoned PRD-3, Planned Residential District; and guided High Density Residential.
Westerly: Single-family homes; zoned R-1, Single Dwelling Unit District and guided low density residential.

Existing Site Features

The subject properties total 19,468 square feet in size and contain two single family homes.

Planning

Guide Plan designation: LD, Low Density Residential
Zoning: R-1, Single-Dwelling Unit District

Parking

Each unit would have two enclosed parking spaces. Limited parking space would be available in front of the garages, and on the street. The project would be code compliant with two enclosed spaces per unit.

Site Circulation

Access to all the housing units would be off the alley in the back of the homes. There are no sidewalks in this area, and no sidewalk is proposed on the City's pedestrian plan. The capacity of the existing street would support the addition of two units on this block. The level of service would not be impacted. (See attached pedestrian plan.)

Landscaping

Based on the perimeter of the site 14 over-story trees would be required. The site plan shows 16 existing and proposed over-story trees around the perimeter of the site.

Grading/Drainage/Utilities

The city engineer has reviewed the proposed plans and found them to be acceptable subject to the comments and conditions outlined in the attached engineering memo. Any approvals of this project would be subject to review and approval of the Nine Mile Creek Watershed Districts, as they are the City's review authority over the grading of the site.

Building/Building Material

The building materials would be a composite siding similar and complementary to the existing townhomes to the south. The walkways and front stoops would be concrete, driveways asphalt, and garage doors paneled. (See attached renderings.)

Height

The proposed height is 1-1/2 stories and would meet the standards for height in the R-1 District as well as the PUD District.

Compliance Table

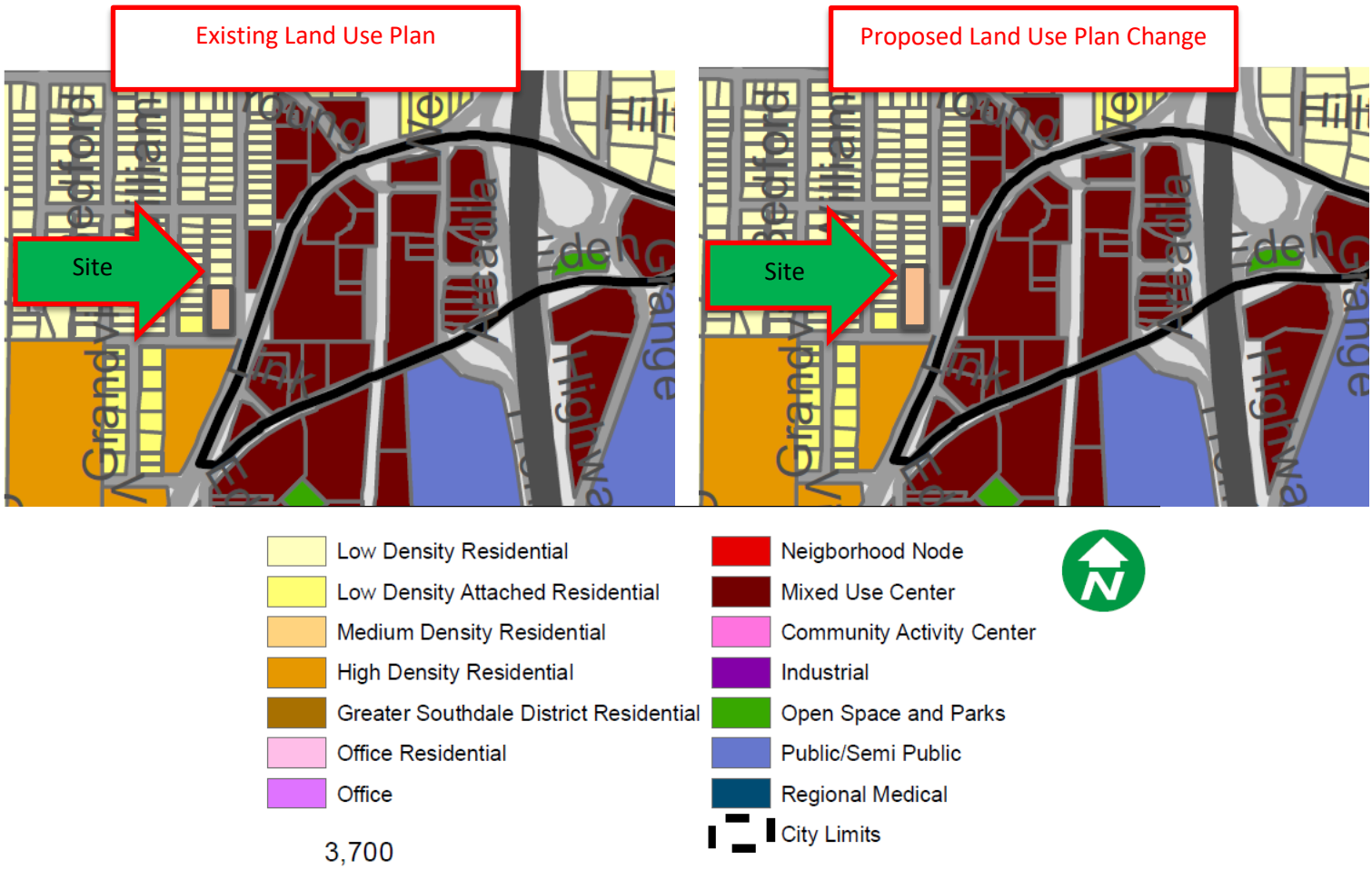
	City Standard (R-1/PRD)	PUD-20	Proposed
Front – Hankerson Ave. Side – North Side – South Rear – West	30-35 feet 10 feet 10 feet 25 feet	35 feet (porch 30') 12 feet 12 feet 20 feet	35 feet (porch 30') 17 feet 10 feet ***20 feet****
Building Height	2-1/2 stories & 35 feet	2-1/2 stories & 33 feet	1-1/2 stories & 25 feet
Building Coverage	.30	.45	***.40****
Density – Comp. Plan	1-5 units per acre	5-12 units per acre (Comp Plan)	***9*****
Density – PRD-3 District	1 unit per 2,900 s.f. = 6 units		4 units

Flexibility through the PUD (meets PUD standard)
****Comprehensive Plan Amendment Required***

Comprehensive Guide Plan Amendment

To accommodate the request, a Comprehensive Plan amendment is requested to Medium Density Residential. The Medium Density Residential District allows between 5-12 units per acre. <u>The map below shows how the Comprehensive Plan would be amended to accommodate the change in designation.</u>

The proposed amendment is reasonable given its proximity to the Grandview District. The property to the south is guided medium density residential and the east is property guided MXC Mixed Use Center.



MDR Medium-Density Residential	Applies to attached housing (townhouses, quads, etc.) and multi-family complexes of moderate density. May also include small institutional uses.	In new development or redevelopment, improve integration of multi-family housing into an interconnected street network and work to create an attractive, pedestrian-friendly street edge.	5 - 12 residential dwelling units/acre

PUD Rezoning

The applicant is requesting a rezoning of this site to PUD, Planned Unit Development District (See attached draft PUD Ordinance.) The findings below are the same as the findings for the approved PUD District to the south, which this project would be part of.

Per Chapter 36 of the City Code the following are the regulations for a PUD:

1. **Purpose and Intent. The purpose of the PUD District is to provide comprehensive procedures and standards intended to allow more creativity and flexibility in site plan design than would be possible under a conventional zoning district. The decision to zone property to PUD is a public policy decision for the City Council to make in its legislative capacity. The purpose and intent of a PUD is to include most or all of the following:**

   a. **provide for the establishment of PUD (planned unit development) zoning districts in appropriate settings and situations to create or maintain a development pattern that is consistent with the City's Comprehensive Plan;**

   b. **promote a more creative and efficient approach to land use within the City, while at the same time protecting and promoting the health, safety, comfort, aesthetics, economic viability, and general welfare of the City;**

   c. **provide for variations to the strict application of the land use regulations in order to improve site design and operation, while at the same time incorporate design elements that exceed the City's standards to offset the effect of any variations. Desired design elements may include: sustainable design, greater utilization of new technologies in building design, special construction materials, landscaping, lighting, stormwater management, pedestrian oriented design, and podium height at a street or <u>transition to residential neighborhoods</u>, parks or other sensitive uses;**

   d. **ensure high quality of design and design compatible with surrounding land uses, including both existing and planned;**

   e. **maintain or improve the efficiency of public streets and utilities;**

   f. **preserve and enhance site characteristics including natural features, wetland protection, trees, open space, scenic views, and screening;**

   g. **allow for mixing of land uses within a development;**

   h. **encourage a variety of housing types including <u>affordable housing</u>; and**

i. ensure the establishment of appropriate transitions between differing land uses.

The project would simply be an extension of the existing PUD Zoning District to the south. The structures would step down from the existing townhomes that are two stories, toward the single-family homes to the north and west to better transition into the neighborhood. The buildings provide an appropriate transition from the high-density development to the south (apartments) to the existing 4-unit, two-story townhomes to these one-story townhomes/duplexes to the single-family homes to the north. The project would provide a housing type the City has not seen much development of. It is an extension of "missing middle" type housing option identified in the Comprehensive Plan. "Examples of housing types may include "missing middle" housing options – a range of multi-unit or clustered housing types compatible in scale with single-family homes that help meet the growing demand for walkable urban living. These housing options may include co-housing, duplexes, triplexes, fourplexes, and other smaller scale multifamily types. Ways to reduce housing costs could include modular building."

2. *Applicability/Criteria*

a. Uses. All permitted uses, permitted accessory uses, conditional uses, and uses allowed by administrative permit contained in the various zoning districts defined in this Title shall be treated as potentially allowable uses within a PUD district, provided they would be allowable on the site under the Comprehensive Plan.

The Medium Density classification of the site suggests "attached housing (townhouses, quads, etc.) and multi-family complexes of moderate density."

b. Eligibility Standards. To be eligible for a PUD district, all development should be in compliance with the following:

i. where the site of a proposed PUD is designated for more than one (1) land use in the Comprehensive Plan, the City may require that the PUD include all the land uses so designated or such combination of the designated uses as the City Council shall deem appropriate to achieve the purposes of this ordinance and the Comprehensive Plan;

The proposal would not include a mixture of land uses. However, it would include a housing type that the City has not seen much construction of over the past 20 years. As described above would provide "missing middle" type housing.

ii. any PUD which involves a single land use type or housing type may be permitted provided that it is otherwise consistent with the objectives of this ordinance and the Comprehensive Plan;

Staff believes this housing development provides a housing type that is needed in the Comprehensive Plan. (See Comprehensive Plan findings on pages 7-10 below.)

iii. permitted densities may be specifically stated in the appropriate planned development designation and shall be in general conformance with the Comprehensive Plan; and

The proposed building density would be 9 units per acre and consistent with the Medium Density Designation in the Comprehensive Plan. The density of the existing Hankerson Townhomes is 11.9 units per acre.

iv. the setback regulation, building coverage and floor area ratio of the most closely related conventional zoning district shall be considered presumptively appropriate, but may be departed from to accomplish the purpose and intent described in #1 above.

The proposed project would require variances from the standards in the R-1 District, however, would be consistent with the PUD-20 regulations. For the reasons stated above, staff believes the purpose and intent of the PUD Ordinance is met.

Subdivision

The applicant is proposing to divide the parcel into four lots so that each building is located on its own lot. The proposed plat would be similar to, and a continuation of the Grandview Townhome plat to the south.

The City is authorized by statute to collect park dedication fees to support the additional demand for parks created by new development when property is subdivided. The City has studied this demand and concluded that new projects generally create additional park demand of $5,000 per lot. Two new lots are being proposed, therefore, a park dedication of $10,000 would be required.

PRIMARY ISSUES/STAFF RECOMMENDATION

Primary Issues

- **Is the Comprehensive Plan Amendment reasonable?**

Yes. Staff believes the proposal is reasonable for the following reasons:

1. The proposed project would meet the following goals and policies of the Comprehensive Plan:

    a. **Multifamily Residential Single-Family Attached.** This land use consists of residential units with common walls, where each unit has direct exterior access. <u>In Edina the most common buildings of this type are townhouses and duplexes</u> (two-family dwellings). <u>Townhouses tend to be clustered close to highway or major road corridors</u>,

while duplexes are often found in narrow strips along major thoroughfares such as Vernon and France Avenues and serve as a buffer for adjacent single-family neighborhood detached housing.

b. **Multifamily.** Multifamily developments are concentrated primarily along the main traffic arteries and are generally located toward the edges of the city, often in proximity to retail business establishments. Concentrations of multifamily developments are found along York Avenue, France Avenue, Vernon Avenue, Lincoln Drive, and Cahill Road.

c. **Integration of multi-unit housing into transitional areas**.
In the past, duplexes were located along many major thoroughfares in Edina as a kind of buffer or transition to the adjacent single-family housing. Today this housing type is in need of updating or replacement in many locations, and high land and redevelopment costs create pressure for higher-density housing types. Townhouse complexes have been constructed in locations such as north France Avenue. The challenge is that in many locations the duplexes are only one lot deep, which makes it difficult to provide an adequate transition to single-family scale.

Multifamily housing can be scaled to be compatible with lower density neighborhoods.

d. **Single-family characteristics**. Attached and multifamily housing should emulate single-family housing in its basic architectural elements – pitched roofs, articulated facades, visible entrances, porches or balconies. Taller buildings should step down to provide a height transition to existing adjacent residential buildings.

Multifamily housing can emulate architectural elements of neighborhood context.

e. **Level of formality.** Design the front and back facades with appropriate levels of formality. The front, as the more public side of the house, will receive the more formal treatment, with the main entrance, porch or steps and landscaping, while trash/recycling storage, play equipment and outdoor storage should be located in the back.

f. **Parking to the rear.** Where rear-loaded or detached garages predominate, parking spaces and garages should be located to the rear of the lot or interior of the block. If this is infeasible, garages should be recessed some distance behind the main façade of the house and surface parking should be placed within side yards to the extent feasible.

Parking to the rear can create attractive pedestrian oriented environments.

g. **Maintaining community character**. With the changing ways people are living, working, and using spaces, there will be changes in how land uses function – both in new and renovated spaces. Edina is frequently on the forefront of innovative practices, particularly related to sustainability and technology. However, this will require some flexibility in development standards. One of the goals of innovating may be to find ways to accommodate missing housing types or to let housing be produced more affordably. Examples of housing types may include "missing middle" housing options – a range of multi-unit or clustered housing types compatible in scale with single-family homes that help meet the growing demand for walkable urban living. These housing options may include co-housing, duplexes, triplexes, fourplexes, and other smaller scale multifamily types. Ways to reduce housing costs could include modular building styles that take advantage of efficiencies in the construction process.

h. ***Future Housing Needs.*** Edina's *Cities of the Future* (2015) report described trends that are shaping the demand for housing in the city. It envisions a future where housing is integrated into mixed use neighborhoods, better meeting the needs of smaller households with fewer children, and an aging population. Characteristics of new housing may include:

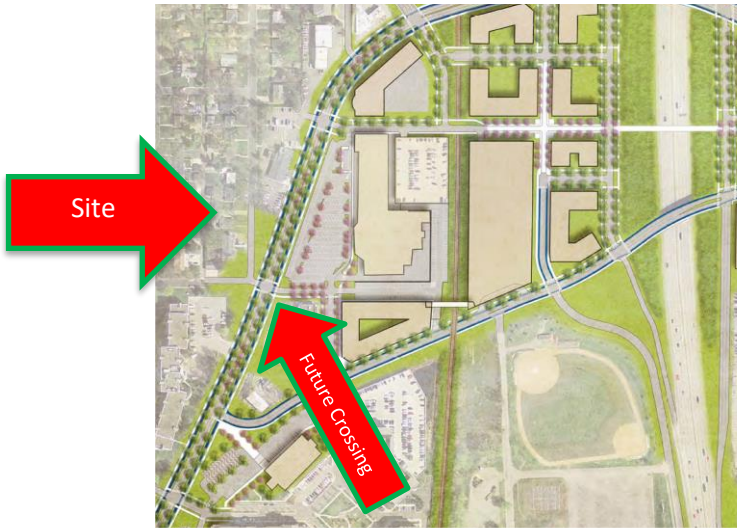
- New housing options primarily are provided as attached (multifamily) apartments and townhouses and small detached lots, with a variety of sizes, uses, and resident types.

2. The proposed amendment is reasonable given its proximity to the Grandview District. The property to the south is guided medium and high density residential and the east is property guided MXC Mixed Use Center.

3. The proposed amendment would allow a development that would create a reasonable land use transition to the single-family residential area to the north. High Density Development is located to the south and east, by developing townhouses at the same height as would be allowed in the single-dwelling unit district would provide a transition and buffer to the single-family homes to the north.

4. The Medium-Density Residential District allows between 5-12 units per acre. The proposal is 9 units per acre.

5. A pedestrian and street connection with Vernon Avenue to Jerry's is proposed in the Grandview Development Framework and Grandview Transportation Study. (See attached pages from both studies.) Higher Densities are typically found near or on streets with higher traffic volume like Vernon Avenue. The Illustration is a rendering of that future connection from the Grandview Transportation Study:



6. Additional density would support the retail uses in the district.

- **Is the Rezoning to PUD reasonable?**

Yes. Staff believes the proposed Rezoning is reasonable for the following reasons:

1. Zoning would provide an appropriate transition from the apartments to the south to the duplexes and single-family homes to the west and north. Proposed heights and setbacks are consistent with the R-1 Zoning to the north.

2. The proposal would meet the PUD criteria in Chapter 36, as outlined on pages 5-7 above, the same as the existing PUD was found to meet the criteria.

3. The proposed project would meet the goals and policies of the Comprehensive Plan as listed above on pages 7-9.

4. The site is unique in its proximity to the Grandview District and higher density development. A recommendation in the Grandview Transportation Plan is that a 52nd Street pedestrian and/or vehicular connection be made to Vernon Avenue. Higher densities are typically located on higher capacity streets and commercial areas.

5. The proposed buildings have been designed to fit the neighborhood and would provide a reasonable transition from the high-density apartments to the south and bank to the east.

6. Rezoning would extend the existing PUD District and would allow for a housing type needed in the City of Edina.

Options for consideration

As noted in the above review, staff is recommending approval of the request. However, a case can be made for both approval and denial. Because the request is for a Comprehensive Plan Amendment and Rezoning, the City has significant discretion to approve or deny when reviewing this application.

Below provide options for the planning commission and city council to consider:

Denial

Comprehensive Plan

Recommend the City Council deny the Comprehensive Plan Amendment to re-guide the site from low density attached to medium density. Denial is based on the following findings:

1. The proposed density is too high for the site. Land uses to the west and north are low density residential. Introducing medium density residential is not reasonable for the middle of this block.

2. This neighborhood is isolated from the Grandview District. The connection from 52nd Street to Vernon has not yet been made.

3. Reasonable use of the property exists today with two single-family residential homes.

Rezoning

Recommend the City Council Deny the Rezoning. Denial is based on the following findings:

1. The existing two single family homes are reasonable uses of the existing properties.

2. The proposed density is too high for the site.

3. Building coverage is too high for this site.

4. The proposal does not meet the criteria for a PUD, Planned Unit Development.

5. The prosed rezoning is not consistent with the Comprehensive Plan.

Approval

Comprehensive Plan

Recommend the City Council approve the Comprehensive Plan Amendment to re-guide the site from low density attached to medium density, which would allow the proposed 9 units per acre. Approval is based on the following findings:

1. The proposed amendment is reasonable given its proximity to the Grandview District. The property to the south is guided medium density residential and the east is property guided MXC Mixed Use Center.

2. The proposed amendment would allow a development that would create a reasonable land use transition to the single-family residential area to the north. Developing townhouses/duplexes at the same height as would be allowed in the single-dwelling unit district would provide a good transition to the single-family homes to the north.

3. Additional density would help support the retail uses in the district.

4. The Medium-Density Residential District allows between 5-12 units per acre. The proposal is 9 units per acre.

5. The proposed project would meet the following goals and policies of the Comprehensive Plan:

    a. Multifamily Residential Single-Family Attached. This land use consists of residential units with common walls, where each unit has direct exterior access. In Edina the most common buildings of this type are townhouses and duplexes (two-family dwellings). Townhouses tend to be clustered close to highway or major road corridors, while duplexes are often found in narrow strips along major thoroughfares such as Vernon and France Avenues and serve as a buffer for adjacent single-family neighborhood detached housing.

    b. Multifamily. Multifamily developments are concentrated primarily along the main traffic arteries and are generally located toward the edges of the city, often in proximity to retail business establishments. Concentrations of multifamily developments are found along York Avenue, France Avenue, Vernon Avenue, Lincoln Drive, and Cahill Road.

    c. Integration of multi-unit housing into transitional areas.
    In the past, duplexes were located along many major thoroughfares in Edina as a kind of buffer or transition to the adjacent single-family housing. Today this housing type is in need of updating or replacement in many locations, and high land and redevelopment costs create pressure for higher-density housing types. Townhouse complexes have been constructed in locations such as north France Avenue. The challenge is that in many locations the duplexes are only one lot deep, which makes it difficult to provide an adequate transition to single-family scale.

d. Single-family characteristics. Attached and multifamily housing should emulate single-family housing in its basic architectural elements – pitched roofs, articulated facades, visible entrances, porches or balconies. Taller buildings should step down to provide a height transition to existing adjacent residential buildings.

e. Level of formality. Design the front and back facades with appropriate levels of formality. The front, as the more public side of the house, will receive the more formal treatment, with the main entrance, porch or steps and landscaping, while trash/recycling storage, play equipment and outdoor storage should be located in the back.

f. Parking to the rear. Where rear-loaded or detached garages predominate, parking spaces and garages should be located to the rear of the lot or interior of the block. If this is infeasible, garages should be recessed some distance behind the main façade of the house and surface parking should be placed within side yards to the extent feasible.

g. Maintaining community character. With the changing ways people are living, working, and using spaces, there will be changes in how land uses function – both in new and renovated spaces. Edina is frequently on the forefront of innovative practices, particularly related to sustainability and technology. However, this will require some flexibility in development standards. One of the goals of innovating may be to find ways to accommodate missing housing types or to let housing be produced more affordably. Examples of housing types may include "missing middle" housing options – a range of multi-unit or clustered housing types compatible in scale with single-family homes that help meet the growing demand for walkable urban living. These housing options may include co-housing, duplexes, triplexes, fourplexes, and other smaller scale multifamily types. Ways to reduce housing costs could include modular building styles that take advantage of efficiencies in the construction process.

h. Future Housing Needs. Edina's *Cities of the Future* (2015) report described trends that are shaping the demand for housing in the city. It envisions a future where housing is integrated into mixed use neighborhoods, better meeting the needs of smaller households with fewer children, and an aging population. Characteristics of new housing may include:

- New housing options primarily are provided as attached (multifamily) apartments and townhouses and small detached lots, with a variety of sizes, uses, and resident types.

Rezoning & Subdivision

Recommend the City Council Approve the Rezoning to PUD, Preliminary Site Plan and Subdivision. Approval is based on the following findings:

1. The site is unique in its proximity to the Grandview District and higher density development. A recommendation in the Grandview Transportation Plan is that 52nd Street connect to Vernon

Avenue by pedestrian or vehicle. Higher densities are typically located on higher capacity streets and commercial areas.

2. The proposed buildings have been designed to fit the neighborhood and would provide a reasonable transition from the high-density apartments to the south and bank to the east.

3. The front setbacks would be consistent to the single-family homes to the north.

4. Provides a housing type not being developed in the City.

5. The proposed project would meet the following goals and policies of the Comprehensive Plan:

    a. Multifamily Residential Single-Family Attached. This land use consists of residential units with common walls, where each unit has direct exterior access. In Edina the most common buildings of this type are townhouses and duplexes (two-family dwellings). Townhouses tend to be clustered close to highway or major road corridors, while duplexes are often found in narrow strips along major thoroughfares such as Vernon and France Avenues and serve as a buffer for adjacent single-family neighborhood detached housing.

    b. Multifamily. Multifamily developments are concentrated primarily along the main traffic arteries and are generally located toward the edges of the city, often in proximity to retail business establishments. Concentrations of multifamily developments are found along York Avenue, France Avenue, Vernon Avenue, Lincoln Drive, and Cahill Road.

    c. Integration of multi-unit housing into transitional areas.
    In the past, duplexes were located along many major thoroughfares in Edina as a kind of buffer or transition to the adjacent single-family housing. Today this housing type is in need of updating or replacement in many locations, and high land and redevelopment costs create pressure for higher-density housing types. Townhouse complexes have been constructed in locations such as north France Avenue. The challenge is that in many locations the duplexes are only one lot deep, which makes it difficult to provide an adequate transition to single-family scale.

    d. Single-family characteristics. Attached and multifamily housing should emulate single-family housing in its basic architectural elements – pitched roofs, articulated facades, visible entrances, porches or balconies. Taller buildings should step down to provide a height transition to existing adjacent residential buildings.

    e. Level of formality. Design the front and back facades with appropriate levels of formality. The front, as the more public side of the house, will receive the more formal treatment, with the main entrance, porch or steps and landscaping, while trash/recycling storage, play equipment and outdoor storage should be located in the back.

    f. Parking to the rear. Where rear-loaded or detached garages predominate, parking spaces and garages should be located to the rear of the lot or interior of the block. If

this is infeasible, garages should be recessed some distance behind the main façade of the house and surface parking should be placed within side yards to the extent feasible.

g. Maintaining community character. With the changing ways people are living, working, and using spaces, there will be changes in how land uses function – both in new and renovated spaces. Edina is frequently on the forefront of innovative practices, particularly related to sustainability and technology. However, this will require some flexibility in development standards. One of the goals of innovating may be to find ways to accommodate missing housing types or to let housing be produced more affordably. Examples of housing types may include "missing middle" housing options – a range of multi-unit or clustered housing types compatible in scale with single-family homes that help meet the growing demand for walkable urban living. These housing options may include co-housing, duplexes, triplexes, fourplexes, and other smaller scale multifamily types. Ways to reduce housing costs could include modular building styles that take advantage of efficiencies in the construction process.

h. Future Housing Needs. Edina's *Cities of the Future* (2015) report described trends that are shaping the demand for housing in the city. It envisions a future where housing is integrated into mixed use neighborhoods, better meeting the needs of smaller households with fewer children, and an aging population. Characteristics of new housing may include:

- New housing options primarily are provided as attached (multifamily) apartments and townhouses and small detached lots, with a variety of sizes, uses, and resident types.

6. The plat would be similar to the townhome plat to the south.

Approval is subject to the following Conditions:

1. The Final Development Plans must be consistent with the Preliminary Development Plans dated March 4, 2024.

2. The Final Landscape Plan must meet all minimum landscaping requirements per Chapter 36 of the Zoning Ordinance. A performance bond, letter-of-credit, or cash deposit must be submitted for one and one-half times the cost amount for completing the required landscaping, screening, or erosion control measures at the time of any building permit. The property owner is responsible for replacing any required landscaping that dies after the project is built.

3. Compliance with all the conditions outlined in the city engineer's memo dated April 2, 2024.

4. Submit a copy of the Nine Mile Creek Watershed District permit. The City may require revisions to the approved plans to meet the district's requirements.

5. Final Rezoning is contingent on the Metropolitan Council approval of the Comprehensive Plan Amendments.

6. Park dedication fee of $10,000 shall be due at the time of issuance of a building permit.

Staff Recommendation

Staff recommends approval of the Comprehensive Plan Amendment, Rezoning and Subdivision subject to the findings and conditions above.

Deadline for a city decision: June 4, 2024

DATE: April 2, 2024

TO: Hankerson - Owner and Development Team

CC: Cary Teague – Community Development Director

FROM: Chad Millner P.E. - Director of Engineering
Ross Bintner P.E. – Engineering Services Manager
Ben Jore P.E. – Senior Project Engineer
Andrew Scipioni, Transportation Planner
Marisa Bayer, Sustainability Manager

RE: **HANKERSON – Development Review**

The Engineering Department has reviewed the subject property for pedestrian facilities, utility connections, grading, flood risk, and storm water. Plans reviewed included civil and landscape dated 3/11/24.

General

1. Deliver as-built records of all utilities including storm, sanitary, and watermain for both public and private post construction.

Survey

2. An existing and proposed site condition survey is required.
3. Show all existing and proposed public and private easements. Drainage & utility easements will be required.

Traffic and Street

4. Construction staging and traffic control plans will be required.
5. Hankerson Ave and 52nd St W were milled and overlaid in 2019. Road patching shall conform to Edina Standard Plates 525, 542, and 544. Mill and overlay for one cohesive street repair.
6. 48 hrs notice is required for road closures related to construction operations. Complete the road closure form from the City's website.

Sanitary and Water Utilities

7. Due to individual ownership proposed, City is requiring one water and sanitary service per residential unit. Water service shall be a minimum of 1-inch. A minimum of 3' horizontal spacing is required between water services. Applicant to make a straight connection from the curb stop/cleanout to the respective mains.
8. Remove all abandoned sanitary and water services to the main.
9. Domestic sanitary shall be sized by the developer's engineer.
10. Sewer and water connection permits required for all connections. City staff to be present to inspect, cost to be paid by developer.
11. A SAC and WAC determination will be required and Met Council and City fees will be calculated from the determination.

Storm Water Utility

26. Applicant proposes an underground chamber to meet the volume and rate control requirements.
27. Evidence of watershed district permit and copies of private maintenance agreement for storm water system in favor of watershed is required for building permit.
28. Retention system engineer required to verify construction of the underground retention systems done per plan.
29. Provide signed plan from underground retention system and confirm it is designed for 80,000lb fire truck load and outriggers.

Grading Erosion and Sediment Control

30. A SWPPP has been submitted. SWPPP to be reviewed at building permit.

Sustainability

26. Edina's Climate Action Plan commits our community to reducing greenhouse gas emissions 45% by 2030 and achieve net-zero emissions by 2050. This project is encouraged to support our Climate Action Plan goals by:
    a. Participating in Xcel Energy's Efficient New Homes program to build a more energy efficient home.
    b. Installing energy-efficient appliances and equipment, receiving rebates from Xcel Energy and CenterPoint Energy.
    c. Electrifying water-heating and heating equipment using heat pumps, receiving rebates from Xcel Energy and CenterPoint Energy.

Other Agency Coordination

27. Nine Mile Creek Watershed permit is required. Plans should be submitted to them for comment. MDH, MPCA and MCES and others as required.

TO: **Cary Teague, Community Development Director**

FROM: **Stephanie Hawkinson, Affordable Housing Development Manager**

DATE: **April 9, 2024**

RE: **Hankerson Avenue Redevelopment Proposal**

Donay Homes is proposing the demolition of two single family homes at 5120/5124 Hankerson for the development of two side-by-side duplexes, thus increasing the number of housing units by two.

Affordable Housing

The Metropolitan Council establishes that a house is affordable to a household whose income is at 80% or below Area Median Income (AMI) if valued at no more than $290,300. Minnesota Housing and the Come Home 2 Edina down payment assistance programs (DPA) have purchase price limits set at $515,200. The current assessed values of 5120/5124 Hankerson are $425,420 and $434,249, which would qualify for the DPA but are not considered affordable to low-moderate income buyers.

Although the term "Naturally Occurring Affordable Housing" or NOAH pertains to multifamily rental housing, it could be argued that the general concept applies to the two Hankerson houses for middle income homeownership opportunities (up to 115% of AMI). The replacement homes that are proposed to sell for $800K do not qualify as affordable by any standard, so this would be a loss of two moderately priced homes. However, if the project were denied, this developer or another, could demolish the homes and redevelop them as new single-family homes at higher price-points. Historically, the average value of a new single-family home from a teardown rebuild has exceeded $1.2 million.

Support for Increasing Density

According to the Federal Reserve Bank of Minneapolis, producing more homes is the single most effective way to ensure long-term housing affordability (based on economic supply/demand principles). The Comprehensive Plan and the Housing Strategy Task force both call for the development of "Missing Middle" housing, which provides increased density and housing supply through duplexes, tri-plexes, up to townhomes and small-scale apartments. The Comprehensive Plan states that "'Missing Middle' housing encompasses housing between the scale of low and high densities, providing both an option to meet needs, and a built form type that can transition between adjacent districts of different levels of scale and intensity." This type of housing is ideally suited in between commercial districts and single-family residential neighborhoods.

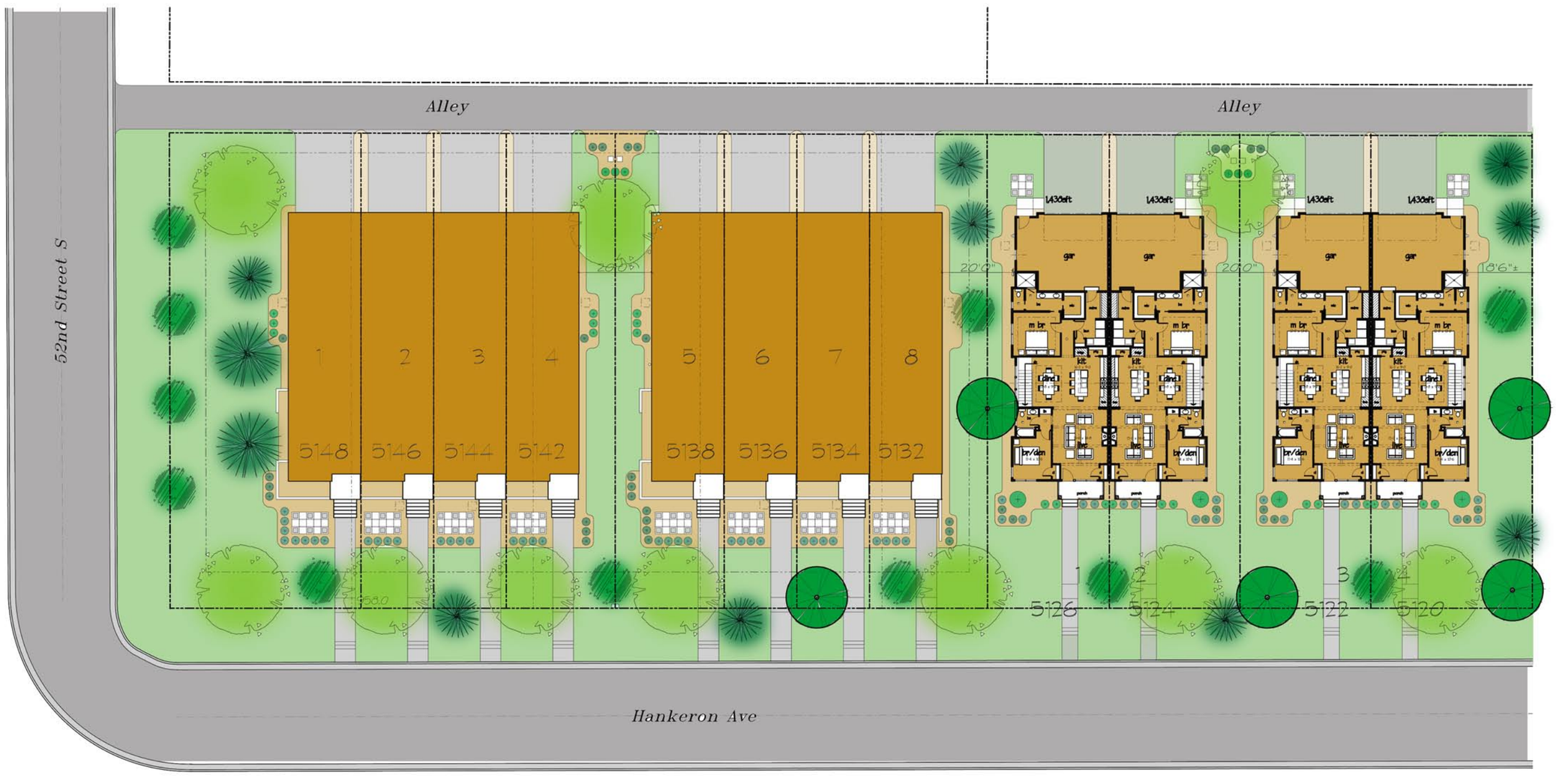
The Housing Strategy Task Force report encourages the development of duplexes or Missing Middle Housing. They include "Support opportunities to accommodate Missing Middle housing within the city, defined as range of multi-unit or clustered housing types compatible in scale with single-family homes." The Task Force also proposed a goal to consider zoning amendments that would expanding housing options, such as "consider[ing] zoning amendments in limited areas (such as transitional areas and activity nodes) and pursue zoning changes to encourage split lots to allow infill, to allow lot splits for infill, single-family ownership housing, detached or attached (zero lot line), on lots after splitting that are 50' or wider (or 3,500 sf or larger)." This type of housing also has market also has market benefits. According to the Edina Maxfield Housing Study: "With the overall aging of the population, more

households are looking for greater convenience and less space in their housing and are selecting twin homes, detached townhomes, and condominiums."

Recommendation
Based on these approved plans, and the location adjacent to a commercial node, I recommend support of the proposal.

Policy consideration
When NOAH properties are lost to make way for higher market rate housing, consider incentives to encourage cost reductions of the new units without disincentivizing increased density.

2 PROJECT STREETSCAPE



1 PROJECT SITE PLANS

TRADITIONS
BY DONNAY HOMES, LLC

9655 - 63rd Avenue North
Maple Grove, Minnesota 55369
(763) 531-0714
Builder #722939

SITE DATA

Current Use: LDR
Proposed Use: MDR 8.85 u/a

Current Zoning: R-1
Proposed Zoning: PUD

Parcel Info:
2011721320172 - 5124 Hankerson Ave
Area - 0.22ac, 9,699sft
2011721320144 - 5120 Hankerson Ave
Area - 0.22ac, 9,769sft

Project Data:
Area - 0.4468ac
Area - 19,468sft
Lot Width - 150'

Setback Information:
Front Yard setback - 30' (min)
Grandview Twnhm Front Setback - 35'
Proposed - 35'
Bldg to Bldg - 20'
Side Yard Setback - 18'6"± to north
Rear Yard Setback - 21'6"± to Alley

Coverage Information:

Building	1,981sft ea	40.7%
Sidewalks	152sft ea	3.1%
Driveway	336sft ea	6.9%
Decks	68sft ea	0%
Total Hardcover Proposed		50.7%

No.	ISSUED/REVISIONS/SUBMISSIONS	DATE

Project Name
GRANDVIEW TOWNHOMES PHASE 2
52nd St & Hankerson Ave
Edina, Minnesota

Drawing Title
PROJECT SITE CONCEPT AND STREETSCAPE

BLDG. No. CONCEPT

Drawn
Checked
Reviewed
Date 8/14/2023
Drawing No. A0.0
of

Submitted
03-01-2024
Planning Division

ELEVATION NOTES:

"Architectural" ASPHALT SHINGLES
MAIN ROOF PITCHES are as Noted
FRONT FACING GABLE ROOF PITCHES are 12/12
SEE ELEVATIONS FOR MORE DETAIL on ROOF PITCHES

12" RAKES - TYPICAL
12" OVERHANGS - TYPICAL

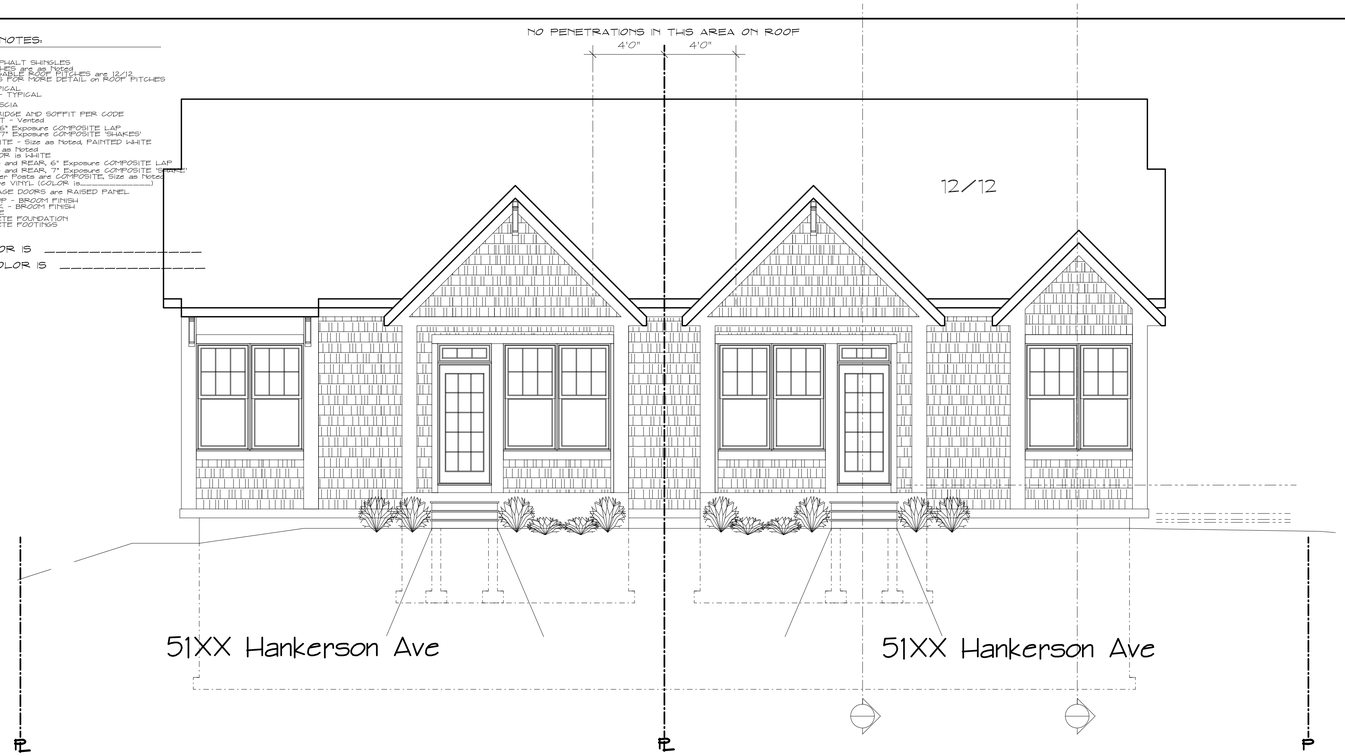
6" ALUMINUM FASCIA

VENT ROOF at RIDGE AND SOFFIT PER CODE
ALUMINUM SOFFIT - Vented
FRONT SIDINGS, 6" Exposure COMPOSITE LAP
FRONT SIDINGS, 7" Exposure COMPOSITE "SHAKES"
TRIM is COMPOSITE - Size as Noted PAINTED WHITE
WINDOW TRIM is as Noted
COLOR is WHITE
SIDING for SIDES and REAR, 6" Exposure COMPOSITE LAP
SIDING for SIDES and REAR, 7" Exposure COMPOSITE "SHAKE"
Corner Posts are COMPOSITE, Size as Noted
ALL WINDOWS are VINYL COLOR is_____
OVERHEAD GARAGE DOORS are RAISED PANEL

CONCRETE STOOP - BROOM FINISH
CONCRETE WALK - BROOM FINISH
CONCRETE DRIVE
POURED CONCRETE FOUNDATION
POURED CONCRETE FOOTINGS

SIDING COLOR IS _____

SHINGLE COLOR IS _____



4 FRONT ELEVATION

3 DETAILS

1 SPECIFICATIONS

2 DETAILS

TRADITIONS
BY DONNAY HOMES, LLC

9655 - 63rd Avenue North
Maple Grove, Minnesota 55369
(763) 531-0714
Builder #722939

Code Analysis

Code Used: 2018 IRC
Code Used: 2020 MNSRC

Classification: IRC-3
Dwelling, Townhome

Construction: VB
Wood Frame, Unrated

Occupancy:
IRC-3, Dwelling, Townhome w/
Accessory, Private Garage

Design Criteria
Roof Snow Load = 50psf
Wind Design Load = 90mph
Exposure = 'B'
Summer Design Temp. 88Db/72Wb Fdeg
Winter Design Temp. = -15 Fdeg
Seismic Design Category = A

Building Areas:
Building Coverage = 3,962sft
Living Space (per unit) = 2,587sft

13d Sprinkler - Not Required

Fire Resistive Construction
Exterior Walls - Not Required
Projections - Not Required
Openings in Walls - Not Required
Party Walls - See Plans
Dwelling/Garage - See Plans

No.	ISSUED/REVISIONS/SUBMISSIONS	DATE

Project Name
Grandview Townhomes of Edina - 2
Hankerson Ave
Edina, Minnesota

Drawing Title
ELEVATIONS

BLDG. No.

Drawn: SKB
Checked
Reviewed
Date: 12/6/23

Drawing No.
A1.0
_____ of _____

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03-01-2024
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2 SIDE ELEVATION

1 REAR ELEVATION

TRADITIONS
BY DONNAY HOMES, LLC
9655 - 63rd Avenue North
Maple Grove, Minnesota 55369
(763) 531-0714
Builder #722939

No.	ISSUED/REVISIONS/SUBMISSIONS	DATE

Project Name
Grandview Townhomes of Edina - 2
Hankerson Ave
Edina, Minnesota

Drawing Title
ELEVATIONS

BLDG. No.

Drawn SKB
Checked
Reviewed
Date 12/6/23

Drawing No. A1.1
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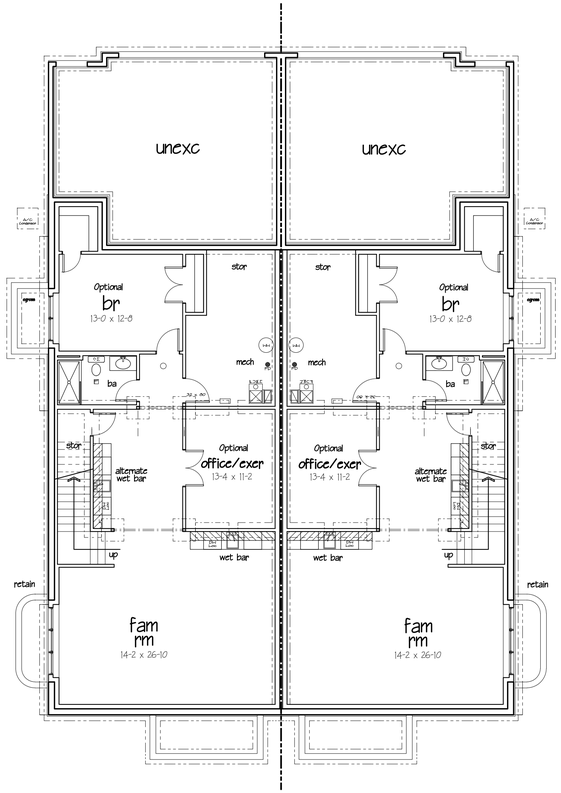
Planning Division

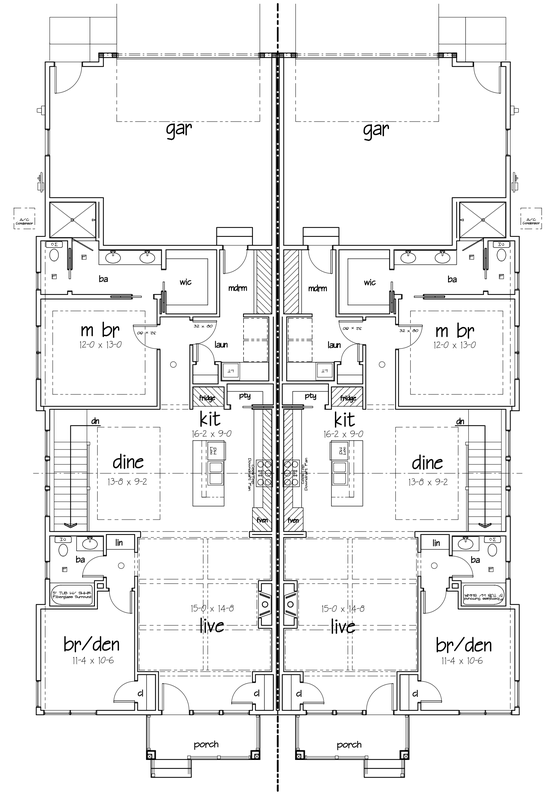
TRADITIONS
BY DONNAY HOMES, LLC

9655 - 63rd Avenue North
Maple Grove, Minnesota 55369
(763) 531-0714
Builder #722939

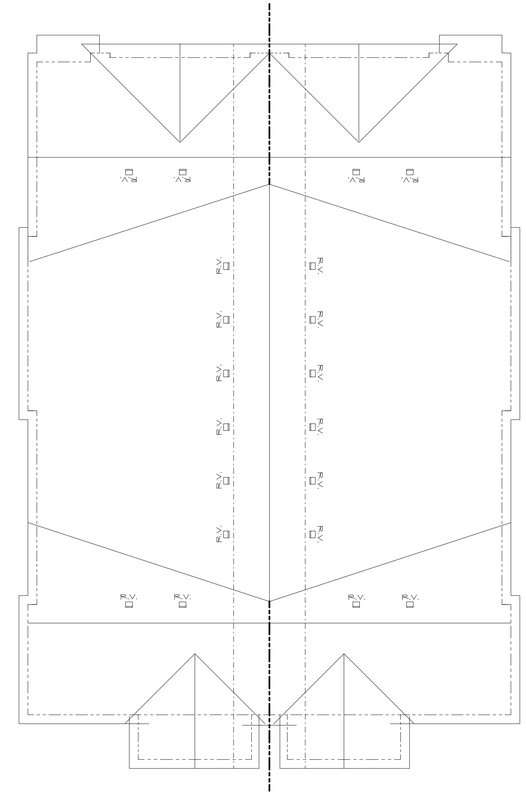
4 NOTES & DETAILS



1 LOWER FLOOR PLAN



2 MAIN FLOOR PLAN



3 ROOF PLAN

No.	ISSUED/REVISIONS/SUBMISSIONS	DATE

Project Name
Grandview Townhomes of Edina - 2
Hankerson Ave
Edina, Minnesota

Drawing Title
BUILDING PLANS

BLDG. No.

Drawn SKB
Checked
Reviewed
Date 12/6/23

Drawing No. A2.0
of

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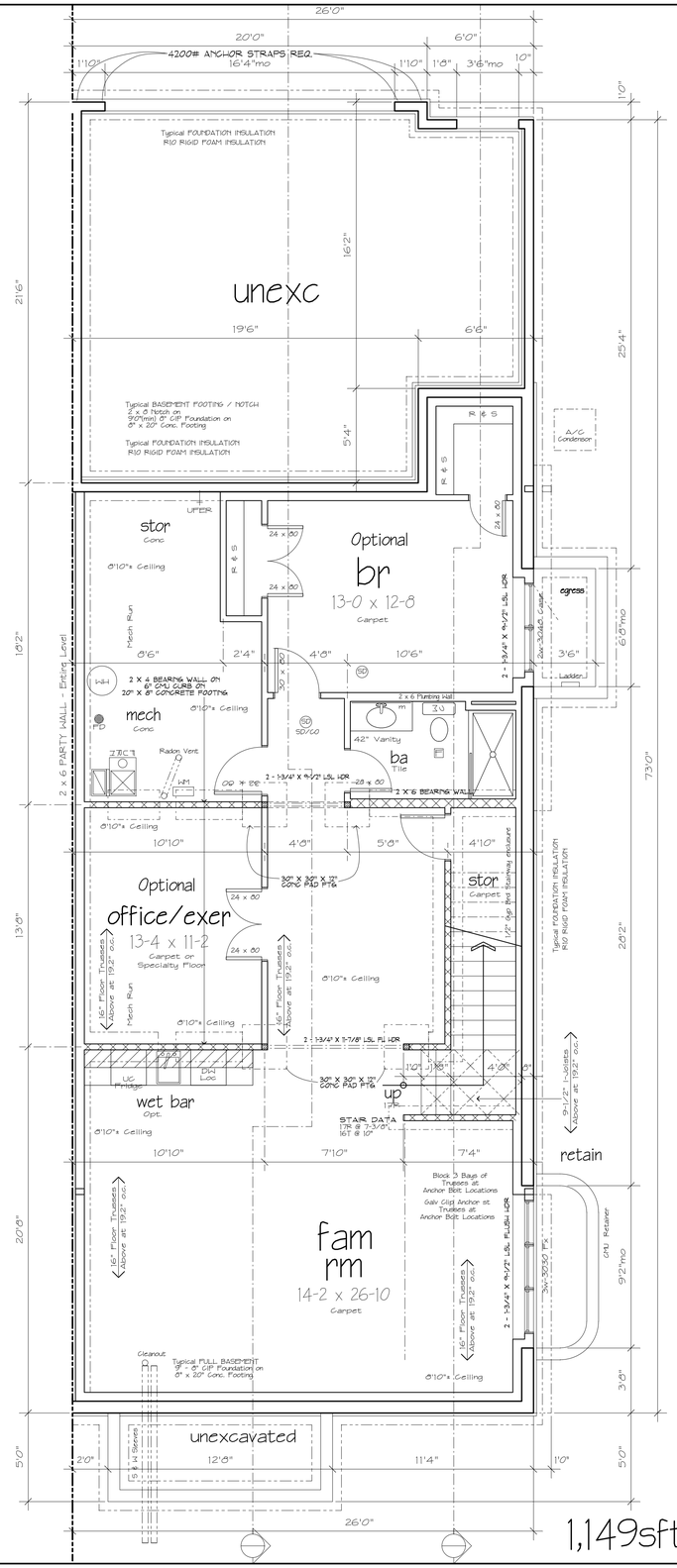

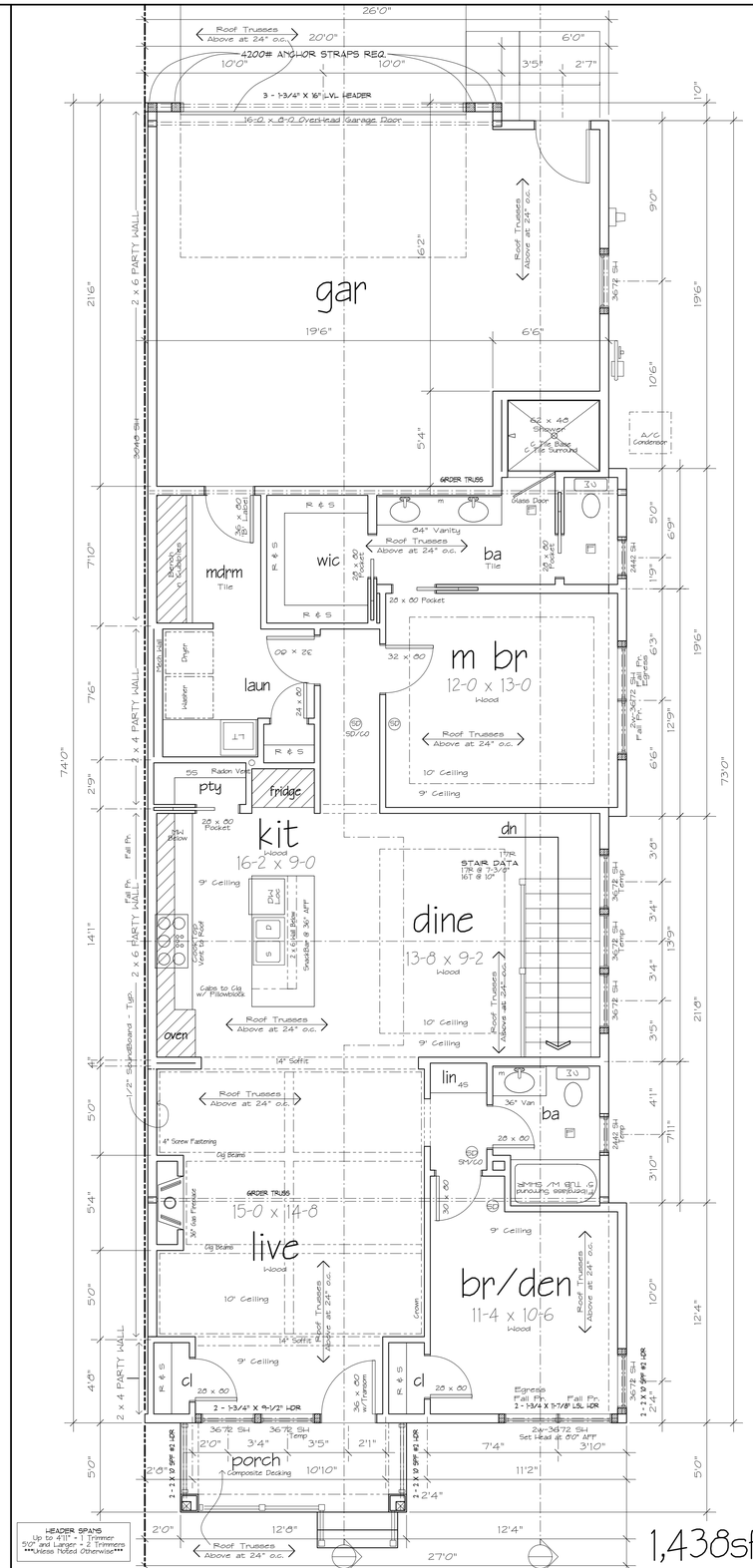

TRADITIONS
BY DONNAY HOMES, LLC

9655 - 63rd Avenue North
Maple Grove, Minnesota 55369
(763) 531-0714
Builder #722939

No.	ISSUED/REVISIONS/SUBMISSIONS	DATE

Project Name
Grandview Townhomes of Edina - 2
Hankerson Ave
Edina, Minnesota

Drawing Title
UNIT PLAN

BLDG. No.

Drawn: SKB
Checked
Reviewed
Date: 12/6/23
Drawing No. A3.1
of

1

2 LOWER LEVEL PLAN 1,149sft

3 MAIN LEVEL PLAN 1,438sft

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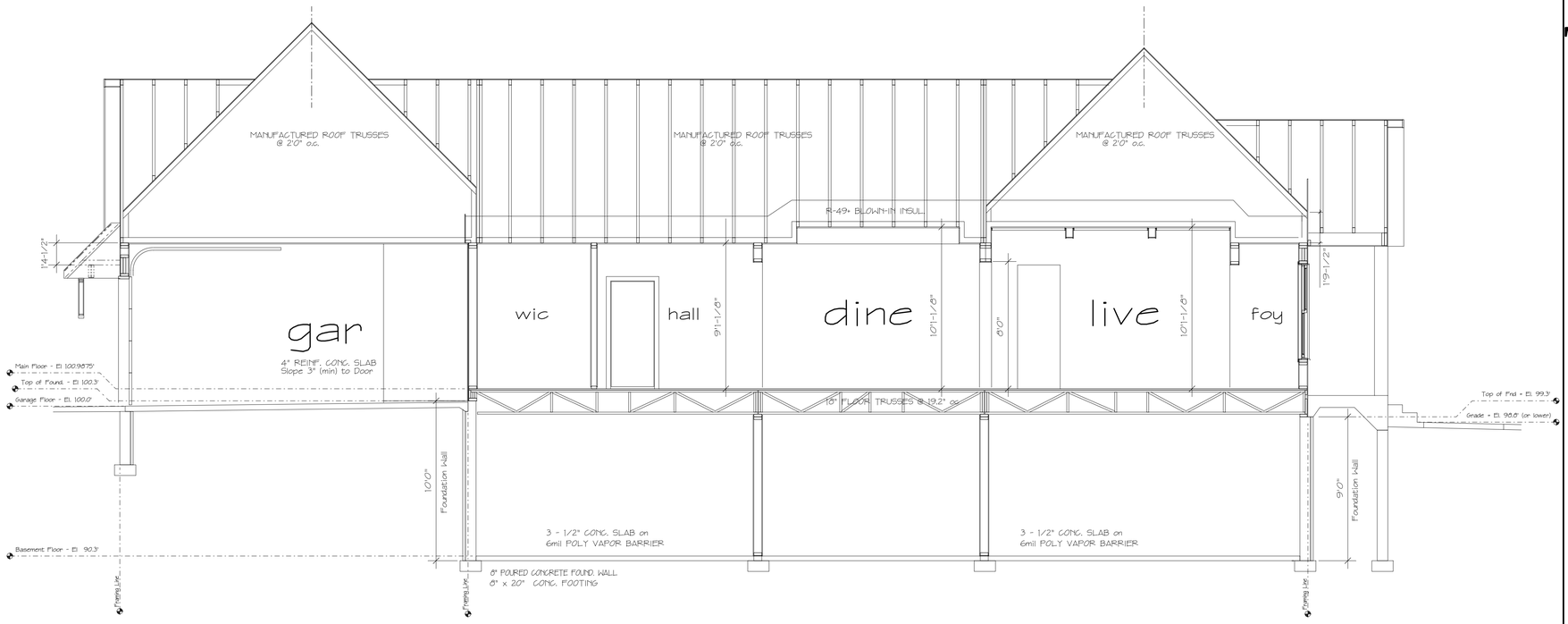
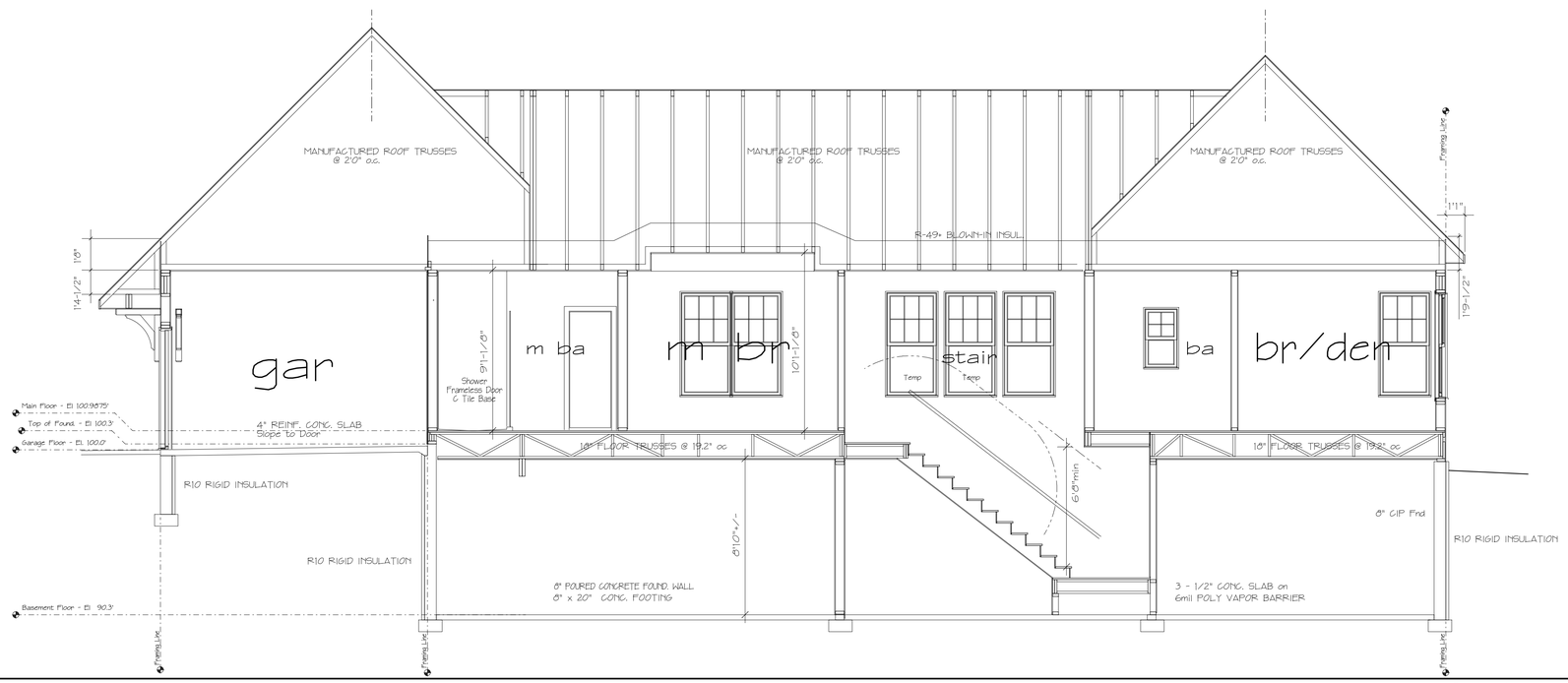


2 SECTIONS



1 SECTIONS

TRADITIONS
BY DONNAY HOMES, LLC
9655 - 63rd Avenue North
Maple Grove, Minnesota 55369
(763) 531-0714
Builder #722939

No.	ISSUED/REVISIONS/SUBMISSIONS	DATE

Project Name
Grandview Townhomes of Edina - 2
Hankerson Ave
Edina, Minnesota

Drawing Title
UNIT SECTIONS

BLDG. No.

Drawn: SKB
Checked
Reviewed
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Drawing No. A4.0
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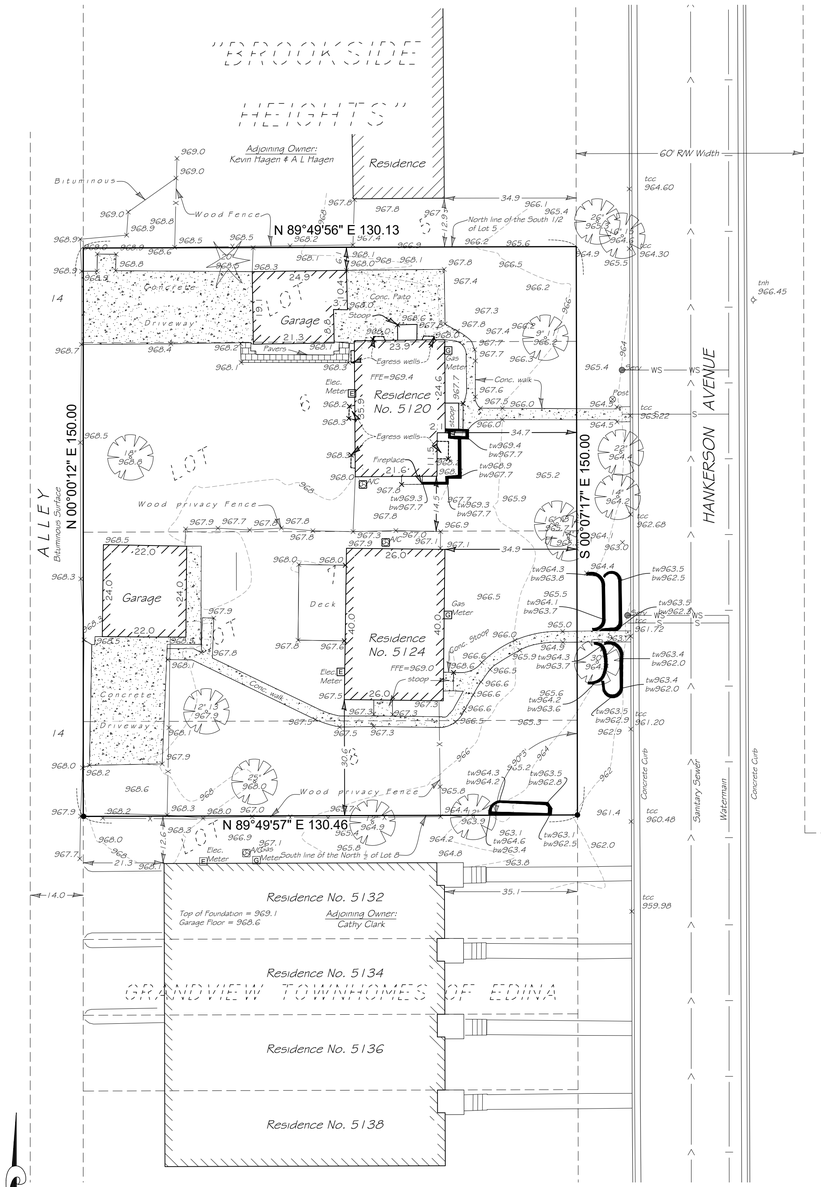


Existing Hardcover	
Lot Area	19,544 sq ft
House No.5124	1,041 sq ft
Garage No.5124	532 sq ft
Stoops No.5124	34 sq ft
Deck No.5124	249 sq ft
Concrete No.5124	1,166 sq ft
Bituminous	64 sq ft
House w/ Fireplace No.5120	845 sq ft
Garage No.5120	445 sq ft
Stoops No.5120	54 sq ft
Concrete Driveway No.5120	878 sq ft
Pavers No. 5120	78 sq ft
Concrete walk & Patio No. 5120	644 sq ft
Total	6,030 sq ft
Percentage	30.85%

Legend

- GAS — Gas Main
- UT — Underground Communications
- UE — Underground Electric
- < — Sanitary Sewer
- << — Storm Sewer
- I — Watermain
- W — Overhead Wires
- SF — Silt Fence or Biorolls
- Existing Contours
- Proposed Contours
- x000.0 — Existing Elevation
- 000.0 — Proposed Elevation
- Proposed Surface Drainage
- Denotes Found Iron Monument
- Denotes Iron Monument Set
- Bollard
- Light
- Power Pole
- Sanitary Manhole
- Storm Manhole
- Water Manhole
- Catch Basin

Personnel

Current Property Owner:

Dennis Bornfleth
5124 Hankerson Avenue
Edina, MN 55436

Developer:

Donnay Homes
9655 63rd Avenue North
Maple Grove, MN 55369
Attn: Steve Benke
612.290.5570

Surveyor:

Demarc Surveying and Engineering
7601 73rd Avenue N.
Brooklyn Park, MN 55428
Attn: Greg Prasch
phone: 763-560-3093
e-mail: gregprasch@demarcinc.com

Miscellaneous Notes

1. Property Address: 5120 & 5124 Hankerson Ave., Edina, MN 55436
   PID No.: 28-117-21-32-0144 & 28-117-21-32-0144
2. Area of Parcel = 19,544 sq. ft.
3. By graphic interpretation only, this property is in Flood Zone "X" (area of minimal flooding) per FEMA panel map number 27053C0361F, dated 11/04/2016
4. Title insurance commitment showing property description and any encumbrances of record not provided, survey subject to change.
   The only easements shown are from plats of record or information provided by client.
5. The land surveyed covers the entire parcel and there are no gaps or overlaps with adjacent parcels.
6. Property located in Section 28, Township 117, Range 21, Hennepin County, Minnesota.
6. Benchmark: Top nut of hydrant at Hankerson Ave. S. and 52nd Street W.
   Elevation = 957.62

Legal Description

Lot 6 and the South Half of Lot 5, Block 9, BROOKSIDE HEIGHTS, Hennepin County, Minnesota.

And

Lot 7 and the North Half of Lot 8, Block 9, BROOKSIDE HEIGHTS, Hennepin County, Minnesota.

Zoning Information

Property Zoned: R-1 Single Dwelling Unit

Setbacks:

Front = 30 feet
Side Street = 15 feet
Side = 10 feet
Rear = 25 feet

SCALE IN FEET
0 20 40 60

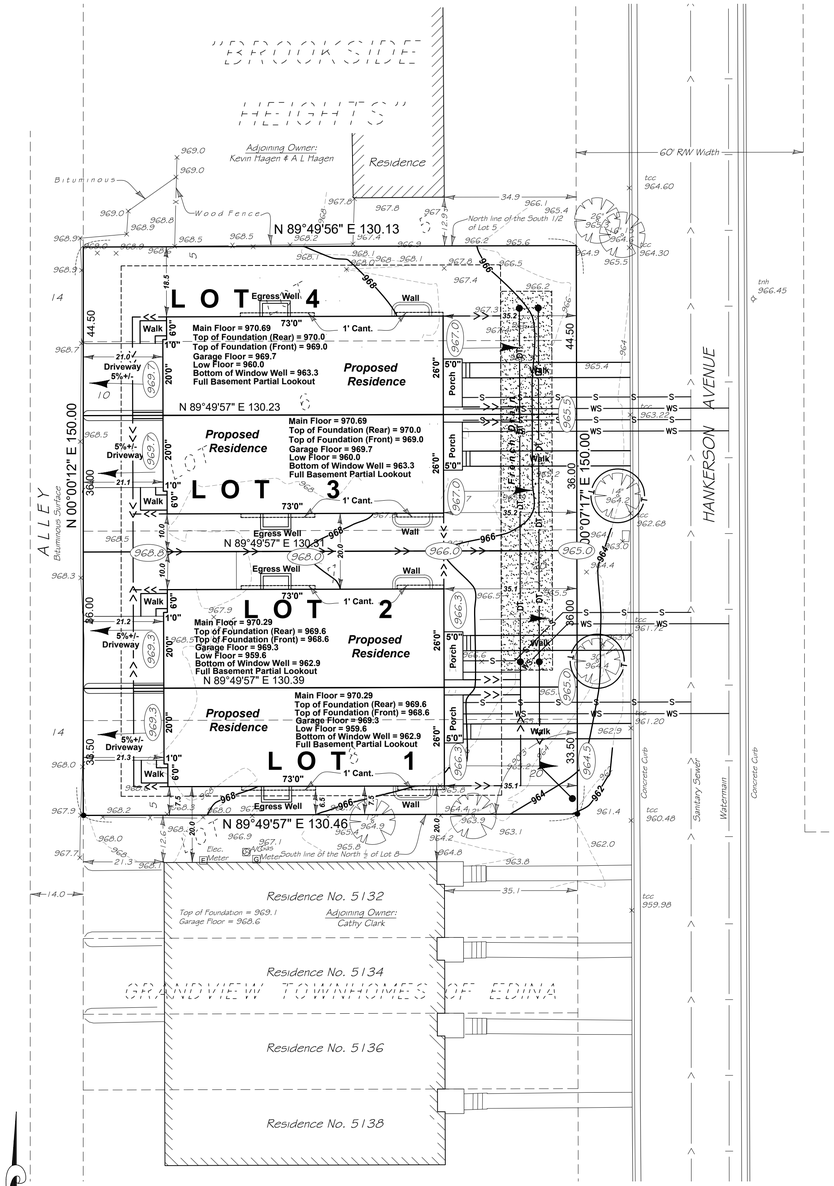
SHEET INDEX

Sheet 1 - Existing Conditions Survey
Sheet 2 - Site Plan
Sheet 3 - Preliminary Plat
Sheet 4 - Grading & Stormwater Erosion Control Plan
Sheet 5 - Utility Plan

F:\survey\brookside heights - hennl\5-9-9 brkside hght - grandview townhome 2nd\01 Surveying - 90570\01 CAD\01 Source\01 Survey Base.dwg

FIELD BY: ?	I certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed land Surveyor under the laws of the State of Minnesota. Surveyed this 14th day of December 2023.	REVISION SUMMARY	DEMARC LAND SURVEYING & ENGINEERING 7601 73rd Avenue North Minneapolis, Minnesota 55428 (763) 560-3093 Demarclnc.com	SURVEY FOR: DONNAY HOMES 9655 63rd Avenue North Maple Grove, MN 55369	TYPE OF SURVEY: EXISTING CONDITIONS	DEVELOPMENT OF: GRANDVIEW TOWNHOMES OF EDINA 2ND ADDITION	PROJECT: 90570A
DRAWN BY: BT	Signed: Gregory R. Prasch / Gregory R. Prasch Registration No. 24992	01-30-24 existing services					FB No: 1109-46
CHECKED BY: GRP							SHEET NO. 1 OF 5

Submitted
03-01-2024
Planning Division



SCALE IN FEET

0 20 40 60

SHEET INDEX

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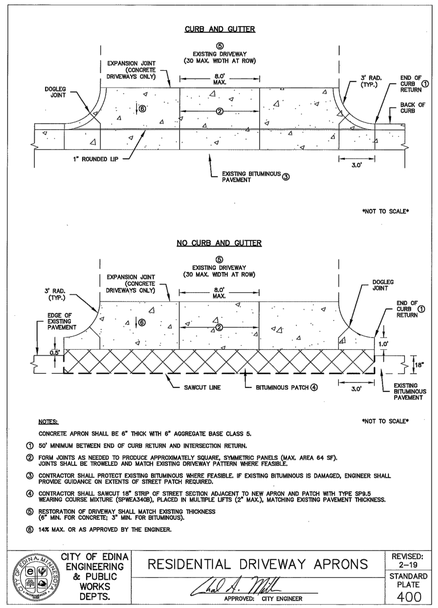

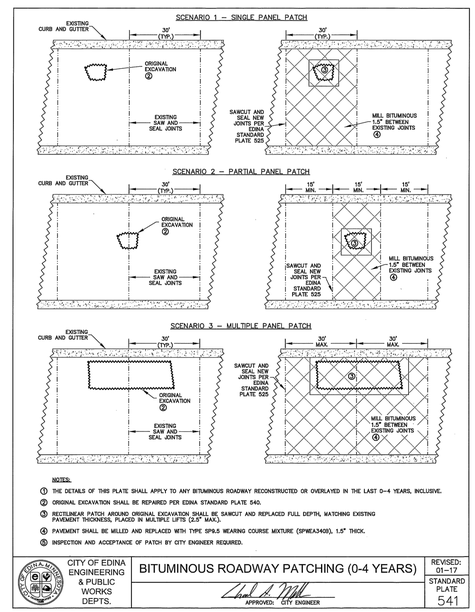

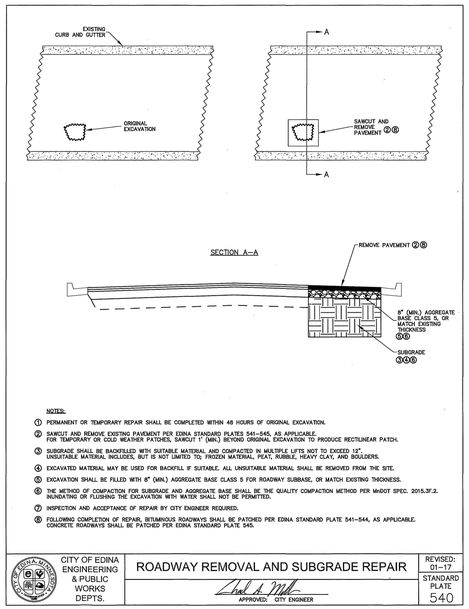

Legend

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- UT — Underground Communications
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- < — Sanitary Sewer
- << — Storm Sewer
- I — Watermain
- W — Overhead Wires
- SF — Silt Fence or Biorolls
- Existing Contours
- Proposed Contours
- x000.0 — Existing Elevation
- 000.0 — Proposed Elevation
- Proposed Surface Drainage
- Denotes Found Iron Monument
- Denotes Iron Monument Set
- Bollard
- Light
- Power Pole
- Sanitary Manhole
- Storm Manhole
- Water Manhole
- Catch Basin

Personnel

Current Property Owner:
Dennis Bornfleth
5124 Hankerson Avenue
Edina, MN 55436

Developer:
Donnay Homes
9655 63rd Avenue North
Maple Grove, MN 55369
Attn: Steve Benke
612.290.5570

Surveyor:
Demarc Surveying and Engineering
7601 73rd Avenue N.
Brooklyn Park, MN 55428
Attn: Greg Prasch
phone: 763-560-3093
e-mail: gregprasch@demarcinc.com

Zoning & Development Information

Existing Zoning Classification - R-1 (Single Dwelling Unit District)

Proposed Zoning Classification - PUD (Planned Unit Development)

Refer to City code for additional requirements and variance information.

Proposed Number of Lots = 4

Area of proposed:
Lot 1, Block 1 = 4,370 sq.ft - (5126 Hankerson Ave.)
Lot 2, Block 1 = 4,693 sq.ft - (5124 Hankerson Ave.)
Lot 3, Block 1 = 4,690 sq.ft - (5122 Hankerson Ave.)
Lot 4, Block 1 = 4,792 sq.ft - (5120 Hankerson Ave.)

Right-of-Way Dedication = 0 sq.ft

Proposed Hardcover

Lot Area	19,544 sq ft
Buildings	7,672 sq ft
Porches	253 sq ft
Driveways	1,498 sq ft
Walks	627 sq ft
Total	10,050 sq ft
Percentage	51.42%

Proposed Building Coverage

Lot Area	19,544 sq ft
Buildings	7,672 sq ft
Porches	253 sq ft
Total	7,925 sq ft
Percentage	40.55%

Miscellaneous Notes

1. Property Address: 5120 & 5124 Hankerson Ave., Edina, MN 55436
PID No.: 28-117-21-32-0144 & 28-117-21-32-0144
2. Area of Parcel = 19,544 sq. ft.
3. By graphic interpretation only, this property is in Flood Zone "X" (area of minimal flooding) per FEMA panel map number 27053C0361F, dated 11/04/2016
4. Title insurance commitment showing property description and any encumbrances of record not provided, survey subject to change.
The only easements shown are from plats of record or information provided by client.
5. The land surveyed covers the entire parcel and there are no gaps or overlaps with adjacent parcels.
6. Property located in Section 28, Township 117, Range 21, Hennepin County, Minnesota.
6. Benchmark: Top nut of hydrant at Hankerson Ave. S. and 52nd Street W.
Elevation = 957.62

Drainage & Utility Easements

Proposed dedicated drainage and utility easements will be thus:
20 feet along Hankerson Avenue
10 feet along the alley
5 feet along the most northerly and southerly lot lines of development.

Legal Description

Lot 6 and the South Half of Lot 5, Block 9, BROOKSIDE HEIGHTS, Hennepin County, Minnesota.

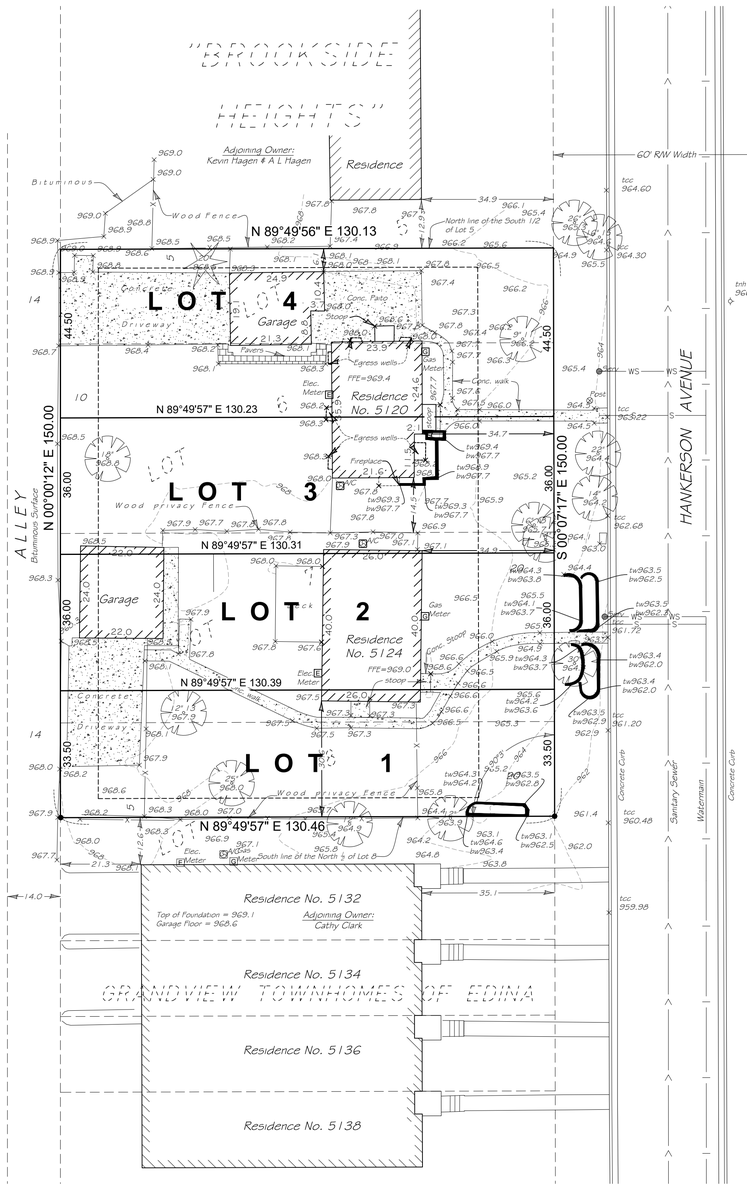
And

Lot 7 and the North Half of Lot 8, Block 9, BROOKSIDE HEIGHTS, Hennepin County, Minnesota.

FIELD BY: ?	I certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed land Surveyor under the laws of the State of Minnesota. Prepared this 17th day of January 2024.	REVISION SUMMARY	DEMARC LAND SURVEYING & ENGINEERING 7601 73rd Avenue North Minneapolis, Minnesota 55428 (763) 560-3093 Demarcinc.com	SURVEY FOR: DONNAY HOMES 9655 63rd Avenue North Maple Grove, MN 55369	TYPE OF SURVEY: SITE PLAN	DEVELOPMENT OF: GRANDVIEW TOWNHOMES OF EDINA 2ND ADDITION	PROJECT: 90570A
DRAWN BY:	Signed: Gregory R. Prasch Registration No. 24992	01-31-24 proposed services					FB No: 1109-46
CHECKED BY: GRP							SHEET NO. 2 OF 5

F:\survey\brookside heights - item15-9-9 brkside hght - grandview townhome 2nd\01 Surveying - 90570\01 CADD\1 Source\01 Survey Base.dwg

Submitted
03-01-2024
Planning Division



Legend

GAS	Gas Main
UT	Underground Communications
UE	Underground Electric
<	Sanitary Sewer
<<	Storm Sewer
I	Watermain
W	Overhead Wires
	Denotes Existing Contours
●	Denotes Found Iron Monument
○	Denotes Iron Monument Set
⊗	Bollard
☼	Light
	Power Pole
Ⓢ	Sanitary Manhole
Ⓓ	Storm Manhole
Ⓦ	Water Manhole
	Catch Basin
x000.0	Denotes Existing Elevation

SCALE IN FEET

0 20 40 60

Personnel

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5 feet along the most northerly and southerly lot lines of development.

Miscellaneous Notes

1. Property Address: 5120 & 5124 Hankerson Ave., Edina, MN 55436
PID No.: 28-117-21-32-0144 & 28-117-21-32-0144
2. Area of Parcel = 19,544 sq. ft.
3. By graphic interpretation only, this property is in Flood Zone "X" (area of minimal flooding) per FEMA panel map number 27053C0361F, dated 11/04/2016
4. Title insurance commitment showing property description and any encumbrances of record not provided, survey subject to change.
The only easements shown are from plats of record or information provided by client.
5. The land surveyed covers the entire parcel and there are no gaps or overlaps with adjacent parcels.
6. Property located in Section 28, Township 117, Range 21, Hennepin County, Minnesota.
6. Benchmark: Top nut of hydrant at Hankerson Ave. S. and 52nd Street W.
Elevation = 957.62
7. Improvements shown are the current existing conditions which will be razed.
Refer to the 'Site Plan' for future improvement locations and elevations

Legal Description

Lot 6 and the South Half of Lot 5, Block 9, BROOKSIDE HEIGHTS, Hennepin County, Minnesota.

And

Lot 7 and the North Half of Lot 8, Block 9, BROOKSIDE HEIGHTS, Hennepin County, Minnesota.

SHEET INDEX
Sheet 1 - Existing Conditions Survey
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Sheet 5 - Utility Plan

FIELD BY: RP	I certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed land Surveyor under the laws of the State of Minnesota. Prepared this 17th day of January 2024. Signed: Gregory R. Prasch Registration No. 24992	REVISION SUMMARY	DEMARC LAND SURVEYING & ENGINEERING 7601 73rd Avenue North Minneapolis, Minnesota 55428 (763) 560-3093 Demarcinc.com	SURVEY FOR: DONNAY HOMES 9655 63rd Avenue North Maple Grove, MN 55369	TYPE OF SURVEY: PRELIMINARY PLAT	DEVELOPMENT OF: GRANDVIEW TOWNHOMES OF EDINA 2ND ADDITION	PROJECT: 90570
DRAWN BY: GRP							FB No: 1109-46
CHECKED BY: GRP							SHEET NO. 3 OF 5

F:\survey\brookside heights - hennl\5-9-9 brkside hght - grandview townhome 2nd\01 Surveying - 90570\01 CAD\01 Source\01 Survey Base.dwg

Submitted
03-01-2024
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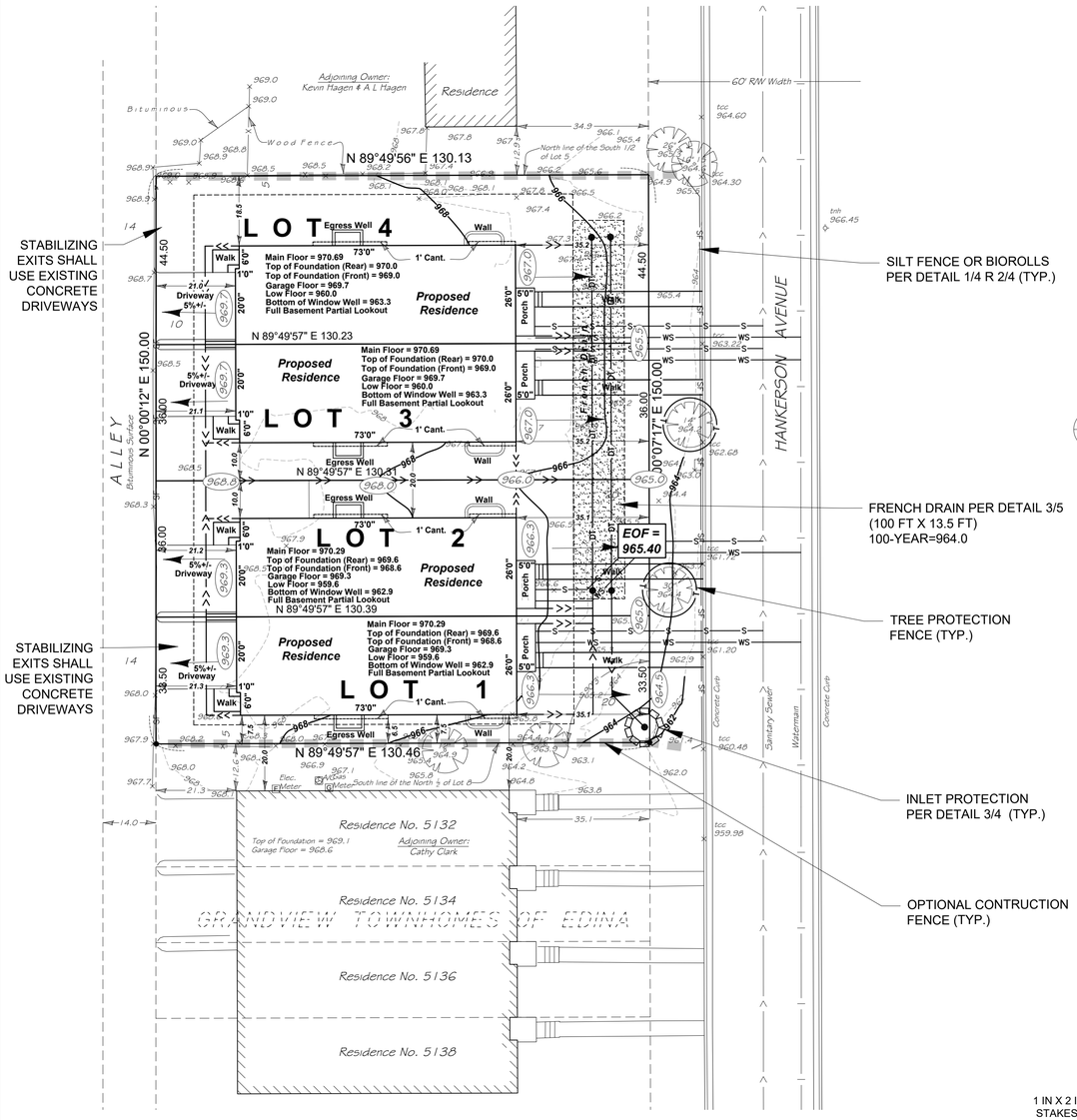
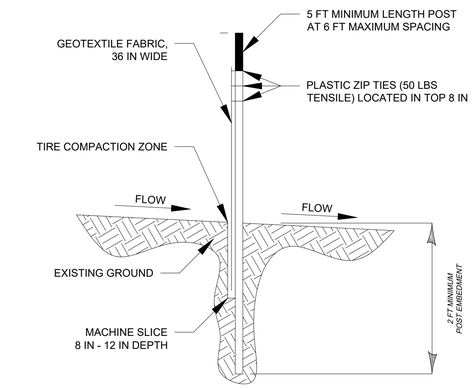




1/4 SILT FENCE (MACHINE SLICED)
NOT TO SCALE

GENERAL GRADING NOTES:
1. THE STANDARD SPECIFICATIONS FOR THE CITY OF EDINA SHALL APPLY EXCEPT WHERE MODIFIED BY THESE DOCUMENTS.
2. THE CONTRACTOR SHALL NOTIFY "GOPHER STATE ONE CALL" 48 HOURS PRIOR TO ANY EXCAVATION. (1-800-252-1166)
3. THE GRADING CONTRACTOR SHALL FIELD VERIFY ALL LOCATIONS AND ELEVATIONS OF UNDERGROUND UTILITIES WITH UTILITY COMPANIES PRIOR TO ANY CONSTRUCTION, AND IMMEDIATELY NOTIFY THE ENGINEER OF ANY CONFLICTS.
4. THE GRADING CONTRACTOR SHALL PROVIDE POSITIVE DRAINAGE ON THE SITE AT ALL TIMES.
5. EXISTING TOPSOIL SHALL BE SALVAGED TO PROVIDE 4" TOPSOIL COVERAGE OVER ALL DISTURBED AREAS TO BE REVEGETATED.
6. THE BUILDING PAD MUST BE PROVIDED WITH A POSITIVE OUTFLOW. THIS WORK SHALL BE INCIDENTAL TO THE GRADING CONTRACT.
7. ANY SEDIMENT REMAINING IN PLACE AFTER THE SILT FENCE HAS BEEN REMOVED SHALL BE DRESSED TO CONFORM WITH THE EXISTING GRADE, PREPARED AND SEEDED OR SODDED AS DIRECTED BY THE CITY ENGINEER.
8. NO FINISHED SLOPE SHALL EXCEED 4H : 1V UNLESS OTHERWISE NOTED.
9. PERMITEE MUST MINIMIZE SOIL COMPACTION. METHODS OF MINIMIZING SOIL COMPACTION INCLUDE THE USE OF TRACKED EQUIPMENT.

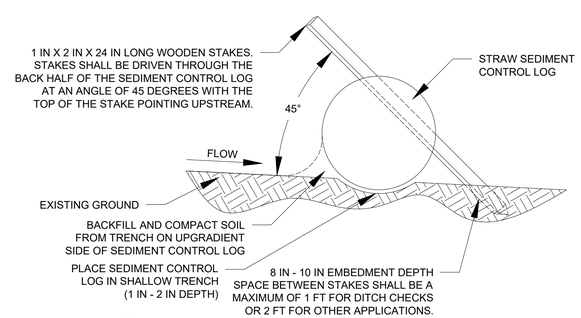
EROSION CONTROL NOTES:
1. ALL EROSION AND SEDIMENT CONTROL BMP'S (I.E. SILT FENCE, BIO-ROLLS, INLET PROTECTION, ETC.) SHALL BE INSTALLED PRIOR TO THE COMMENCEMENT OF ANY CONSTRUCTION ACTIVITY.
2. INLET PROTECTION SHALL BE INSTALLED AT ANY INLET THAT MAY RECEIVE RUNOFF FROM THE DISTURBED AREAS OF THE PROJECT. INLET PROTECTION MAY BE REMOVED FOR A PARTICULAR INLET IF A SPECIFIC SAFETY CONCERN (FLOODING / FREEZING) HAS BEEN IDENTIFIED. THE PERMITTED MUST RECEIVE WRITTEN CORRESPONDENCE FROM THE CITY ENGINEER VERIFYING THE NEED FOR REMOVAL.
3. INSTALL SEDIMENT CONTROL BMP'S, SUCH AS SILT FENCE, AROUND ALL STOCKPILES.
4. RETAIN AND PROTECT AS MUCH NATURAL VEGETATION AS FEASIBLE. WHEN VEGETATION IS REMOVED DURING DEVELOPMENT, THE EXPOSED CONDITION OF LAND SHALL BE KEPT TO THE SHORTEST PRACTICAL PERIOD OF TIME, BUT NOT LONGER THAN 60 DAYS. ANY EXPOSED AREAS EXCEEDING THIS TIME-FRAME SHALL BE TEMPORARILY STABILIZED (STRAW MULCH, WOODCHIPS, ROCK). AREAS BEING USED FOR MATERIAL STORAGE AND AREAS UNDER CONSTRUCTION ARE EXEMPT FROM TEMPORARY STABILIZATION.
5. ANY STEEP SLOPES (3H : 1V OR STEEPER) EXPOSED DURING CONSTRUCTION SHALL BE PROTECTED WITH TEMPORARY VEGETATION, MULCHING OR BY OTHER MEANS ACCEPTABLE TO THE BUILDING OFFICIAL WITHIN 14 DAYS OF CEASING LAND DISTURBING ACTIVITIES ON THE STEEP SLOPES. STOCKPILES MAY BE PROTECTED BY AN ANCHORED TARP OR PLASTIC SHEET.
6. PROVIDE DUST CONTROL AS NECESSARY. DUST CONTROL CAN INCLUDE WATER.
7. REMOVE ALL SOILS AND SEDIMENTS TRACKED OR OTHERWISE DEPOSITED ONTO PUBLIC PAVEMENT AREAS ON A DAILY BASIS OR AS NEEDED.
8. ALL EROSION AND SEDIMENT CONTROL BMP'S SHALL BE INSPECTED EVERY 7 DAYS, OR WITHIN 24 HOURS OF ALL RAIN EVENTS GREATER THAN 1.0" IN 24 HOURS. CORRECTIVE ACTION REQUIRED SHALL BE INITIATED IMMEDIATELY.
9. SILT FENCE, BIO-ROLLS AND INLET PROTECTION DEVICES MUST BE REPAIRED, REPLACED OR SUPPLEMENTED WHEN THEY BECOME NONFUNCTIONAL OR THE SEDIMENT REACHES 1/3 THE HEIGHT OF THE DEVICE. THESE REPAIRS MUST BE MADE WITHIN 24 HOURS OF DISCOVERY, OR AS SOON AS FIELD CONDITIONS ALLOW.
10. AFTER FINAL GRADING HAS BEEN COMPLETED, EXPOSED SOILS MUST BE PERMANENTLY STABILIZED AS SOON AS POSSIBLE. PERMANENT STABILIZATION SHALL CONSIST OF 4 INCHES TOPSOIL, AND SEED, MULCH AND FERTILIZER APPLIED BY METHODS AND RATES RECOMMENDED IN MN/DOT SPECIFICATION 2575 AND MN/DOT SEEDING MANUAL, OR SOD. THE SEED MIX SHALL BE MN/DOT 25-151.
11. NO CONCRETE WASHOUT ALLOWED ON SITE, TRUCK BASED SELF CONTAINMENT WASHOUT DEVICES REQUIRED.
12. OIL STAINS ON CITY STREETS SHALL BE CLEANED UP WITH FLOOR DRY, AND DISPOSED OF AS A HAZARDOUS WASTE MATERIAL.
13. ALL HAZARDOUS WASTE SHALL BE STORED CLEANED UP AND DISPOSED OF PER EPA STANDARDS.
14. ALL EROSION AND SEDIMENT CONTROL DEVICES SHALL BE MAINTAINED UNTIL ALL DISTURBED AREAS HAVE BEEN PERMANENTLY STABILIZED.
15. ALL EROSION AND SEDIMENT CONTROL DEVICES SHALL BE REMOVED FROM THE SITE AFTER PERMANENT STABILIZATION HAS BEEN ESTABLISHED.
16. TEMPORARY PUMPED DISCHARGE POLLUTION PREVENTION TECHNIQUES: "DANDY DEWATERING BAG" BROCK WHITE CO. USA.
17. CONTACT PERSON FOR SITE CLEANLINESS AND MAINTENANCE OF THE EROSION AND SEDIMENT CONTROLS: PAUL DONNAY (763) 531-0714
18. NINE MILE CREEK WATERSHED DISTRICT: (952) 835-2078

Existing Hardcover

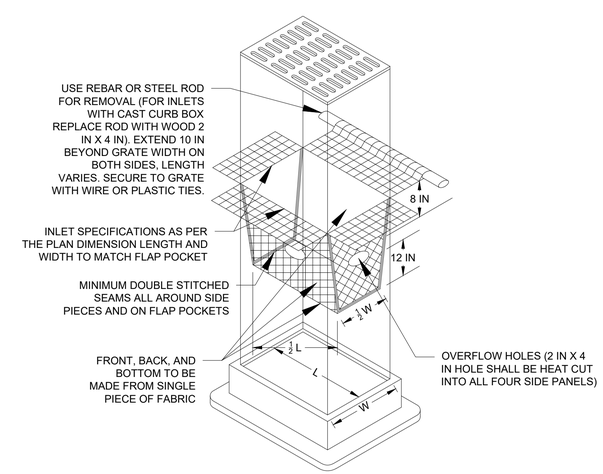
Lot Area	19,544 sq ft
House No.5124	1,041 sq ft
Garage No.5124	532 sq ft
Stoops No.5124	34 sq ft
Deck No.5124	249 sq ft
Concrete No.5124	1,166 sq ft
Bituminous	64 sq ft
House w/ Fireplace No.5120	845 sq ft
Garage No.5120	445 sq ft
Stoops No.5120	54 sq ft
Concrete Drveway No.5120	878 sq ft
Pavers No. 5120	78 sq ft
Concrete walk & Patio No. 5120	644 sq ft
Total	6,030 sq ft
Percentage	30.85%

Proposed Hardcover

Lot Area	19,544 sq ft
Buildings	7,672 sq ft
Porches	253 sq ft
Drveways	1,498 sq ft
Walks	627 sq ft
Total	10,050 sq ft
Percentage	51.42%



2/4 BIOROLLS
NOT TO SCALE



3/4 INLET PROTECTION
NOT TO SCALE

0 20 40
SCALE IN FEET

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DRAWN BY: ABL			
CHECKED BY: GRP	JEFFREY A. PRASCH, P.E. DATE: 01.25.24 LIC. NO.: 52706		

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GRANDVIEW TOWNHOMES OF EDINA
5132-5148 HANKERSON AVENUE
EDINA, MINNESOTA

GRADING & STORMWATER EROSION CONTROL PLAN

PROJECT: 90570A
SHEET NO. 4 OF 5

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Submitted
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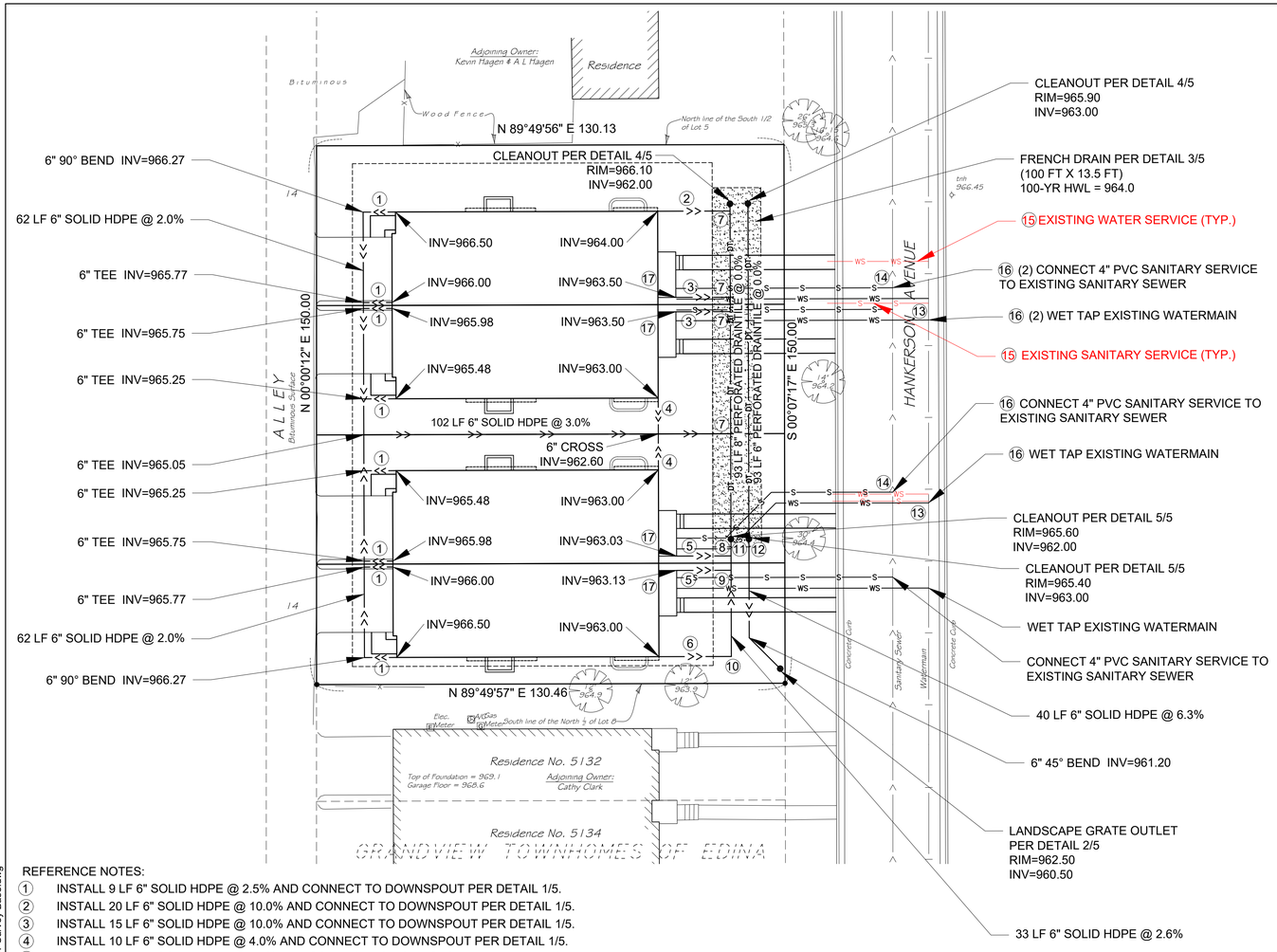
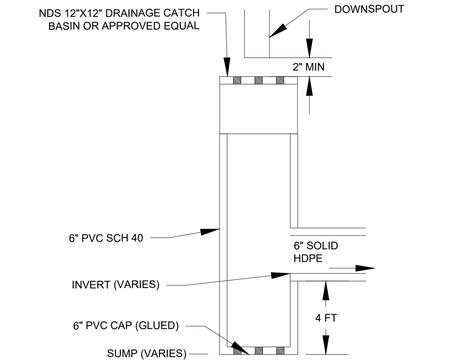
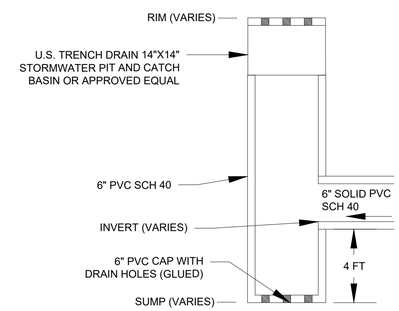




1/5 DOWNSPOUT SUMP
NOT TO SCALE



2/5 LANDSCAPE GRATE/SUMP
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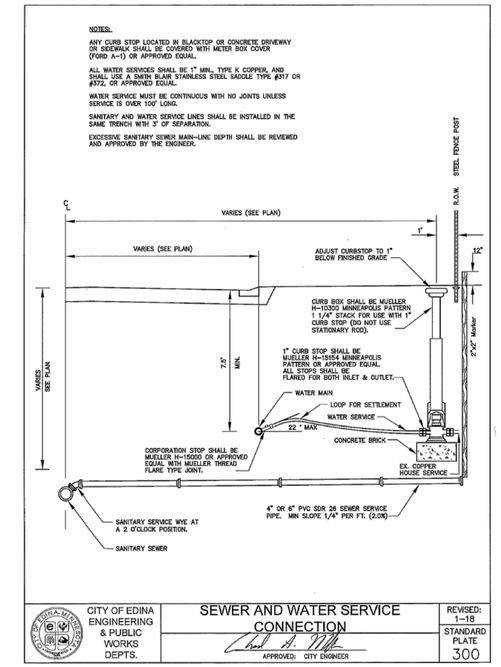

REFERENCE NOTES:

1. INSTALL 9 LF 6" SOLID HDPE @ 2.5% AND CONNECT TO DOWNSPOUT PER DETAIL 1/5.
2. INSTALL 20 LF 6" SOLID HDPE @ 10.0% AND CONNECT TO DOWNSPOUT PER DETAIL 1/5.
3. INSTALL 15 LF 6" SOLID HDPE @ 10.0% AND CONNECT TO DOWNSPOUT PER DETAIL 1/5.
4. INSTALL 10 LF 6" SOLID HDPE @ 4.0% AND CONNECT TO DOWNSPOUT PER DETAIL 1/5.
5. INSTALL 15 LF 6" SOLID HDPE @ 6.0% AND CONNECT TO DOWNSPOUT PER DETAIL 1/5.
6. INSTALL 20 LF 6" SOLID HDPE @ 7.0% AND CONNECT TO DOWNSPOUT PER DETAIL 1/5.
7. INSTALL 8" X 6" TEE INV=962.00
8. INSTALL 6" TEE INV=962.13
9. INSTALL 6" TEE INV=962.35
10. INSTALL 6" 90° BEND INV=962.86
11. INSTALL 6" X 8" INCREASER AND TRANSITION TO PERFORATED DRAINTILE AT CLEANOUT.
12. TRANSITION TO PERFORATED DRAINTILE AT CLEANOUT.
13. (2) INSTALL 1" COPPER WATER SERVICE AND CURB STOP FOR EACH UNIT.
14. (2) INSTALL 4" SANITARY SERVICE AND CLEANOUT FOR EACH UNIT.
15. VERIFY LOCATION OF EXISTING SERVICES AND REMOVE TO THE MAIN.
16. INSTALL PROPOSED SERVICES WITHIN SAME TRENCH TO MINIMIZE ROADWAY DISTURBANCE. PATCH ROADWAY PER CITY STANDARD PLATES 540 AND 541 ON SHEET 2.
17. COORDINATE LOCATION OF SERVICES WITH MECHANICAL/ARCHITECTURAL PLANS.

NOTES:

1. ALL SANITARY SERVICES SHALL HAVE A MINIMUM OF 2.5% SLOPE.
2. ALL SERVICES SHALL BE CONSTRUCTED PER CITY STANDARD PLATE 300.
3. ALL DRAINTILE SHALL BE PERFORATED WITHIN THE FRENCH DRAIN.
4. ALL STORM SEWER OUTSIDE OF FRENCH DRAIN SHALL BE SOLID PIPE.

SHEET INDEX

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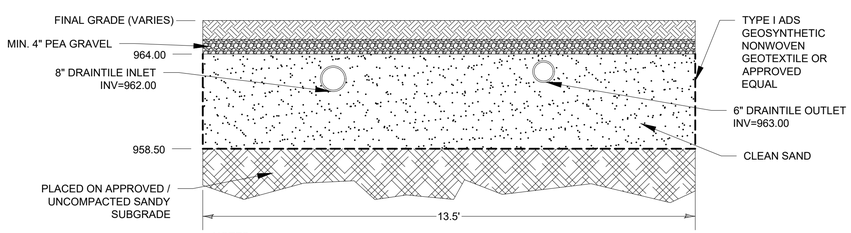
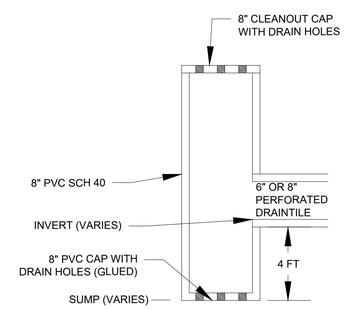
0 20 40
SCALE IN FEET



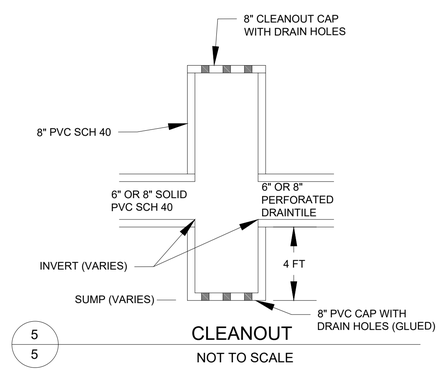
NOTES:

1. CONSTRUCT BOTTOM OF FRENCH DRAIN SYSTEM ON A LEVEL PLANE.
2. IF CLAYEY SOILS ARE ENCOUNTERED AT BOTTOM OF FRENCH DRAIN, EXCAVATE DOWN TO SANDY SOILS AND REPLACE CLAY SOIL WITH SANDY SOIL ONSITE TO 960.00 ELEVATION.

3/5 FRENCH DRAIN
NOT TO SCALE



4/5 CLEANOUT
NOT TO SCALE



5/5 CLEANOUT
NOT TO SCALE

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DRAWN BY: ABL		01.31.24	EXIST./PROP. SERVICES, REFERENCE NOTES
CHECKED BY: GRP	JEFFREY A. PRASCH, P.E. DATE: 01.25.24 LIC. NO.: 52706		

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GRANDVIEW TOWNHOMES OF EDINA 2ND ADDITION
5120-5124 HANKERSON AVENUE
EDINA, MINNESOTA

UTILITY PLAN

PROJECT: 90570A
SHEET NO. 5 OF 5

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Submitted
03 JAN 2024
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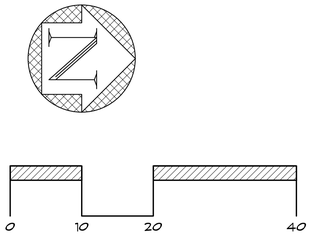

LANDSCAPE SCHEDULE

QN	KEY	COMMON NAME	BOTANICAL NAME	SIZE/ROOT	FULL SIZE	% OF SPECIES
1	ABM	AUTUMN BLAZE MAPLE	ACER x FREEMANII 'JEFFERS RED'	3-1/2" / B&B	50' - 60', 40'	
1	GSL	GREENSPIRE LINDEN	QUERCUS BICOLOR	3-1/2" / B&B	50' - 60', 40' - 50'	
1	HCK	HACKBERRY	CELTIS OCCIDENTALIS	3-1/2" / B&B	40' - 60', 25' - 30'	
1	BHS	BLACK HILLS SPRUCE	PICEA GLAUCA DENSATA	8' / B&B		
0	RSC	RED SPLENDOR CRABAPPLE	MALUS 'RED SPLENDOR'	6' / B&B	20' - 25', 20'	
2	RPC	PRAIRIFIRE CRABAPPLE	MALUS 'RED SPLENDOR'	6' / B&B	20' - 25', 20'	
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MJ	MANEY JUNIPER	JUNIPERUS CHINENSIS 'MANEY'	2" / POT			
PGJ	PFITZER GREEN JUNIPER	JUNIPERUS CHINENSIS 'PFITZER'	2" / POT			
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WA	WOODWARD ARBORVITAE	THUJA OCCIDENTALIS 'WOODWARDII'	2" / POT			
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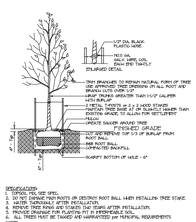
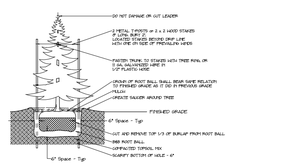


DECIDUOUS TREE DETAIL



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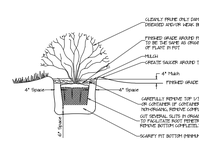
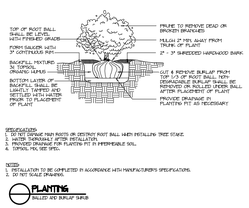
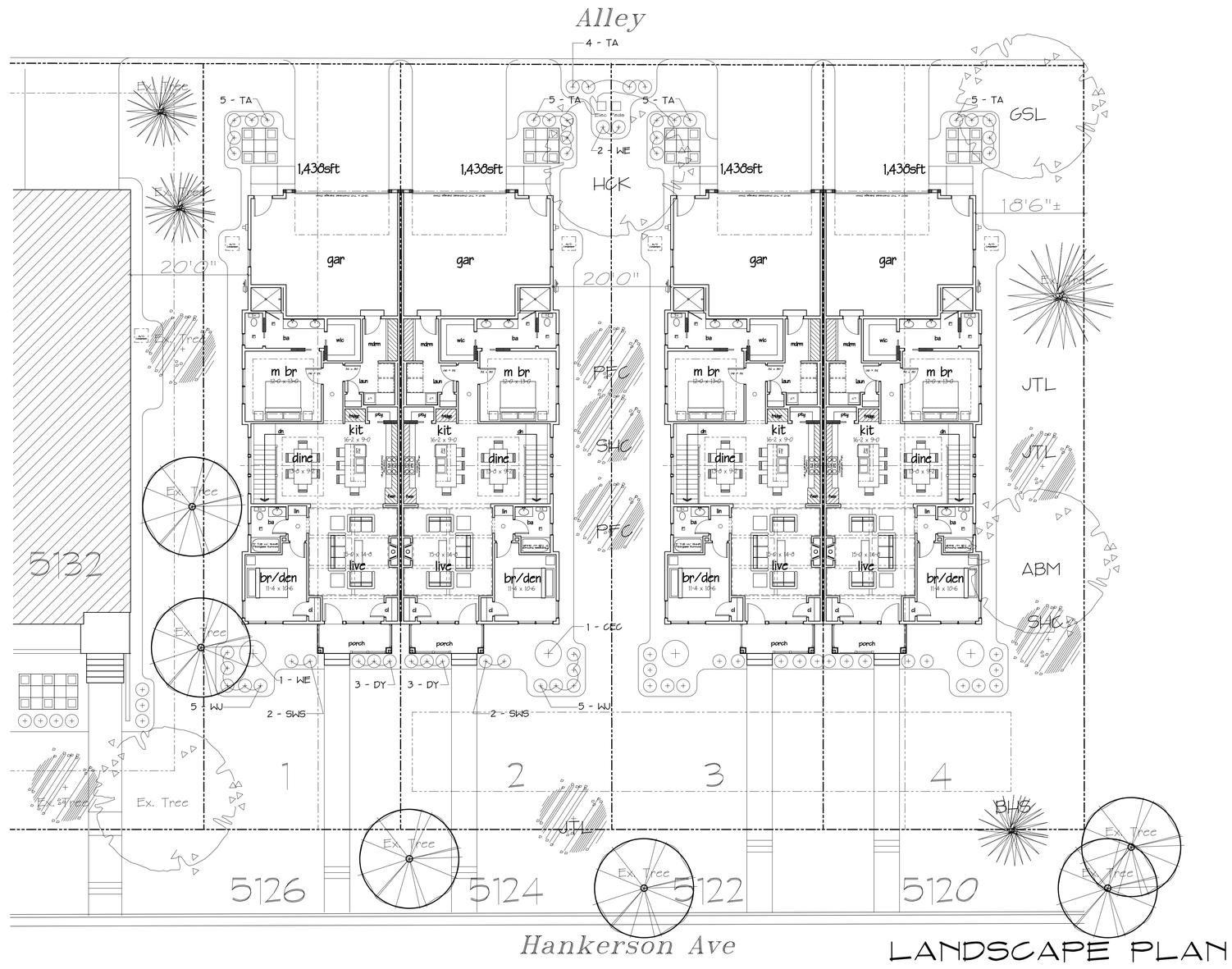


SHRUB DETAIL - POTTED



PLANTING
BALLED AND BURLAP SHRUB



LANDSCAPE PLAN

donnay homes
MORE HOME FOR YOUR MONEY
9655 - 63rd Avenue North
Maple Grove, Minnesota 55369
(612) 531-0714
Builder #20593848

No.	ISSUED/REVISIONS/SUBMISSIONS	DATE
A	Issued / Land Use App	2/10/24

Project Name
GRANDVIEW TOWNHOMES
PHASE 2
52nd St & Hankerson Ave
Edina, Minnesota

Drawing Title
LANDSCAPE
PLAN

BLDG. No.

Drawn

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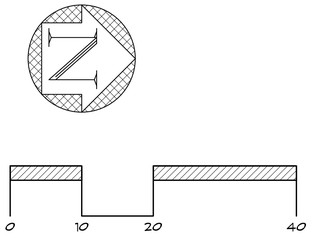
Submitted
03 JAN 2024
Planning Division



LANDSCAPE SCHEDULE

QN	KEY	COMMON NAME	BOTANICAL NAME	SIZE/ROOT	FULL SIZE	% OF SPECIES
1	ABM	AUTUMN BLAZE MAPLE	ACER x FREEMANII 'JEFFERS RED'	3-1/2" / B&B	50' - 60', 40'	
1	GSL	GREENSPIRE LINDEN	QUERCUS BICOLOR	3-1/2" / B&B	50' - 60', 40' - 50'	
1	HCK	HACKBERRY	CELTIS OCCIDENTALIS	3-1/2" / B&B	40' - 60', 25' - 30'	
1	BHS	BLACK HILLS SPRUCE	PICEA GLAUCA DENSATA	8' / B&B		
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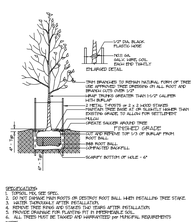

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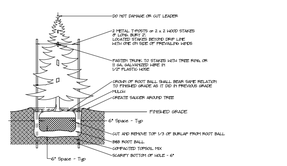

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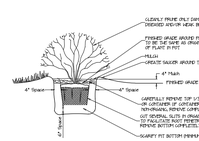

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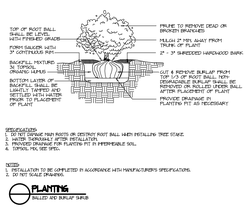

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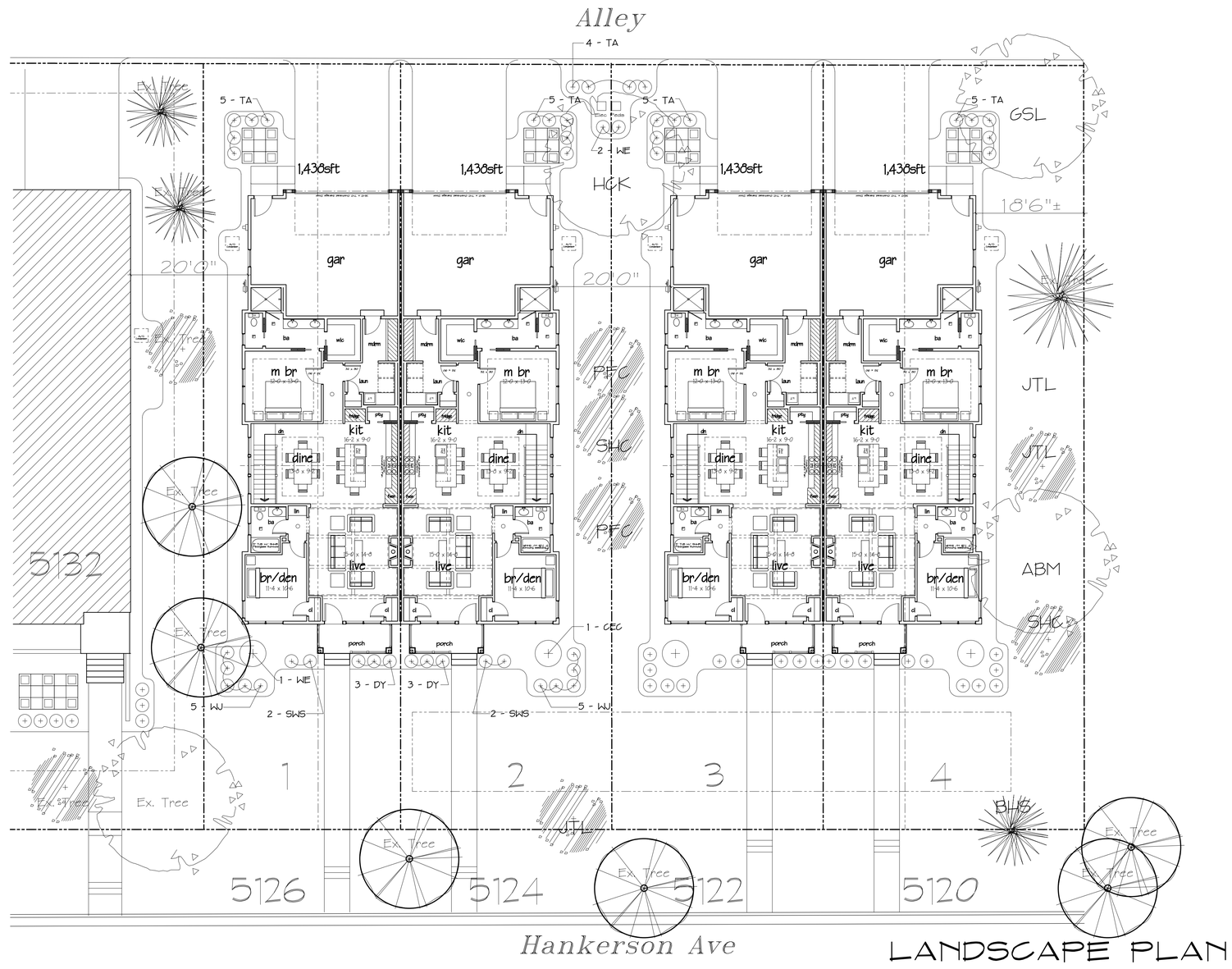

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Submitted
03-01-2024
Planning Division

Date: February 19, 2024

Re: Hankerson Avenue Redevelopment
5120 and 5124 Hankerson Ave.
PID 28-117-21-32-0144, PID 28-117-21-32-0172

Background: This project is adjacent to the previously approved, built, and fully occupied, Grandview Townhomes of Edina PUD; we have endeavored to create a complimentary project with a different style of unit to address community feedback.
We encountered considerable demand for additional homes in this area during the 2021 and 2022 Parade of Homes, and specifically for the type of homes found in this proposal.
Many commenters were asking to be able to move within the City of Edina; they are strongly connected to the Community and wish to stay in Edina. But their housing needs have changed. They are looking for greater convenience and less space in their homes, one-level living, and proximity to community services.
Unit Pricing starting in the $800's is an affordable, move-down product to the cohort these market-rate units are aimed at. The homes these residents leave then become available to new residents, or existing resident move-ups. The real estate process then continues helping new residents to move into Edina; this project could free up 4, truly affordable homes in the city to new residents.
The proposed $800k price point is also similar to the value of several remodeled single-family homes on this block.

Neighborhood Meeting: This was held on February 7, 2024, at the Edina Library starting at 5pm. Approximately 12 resident families were represented out of 199 letters sent. Outside of 2 immediate neighbors to the property, the project received generally positive comments. The most common issue the neighbors expressed concern about was speeding in the alley behind this proposal and they asked for assistance in communicating this to the city.
We do feel this is a city issue not related to this proposal, but we also express concern that traffic and other laws are being upheld in the city of Edina. We want to remind the Planning Commission and Council that we have worked with the city's Administrative and Health departments to deal with parking and trash issues in the area.

Proposal: The project consists of building a total of 4 units on the 2 subject properties in 2 – two-unit Rambler Townhome Buildings. The project also includes the removal of two, older rambler homes; one built in 1956 and one built in 1953. Both 1950's homes suffer from minimal insulation, dated and worn finishes, and mechanicals. The 1953 home has only 2 bedrooms and 1,260sft. The 1956 home has only 1,024sft on the main floor, 3 small bedrooms, and no consideration for aging-in-place without significant reconstruction. Current statistics show that without continual maintenance and updates, 1950's finishes have an approximate 25-year lifespan. Without that maintenance deterioration follows.

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03-01-2024
Planning Division

New Building Information: the units will each have a 2-car garage, with extra storage space. Units will be Ramblers, with an unfinished basement. The finished Living Space will be 1,438sft with an additional 1,100sft available in the lower level if desired. The units will have up to 4 bedrooms and 3 baths.

We are proposing to amend and expand the existing Grandview Townhomes PUD to include these 2 parcels. Given the deviations approved in the existing PUD, amending it is the cleanest mechanism to entitle this project.

- Rezoning these properties, and expanding the Existing PUD offers several benefits to the City and the Existing Residents of Grandview Townhomes.
    - It allows a seamless addition of these 4 units to the existing Grandview Townhomes PUD; this simplifies the documentation of any deviations required to allow the project to be built.
    - It also allows for the merging of this project into the existing 9-mile Creek Watershed plans, simplifying the agreements and future compliance.

Project Specifications:

- Project Density is 8.95units/acre, requiring a Comprehensive Plan amendment to Medium Density Residential (5 to 12 units per acre) from Low Density Residential (1 to 5 units per acre.
    - Currently the density of these lots is 6.69u/a; the current average density of the Brookside Neighborhood, of which this parcel is part of, is also 6.69u/a.
    - <u>The Brookside neighborhood is **not** Low density Residential; it is already Medium Density Residential, but not acknowledged as such by the City's Land Use Map.</u>
- We are requesting a rezoning from R1 to PUD (amending the existing Grandview Townhomes PUD).
    - Building Hardcover is ~40% (less than Grandview Townhomes which was >42%).
    - Setbacks from Hankerson and the Alley are identical to Grandview Townhomes, Setback from the North Property Line is 18'6".
    - Building to building distance to 5112 Hankerson is about 31'.
- The project is divided into 2 – 2-unit Townhome Buildings.
    - The proposed units are Single-level living, Rambler-style townhomes over a basement.
        - During our sales period for Grandview Townhomes, we heard <u>considerable</u> demand for this style of unit from existing Edina residents who chose to visit our model and, after expressing a desire to downsize, asked us repeatedly when we would be building a product that was appropriate for 'Move Down' or 'Empty Nester' buyers from Edina.
    - As in Grandview Townhomes, developing the project as 2 buildings allows for a better relationship meeting the various existing grades around the site, the alley, and Hankerson Ave. The building size is similar to others in the area, even on this block. It also presents a less 'monolithic' building than a single 4-unit building would be.
    - Building to Building distance is 20', which is the same as Grandview Townhomes.
    - New building heights are from 20' to 25'above adjacent grade.
- The building design is complementary to Grandview Townhomes but also has its own style.

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03-01-2024
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- The siding is composite, pre-finished material with a 25-year (min) finish warranty.

- Unit Pricing can start in the low $800's (subject to industry pricing).
- Building information
  - Units each has a 2-car garage, with extra storage space.
  - Units will be Ramblers, with an unfinished basement.
  - Basement finishes are available.
  - Units will have up to 4 bedrooms and 4 baths.

Exterior finishes are similar, and complementary, to Grandview Townhomes.

Our goal has been to create a project that enhances both the existing single-family homes in the area and the neighboring Grandview Townhomes project. We believe this project strongly contributes to Workforce Housing, fits the definition of Lifestyle Housing, and includes additional elements of the Edina 2040 Comprehensive Plan listed below.

From Edina Comprehensive Plan, Housing Chapter – Housing Choice
Goal 4: Support the development of a wide range of housing options to meet the diverse needs and preferences of the existing and future Edina community.

1. Promote increased housing opportunities and a diversity of housing types by promoting the creative and innovative use of land guided for residential or commercial mixed-use while promoting transit use and other mobility alternatives.
2. Promote a vision of community that is inclusive of a range of ages, incomes, abilities, and other demographics, and offers a range of housing options.
3. Promote affordable and workforce housing that includes a range of housing prices and options, based on the principle that those who contribute to the community should have the opportunity to live here. Also, this housing vision strengthens and reinvigorates community institutions and makes the city an attractive destination for young families.
4. Promote lifecycle housing to support a range of housing options that meet people's preferences and circumstances in all stages of life.
5. Protect and maintain lifecycle housing that is important for attracting young families.
6. Retain and expand housing ownership options, while also supporting a balance between ownership and rental households.

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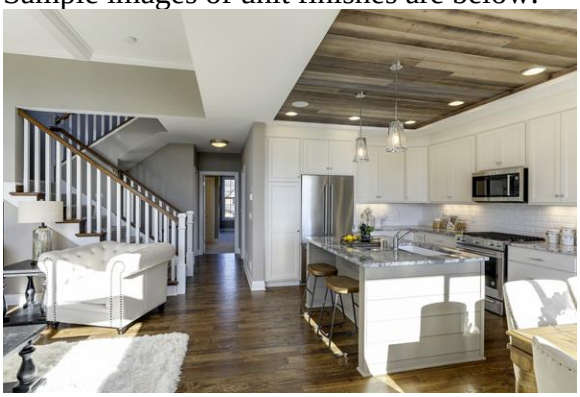
See the image of the built Grandview Townhomes above.

Sample images of unit finishes are below.



<u>PUD Regulation Discussion:</u>

1. <u>Purpose and Intent.</u> The purpose of the PUD District is to provide comprehensive procedures and standards intended to allow more creativity and flexibility in site plan design than would be possible under a conventional zoning district. The decision to zone property to PUD is a public policy decision for the City Council to make in its legislative capacity. The purpose and intent of a PUD is to include most or all of the following:

a. provide for the establishment of PUD (planned unit development) zoning districts in appropriate settings and situations to create or maintain a development pattern that is consistent with the City's Comprehensive Plan;

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03-01-2024
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b. promote a more creative and efficient approach to land use within the City, while at the same time protecting and promoting the health, safety, comfort, aesthetics, economic viability, and general welfare of the City;
c. provide for variations to the strict application of the land use regulations in order to improve site design and operation, while at the same time incorporate design elements that exceed the City's standards to offset the effect of any variations. Desired design elements may include: sustainable design, greater utilization of new technologies in building design, special construction materials, landscaping, lighting, stormwater management, pedestrian oriented design, and podium height at a street or transition to residential neighborhoods, parks or other sensitive uses;
d. ensure high quality of design and design compatible with surrounding land uses, including both existing and planned;
e. maintain or improve the efficiency of public streets and utilities;
f. preserve and enhance site characteristics including natural features, wetland protection, trees, open space, scenic views, and screening;
g. allow for mixing of land uses within a development;
h. encourage a variety of housing types including affordable housing; and
i. ensure the establishment of appropriate transitions between differing land uses.

The project would be an improvement over the existing conditions of the site; the proposed buildings provide an appropriate transition from the higher-density apartment development and the Grandview Townhomes of Edina project to the south, to the duplexes to the west, and single-family homes to the north and west. It is "missing middle" type housing identified as desirable in the Comprehensive Plan. Examples of "missing middle" housing options include a range of multi-unit or clustered housing types compatible in scale with single-family homes that help meet the growing demand for walkable urban living. The Housing Strategy Task Force report encourages the development of duplexes or Missing Middle Housing. The Task Force also proposed a goal to consider zoning amendments that would expand housing options, such as "considering zoning amendments in limited areas (such as transitional areas and activity nodes) and pursue zoning changes to encourage split lots to allow infill, to allow lot splits for infill, single-family ownership housing, detached or attached (zero lot line), on lots after splitting that are 50' or wider (or 3,500 sf or larger)."

Comments from the Edina HRA support this kind of redevelopment. This type of housing also has market benefits; it contributes to Life-cycle Housing. According to the Edina Maxfield Housing Study: "With the overall aging of the population, more households are looking for greater convenience and less space in their housing and are selecting twin homes, detached townhomes, and condominiums." Twin homes provide an outlet for existing homeowners to move within Edina, freeing up other Edina homes for new families. The pattern can continue with the family homes being vacated, becoming available to residents who may be Edina renters.

This project also acknowledges the current, dominant, building patterns in the area; Street Facing front doors and Alley facing rear garage doors, which is a significant factor in the need for the PUD classification, at the same time adding modestly to the area's density.

2. <u>Applicability/Criteria</u>

a. Uses. All permitted uses, permitted accessory uses, conditional uses, and uses allowed by administrative permit contained in the various zoning districts defined in this Title shall be treated as potentially allowable uses within a PUD district, provided they would be allowable on the site under the Comprehensive Plan.

Submitted
03-01-2024
Planning Division

Allowed uses for this project would be the same as those allowed under R2, PRD-2 and PRD-3 Zoning Districts, all of which could be considered for this site. The Medium Density proposed classification of the site suggests "attached housing (townhouses, quads, etc.) and multi-family complexes of moderate density." The Current Density in the neighborhood is 6.68 units/acre, which is Medium Density according to the Comprehensive Plan, and the current homes in the neighborhood are detached single-family and twin homes.
This project is proposing 'Twin homes', as such, it is an existing and permitted use in the neighborhood.

b. Eligibility Standards. To be eligible for a PUD district, all development should be in compliance with the following:

i. where the site of a proposed PUD is designated for more than one (1) land use in the Comprehensive Plan, the City may require that the PUD include all the land uses so designated or such combination of the designated uses as the City Council shall deem appropriate to achieve the purposes of this ordinance and the Comprehensive Plan;

The proposal would not include a mixture of land uses. However, it would include a housing type that the City has not seen much construction of over the past 20 years. As described above, the project would provide "missing middle", Twin home - Townhome-type housing.

ii. any PUD that involves a single land use type or housing type may be permitted provided that it is otherwise consistent with the objectives of this ordinance and the Comprehensive Plan;

The Applicant believes this housing development provides a housing type, Twin homes, that is listed as needed in the Comprehensive Plan.

iii. permitted densities may be specifically stated in the appropriate planned development designation and shall be in general conformance with the Comprehensive Plan; and

The proposed building density would be 8.95 units per acre and consistent with a Medium Density Designation in the Comprehensive Plan. The Site consists of 3 lots. We are proposing a modest densification to 4 units.

iv. the setback regulation, building coverage and floor area ratio of the most closely related conventional zoning district shall be considered presumptively appropriate, but may be departed from to accomplish the purpose and intent described in #1 above.

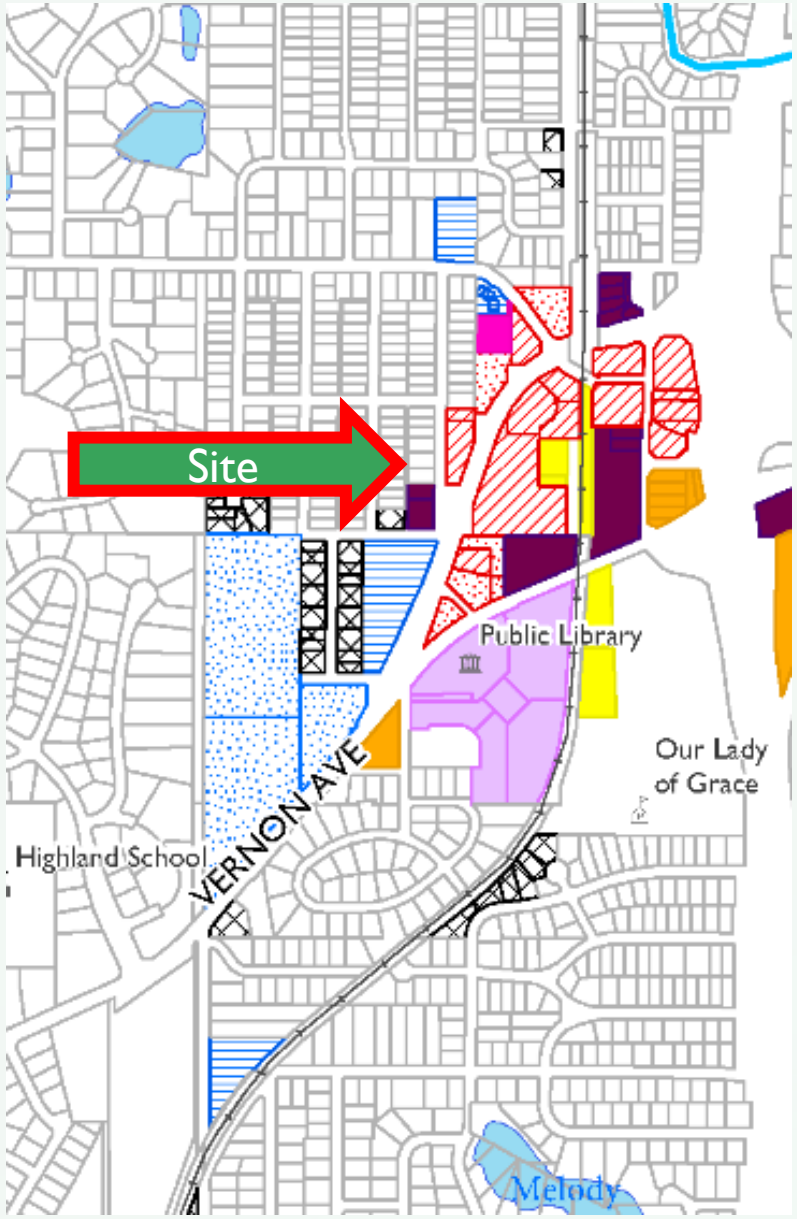
The proposed project does require variances from the already established standards of a R-1, R-2, or a PRD Districts; the setbacks, building coverage, and floor area ratio standards we are asking for already exist in the immediately adjacent Grandview Townhome PUD. For the reasons stated above, we believe the purpose and intent of the PUD Ordinance are met, and amending the existing PUD is appropriate.

Conclusion: Donnay Home believes that, like the Grandview Townhomes project next door, the addition of this development to the neighborhood will be a benefit to the entire Brookside neighborhood, meet the stated goals of the adjacent Grandview Small Area Plan and the Edina

Submitted
03-01-2024
Planning Division

Comprehensive Plan, turn barriers into opportunities, provide additional buffer to the neighborhood from the Vernon Ave Commercial area, enhance property values in the neighborhood, improve the Vernon and Hankerson streetscape, offer a type of housing that is in short supply in the City of Edina, and compliment walkability in the Brookside neighborhood.

Additional Streetscape images follow:



Zoning Map

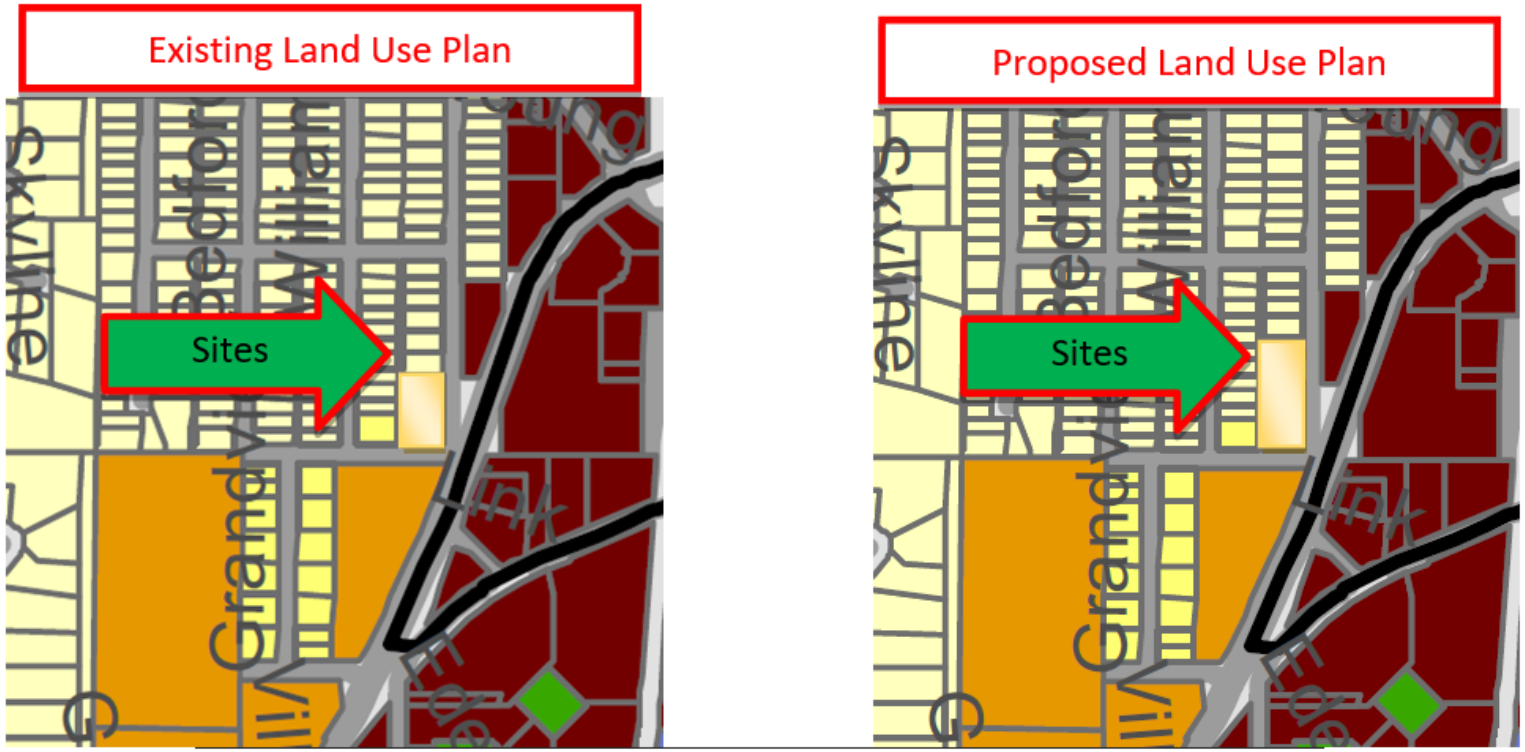
Legend

- Lakes
- Creeks
- Edina Boundary

Zoning

- R-1 - Single Dwelling Unit
- R-2 - Double Dwelling Unit
- PRD-1 - Planned Residence
- PRD-2 - Planned Residence
- PRD-3 - Planned Residence
- PRD-4 - Planned Residence
- PRD-5 - Planned Residence
- PCD-1 - Planned Commercial
- PCD-2 - Planned Commercial
- PCD-3 - Planned Commercial
- PCD-4 - Planned Commercial
- POD-1 - Planned Office
- POD-2 - Planned Office
- RMD - Regional Medical
- PID - Planned Industrial
- PUD - Planned Unit Development
- APD - Automotive Parking
- PSR-4 - Planned Residence
- MDD-4 - Mixed Development
- MDD-5 - Mixed Development
- MDD-6 - Mixed Development

The CITY of EDINA



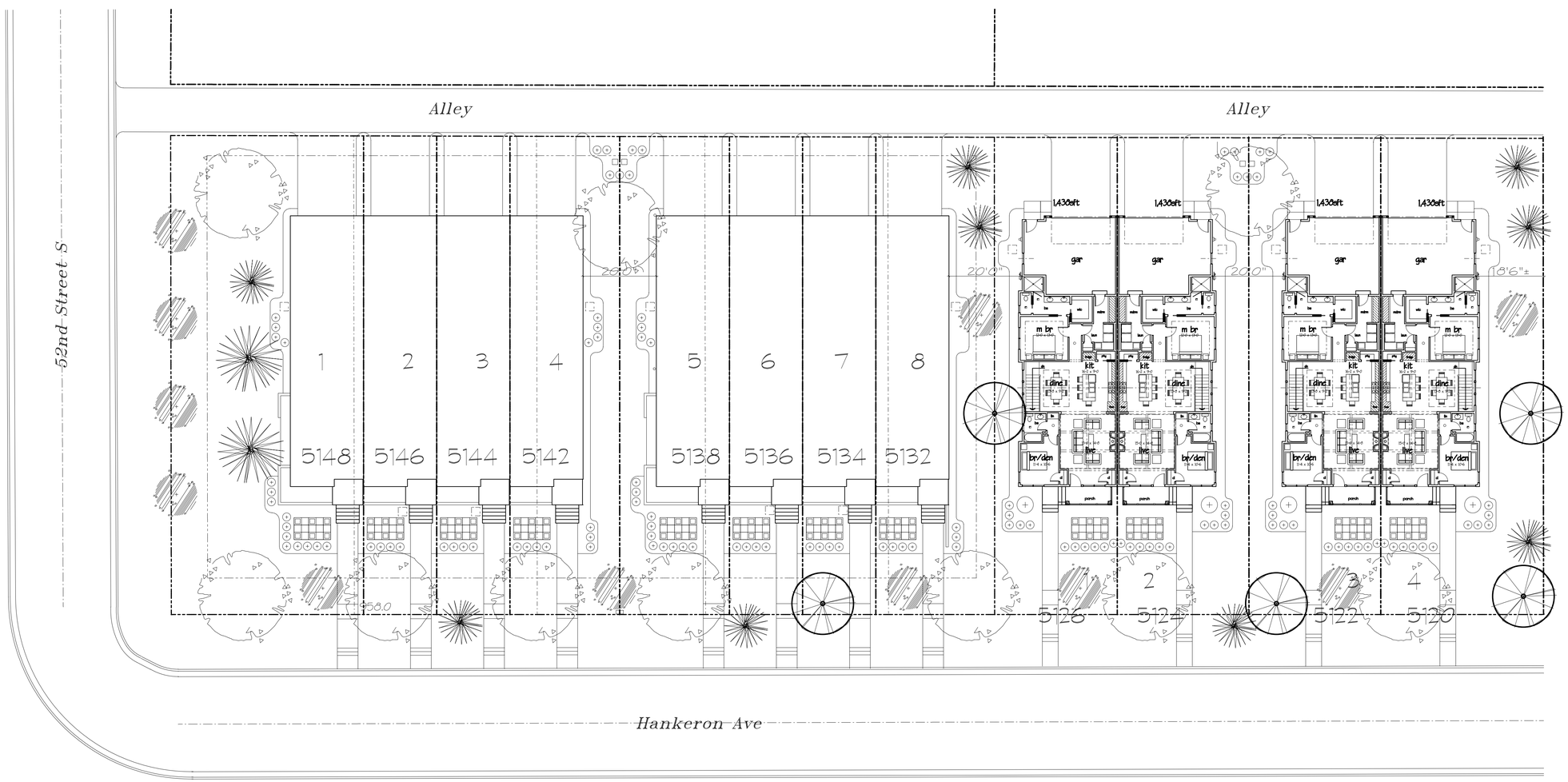
- Low Density Residential
- Low Density Attached Residential
- Medium Density Residential
- High Density Residential
- Greater Southdale District Residential
- Office Residential
- Office
- Neigborhood Node
- Mixed Use Center
- Community Activity Center
- Industrial
- Open Space and Parks
- Public/Semi Public
- Regional Medical
- City Limits

3,700

Edina 2040 Comprehensive Plan

Edina, Minnesota

2 PROJECT STREETSCAPE



1 PROJECT SITE PLANS

TRADITIONS
BY DONNAY HOMES, LLC

9655 - 63rd Avenue North
Maple Grove, Minnesota 55369
(763) 531-0714
Builder #722939

SITE DATA

Current Use: LDR
Proposed Use: MDR

Current Zoning: R-1
Proposed Zoning: PUD

Parcel Info:
2811721320172 - 5124 Hankerson Ave
Area - 0.22ac, 9,699sft
2811721320144 - 5120 Hankerson Ave
Area - 0.22ac, 9,769sft

Project Data:
Area - 0.4468ac
Area - 19,468sft
Lot Width - 150'

Setback Information:
Front Yard setback - 30' (min)
Grandview Twnhm Front Setback - 35'
Proposed - 35'
Bldg to Bldg - 20'
Side Yard Setback - 18'6"± to north
Rear Yard Setback - 21'6"± to Alley

Coverage Information:

Building	1,981sft ea	40.7%
Sidewalks	152sft ea	3.1%
Driveway	336sft ea	6.9%
Decks	68sft ea	0%
Total Hardcover Proposed		50.7%

No.	ISSUED/REVISIONS/SUBMISSIONS	DATE

Project Name
GRANDVIEW TOWNHOMES PHASE 2
52nd St & Hankerson Ave
Edina, Minnesota

Drawing Title
PROJECT SITE CONCEPT AND STREETSCAPE

BLDG. No. CONCEPT

Drawn
Checked
Reviewed
Date 01/14/2023
Drawing No. A0.0
of

PDF created with pdfFactory Pro trial version www.pdffactory.com

PEDESTRIAN NETWORK

The goal of the City's pedestrian network is to provide safe movement for all ages and abilities and to encourage active lifestyles. It should provide network continuity with broad geographic coverage and without notable gaps. Figure 3.2 below indicates locations of existing and future proposed pedestrian facilities. Refer to "Pedestrian Facilities" design guidelines in Chapter 6 for specific guidance regarding the application of these facilities.

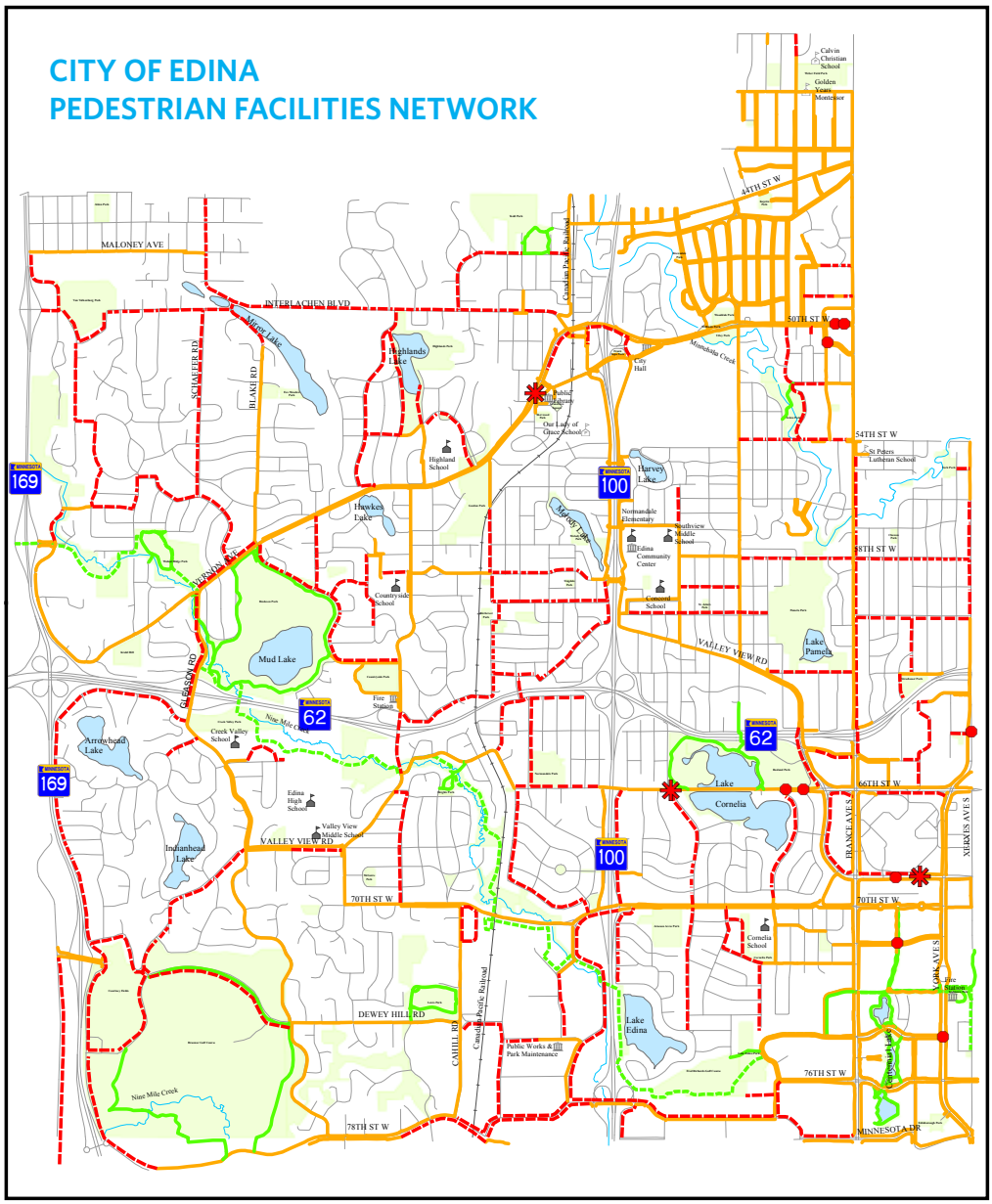


Figure 3.2. Edina Living Streets Classification Map

BICYCLE NETWORK

Edina's Living Streets should provide safe, convenient and comfortable access for bicyclists throughout the city. Edina's network of Living Streets shall accommodate all types, levels, and ages of bicyclists. Figure 3.3 below indicates locations of existing and future proposed bicycle facilities. Refer to "Bicycle Facilities" design guidelines in Chapter 6 for specific guidance regarding the application of these facilities.

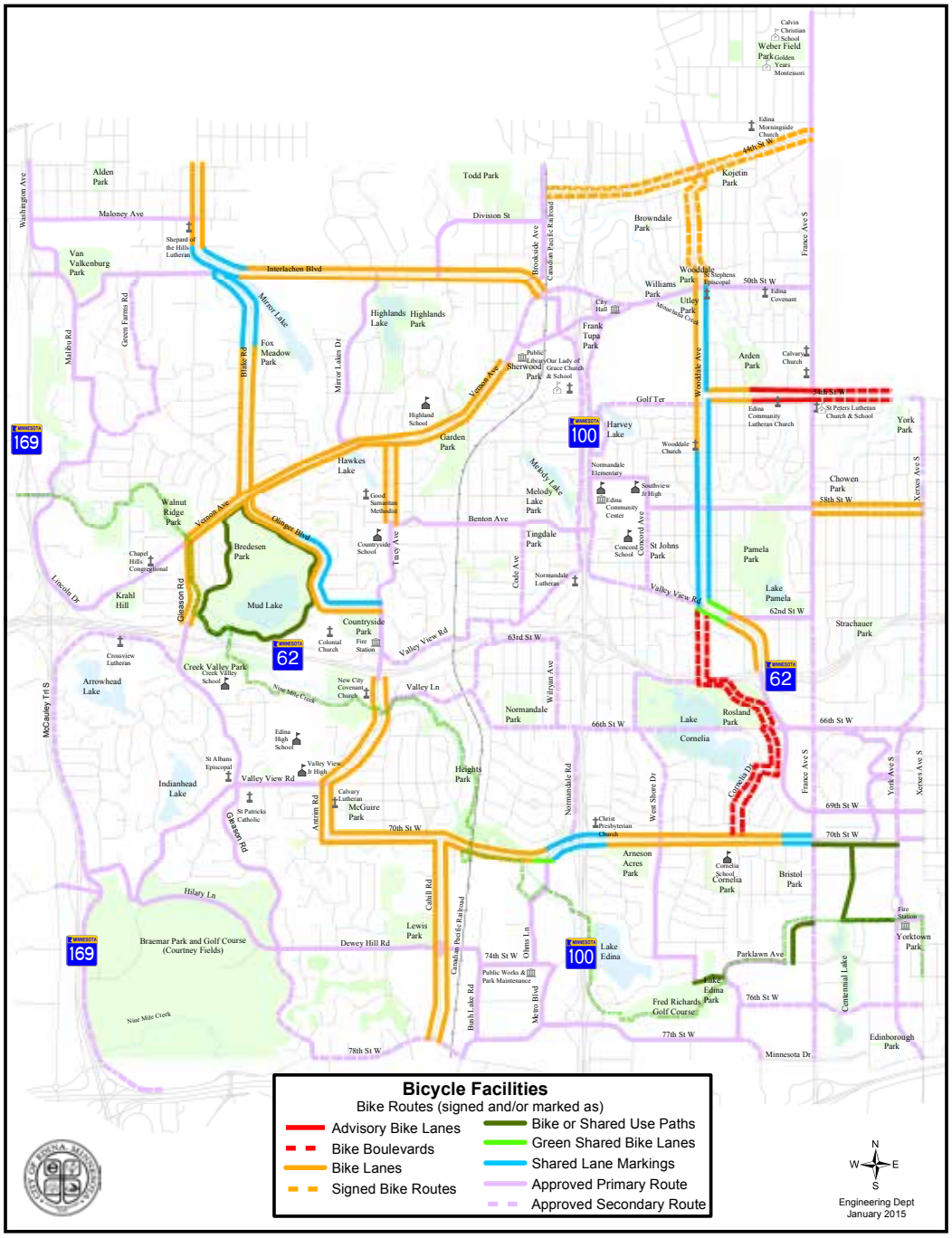


Figure 3.3. Bicycle Facilities Network

Appendix A: The pyramid of discretion

The pyramid framework illustrates how much discretion the city has to make land use decisions based on the role it is playing.

The CITY of EDINA

ORDINANCE NO. 2024-___
AN ORDINANCE AMENDING THE ZONING ORDINANCE
FOR THE PUD-20, PLANNED UNIT DEVELOPMENT-20 ZONING DISTRICT

The City Of Edina Ordains:

Section 1. Chapter 36, Article VIII, Division 4 is hereby amended as follows:

Sec. 36-507 Planned Unit Development District-20 (PUD-20) – 5120, 5124, 5132 and 5136-48 Hankerson Avenue – Hankerson Townhomes

(a) *Legal description:*

The South Half of Lot 8 and all of Lots 6, 7, 9, 10, 11 and 12, Block 9, Brookside Heights, Hennepin County, Minnesota

(b) Approved Plans. Incorporated herein by reference are the re-development plans, dated May 4, 2020 except as amended by City Council Resolution No. 2020-92 on file in the Office of the Planning Department.

Phase 2. Incorporated herein by reference are the plans, dated _____ except as amended by City Council Resolution No. 2024-___ on file in the Office of the Planning Department.

(c) Principal Uses:

Multi-family Townhomes/Condos.

(d) Accessory Uses:

All accessory uses allowed in the R1 Zoning District.

(e) Conditional Uses:

None

(f) Signs shall be regulated per the R-1 Zoning District.

DRAFT

Existing text – XXXX
Stricken text – ~~XXXX~~
Added text – XXXX

(g) Development Standards. In addition to the development standards per the R-1 Zoning District, the following shall apply:

	Standard
Front – Hankerson Ave. Front – 52nd Avenue Side – North Side/Rear – West	35 feet (porch 30') 32 feet 12 feet 20 feet
Building Height	2-1/2 stories & 33 feet 1-1/2 stories & 25 feet (two-unit townhomes)
Building Coverage	.45
Density	~~8~~ 12 units

Section 2. This ordinance is effective upon approval.

First Reading:

Second Reading:

Published:

Attest: _____ _____

Sharon Allison, City Clerk James B. Hovland, Mayor

Please publish in the Edina Sun Current on:

Send two affidavits of publication.

Bill to Edina City Clerk

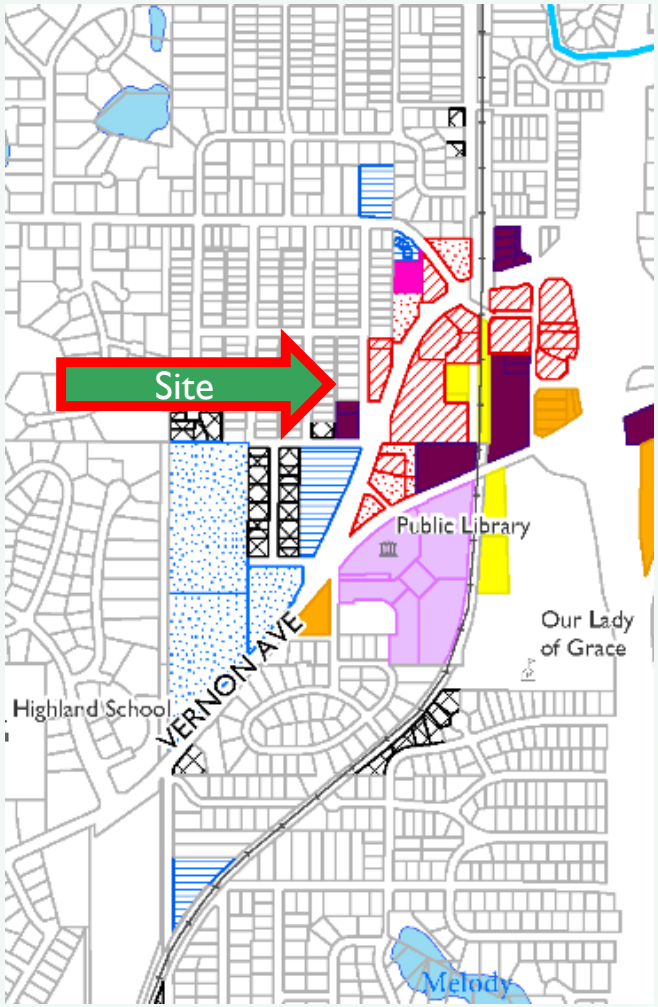
DRAFT

The CITY of EDINA

Comprehensive Plan Amendment, Rezoning & Subdivision

5120 and 5124 Hankerson Avenue

EdinaMN.gov



Zoning Map

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- Creeks
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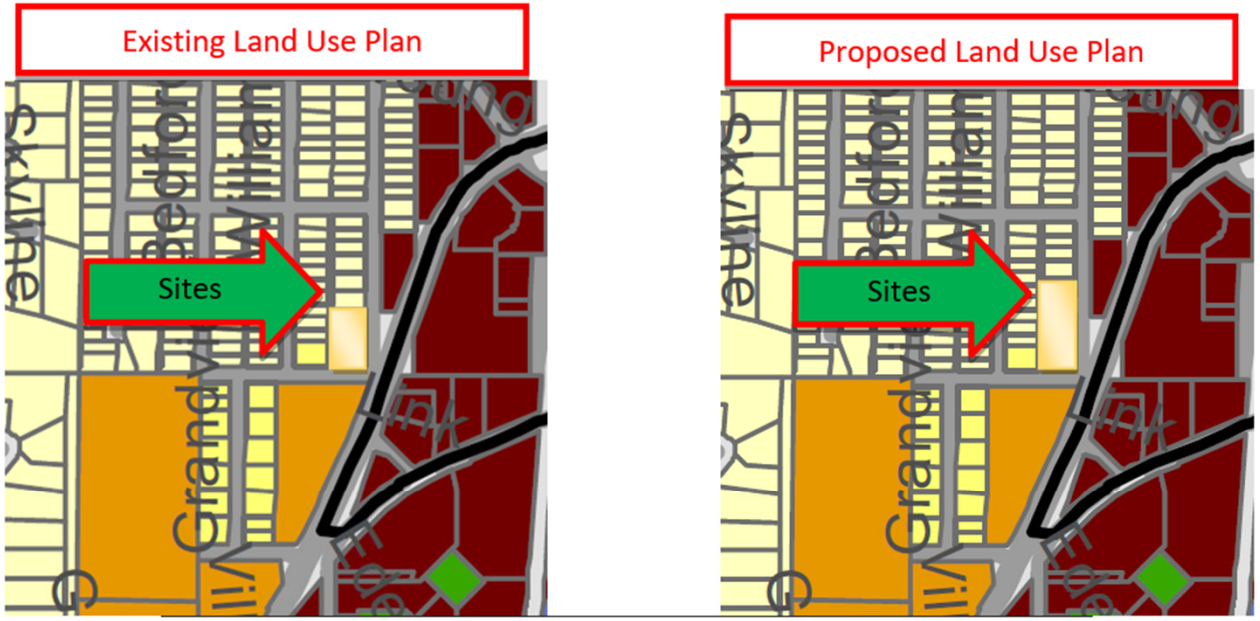
The CITY of EDINA



Low Density Residential
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Office

Neigborhood Node
Mixed Use Center
Community Activity Center
Industrial
Open Space and Parks
Public/Semi Public
Regional Medical
City Limits

3,700

Edina 2040 Comprehensive Plan

Edina, Minnesota

Oct 2022
See more dates
Caribou Coffee
W 51st St
5120
Jerry's Foods
Google

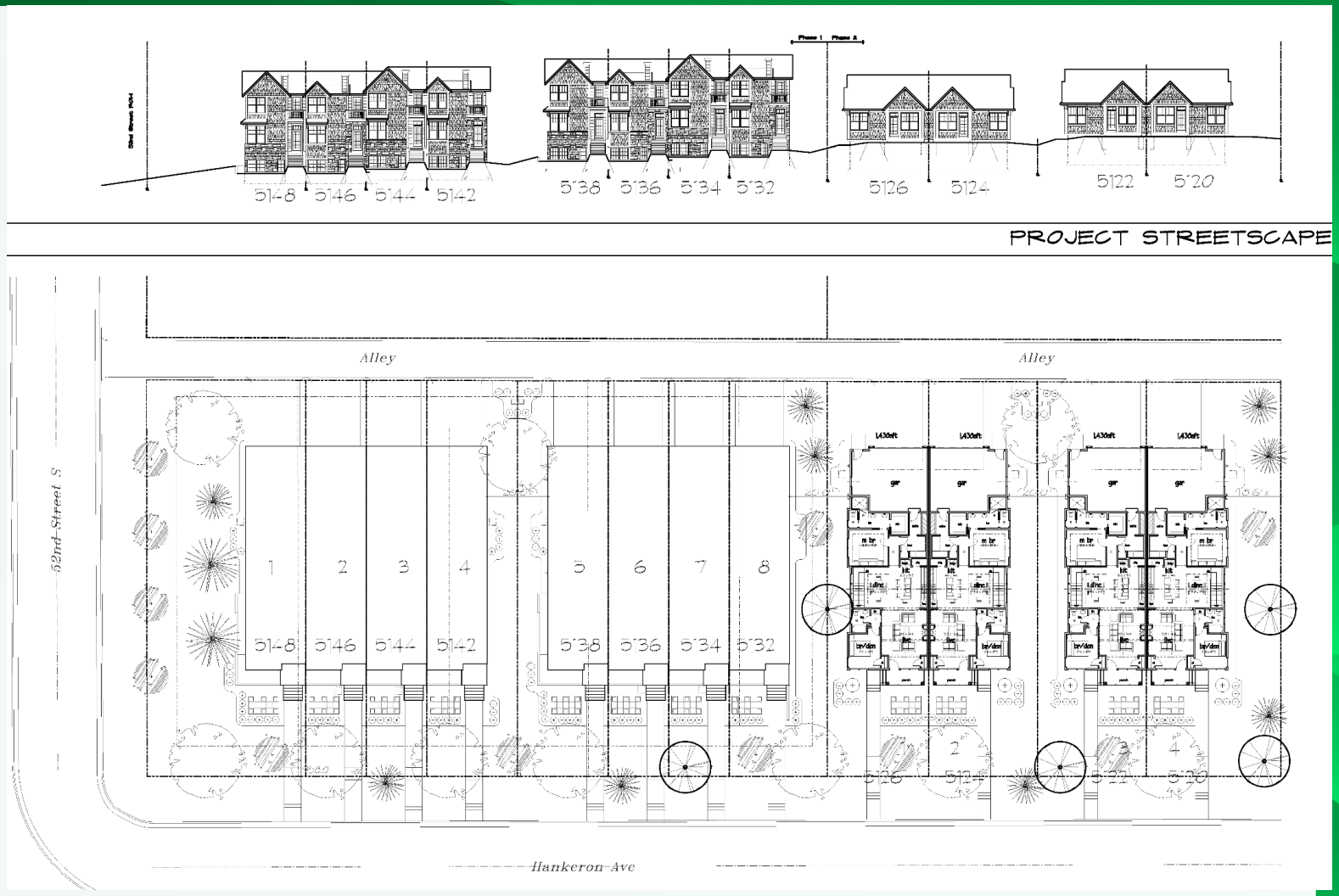
Hankerson Ave
© 2023 Google

Streetscape images follow:

Sketch Plan

The CITY of EDINA

Sketch Plan



The CITY of EDINA

2 PROJECT STREETSCAPE

1 PROJECT SITE PLANS

Maple Grove, Minnesota 55369
(763) 531-0714
Builder #722939

SITE DATA

Current Use: LDR
Proposed Use: MDR 8.85 u/a

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Proposed Zoning: PUD

Parcel Info:
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2011721320144 - 5120 Hankerson Ave
Area - 0.22ac, 9,769sft

Area - 0.4460ac
Area - 19,460sft
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Setback Information:
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Grandview Twnhm Front Setback - 35'
Proposed - 35'
Bldg to Bldg - 20'
Side Yard Setback - 10'6"± to north
Rear Yard Setback - 21'6"± to Alley

Coverage Information:

Building	1,901sft ea	40.7%
Sidewalks	152sft ea	3.1%
Driveway	336sft ea	6.9%
Decks	60sft ea	0%
Total Hardcover Proposed		50.7%

No	ISSUED/REVISIONS/SUBMISSIONS	DATE

Project Name
GRANDVIEW TOWNHOMES PHASE 2
52nd St & Hankerson Ave
Edina, Minnesota

Drawing Title
PROJECT SITE CONCEPT AND STREETSCAPE

BLDG. No. CONCEPT
Drawn
Checked
Reviewed
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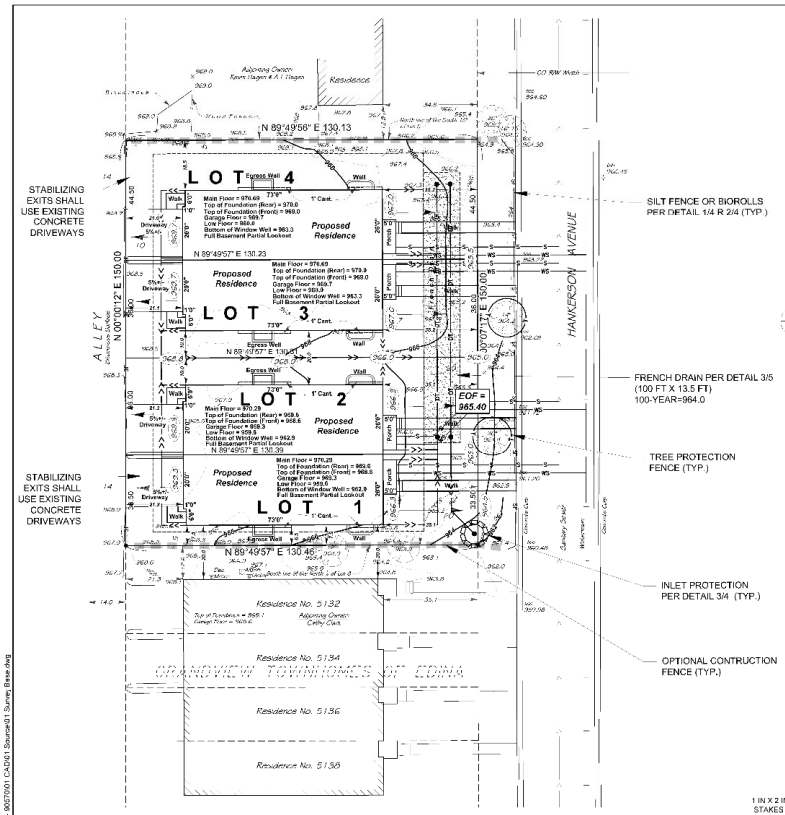
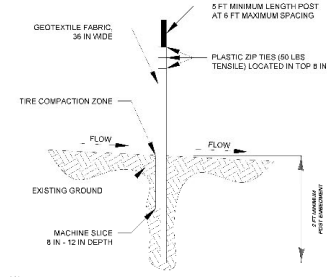
OF





1/4 SILT FENCE (MACHINE SLICED)
NOT TO SCALE

Existing Hardcover

Lot Area	*19,544 sq ft*
House No.5124	*1,041 sq ft*
Garage No.5124	*532 sq ft*
Stoops No.5124	*34 sq ft*
Deck No.5124	*249 sq ft*
Concrete No.5124	*1,166 sq ft*
Bituminous	*64 sq ft*
House w/ Fireplace No.5120	*845 sq ft*
Garage No.5120	*445 sq ft*
Stoops No.5120	*54 sq ft*
Concrete Driveway No.5120	*678 sq ft*
Pavers No. 5120	*76 sq ft*
Concrete walk 6	*644 sq ft*
Patio No. 5120	
Total	*6,030 sq ft*
Percentage	*30.85%*

Proposed Hardcover

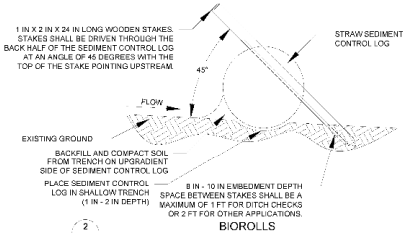
Lot Area	*19,544 sq ft*
Buildings	*7,672 sq ft*
Porches	*253 sq ft*
Driveways	*1,498 sq ft*
Walks	*627 sq ft*
Total	*10,050 sq ft*
Percentage	*51.42%*

GENERAL GRADING NOTES:

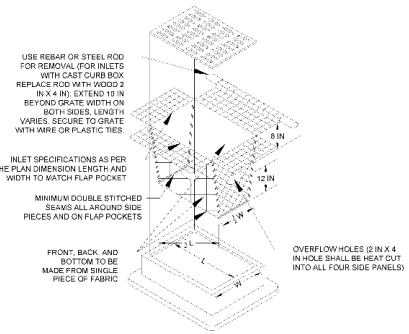
1. THE STANDARD SPECIFICATIONS FOR THE CITY OF EDINA SHALL APPLY EXCEPT WHERE MODIFIED BY THESE DOCUMENTS.
2. THE CONTRACTOR SHALL NOTIFY "GOPHER STATE ONE CALL" 48 HOURS PRIOR TO ANY EXCAVATION. (1-800-252-1166)
3. THE GRADING CONTRACTOR SHALL FIELD VERIFY ALL LOCATIONS AND ELEVATIONS OF UNDERGROUND UTILITIES WITH UTILITY COMPANIES PRIOR TO ANY CONSTRUCTION, AND IMMEDIATELY NOTIFY THE ENGINEER OF ANY CONFLICTS.
4. THE GRADING CONTRACTOR SHALL PROVIDE POSITIVE DRAINAGE ON THE SITE AT ALL TIMES.
5. EXISTING TOPSOIL SHALL BE SALVAGED TO PROVIDE 4" TOPSOIL COVERAGE OVER ALL DISTURBED AREAS TO BE REVEGETATED.
6. THE BUILDING PAD MUST BE PROVIDED WITH A POSITIVE OUTFLOW. THIS WORK SHALL BE INCIDENTAL TO THE GRADING CONTRACT.
7. ANY SEDIMENT REMAINING IN PLACE AFTER THE SILT FENCE HAS BEEN REMOVED SHALL BE DRESSED TO CONFORM WITH THE EXISTING GRADE, PREPARED AND SEEDED OR SODDED AS DIRECTED BY THE CITY ENGINEER.
8. NO FINISHED SLOPE SHALL EXCEED 4H : 1V UNLESS OTHERWISE NOTED.
9. PERMITTEE MUST MINIMIZE SOIL COMPACTION. METHODS OF MINIMIZING SOIL COMPACTION INCLUDE THE USE OF TRACKED EQUIPMENT.

EROSION CONTROL NOTES:

1. ALL EROSION AND SEDIMENT CONTROL BMP'S (I.E. SILT FENCE, BIO-ROLLS, INLET PROTECTION, ETC.) SHALL BE INSTALLED PRIOR TO THE COMMENCEMENT OF ANY CONSTRUCTION ACTIVITY.
2. INLET PROTECTION SHALL BE INSTALLED AT ANY INLET THAT MAY RECEIVE RUNOFF FROM THE DISTURBED AREAS OF THE PROJECT. INLET PROTECTION MAY BE REMOVED FOR A PARTICULAR INLET IF A SPECIFIC SAFETY CONCERN (FLOODING / FREEZING) HAS BEEN IDENTIFIED. THE PERMITTED MUST RECEIVE WRITTEN CORRESPONDENCE FROM THE CITY ENGINEER VERIFYING THE NEED FOR REMOVAL.
3. INSTALL SEDIMENT CONTROL BMP'S, SUCH AS SILT FENCE, AROUND ALL STOCKPILES.
4. RETAIN AND PROTECT AS MUCH NATURAL VEGETATION AS FEASIBLE. WHEN VEGETATION IS REMOVED DURING DEVELOPMENT, THE EXPOSED CONDITION OF LAND SHALL BE KEPT TO THE SHORTEST PRACTICAL PERIOD OF TIME, BUT NOT LONGER THAN 60 DAYS. ANY EXPOSED AREAS EXCEEDING THIS TIME-FRAME SHALL BE TEMPORARILY STABILIZED (STRAW MULCH, WOODCHIPS, ROCK). AREAS BEING USED FOR MATERIAL STORAGE AND AREAS UNDER CONSTRUCTION ARE EXEMPT FROM TEMPORARY STABILIZATION.
5. ANY STEEP SLOPES (3H : 1V OR STEEPER) EXPOSED DURING CONSTRUCTION SHALL BE PROTECTED WITH TEMPORARY VEGETATION, MULCHING OR BY OTHER MEANS ACCEPTABLE TO THE BUILDING OFFICIAL WITHIN 14 DAYS OF CEASING LAND DISTURBING ACTIVITIES ON THE STEEP SLOPES. STOCKPILES MAY BE PROTECTED BY AN ANCHORED TARP OR PLASTIC SHEET.
6. PROVIDE DUST CONTROL AS NECESSARY. DUST CONTROL CAN INCLUDE WATER.
7. REMOVE ALL SOILS AND SEDIMENTS TRACKED OR OTHERWISE DEPOSITED ONTO PUBLIC PAVEMENT AREAS ON A DAILY BASIS OR AS NEEDED.
8. ALL EROSION AND SEDIMENT CONTROL BMP'S SHALL BE INSPECTED EVERY 7 DAYS, OR WITHIN 24 HOURS OF ALL RAIN EVENTS GREATER THAN 1.0" IN 24 HOURS. CORRECTIVE ACTION REQUIRED SHALL BE INITIATED IMMEDIATELY.
9. SILT FENCE, BIO-ROLLS AND INLET PROTECTION DEVICES MUST BE REPAIRED, REPLACED OR SUPPLEMENTED WHEN THEY BECOME NONFUNCTIONAL OR THE SEDIMENT REACHES 1/3 THE HEIGHT OF THE DEVICE. THESE REPAIRS MUST BE MADE WITHIN 24 HOURS OF DISCOVERY, OR AS SOON AS FIELD CONDITIONS ALLOW.
10. AFTER FINAL GRADING HAS BEEN COMPLETED, EXPOSED SOILS MUST BE PERMANENTLY STABILIZED AS SOON AS POSSIBLE. PERMANENT STABILIZATION SHALL CONSIST OF 4 INCHES TOPSOIL AND SEED, MULCH AND FERTILIZER APPLIED BY METHODS AND RATES RECOMMENDED IN MN/DOT SPECIFICATION 2575 AND MN/DOT SEEDING MANUAL, OR SOD. THE SEED MIX SHALL BE MN/DOT 25-151.
11. NO CONCRETE WASHOUT ALLOWED ON SITE, TRUCK BASED SELF CONTAINMENT WASHOUT DEVICES REQUIRED.
12. OIL STAINS ON CITY STREETS SHALL BE CLEANED UP WITH FLOOR DRY, AND DISPOSED OF AS A HAZARDOUS WASTE MATERIAL.
13. ALL HAZARDOUS WASTE SHALL BE STORED CLEANED UP AND DISPOSED OF PER EPA STANDARDS.
14. ALL EROSION AND SEDIMENT CONTROL DEVICES SHALL BE MAINTAINED UNTIL ALL DISTURBED AREAS HAVE BEEN PERMANENTLY STABILIZED.
15. ALL EROSION AND SEDIMENT CONTROL DEVICES SHALL BE REMOVED FROM THE SITE AFTER PERMANENT STABILIZATION HAS BEEN ESTABLISHED.
16. TEMPORARY PUMPED DISCHARGE POLLUTION PREVENTION TECHNIQUES: "DANDY DEWATERING BAG" BROCK WHITE CO. USA.
17. CONTACT PERSON FOR SITE CLEANLINESS AND MAINTENANCE OF THE EROSION AND SEDIMENT CONTROLS: PAUL DONNAY (763) 531-0714
18. NINE MILE CREEK WATERSHED DISTRICT: (952) 835-2078



2/4 BIOROLLS
NOT TO SCALE



3/4 INLET PROTECTION
NOT TO SCALE

SHEET INDEX

Sheet 1 - Existing Conditions Survey
Sheet 2 - Site Plan
Sheet 3 - Preliminary Plat
Sheet 4 - Grading & Stormwater Erosion Control Plan
Sheet 5 - Utility Plan

0 20 40
SCALE IN FEET

DESIGNED BY: JAP
DRAWN BY: ABL
CHECKED BY: GRP

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

JEFFREY A. PRASCH, P.E.
DATE: 01.25.24 LIC. NO.: 52706

REVISIONS	
01.31.24	PROP. SERVICES

DEMARC
LAND SURVEYING & ENGINEERING
7601 73RD AVENUE N, BROOKLYN PARK, MN 55428
PHONE: 763.560.3093 FAX: 763.560.3522
www.Demarcinc.com

DONNAY HOMES
9655 63RD AVENUE NORTH
MAPLEGROVE, MINNESOTA 55369
763.531.0714

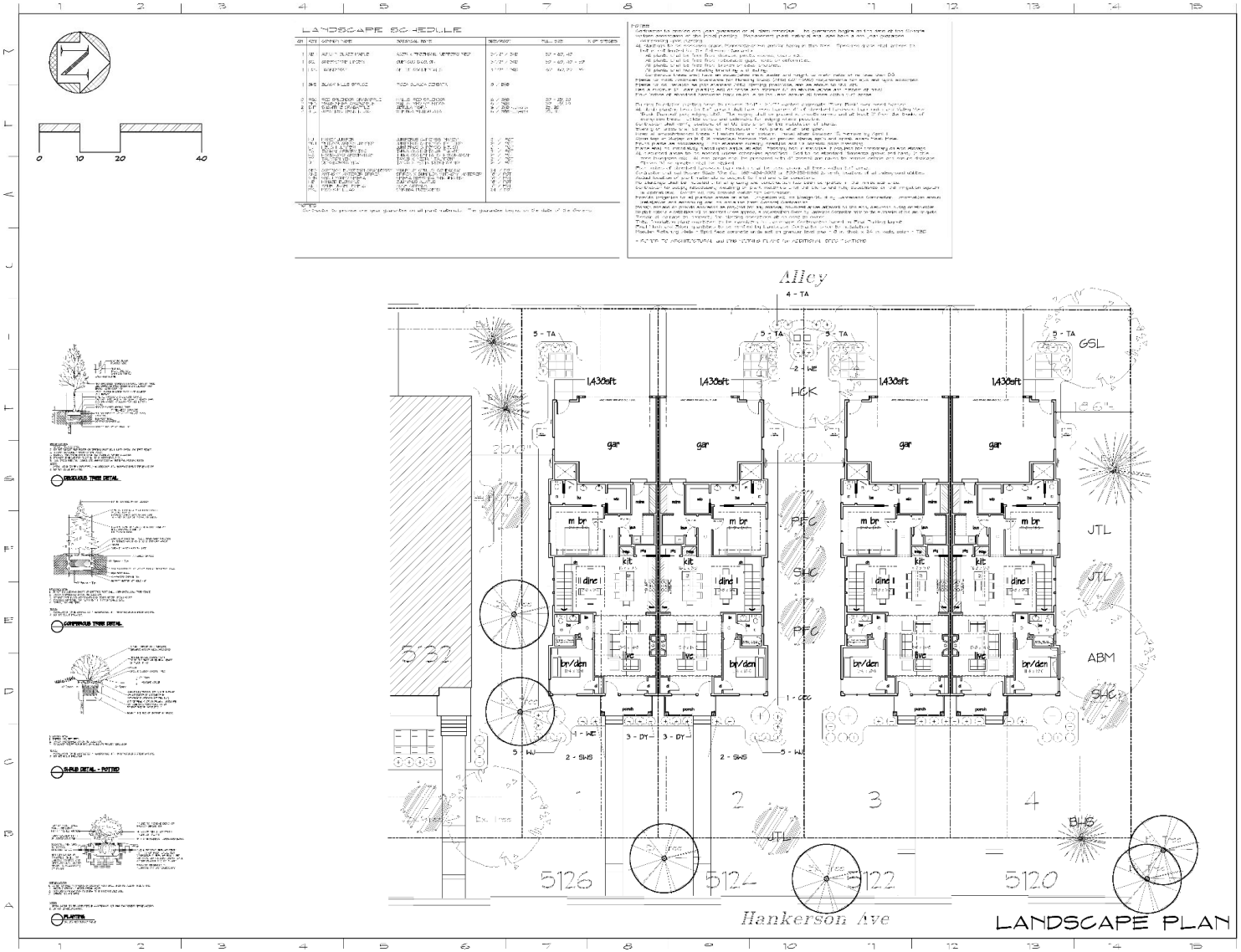
GRANDVIEW TOWNHOMES OF EDINA
5132-5148 HANKERSON AVENUE
EDINA, MINNESOTA

GRADING & STORMWATER EROSION CONTROL PLAN

PROJECT: 90570A
SHEET NO. 4 OF 5

Submitted CUP/VAR/PUD/SUP/SDR/PRELIM/FP/SP Planning Division

F:\Survey\grandview heights - hanker\5-5-3 inside light - grandview townhome 2\001 Surveying - 90570\001 CAD\01 Sourced\1 Survey Base.dwg



PDF created with pdfFactory Pro trial version www.pdffactory.com

Request Requires

- A Comprehensive Plan Amendment to re-guide the site from Low Density Residential to Medium Density Residential, which would allow 5-12 units per acre.

- A Rezoning from R-1, Single-Dwelling Unit District to PUD-20, Planned Unit Development, to be part of the adjacent Grandview Townhome PUD. Flexibility through the PUD would be for structure setback, building coverage and lot area per dwelling, to match the existing PUD to the south.

- Subdivision/Preliminary Plat.

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Appendix A: The pyramid of discretion

The pyramid framework illustrates how much discretion the city has to make land use decisions based on the role it is playing.

The CITY of EDINA

An argument can be made for and against the proposed development. The City has complete discretion to approve or deny.

Staff has provided the Commission with alternatives to consider for approval and denial of the request.

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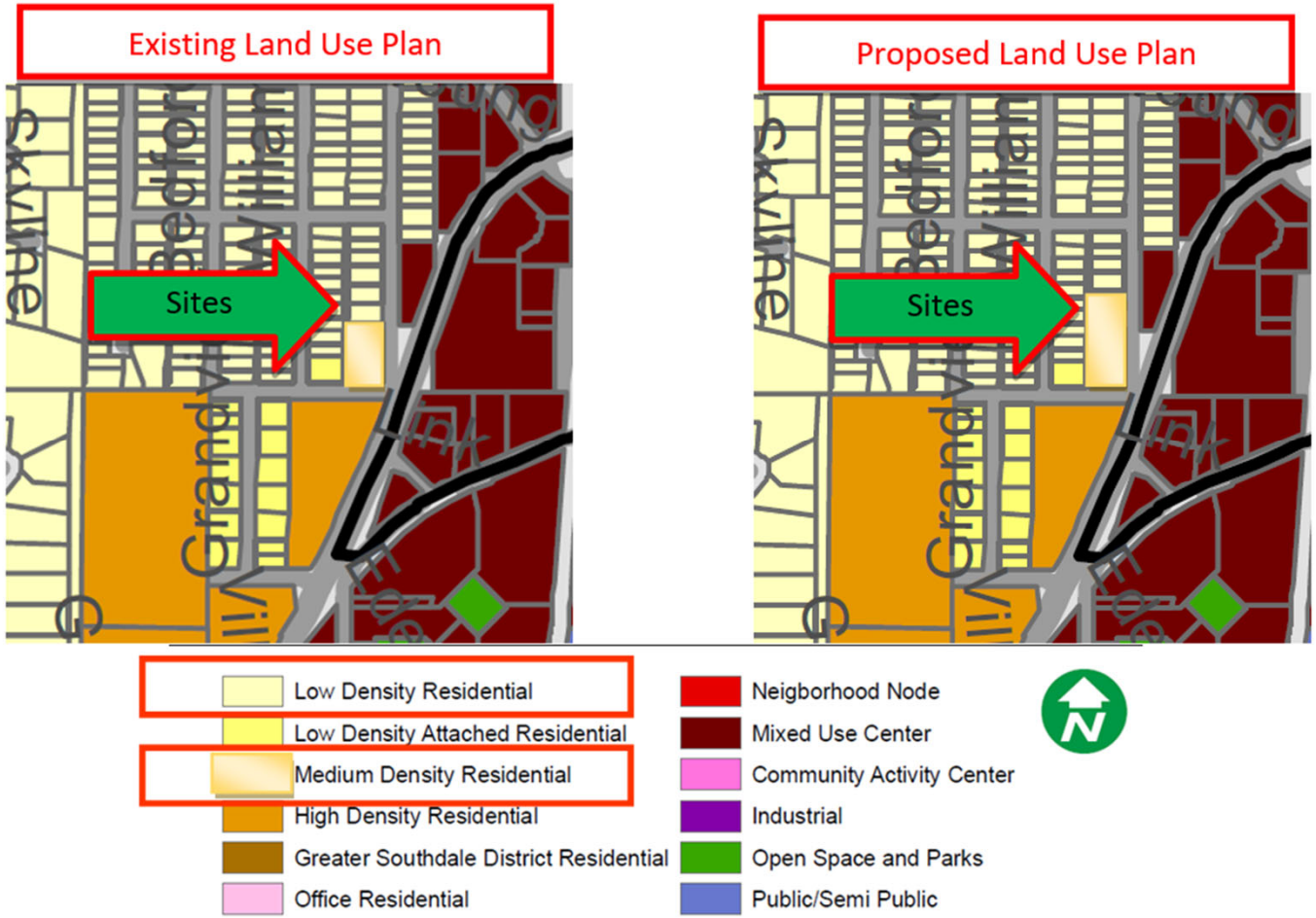
Primary Issues

- **Is the Comprehensive Plan Amendment reasonable?**
- **Is the Rezoning to PUD reasonable?**

The CITY of EDINA



The CITY of EDINA

Primary Issues

- **Is the Comprehensive Plan Amendment reasonable?**

  1. The proposed project would meet goals and policies of the Comprehensive Plan:

     - **Multifamily.** Multifamily developments are concentrated primarily along the main traffic arteries and are generally located toward the edges of the city, often in proximity to retail business establishments. Concentrations of multifamily developments are found along York Avenue, France Avenue, Vernon Avenue, Lincoln Drive, and Cahill Road.
     - **Integration of multi-unit housing into transition areas**.
     - **Single-family characteristics**. Attached and multifamily housing should emulate single-family housing in its basic architectural elements – pitched roofs, articulated facades, visible entrances, porches or balconies. Taller buildings should step down to provide a height transition to existing adjacent residential buildings.

The CITY of EDINA

Primary Issues

- **Is the Comprehensive Plan Amendment reasonable?**
  - **Parking to the rear.** Where rear-loaded or detached garages predominate, parking spaces and garages should be located to the rear of the lot or interior of the block. If this is infeasible, garages should be recessed some distance behind the main façade of the house and surface parking should be placed within side yards to the extent feasible.
  - **Maintaining community character**. With the changing ways people are living, working, and using spaces, there will be changes in how land uses function – both in new and renovated spaces. Edina is frequently on the forefront of innovative practices, particularly related to sustainability and technology. However, this will require some flexibility in development standards. One of the goals of innovating may be to find ways to accommodate missing housing types or to let housing be produced more affordably. Examples of housing types may include "missing middle" housing options – a range of multi-unit or clustered housing types compatible in scale with single-family homes that help meet the growing demand for walkable urban living. These housing options may include co-housing, duplexes, triplexes, fourplexes, and other smaller scale multifamily types. Ways to reduce housing costs could include modular building styles that take advantage of efficiencies in the construction process.

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Missing Middle Housing is a range of house-scale buildings with multiple units —compatible in scale and form with detached single-family homes—located in a walkable neighborhood.

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Primary Issues

- **Is the Comprehensive Plan Amendment reasonable?**

2. The proposed amendment is reasonable given its proximity to the Grandview District. The property to the south is guided medium and high density residential and the east is property guided MXC Mixed Use Center.

3. The proposed amendment would allow a development that would create a reasonable land use transition to the single-family residential area to the north. High Density Development is located to the south and east, by developing townhouses at the same height as would be allowed in the single-dwelling unit district would provide a transition and buffer to the single-family homes to the north.

4. The Medium-Density Residential District allows between 5-12 units per acre. The proposal is 9 units per acre.

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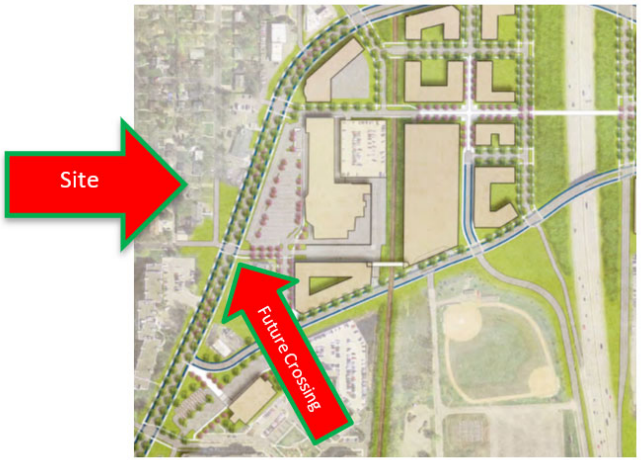
Primary Issues

- **Is the Comprehensive Plan Amendment reasonable?**

5. A pedestrian and street connection with Vernon Avenue to Jerry's is proposed in the Grandview Development Framework and Grandview Transportation Study. (See attached pages from both studies.) Higher Densities are typically found near or on streets with higher traffic volume like Vernon Avenue. The Illustration is a rendering of that future connection from the Grandview Transportation Study:

6. Additional density would support the retail uses in the district.



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Primary Issues

- **Is the Rezoning to PUD reasonable?**

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Primary Issues

- **Is the Rezoning to PUD reasonable?**

1. **The purpose and intent of a PUD is to include most or all of the following:**

   a. **provide for the establishment of PUD (planned unit development) zoning districts in appropriate settings and situations to create or maintain a development pattern that is consistent with the City's Comprehensive Plan; This area was found to be an appropriate setting.**

   b. **promote a more creative and efficient approach to land use within the City, while at the same time protecting and promoting the health, safety, comfort, aesthetics, economic viability, and general welfare of the City; An efficient approach to provide housing lacking in the city (Missing middle)**

   c. **provide for variations to the strict application of the land use regulations in order to improve site design and operation, while at the same time incorporate design elements that exceed the City's standards to offset the effect of any variations. Desired design elements may include: sustainable design, greater utilization of new technologies in building design, special construction materials, landscaping, lighting, stormwater management, pedestrian oriented design, and podium height at a street or transition to residential neighborhoods, parks or other sensitive uses;**

   d. **ensure high quality of design and design compatible with surrounding land uses, including both existing and planned;**

   e. **maintain or improve the efficiency of public streets and utilities;**

   f. **preserve and enhance site characteristics including natural features, wetland protection, trees, open space, scenic views, and screening;**

   g. **allow for mixing of land uses within a development;**

   h. **encourage a variety of housing types including affordable housing; and**

   i. **ensure the establishment of appropriate transitions between differing land uses.**

Primary Issues

- **Is the Rezoning to PUD reasonable?**

  - The project would simply be an extension of the existing PUD Zoning District to the south.
  - The structures would step down from the existing townhomes that are two stories, toward the single-family homes to the north and west to better transition into the neighborhood.
  - The buildings provide an appropriate transition from the high-density development to the south (apartments) to the existing 4-unit, two-story townhomes to these one-story townhomes/duplexes to the single-family homes to the north.
  - The project would provide a housing type the City has not seen much development of. It is an extension of "missing middle" type housing option identified in the Comprehensive Plan. "Examples of housing types may include "missing middle" housing options – a range of multi-unit or clustered housing types compatible in scale with single-family homes that help meet the growing demand for walkable urban living.

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Options

The CITY of EDINA

Recommendation

The CITY of EDINA

CITY OF EDINA

4801 West 50th Street
Edina, MN 55424
www.edinamn.gov

Date: April 11, 2024 **Agenda Item #**: VI.B.

To: Planning Commission **Item Type:**

From: Cary Teague, Community Development Director

Item Activity:

Subject: Site Plan with Variances and Subdivision – Edina Endodontics, 7300 Metro Boulevard

---

ACTION REQUESTED:
Recommend the City Council approve the requests.

INTRODUCTION:
Hempel Real Estate, on behalf of Edina Endodontics, is proposing to build a new 6,000 square foot dental office in the parking lot of 7300 Metro Boulevard. The existing site contains a 110,790 square foot office building that is served by a surface parking lot. The lot is 5.47 acres in size. The property is zoned Planned Office District, in which dental offices are a permitted use. (See attached plans and narrative.)

The request requires the following:

- Site Plan Review.
- Variances: First floor ceiling height from 20 to 10 feet and side yard setback from 20 to 10 feet.
- Subdivision.

The applicant went through the sketch plan process and has made revisions to the plans based on the feedback from the Planning Commission and City Council. The most significant change includes eliminating the parking lot in front of the building.

ATTACHMENTS:

Staff Report

Engineering Memo

Proposed Plans and Renderings

Site Location, Zoning and Comp. Plan

Traffic and Parking Study

Staff Presentation

STAFF REPORT

Date: April 11, 2024

To: Planning Commission

From: Cary Teague, Community Development Director

Subject: Site Plan with Variances and Subdivision – Edina Endodontics, 7300 Metro Boulevard

Information / Background:

Hempel Real Estate, on behalf of Edina Endodontics, is proposing to build a new 6,000 square foot dental office in the parking lot of 7300 Metro Boulevard. The existing site contains a 110,790 square foot office building that is served by a surface parking lot. The lot is 5.47 acres in size. The property is zoned Planned Office District, in which dental offices are a permitted use. (See attached plans and narrative.)

The request requires the following:

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- Subdivision.

The applicant went through the sketch plan process and has revised the plans based on the feedback from the Planning Commission and City Council. The most significant change includes eliminating the parking lot in front of the building.

SUPPORTING INFORMATION

Surrounding Land Uses

Northerly: Offices; zoned POD-2, Planned Office District – 2
Easterly: Highway 100
Southerly: Office buildings; zoned POD-2, Planned Office District – 2
Westerly: Office buildings; zoned POD-2, Planned Office District – 2

Existing Site Features

The subject site contains an office building with surface parking and perimeter landscaping.

Planning

Guide Plan designation:	O, Office
Zoning:	POD-2, Planned Office District – 2

Site Circulation/Traffic

The proposal would relocate one drive entrance to the site, shifting slightly to the north, which would provide more green space including a rain garden in front of the building. Primary parking would be along the side of the building. which would help traffic conflict on Edina Industrial Boulevard. There would be boulevard sidewalk proposed along Edina Industrial Boulevard in the future. Construction of this segment on this property is not necessary at this time, as there is adequate space within the existing right-of-way to construct in the future.

Stantec Consulting conducted a traffic study. (See attached traffic study.) The study concludes that the existing roadways would have minimal impact on the adjacent roadways and can support the project.

Parking

The proposed 6,000 square foot bank would require 20 off-street parking stalls per Section 36-1311 (v) of the Edina City Code. The applicant is proposing 31 parking stalls on their new proposed lot, therefore, is code compliant. Calculating the existing building and new dental office 389 spaces would be required. The proposed plans would provide 412 spaces.

As a condition of approval, bicycle parking must be installed on the site. City Code would require 1 bike parking stalls based on the number of parking stalls proposed.

Landscaping

Based on the size of the entire site, site 48 over-story trees are required to be planted. There are 58 overstory trees on the site that would remain and is proposing to add 6 over-story trees with a full complement of understory shrubs and bushes.

Grading/Drainage/Utilities

The city engineer has reviewed the proposed plans and found them to be acceptable. (See attached engineering review memo.)

Building/Building Material

The building materials would be a combination of stone and metal panel. (See attached renderings and building materials.)

Mechanical Equipment/Trash Enclosures

Any rooftop and/or ground level equipment and trash enclosures would have to be screened if visible from adjacent property lines. The trash area in the northwest corner of the site shall be screened.

Subdivision

The applicant is proposing to divide the parcel into two lots, so the new dental clinic is located on its own lot. There is no minimum lot size in the POD Zoning District. For setback purposes, per Section 36-618 (2) the interior lot lines are not considered, only the perimeter or zoning district boundary. All existing and proposed structures meet the required setbacks with the exception of the proposed variance.

The City is authorized by statute to collect park dedication fees to support the additional demand for parks created by new development when property is subdivided. The City has studied this demand and concluded that new projects generally create additional park demand of approximately $7,1000 per buildable acre (43,560 square feet). The new lot would be 30,419 square feet. Therefore, a park dedication of $4,900 would be required.

Compliance Table

	City Standard (POD-2) (Setback measured to the property line)	**Proposed (Setback measured to the property line)**
Building Setbacks Front – Metro Boulevard Side – North Side – South Rear	 35 feet 20 feet 20 feet 20 feet	 90 feet 200+ feet ***10 feet**** 21 feet
Building height	8 stories & 96 feet	1 story
First floor ceiling height	20 feet	***10 feet****
Floor area ratio (FAR)	.50% 110,790 s.f. existing building plus 6,000 s.f. proposed = 116,790 s.f. total Site area = 238,321s.f.	.50%
Parking	Dental = 1 space/300 s.f. = 20 spaces required Existing building & dental office = 389 spaces	31 spaces proposed 412 spaces total existing and proposed spaces

Variance Requested

PRIMARY ISSUE/STAFF RECOMMENDATION

Primary Issues

- **Is the proposed Variance for first floor height justified?**

Yes, staff believes the requested variances are justified. Per Chapter 36 of the City Code, a variance should not be granted unless it is found that the enforcement of the Ordinance would cause practical difficulties in complying with the Zoning Ordinance and that the use is reasonable. As demonstrated below, staff believes the proposal meets the variance standards, when applying the three conditions:

Minnesota Statutes and Section 36-98 of the Edina Zoning Ordinance require that a variance shall not be granted unless the following findings are made:

1. ***The variances would be in harmony with the general purposes and intent of the ordinance.***

The intent of this regulation was to provide active building frontage and flexible reuse of first floor space. This Ordinance went into effect in 2020 and were recommended as part of the Southdale Design Experience Guidelines. However, since the ordinance went into effect most development proposals have either requested a variance from them, or requested flexibility from the standards through the PUD, Planned Unit Development Process. Issues raised with the current regulations by Planning Commission, City Council and developers during those reviews include that these standards run contrary to the City's sustainability goals regarding energy efficiency in buildings, causes buildings to be taller, and adds expense to buildings.

Below are examples from recent projects that didn't meet the standards.

Project	20 Foot 1st Floor Ceiling Height
Edina Fire Station	11 feet
Eden 100 Apartment (not built)	13 feet
First National Bank	13 feet
7200-7250 France (not built)	16 feet
Aeon 76th	10 feet
Maison Green	12 feet

The Planning Commission is currently working on an Ordinance Amendment to revise or eliminate these two requirements outside of the Greater Southdale Area.

2. ***The variance would be consistent with the Comprehensive Plan.***

The Comprehensive Plan does not regulate first floor building height. The proposed dental office is consistent with the Comprehensive Plan. Below is the land use categories and description.

Categories	Description, Land Uses
OR Office Residential	Transitional areas along major thoroughfares or between higher-intensity districts and residential districts. Many existing highway-oriented commercial areas are anticipated to transition to this more mixed -use character. Primary uses are offices, attached or multifamily housing. Secondary uses: Limited retail and service uses (not including "big box" retail), limited industrial (fully enclosed), institutional uses, parks and open space. Vertical mixed-use should be encouraged and may be required on larger sites.

3. ***There are practical difficulties in complying with the ordinance. The term "practical difficulties" means the following:***

   i. ***The property owner proposes to use the property in a reasonable manner not permitted by the Zoning Ordinance.***

   Reasonable use does not mean that the applicant must show the land cannot be put to any reasonable use without the variance. Rather, the applicant must show that there are practical difficulties in complying with the code and that the proposed use is reasonable. "Practical difficulties" may include functional and aesthetic concerns.

   These variances are reasonable given the recent decisions (Variances and PUDs) that allowed reduced first floor building height. Additionally, these variances are reasonable given the small size of the proposed building, and that a 10-foot ceiling height would be more energy efficient. A 10-foot ceiling height would not impact reuse of the building for different users in the future. The City is also considering an amendment to this regulation to reduce or eliminate it outside of the Greater Southdale Area.

   ii. ***The plight of the landowner is due to circumstances unique to the property not created by the landowner.***

   Yes. The circumstances would be unique to the property in raising the first-floor ceiling height to 20 feet for the new buildings proposed, compared the existing buildings in the area. As mentioned above, an ordinance amendment to reduce or eliminate this regulation is being considered.

   iii. ***The variance, if granted, will not alter the essential character of the locality.***

   The variances would not alter the essential character of the locality. The existing buildings within this area have not been subject to this regulation, as the ordinance is only a few years old. There are no buildings in this area with 20-foot first floor ceiling height.

- **Is the proposed Variance for side yard setback justified?**

Yes, staff believes the requested variances are justified. Per Chapter 36 of the City Code, a variance should not be granted unless it is found that the enforcement of the Ordinance would cause practical difficulties in complying with the Zoning Ordinance and that the use is reasonable. As

demonstrated below, staff believes the proposal meets the variance standards, when applying the three conditions:

Minnesota Statutes and Section 36-98 of the Edina Zoning Ordinance require that a variance shall not be granted unless the following findings are made:

1. The variances would be in harmony with the general purposes and intent of the ordinance.

The intent of the side yard setback is to provide adequate spacing between buildings. The building to the south is setback further on the lot, 165 feet back from the street and is 60 feet back from the side lot line. The rear of the proposed dental office would be over 70 feet away. Staff believes there is adequate spacing between the buildings.

2. The variance would be consistent with the Comprehensive Plan.

The proposed dental office is consistent with the Comprehensive Plan. Below is the land use categories and description.

Categories	Description, Land Uses
OR Office Residential	Transitional areas along major thoroughfares or between higher-intensity districts and residential districts. Many existing highway-oriented commercial areas are anticipated to transition to this more mixed -use character. Primary uses are offices, attached or multifamily housing. Secondary uses: Limited retail and service uses (not including "big box" retail), limited industrial (fully enclosed), institutional uses, parks and open space. Vertical mixed-use should be encouraged and may be required on larger sites.

3. There are practical difficulties in complying with the ordinance. The term "practical difficulties" means the following:

i. The property owner proposes to use the property in a reasonable manner not permitted by the Zoning Ordinance.

Reasonable use does not mean that the applicant must show the land cannot be put to any reasonable use without the variance. Rather, the applicant must show that there are practical difficulties in complying with the code and that the proposed use is reasonable. "Practical difficulties" may include functional and aesthetic concerns.

These variances are reasonable given the distance separation from the closest building at over 70 feet. The reason for the request is that the building could be moved up closer to the street, as recommended in the sketch plan. With a closer setback to the side lot line, they are able to provide parking along the side of the building and not disturb the location of the existing drive-aisle.

ii. The plight of the landowner is due to circumstances unique to the property not created by the landowner.

Yes. The unique circumstances are the existing drive-aisle locations, the desire to pull the building up closer to the street and still provide convenient parking so that patients would not have to cross over drive-aisles while walking from their vehicles to the dental office.

iii. The variance, if granted, will not alter the essential character of the locality.

The variances would not alter the essential character of the locality. The existing buildings within this area have not been subject to this regulation, as the ordinance is only a few years old. There are no buildings in this area with 20-foot first floor ceiling height.

STAFF RECOMMENDATION

Recommend the City Council approve the Site Plan, Subdivision, building setback variance from 20 to 10 feet and first floor building height from 20 to 10 feet at 7300 Metro Boulevard.

Approval is based on the following findings:

1. The requested variances meet the variance criteria.

2. The intent of the side yard setback is to provide adequate spacing between buildings. The proposed dental office would allow for adequate space from the nearest building. The building to the south is setback further on the lot, 165 feet back from the street and is 60 feet back from the side lot line. The rear of the proposed dental office would be over 70 feet away.

3. These variances are reasonable given the distance separation from the closest building at over 70 feet. The reason for the request is that the building could be moved up closer to the street, as recommended in the sketch plan. With a closer setback to the side lot line, they are able to provide parking along the side of the building and not disturb the location of the existing drive-aisle.

4. The proposed use of the building is consistent with the Comprehensive Plan.

5. There are no buildings in this area with 20-foot first floor ceiling height.

6. Stantec conducted a traffic and parking study and concluded that the project would have minimal impact on existing roads and there would be adequate parking.

Approval is subject to the following Conditions:

1. The Plan must be consistent with the Plans date stamped March 11, 2024.

2. The Final Landscape Plan must meet all minimum landscaping requirements per Chapter 36 of the Zoning Ordinance. A performance bond, letter-of-credit, or cash deposit must

be submitted for one and one-half times the cost amount for completing the required landscaping, screening, or erosion control measures at the time of any building permit.

3. Provision of code compliant bike racks for each use near the building entrances.

4. The Final Lighting Plan must meet all minimum requirements per Section 36-1260 of the City Code.

5. Roof-top mechanical equipment and ground level equipment must be screened per Section 36-1459 of the City Code.

6. Submit a copy of the Nine Mile Creek Watershed District permit. The City may require revisions to the approved plans to meet the district's requirements.

7. Hours of construction must be consistent with City Code.

8. Compliance with all the conditions outlined in the city engineer's memo dated April 2, 2024.

9. Per City requirements, a Travel Demand Management (TDM) plan is required for this project. The goal of the TDM plan is to reduce vehicular trips during peak hours and carbon emissions from vehicles. TDM strategies for this site include:
   - Providing maps that show the area bus routes and schedules.
   - Providing maps of bicycle and pedestrian facilities.
   - Providing information on starting and joining commuter programs.
   - Providing bicycle parking spaces for employees and facility users.

Staff Recommendation

Staff recommends approval subject to the findings and conditions above.

Deadline for a city decision: June 4, 2024

DATE: April 2, 2024

TO: 7300 Metro Boulevard - Owner and Development Team

CC: Cary Teague – Community Development Director

FROM: Chad Millner P.E. - Director of Engineering
Ross Bintner P.E. – Engineering Services Manager
Ben Jore P.E. – Senior Project Engineer
Andrew Scipioni, Transportation Planner
Marisa Bayer, Sustainability Manager

RE: **7300 Metro Boulevard – Development Review**

The Engineering Department has reviewed the subject property for pedestrian facilities, utility connections, grading, flood risk, and storm water. Plans reviewed included civil and landscape dated 3/11/24.

General

1. Deliver as-built records of all utilities including storm, sanitary, and watermain for both public and private post construction.

Survey

2. An existing and proposed site condition survey is required.
3. Show all existing and proposed public and private easements. Drainage & utility easements will be required.

Living Streets

4. Design sidewalks to meet ADA requirements.

Traffic and Street

5. Construction staging and traffic control plans will be required.
6. Building access roads and entrances will need to accommodate Edina's ladder fire truck.
7. Confirm all roads are designed for Edina's 80,000lb fire truck load.
8. 48 hrs notice is required for road closures related to construction operations. Complete the road closure form from the City's website.
9. Consider impacts from deliveries to pedestrian and vehicular traffic.

Sanitary and Water Utilities

10. Sanitary service is shown to connect to the trunk line off the property to the west. Permission and private easement from the property owner is required to make the sanitary connection.
11. Remove all abandoned sanitary and water services to the main.
12. Domestic water shall be sized by the developer's engineer.
13. Domestic sanitary shall be sized by the developer's engineer.

14. Sewer and water connection permits required for all connections. City staff to be present to inspect, cost to be paid by developer.
15. Separate meters for fire and domestic services will be required.
16. A SAC and WAC determination will be required and Met Council and City fees will be calculated from the determination.

Storm Water Utility

26. Provide hydraulic and hydrologic report meeting watershed and state construction site permit requirements.
27. Provide geotechnical report with soil borings.
28. The property is within a local flood zone. The 100yr HWL of the local flood zone is 827.80. The FFE of the proposed dental office is 829.80. The dental office meets the freeboard requirement.
29. Applicant proposes to fill existing flood storage and provide compensatory storage by oversizing ponds and underground storm chambers used to provide rate and volume control meeting Nine Mile Watershed standards. City will confirm compensatory flood storage provided causes no net increase in flood risk to structures or neighboring properties in 1% probability event by modeling proposed system in XP-SWMM. Results will be provided to developer's engineer. Developer must modify storage to provide no increase in risk, if results show increase.
30. Evidence of watershed district permit and copies of private maintenance agreement for storm water system in favor of watershed is required for building permit.
31. Retention system engineer required to verify construction of the underground retention systems done per plan.
32. Provide signed plan from underground retention system and confirm it is designed for 80,000lb fire truck load and outriggers.

Grading Erosion and Sediment Control

33. A SWPPP has been submitted. SWPPP to be reviewed at building permit.

Sustainability

34. Edina's Climate Action Plan commits our community to reducing greenhouse gas emissions 45% by 2030 and achieve net-zero emissions by 2050. This project can take steps to support our Climate Action Plan goals by:
    a. Participating in Xcel Energy's New Building programs, like Energy Efficient Buildings, to receive no-cost support to identify energy-saving opportunities in your design.
    b. Contacting CenterPoint Energy for their Builder and Developer Programs. The program assists in finding rebates and other programs that can help builders, developers, and their customers save energy and money.
    c. Building external trash enclosure/screen that make enough room for an organics bin along with trash and recycling. Business communities are finding separating organic waste reduces their waste management expenses and their greenhouse gas emissions.
    d. Consider electric vehicle charging capability for at least 15% of new parking stalls.
35. If this project receives new Planned Unit Development (PUD) zoning or financial assistance from the City of Edina or Edina Housing & Redevelopment Authority, it will need to comply with the Sustainable Building Policy.

Other Agency Coordination

36. Nine Mile Creek Watershed permit is required. Plans should be submitted to them for comment. MDH, MPCA and MCES and others as required.

Planning Division
Submitted
03-11-2024

EDINA DENTAL OFFICE

EDINA, MINNESOTA

ISSUED FOR: CITY SUBMITTAL

SITE LOCATION MAP N

SITE LOCATION

PROJECT CONTACTS		
	NAME & ADDRESS	CONTACT
CIVIL ENGINEER	CIVIL SITE GROUP 5000 GLENWOOD AVE GOLDEN VALLEY, MN 55422	ROBBIE LATTA 612-615-0060 RLATTA@CIVILSITEGROUP.COM
LANDSCAPE ARCHITECT	CIVIL SITE GROUP 5000 GLENWOOD AVE GOLDEN VALLEY, MN 55422	PATRICK SARVER 612-615-0060 PSARVER@CIVILSITEGROUP.COM
PROPERTY OWNER/DEVELOPER	HEMPEL REAL ESTATE. 10050 CROSSTOWN CIRCLE, SUITE 600 EDEN PRAIRIE, MN 55334	BRENT JACOBSON 612-355-2609 BJACOBSON@HEMPELCOMPANIES.COM
ARCHITECT	DRJ ARCHITECTURE , INC. 333 WASHINGTON AVENUE N, #210 MINNEAPOLIS, MN 55401	CHRIS WHITEHOUSE 6126762700 CWHITEHOUSE@DJR-INC.COM
SURVEYOR	CIVIL SITE GROUP 5000 GLENWOOD AVE GOLDEN VALLEY, MN 55422	RORY SYNSTELIEN 612-615-0060 RORY@CIVILSITEGROUP.COM
GEOTECHNICAL ENGINEER	HAUGO GEOTECHNICAL SERVICES 2825 CEDAR AVE S MINNEAPOLIS, MN 55407	PAUL GIONFRIDDO 612-729-2659 PAULGIONFRIDDO@GMAIL.COM

ALL EXISTING UTILITY LOCATIONS SHOWN ARE APPROXIMATE. CONTACT "GOPHER STATE ONE CALL" (651-454-0002 OR 800-252-1166) FOR UTILITY LOCATIONS, 48 HOURS PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL REPAIR OR REPLACE ANY UTILITIES THAT ARE DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.

811 Know what's below. Call before you dig.

SHEET INDEX

SHEET NUMBER	SHEET TITLE
C0.0	TITLE SHEET
C0.1	PROJECT NOTES
C1.0	REMOVALS PLAN
C1.1	TREE PRESERVATION PLAN
C1.2	REMOVALS PLAN - WEST
C2.0	SITE PLAN
C3.0	GRADING PLAN
C4.0	UTILITY PLAN
C4.1	SANITARY CONNECTION
C5.0	CIVIL DETAILS
C5.1	CIVIL DETAILS
C5.2	CIVIL DETAILS
L1.0	LANDSCAPE PLAN
L1.1	LANDSCAPE PLAN NOTES & DETAILS
SW1.0	SWPPP - EXISTING CONDITIONS
SW1.1	SWPPP - PROPOSED CONDITIONS
SW1.2	SWPPP - DETAILS
SW1.3	SWPPP - NARRATIVE

CivilSite GROUP
Civil Engineering • Surveying • Landscape Architecture
5000 Glenwood Avenue
Golden Valley, MN 55422
civilsitegroup.com 612-615-0060

PRELIMINARY: NOT FOR CONSTRUCTION

PROJECT
EDINA DENTAL OFFICE
7300 METRO BOULEVARD, EDINA, MINNESOTA 55439

HEMPEL REAL ESTATE
10050 CROSSTOWN CIRCLE, SUITE 600, EDEN PRAIRIE, MN 55334

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

Robert A. Latta
DATE 3/11/24 LICENSE NO. 59612

ISSUE/SUBMITTAL SUMMARY

DATE	DESCRIPTION
03/11/2024	CITY SUBMITTAL

DRAWN BY	JR
REVIEWED BY	RL
PROJECT MANAGER	RL
PROJECT NUMBER	23353

REVISION SUMMARY

DATE	DESCRIPTION

TITLE SHEET

C0.0

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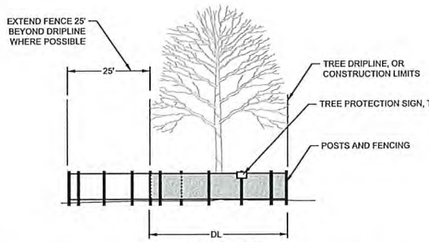
Planning Division
Submitted 04
03 / 14

REMOVAL NOTES:

1. ALL EXISTING UTILITY LOCATIONS SHOWN ARE APPROXIMATE. CONTACT "GOPHER STATE ONE CALL" (651-454-0002 OR 800-252-1166) FOR UTILITY LOCATIONS, 48 HOURS PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL REPAIR OR REPLACE ANY UTILITIES THAT ARE DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.
2. SEE STORM WATER POLLUTION PREVENTION PLAN (SWPPP) PLAN FOR CONSTRUCTION STORM WATER MANAGEMENT PLAN.
3. REMOVAL OF MATERIALS NOTED ON THE DRAWINGS SHALL BE IN ACCORDANCE WITH MNDOT, STATE AND LOCAL REGULATIONS.
4. REMOVAL OF PRIVATE UTILITIES SHALL BE COORDINATED WITH UTILITY OWNER PRIOR TO CONSTRUCTION ACTIVITIES.
5. EXISTING PAVEMENTS SHALL BE SAWCUT IN LOCATIONS AS SHOWN ON THE DRAWINGS OR THE NEAREST JOINT FOR PROPOSED PAVEMENT CONNECTIONS.
6. REMOVED MATERIALS SHALL BE DISPOSED OF TO A LEGAL OFF-SITE LOCATION AND IN ACCORDANCE WITH STATE AND LOCAL REGULATIONS.
7. ABANDON, REMOVAL, CONNECTION, AND PROTECTION NOTES SHOWN ON THE DRAWINGS ARE APPROXIMATE. COORDINATE WITH PROPOSED PLANS.
8. EXISTING ON-SITE FEATURES NOT NOTED FOR REMOVAL SHALL BE PROTECTED THROUGHOUT THE DURATION OF THE CONTRACT.
9. PROPERTY LINES SHALL BE CONSIDERED GENERAL CONSTRUCTION LIMITS UNLESS OTHERWISE NOTED ON THE DRAWINGS. WORK WITHIN THE GENERAL CONSTRUCTION LIMITS SHALL INCLUDE STAGING, DEMOLITION AND CLEAN-UP OPERATIONS AS WELL AS CONSTRUCTION SHOWN ON THE DRAWINGS.
10. MINOR WORK OUTSIDE OF THE GENERAL CONSTRUCTION LIMITS SHALL BE ALLOWED AS SHOWN ON THE PLAN AND PER CITY REQUIREMENTS. FOR ANY WORK ON ADJACENT PRIVATE PROPERTY, THE CONTRACTOR SHALL OBTAIN WRITTEN PERMISSION FROM THE ADJACENT PROPERTY OWNER PRIOR TO ANY WORK.
11. DAMAGE BEYOND THE PROPERTY LIMITS CAUSED BY CONSTRUCTION ACTIVITY SHALL BE REPAIRED IN A MANNER APPROVED BY THE ENGINEER/LANDSCAPE ARCHITECT OR IN ACCORDANCE WITH THE CITY.
12. PROPOSED WORK (BUILDING AND CIVIL) SHALL NOT DISTURB EXISTING UTILITIES UNLESS OTHERWISE SHOWN ON THE DRAWINGS AND APPROVED BY THE CITY PRIOR TO CONSTRUCTION.
13. SITE SECURITY MAY BE NECESSARY AND PROVIDED IN A MANNER TO PROHIBIT VANDALISM, AND THEFT, DURING AND AFTER NORMAL WORK HOURS, THROUGHOUT THE DURATION OF THE CONTRACT. SECURITY MATERIALS SHALL BE IN ACCORDANCE WITH THE CITY.
14. VEHICULAR ACCESS TO THE SITE SHALL BE MAINTAINED FOR DELIVERY AND INSPECTION ACCESS DURING NORMAL OPERATING HOURS. AT NO POINT THROUGHOUT THE DURATION OF THE CONTRACT SHALL CIRCULATION OF ADJACENT STREETS BE BLOCKED WITHOUT APPROVAL BY THE CITY PRIOR TO CONSTRUCTION ACTIVITIES.
15. ALL TRAFFIC CONTROLS SHALL BE PROVIDED AND ESTABLISHED PER THE REQUIREMENTS OF THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MMUTCD) AND THE CITY. THIS SHALL INCLUDE, BUT NOT BE LIMITED TO, SIGNAGE, BARRICADES, FLASHERS, AND FLAGGERS AS NEEDED. ALL PUBLIC STREETS SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES. NO ROAD CLOSURES SHALL BE PERMITTED WITHOUT APPROVAL BY THE CITY.
16. SHORING FOR BUILDING EXCAVATION MAY BE USED AT THE DISCRETION OF THE CONTRACTOR AND AS APPROVED BY THE OWNERS REPRESENTATIVE AND THE CITY PRIOR TO CONSTRUCTION ACTIVITIES.
17. STAGING, DEMOLITION, AND CLEAN-UP AREAS SHALL BE WITHIN THE PROPERTY LIMITS AS SHOWN ON THE DRAWINGS AND MAINTAINED IN A MANNER AS REQUIRED BY THE CITY.
18. ALL EXISTING SITE TRAFFIC/REGULATORY SIGNAGE TO BE INVENTORIED AND IF REMOVED FOR CONSTRUCTION SHALL BE RETURNED TO LGU.
19. ALL EXISTING UTILITY LOCATIONS SHOWN ARE APPROXIMATE. CONTACT "GOPHER STATE ONE CALL" (651-454-0002 OR 800-252-1166) FOR UTILITY LOCATIONS, 48 HOURS PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL REPAIR OR REPLACE ANY UTILITIES THAT ARE DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.



FURNISH A AND INSTALL TEMPORARY FENCE AT THE TREE'S DRIP LINE OR CONSTRUCTION LIMITS AS SHOWN ON PLAN, PRIOR TO ANY CONSTRUCTION. WHERE POSSIBLE PLACE FENCE 25' BEYOND DRIP LINE. PLACE TREE PROTECTION SIGN ON POSTS, ONE PER INDIVIDUAL TREE (FACING CONSTRUCTION ACTIVITY), OR ONE EVERY 100' LF ALONG A GROVE OR MULTI-TREE PROTECTION AREA.

1 TREE PROTECTION
NTS

SITE LAYOUT NOTES:

1. ALL EXISTING UTILITY LOCATIONS SHOWN ARE APPROXIMATE. CONTACT "GOPHER STATE ONE CALL" (651-454-0002 OR 800-252-1166) FOR UTILITY LOCATIONS, 48 HOURS PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL REPAIR OR REPLACE ANY UTILITIES THAT ARE DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.
2. CONTRACTOR SHALL VERIFY LOCATIONS AND LAYOUT OF ALL SITE ELEMENTS PRIOR TO BEGINNING CONSTRUCTION, INCLUDING BUT NOT LIMITED TO, LOCATIONS OF EXISTING AND PROPOSED PROPERTY LINES, EASEMENTS, SETBACKS, UTILITIES, BUILDINGS AND PAVEMENTS. CONTRACTOR IS RESPONSIBLE FOR FINAL LOCATIONS OF ALL ELEMENTS FOR THE SITE. ANY REVISIONS REQUIRED AFTER COMMENCEMENT OF CONSTRUCTION, DUE TO LOCATIONAL ADJUSTMENTS SHALL BE CORRECTED AT NO ADDITIONAL COST TO OWNER. ADJUSTMENTS TO THE LAYOUT SHALL BE APPROVED BY THE ENGINEER/LANDSCAPE ARCHITECT PRIOR TO INSTALLATION OF MATERIALS. STAKE LAYOUT FOR APPROVAL.
3. THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS PRIOR TO CONSTRUCTION, INCLUDING A RIGHT-OF-WAY AND STREET OPENING PERMIT.
4. THE CONTRACTOR SHALL VERIFY RECOMMENDATIONS NOTED IN THE GEO TECHNICAL REPORT PRIOR TO INSTALLATION OF SITE IMPROVEMENT MATERIALS.
5. CONTRACTOR SHALL FIELD VERIFY COORDINATES AND LOCATION DIMENSIONS & ELEVATIONS OF THE BUILDING AND STAKE FOR REVIEW AND APPROVAL BY THE OWNERS REPRESENTATIVE PRIOR TO INSTALLATION OF FOOTING MATERIALS.
6. LOCATIONS OF STRUCTURES, ROADWAY PAVEMENTS, CURBS AND GUTTERS, BOLLARDS, AND WALKS ARE APPROXIMATE AND SHALL BE STAKED IN THE FIELD, PRIOR TO INSTALLATION, FOR REVIEW AND APPROVAL BY THE ENGINEER/LANDSCAPE ARCHITECT.
7. CURB DIMENSIONS SHOWN ARE TO FACE OF CURB. BUILDING DIMENSIONS ARE TO FACE OF CONCRETE FOUNDATION. LOCATION OF BUILDING IS TO BUILDING FOUNDATION AND SHALL BE AS SHOWN ON THE DRAWINGS.
8. THE CONTRACTOR SHALL SUBMIT SHOP DRAWINGS OR SAMPLES AS SPECIFIED FOR REVIEW AND APPROVAL BY THE ENGINEER/LANDSCAPE ARCHITECT PRIOR TO FABRICATION FOR ALL PREFABRICATED SITE IMPROVEMENT MATERIALS SUCH AS, BUT NOT LIMITED TO THE FOLLOWING, FURNISHINGS, PAVEMENTS, WALLS, RAILINGS, BENCHES, FLAGPOLES, LANDING PADS FOR CURB RAMPS, AND LIGHT AND POLES. THE OWNER RESERVES THE RIGHT TO REJECT INSTALLED MATERIALS NOT PREVIOUSLY APPROVED.
9. PEDESTRIAN CURB RAMPS SHALL BE CONSTRUCTED WITH TRUNCATED DOME LANDING AREAS IN ACCORDANCE WITH A.D.A. REQUIREMENTS-SEE DETAIL.
10. CROSSWALK STRIPING SHALL BE 24" WIDE WHITE PAINTED LINE, SPACED 48" ON CENTER PERPENDICULAR TO THE FLOW OF TRAFFIC. WIDTH OF CROSSWALK SHALL BE 5' WIDE. ALL OTHER PAVEMENT MARKINGS SHALL BE WHITE IN COLOR UNLESS OTHERWISE NOTED OR REQUIRED BY ADA OR LOCAL GOVERNING BODIES.
11. SEE SITE PLAN FOR CURB AND GUTTER TYPE. TAPER BETWEEN CURB TYPES-SEE DETAIL.
12. ALL CURB RADII ARE MINIMUM 3' UNLESS OTHERWISE NOTED.
13. CONTRACTOR SHALL REFER TO FINAL PLAT FOR LOT BOUNDARIES, NUMBERS, AREAS AND DIMENSIONS PRIOR TO SITE IMPROVEMENTS.
14. FIELD VERIFY ALL EXISTING SITE CONDITIONS, DIMENSIONS.
15. PARKING IS TO BE SET PARALLEL OR PERPENDICULAR TO EXISTING BUILDING UNLESS NOTED OTHERWISE.
16. ALL PARKING LOT PAINT STRIPING TO BE WHITE, 4" WIDE TYP.
17. BITUMINOUS PAVING TO BE "LIGHT DUTY" UNLESS OTHERWISE NOTED. SEE DETAIL SHEETS FOR PAVEMENT SECTIONS.
18. ALL TREES THAT ARE TO REMAIN ARE TO BE PROTECTED FROM DAMAGE WITH A CONSTRUCTION FENCE AT THE DRIP LINE. SEE LANDSCAPE DOCUMENTS.
19. CONTRACTOR IS RESPONSIBLE TO INSTALL ANY SIDEWALK AND CURBING PER DESIGN PLAN. CONTRACTOR TO VERIFY ALL CURBS AND SIDEWALKS WILL DRAIN PROPERLY IN FIELD CONDITIONS. **CONTRACTOR MUST CONTACT THE CIVIL ENGINEER 24-HOURS PRIOR TO ANY CURB AND/OR SIDEWALK INSTALLATION TO REVIEW AND INSPECT CURB STAKES. CONTRACTOR IS RESPONSIBLE FOR ANY CURB OR SIDEWALK REPLACEMENT IF THIS PROCEDURE IS NOT FOLLOWED.**

OPERATIONAL NOTES	
SNOW REMOVAL	ALL SNOW SHALL BE STORED ON-SITE OUTSIDE PARKING LOT. WHEN FULL, REMOVAL CO. SHALL REMOVE EXCESS OFF-SITE.
TRASH REMOVAL	TRASH SHALL BE PLACED IN EXTERIOR TRASH AREA AND REMOVED BY COMMERCIAL CO. WEEKLY.
DELIVERIES	DELIVERIES SHALL OCCUR AT THE FRONT DOOR VIA STANDARD COMMERCIAL DELIVERY VEHICLES (UPS, FED-EX, USPS).

SITE DATA		
ZONING SUMMARY		
EXISTING ZONING	POD-1 - PLANNED OFFICE	
PROPOSED ZONING		
	REQUIRED	PROVIDED
BUILDING SETBACKS		
FRONT STREET	35'	
REAR YARD	20'	
SIDE STREET	35'	
INTERIOR SIDE YARD	20'	
PARKING SETBACKS		
FRONT - (FROM ROW)	20'	
REAR	10'	
SIDE	10'	
	REQUIRED	PROVIDED
PARKING SUMMARY		
PARKING STALL DIMENSIONS	8.5' X 18'	
COMPACT PARKING STALL DIMENSIONS	7.5' X 16'	
DRIVE AISLE DIMENSIONS	24'	
FIRE ACCESS ROAD DIMENSIONS		
	REQUIRED	PROVIDED
PARKING STALLS - SURFACE	23	
PARKING STALLS - GARAGE	N/A	

GENERAL GRADING NOTES:

1. **CONTRACTOR SHALL VERIFY ALL BUILDING ELEVATIONS, (FFE, LFE, GFE), PRIOR TO CONSTRUCTION BY CROSS CHECKING WITH ARCHITECTURAL, STRUCTURAL AND CIVIL ELEVATIONS FOR EQUIVALENT "100" ELEVATIONS. THIS MUST BE DONE PRIOR TO EXCAVATION AND INSTALLATION OF ANY FOOTING MATERIALS. VERIFICATION OF THIS COORDINATION SHALL BE CONFIRMED IN WRITING BY CIVIL, SURVEYOR, ARCHITECTURAL, STRUCTURAL AND CONTRACTOR PRIOR TO CONSTRUCTION.**
2. ALL EXISTING UTILITY LOCATIONS SHOWN ARE APPROXIMATE. CONTACT "GOPHER STATE ONE CALL" (651-454-0002 OR 800-252-1166) FOR UTILITY LOCATIONS, 48 HOURS PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL REPAIR OR REPLACE ANY UTILITIES THAT ARE DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.
3. SEE SITE PLAN FOR HORIZONTAL LAYOUT & GENERAL GRADING NOTES.
4. THE CONTRACTOR SHALL COMPLETE THE SITE GRADING CONSTRUCTION (INCLUDING BUT NOT LIMITED TO SITE PREPARATION, SOIL CORRECTION, EXCAVATION, EMBANKMENT, ETC.) IN ACCORDANCE WITH THE REQUIREMENTS OF THE OWNER'S SOILS ENGINEER. ALL SOIL TESTING SHALL BE COMPLETED BY THE OWNER'S SOILS ENGINEER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL REQUIRED SOIL TESTS AND INSPECTIONS WITH THE SOILS ENGINEER.
5. ANY ELEMENTS OF AN EARTH RETENTION SYSTEM AND RELATED EXCAVATIONS THAT FALL WITHIN THE PUBLIC RIGHT OF WAY WILL REQUIRE A "RIGHT OF WAY EXCAVATION PERMIT". CONTRACTOR IS RESPONSIBLE FOR AQUIRING THIS PERMIT PRIOR TO CONSTRUCTION IF APPLICABLE
6. ALL EXISTING UTILITY LOCATIONS SHOWN ARE APPROXIMATE. CONTACT "GOPHER STATE ONE CALL" (651-454-0002 OR 800-252-1166) FOR UTILITY LOCATIONS, 48 HOURS PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL REPAIR OR REPLACE ANY UTILITIES THAT ARE DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.
7. GRADING AND EXCAVATION ACTIVITIES SHALL BE PERFORMED IN ACCORDANCE WITH THE NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT REQUIREMENTS & PERMIT REQUIREMENTS OF THE CITY.
8. PROPOSED SPOT GRADES ARE FLOW-LINE FINISHED GRADE ELEVATIONS, UNLESS OTHERWISE NOTED.
9. GRADES OF WALKS SHALL BE INSTALLED WITH 5% MAX. LONGITUDINAL SLOPE AND 1% MIN. AND 2% MAX. CROSS SLOPE, UNLESS OTHERWISE NOTED.
10. PROPOSED SLOPES SHALL NOT EXCEED 3:1 UNLESS INDICATED OTHERWISE ON THE DRAWINGS. MAXIMUM SLOPES IN MAINTAINED AREAS IS 4:1.
11. PROPOSED RETAINING WALLS, FREESTANDING WALLS, OR COMBINATION OF WALL TYPES GREATER THAN 4' IN HEIGHT SHALL BE DESIGNED AND ENGINEERED BY A REGISTERED RETAINING WALL ENGINEER. DESIGN DRAWINGS SHALL BE SUBMITTED FOR REVIEW AND APPROVAL PRIOR TO CONSTRUCTION.
12. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTENANCE OF GRADE STAKES THROUGHOUT THE DURATION OF CONSTRUCTION TO ESTABLISH PROPER GRADES. THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR A FINAL FIELD CHECK OF FINISHED GRADES ACCEPTABLE TO THE ENGINEER/LANDSCAPE ARCHITECT PRIOR TO TOPSOIL AND SODDING ACTIVITIES.
13. IF EXCESS OR SHORTAGE OF SOIL MATERIAL EXISTS, THE CONTRACTOR SHALL TRANSPORT ALL EXCESS SOIL MATERIAL OFF THE SITE TO AN AREA SELECTED BY THE CONTRACTOR, OR IMPORT SUITABLE MATERIAL TO THE SITE.
14. EXCAVATE TOPSOIL FROM AREAS TO BE FURTHER EXCAVATED OR REGRADED AND STOCKPILE IN AREAS DESIGNATED ON THE SITE. THE CONTRACTOR SHALL SALVAGE ENOUGH TOPSOIL FOR RESPREADING ON THE SITE AS SPECIFIED. EXCESS TOPSOIL SHALL BE PLACED IN EMBANKMENT AREAS, OUTSIDE OF BUILDING PADS, ROADWAYS AND PARKING AREAS. THE CONTRACTOR SHALL SUBCUT CUT AREAS, WHERE TURF IS TO BE ESTABLISHED, TO A DEPTH OF 6 INCHES. RESPREAD TOPSOIL IN AREAS WHERE TURF IS TO BE ESTABLISHED TO A MINIMUM DEPTH OF 6 INCHES.
15. FINISHED GRADING SHALL BE COMPLETED. THE CONTRACTOR SHALL UNIFORMLY GRADE AREAS WITHIN LIMITS OF GRADING, INCLUDING ADJACENT TRANSITION AREAS. PROVIDE A SMOOTH FINISHED SURFACE WITHIN SPECIFIED TOLERANCES, WITH UNIFORM LEVELS OR SLOPES BETWEEN POINTS WHERE ELEVATIONS ARE SHOWN, OR BETWEEN SUCH POINTS AND EXISTING GRADES. AREAS THAT HAVE BEEN FINISH GRADED SHALL BE PROTECTED FROM SUBSEQUENT CONSTRUCTION OPERATIONS, TRAFFIC AND EROSION. REPAIR ALL AREAS THAT HAVE BECOME RUTTED BY TRAFFIC OR ERODED BY WATER OR HAS SETTLED BELOW THE CORRECT GRADE. ALL AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED TO EQUAL OR BETTER THAN ORIGINAL CONDITION OR TO THE REQUIREMENTS OF THE NEW WORK.
16. PRIOR TO PLACEMENT OF THE AGGREGATE BASE, A TEST ROLL WILL BE REQUIRED ON THE STREET AND/OR PARKING AREA SUBGRADE. THE CONTRACTOR SHALL PROVIDE A LOADED TANDEM AXLE TRUCK WITH A GROSS WEIGHT OF 25 TONS. THE TEST ROLLING SHALL BE AT THE DIRECTION OF THE SOILS ENGINEER AND SHALL BE COMPLETED IN AREAS AS DIRECTED BY THE SOILS ENGINEER. THE SOILS ENGINEER SHALL DETERMINE WHICH SECTIONS OF THE STREET OR PARKING AREA ARE UNSTABLE. CORRECTION OF THE SUBGRADE SOILS SHALL BE COMPLETED IN ACCORDANCE WITH THE REQUIREMENTS OF THE SOILS ENGINEER. NO TEST ROLL SHALL OCCUR WITHIN 10' OF ANY UNDERGROUND STORM RETENTION/DETENTION SYSTEMS.
17. TOLERANCES
    - 17.1. THE BUILDING SUBGRADE FINISHED SURFACE ELEVATION SHALL NOT VARY BY MORE THAN 0.30 FOOT ABOVE, OR 0.30 FOOT BELOW, THE PRESCRIBED ELEVATION AT ANY POINT WHERE MEASUREMENT IS MADE.
    - 17.2. THE STREET OR PARKING AREA SUBGRADE FINISHED SURFACE ELEVATION SHALL NOT VARY BY MORE THAN 0.05 FOOT ABOVE, OR 0.10 FOOT BELOW, THE PRESCRIBED ELEVATION OF ANY POINT WHERE MEASUREMENT IS MADE.
    - 17.3. AREAS WHICH ARE TO RECEIVE TOPSOIL SHALL BE GRADED TO WITHIN 0.30 FOOT ABOVE OR BELOW THE REQUIRED ELEVATION, UNLESS DIRECTED OTHERWISE BY THE ENGINEER.
    - 17.4. TOPSOIL SHALL BE GRADED TO PLUS OR MINUS 1/2 INCH OF THE SPECIFIED THICKNESS.
18. MAINTENANCE
    - 18.1. THE CONTRACTOR SHALL PROTECT NEWLY GRADED AREAS FROM TRAFFIC AND EROSION, AND KEEP AREA FREE OF TRASH AND DEBRIS.
    - 18.2. CONTRACTOR SHALL REPAIR AND REESTABLISH GRADES IN SETTLED, ERODED AND RUTTED AREAS TO SPECIFIED TOLERANCES. DURING THE CONSTRUCTION, IF REQUIRED, AND DURING THE WARRANTY PERIOD, ERODED AREAS WHERE TURF IS TO BE ESTABLISHED SHALL BE RESEEDED AND MULCHED.
    - 18.3. WHERE COMPLETED COMPACTED AREAS ARE DISTURBED BY SUBSEQUENT CONSTRUCTION OPERATIONS OR ADVERSE WEATHER, CONTRACTOR SHALL SCARIFY, SURFACE, RESHAPE, AND COMPACT TO REQUIRED DENSITY PRIOR TO FURTHER CONSTRUCTION.

GENERAL UTILITY NOTES:

1. ALL EXISTING UTILITY LOCATIONS SHOWN ARE APPROXIMATE. CONTACT "GOPHER STATE ONE CALL" (651-454-0002 OR 800-252-1166) FOR UTILITY LOCATIONS, 48 HOURS PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL REPAIR OR REPLACE ANY UTILITIES THAT ARE DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.
2. SEE SITE PLAN FOR HORIZONTAL DIMENSIONS AND LAYOUT.
3. CONTRACTOR SHALL FIELD VERIFY LOCATION AND ELEVATION OF EXISTING UTILITIES AND TOPOGRAPHIC FEATURES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER OF DISCREPANCIES OR VARIATIONS FROM THE PLANS.
4. UTILITY INSTALLATION SHALL CONFORM TO THE CURRENT EDITION OF "STANDARD SPECIFICATIONS FOR WATER MAIN AND SERVICE LINE INSTALLATION" AND "SANITARY SEWER AND STORM SEWER INSTALLATION" AS PREPARED BY THE CITY ENGINEERS ASSOCIATION OF MINNESOTA (CEAM), AND SHALL CONFORM WITH THE REQUIREMENTS OF THE CITY AND THE PROJECT SPECIFICATIONS.
5. CASTINGS SHALL BE SALVAGED FROM STRUCTURE REMOVALS AND RE-USED OR PLACED AT THE DIRECTION OF THE OWNER.
6. ALL WATER PIPE SHALL BE CLASS 52 DUCTILE IRON PIPE (DIP) AWWA C151, ASME B16.4, AWWA C110, AWWA C153 UNLESS OTHERWISE NOTED.
7. ALL SANITARY SEWER SHALL BE SDR 26 POLYVINYL CHLORIDE (PVC) ASTM D3034 & F679, OR SCH 40 ASTM D1785, 2665, ASTM F794, 1866) UNLESS OTHERWISE NOTED.
8. ALL STORM SEWER PIPE SHALL BE HDPE ASTM F714 & F2306 WITH ASTM D3212 SPEC FITTINGS UNLESS OTHERWISE NOTED.
9. PIPE LENGTHS SHOWN ARE FROM CENTER TO CENTER OF STRUCTURE OR TO END OF FLARED END SECTION.
10. UTILITIES ON THE PLAN ARE SHOWN TO WITHIN 5' OF THE BUILDING FOOTPRINT. THE CONTRACTOR IS ULTIMATELY RESPONSIBLE FOR THE FINAL CONNECTION TO BUILDING LINES. COORDINATE WITH ARCHITECTURAL AND MECHANICAL PLANS.
11. CATCH BASINS AND MANHOLES IN PAVED AREAS SHALL BE SUMPED 0.04 FEET. ALL CATCH BASINS IN GUTTERS SHALL BE SUMPED 0.15 FEET PER DETAILS. RIM ELEVATIONS SHOWN ON THIS PLAN DO NOT REFLECT SUMPED ELEVATIONS.
12. ALL FIRE HYDRANTS SHALL BE LOCATED 5 FEET BEHIND BACK OF CURB UNLESS OTHERWISE NOTED.
13. HYDRANT TYPE, VALVE, AND CONNECTION SHALL BE IN ACCORDANCE WITH CITY REQUIREMENTS. HYDRANT EXTENSIONS ARE INCIDENTAL.
14. A MINIMUM OF 8 FEET OF COVER IS REQUIRED OVER ALL WATERMAIN, UNLESS OTHERWISE NOTED. EXTRA DEPTH MAY BE REQUIRED TO MAINTAIN A MINIMUM OF 18" VERTICAL SEPARATION TO SANITARY OR STORM SEWER LINES. EXTRA DEPTH WATERMAIN IS INCIDENTAL.
15. A MINIMUM OF 18 INCHES OF VERTICAL SEPARATION AND 10 FEET OF HORIZONTAL SEPARATION IS REQUIRED FOR ALL UTILITIES, UNLESS OTHERWISE NOTED.
16. ALL CONNECTIONS TO EXISTING UTILITIES SHALL BE IN ACCORDANCE WITH CITY STANDARDS AND COORDINATED WITH THE CITY PRIOR TO CONSTRUCTION.
17. CONNECTIONS TO EXISTING STRUCTURES SHALL BE CORE-DRILLED.
18. COORDINATE LOCATIONS AND SIZES OF SERVICE CONNECTIONS WITH THE MECHANICAL DRAWINGS.
19. COORDINATE INSTALLATION AND SCHEDULING OF THE INSTALLATION OF UTILITIES WITH ADJACENT CONTRACTORS AND CITY STAFF.
20. ALL STREET REPAIRS AND PATCHING SHALL BE PERFORMED PER THE REQUIREMENTS OF THE CITY. ALL PAVEMENT CONNECTIONS SHALL BE SAWCUT. ALL TRAFFIC CONTROLS SHALL BE PROVIDED BY THE CONTRACTOR AND SHALL BE ESTABLISHED PER THE REQUIREMENTS OF THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MMUTCD) AND THE CITY. THIS SHALL INCLUDE BUT NOT BE LIMITED TO SIGNAGE, BARRICADES, FLASHERS, AND FLAGGERS AS NEEDED. ALL PUBLIC STREETS SHALL BE OPEN TO TRAFFIC AT ALL TIMES. NO ROAD CLOSURES SHALL BE PERMITTED WITHOUT APPROVAL BY THE CITY.
21. ALL STRUCTURES, PUBLIC AND PRIVATE, SHALL BE ADJUSTED TO PROPOSED GRADES WHERE REQUIRED. THE REQUIREMENTS OF ALL OWNERS MUST BE COMPLIED WITH. STRUCTURES BEING RESET TO PAVED AREAS MUST MEET OWNERS REQUIREMENTS FOR TRAFFIC LOADING.
22. CONTRACTOR SHALL COORDINATE ALL WORK WITH PRIVATE UTILITY COMPANIES.
23. CONTRACTOR SHALL COORDINATE CONNECTION OF IRRIGATION SERVICE TO UTILITIES. COORDINATE THE INSTALLATION OF IRRIGATION SLEEVES NECESSARY AS TO NOT IMPACT INSTALLATION OF UTILITIES.
24. CONTRACTOR SHALL MAINTAIN AS-BUILT PLANS THROUGHOUT CONSTRUCTION AND SUBMIT THESE PLANS TO ENGINEER UPON COMPLETION OF WORK.
25. ALL JOINTS AND CONNECTIONS IN STORM SEWER SYSTEM SHALL BE GASTIGHT OR WATERTIGHT. APPROVED RESILIENT RUBBER JOINTS MUST BE USED TO MAKE WATERTIGHT CONNECTIONS TO MANHOLES, CATCHBASINS, OR OTHER STRUCTURES.
26. ALL PORTIONS OF THE STORM SEWER SYSTEM LOCATED WITHIN 10 FEET OF THE BUILDING OR WATER SERVICE LINE MUST BE TESTED IN ACCORDANCE WITH MN RULES, CHAPTER 4714, SECTION 1109.0.
27. FOR ALL SITES LOCATED IN CLAY SOIL AREAS, DRAIN TILE MUST BE INSTALLED AT ALL LOW POINT CATCH BASINS 25' IN EACH DIRECTION. SEE PLAN AND DETAIL. INSTALL LOW POINT DRAIN TILE PER PLANS AND GEOTECHNICAL REPORT RECOMMENDATIONS AND REQUIREMENTS.

Know what's **below.**
Call before you dig.

CivilSite GROUP
Civil Engineering • Surveying • Landscape Architecture
5000 Glenwood Avenue
Golden Valley, MN 55422
civilsitegroup.com 612-615-0060

PRELIMINARY: NOT FOR CONSTRUCTION

PROJECT
EDINA DENTAL OFFICE
7300 METRO BOULEVARD, EDINA, MINNESOTA 55439

HEMPEL REAL ESTATE
10050 CROSSTOWN CIRCLE, SUITE 600, EDEN PRAIRIE, MN 55334

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

Robert A. Latta
DATE 3/11/24 LICENSE NO. 59612

ISSUE/SUBMITTAL SUMMARY

DATE	DESCRIPTION
03/11/2024	CITY SUBMITTAL

DRAWN BY	JR
REVIEWED BY	RL
PROJECT MANAGER	RL
PROJECT NUMBER	23355

REVISION SUMMARY

DATE	DESCRIPTION

PROJECT NOTES

C0.1

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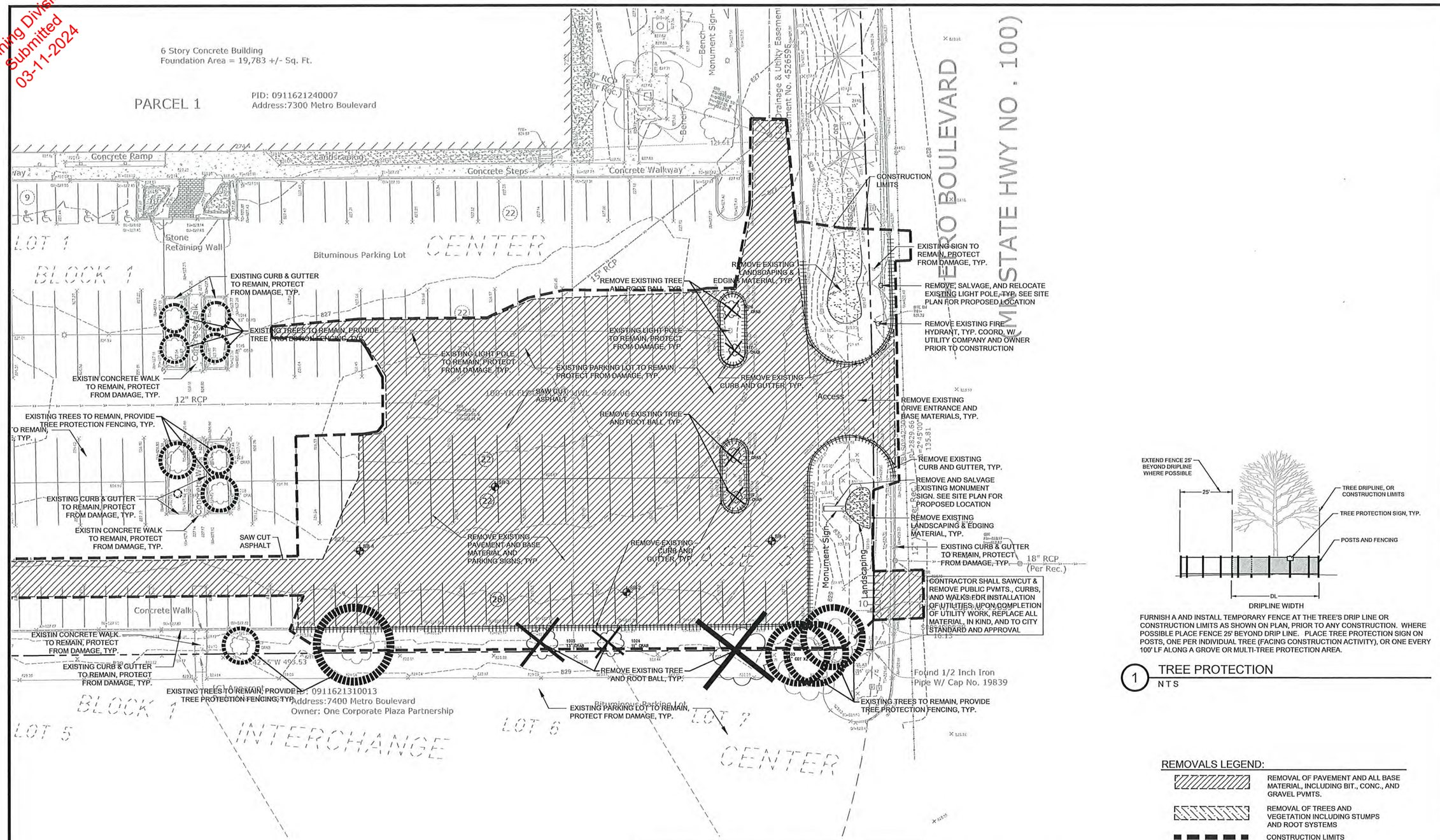
Planning Division
Submitted
03-11-2024



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1 TREE PROTECTION
NTS

CITY OF EDINA REMOVAL NOTES:

1. RESERVED FOR CITY SPECIFIC REMOVAL NOTES.

EROSION CONTROL NOTES:

SEE SWPPP ON SHEETS SW1.0 - SW1.3

OWNER INFORMATION

HEMPEL REAL ESTATE
10050 CROSSTOWN CIRCLE, SUITE 600
EDEN PRAIRIE, MN 55334
BRENT JACOBSON
612-355-2609
BJACOBSON@HEMPELCOMPANIES.COM

SEE SHEET C0.1 FOR GENERAL REMOVAL NOTES

REMOVALS LEGEND:

- REMOVAL OF PAVEMENT AND ALL BASE MATERIAL, INCLUDING BIT., CONC., AND GRAVEL PVMTS.
- REMOVAL OF TREES AND VEGETATION INCLUDING STUMPS AND ROOT SYSTEMS
- CONSTRUCTION LIMITS
- PROPERTY LINE
- REMOVE CURB AND GUTTER. IF IN RIGHT-OF-WAY, COORDINATE WITH LOCAL GOVERNING UNIT.
- TREE PROTECTION
- TREE REMOVAL - INCLUDING ROOTS AND STUMPS

Know what's below. Call before you dig.

1" = 20'-0"
10'-0" 0 20'-0"

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5000 Glenwood Avenue
Golden Valley, MN 55422
civilsitegroup.com 612-615-0060

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PROJECT
EDINA DENTAL OFFICE
7300 METRO BOULEVARD, EDINA, MINNESOTA 55439

HEMPEL REAL ESTATE
10050 CROSSTOWN CIRCLE, SUITE 600, EDEN PRAIRIE, MN 55334

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Robert A. Latta
DATE 3/11/24 LICENSE NO. 59612

ISSUE/SUBMITTAL SUMMARY

DATE	DESCRIPTION
03/11/2024	CITY SUBMITTAL

DRAWN BY	JR
REVIEWED BY	RL
PROJECT MANAGER	RL
PROJECT NUMBER	23359

REVISION SUMMARY

DATE	DESCRIPTION

REMOVALS PLAN

C1.0

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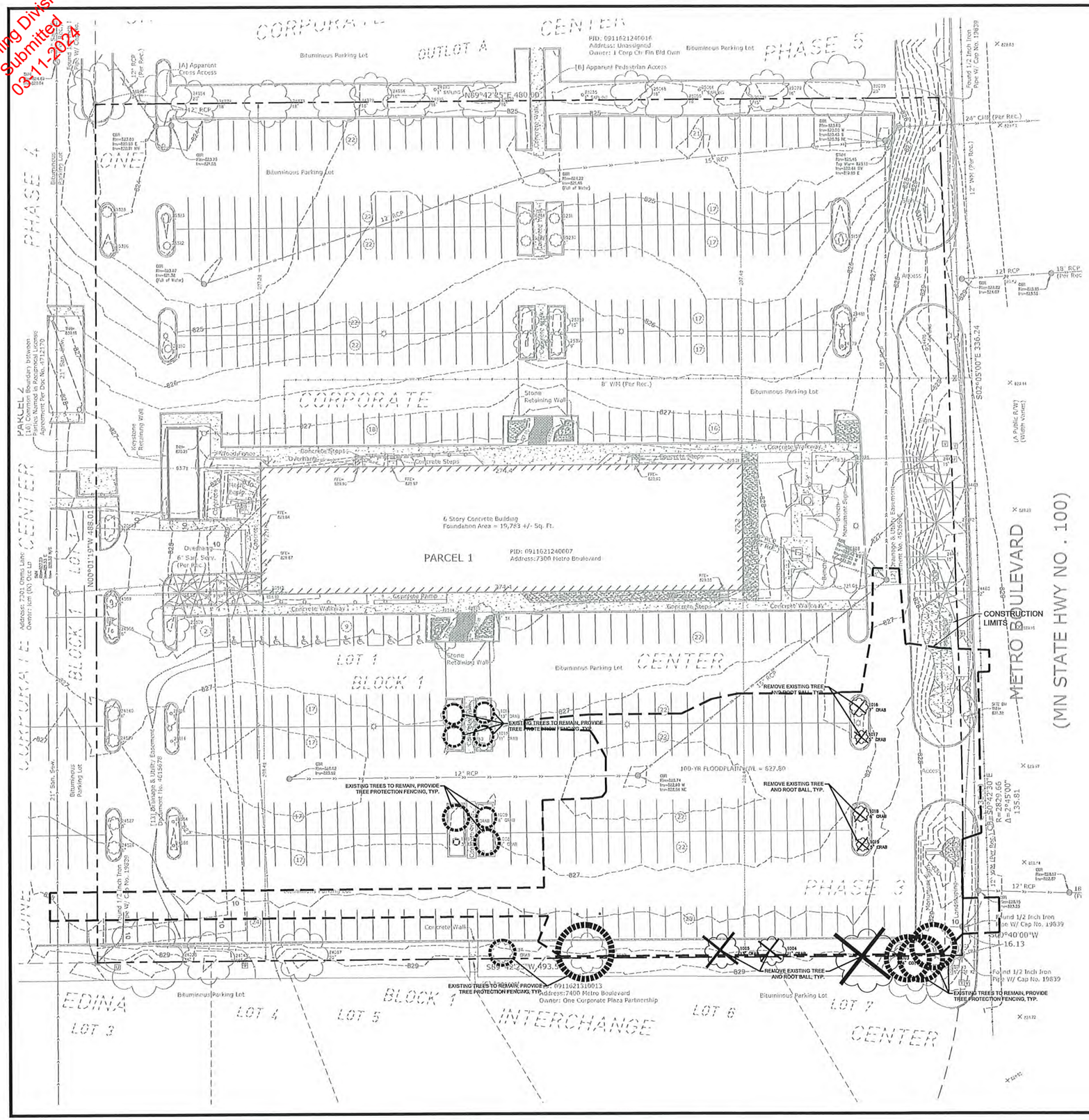
Planning Division
Submitted
03-11-2024



TREE SURVEY & TREE REPLACEMENT CALCULATIONS						
TREE#	TREE SPECIES	SIZE CAL. INCH	PROTECTED	HERITAGE	REMOVED	MITIGATION INCHES
24972	DECIDUOUS	18	Y		N	
24973	DECIDUOUS	18	Y		N	
24979	DECIDUOUS	18	Y		N	
24985	DECIDUOUS	18	Y		N	
24992	DECIDUOUS	16	Y		N	
25056	DECIDUOUS	18	Y		N	
25071	DECIDUOUS	15	Y		N	
25159	DECIDUOUS	5	Y		N	
24481	DECIDUOUS	8	Y		N	
25231	DECIDUOUS	4	N		N	
25244	DECIDUOUS	5	Y		N	
25230	DECIDUOUS	5	Y		N	
25252	DECIDUOUS	4	N		N	
25389	DECIDUOUS	10	Y		N	
25381	DECIDUOUS	10	Y		N	
25390	DECIDUOUS	9	Y		N	
25380	DECIDUOUS	10	Y		N	
25323	DECIDUOUS	4	N		N	
25325	DECIDUOUS	4	N		N	
25326	DECIDUOUS	5	Y		N	
25322	DECIDUOUS	3	N		N	
25350	DECIDUOUS	3	N		N	
20673	DECIDUOUS	10	Y		N	
24569	DECIDUOUS	5	Y		N	
24568	DECIDUOUS	4	N		N	
20509	DECIDUOUS	16	Y		N	
20510	PINE	18	Y		N	
20511	PINE	18	Y		N	
20512	PINE	22	Y		N	
20102	DECIDUOUS	10	Y		N	
20111	DECIDUOUS	10	Y		N	
20225	DECIDUOUS	26	Y		N	
12	OAK	3	N		N	
20238	HONEYLOCUST	36	Y	Y	N	
20011	DECIDUOUS	16	Y		N	
24464	PINE	15	Y		N	
24463	PINE	15	Y		N	
24462	PINE	18	Y		N	
24461	PINE	15	Y		N	
24460	PINE	22	Y		N	
24540	DECIDUOUS	6	Y		N	
24539	DECIDUOUS	5	Y		N	
24916	DECIDUOUS	1	N		N	
24914	DECIDUOUS	1	N		N	
24847	DECIDUOUS	10	Y		N	
24845	DECIDUOUS	9	Y		N	
24825	DECIDUOUS	12	Y		N	
24831	DECIDUOUS	11	Y		N	
24751	DECIDUOUS	6	Y		Y	4.5
24749	DECIDUOUS	4	N		Y	
24746	DECIDUOUS	5	Y		Y	3.75
24748	DECIDUOUS	5	Y		Y	3.75
24465	DECIDUOUS	9	Y		N	
24663	DECIDUOUS	7	Y		N	
24644	DECIDUOUS	10	Y		N	
24643	DECIDUOUS	1	N		N	
24584	DECIDUOUS	1	N		N	
24527	DECIDUOUS	5	Y		N	
24526	DECIDUOUS	5	Y		N	
24586	DECIDUOUS	1	N		N	
24023	DECIDUOUS	30	Y	Y	N	
24045	DECIDUOUS	20	Y		N	
24067	DECIDUOUS	20	Y		N	
24102	DECIDUOUS	9	Y		N	
24113	DECIDUOUS	20	Y		N	
24138	DECIDUOUS	15	Y		Y	11.25
24151	DECIDUOUS	12	Y		Y	9
24168	DECIDUOUS	30	Y	Y	Y	30
24187	DECIDUOUS	24	Y		N	
24188	DECIDUOUS	27	Y		N	
24189	DECIDUOUS	18	Y		N	

	TREE COUNT	CALIPER INCHES
TOTAL HERITAGE TREES	3	96
TOTAL TREES PRESERVED	64	751
TOTAL TREES REMOVED	7	77
TOTAL TREES SURVEYED	71	828

REMOVALS LEGEND:

- CONSTRUCTION LIMITS
- PROPERTY LINE
- TREE PROTECTION
- TREE REMOVAL - INCLUDING ROOTS AND STUMPS

Know what's below. Call before you dig.

1" = 30'-0"
15'-0" 0 30'-0"

CivilSite GROUP
Civil Engineering • Surveying • Landscape Architecture
5000 Glenwood Avenue
Golden Valley, MN 55422
civilsitegroup.com 612-615-0060

PRELIMINARY: NOT FOR CONSTRUCTION

PROJECT
EDINA DENTAL OFFICE
7300 METRO BOULEVARD, EDINA, MINNESOTA 55439

HEMPEL REAL ESTATE
10050 CROSSTOWN CIRCLE, SUITE 600, EDEN PRAIRIE, MN 55334

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED LANDSCAPE ARCHITECT UNDER THE LAWS OF THE STATE OF MINNESOTA.

Patrick J. Sarver
DATE 3/11/24 LICENSE NO. 24904

ISSUE/SUBMITTAL SUMMARY

DATE	DESCRIPTION
03/11/2024	CITY SUBMITTAL

DRAWN BY	JR
REVIEWED BY	RL
PROJECT MANAGER	RL
PROJECT NUMBER	23359

REVISION SUMMARY

DATE	DESCRIPTION

TREE PRESERVATION PLAN

C1.1

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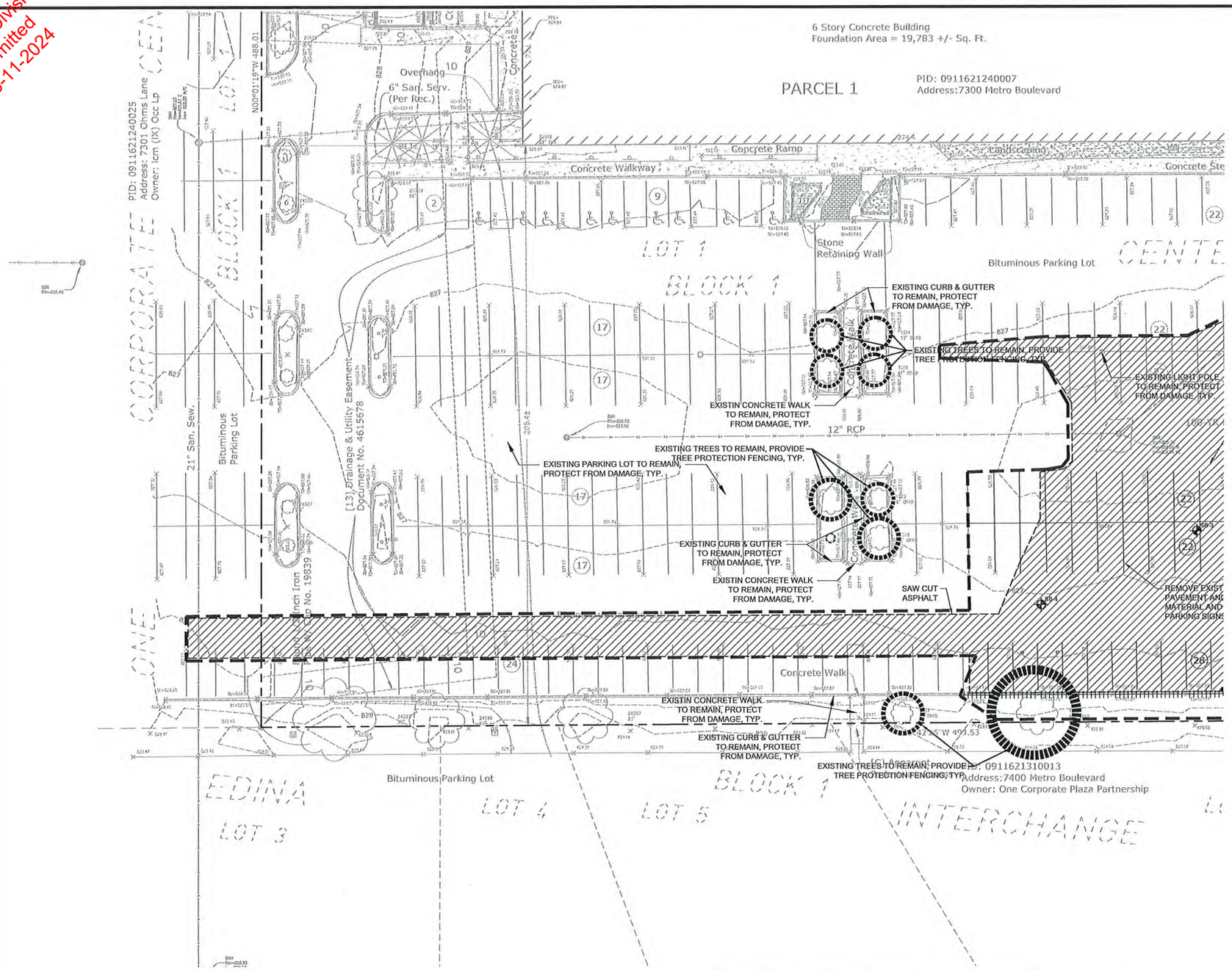
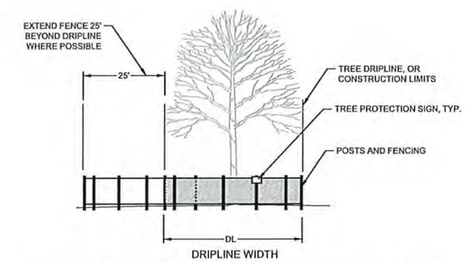
Submitted
03-11-2024





FURNISH A AND INSTALL TEMPORARY FENCE AT THE TREE'S DRIP LINE OR CONSTRUCTION LIMITS AS SHOWN ON PLAN, PRIOR TO ANY CONSTRUCTION. WHERE POSSIBLE PLACE FENCE 25' BEYOND DRIP LINE. PLACE TREE PROTECTION SIGN ON POSTS, ONE PER INDIVIDUAL TREE (FACING CONSTRUCTION ACTIVITY), OR ONE EVERY 100' LF ALONG A GROVE OR MULTI-TREE PROTECTION AREA.

1 TREE PROTECTION
NTS

REMOVALS LEGEND:

- REMOVAL OF PAVEMENT AND ALL BASE MATERIAL, INCLUDING BIT., CONC., AND GRAVEL PVMTS.
- REMOVAL OF TREES AND VEGETATION INCLUDING STUMPS AND ROOT SYSTEMS
- CONSTRUCTION LIMITS
- PROPERTY LINE
- REMOVE CURB AND GUTTER. IF IN RIGHT-OF-WAY, COORDINATE WITH LOCAL GOVERNING UNIT.
- TREE PROTECTION
- TREE REMOVAL - INCLUDING ROOTS AND STUMPS

CITY OF EDINA REMOVAL NOTES:

1. RESERVED FOR CITY SPECIFIC REMOVAL NOTES.

EROSION CONTROL NOTES:

SEE SWPPP ON SHEETS SW1.0 - SW1.3

OWNER INFORMATION

HEMPEL REAL ESTATE
10050 CROSSTOWN CIRCLE, SUITE 600
EDEN PRAIRIE, MN 55334
BRENT JACOBSON
612-355-2609
BJACOBSON@HEMPELCOMPANIES.COM

SEE SHEET C0.1 FOR GENERAL REMOVAL NOTES

Know what's below. Call before you dig.

1" = 20'-0"
10'-0" 0 20'-0"

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Golden Valley, MN 55422
civilsitegroup.com 612-615-0060

PRELIMINARY: NOT FOR CONSTRUCTION

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EDINA DENTAL OFFICE
7300 METRO BOULEVARD, EDINA, MINNESOTA 55439

HEMPEL REAL ESTATE
10050 CROSSTOWN CIRCLE, SUITE 600, EDEN PRAIRIE, MN 55334

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Robert A. Latta
DATE 3/11/24 LICENSE NO. 59612

ISSUE/SUBMITTAL SUMMARY

DATE	DESCRIPTION
03/11/2024	CITY SUBMITTAL

DRAWN BY	JR
REVIEWED BY	RL
PROJECT MANAGER	RL
PROJECT NUMBER	22355

REVISION SUMMARY

DATE	DESCRIPTION

REMOVALS PLAN - WEST

C1.2

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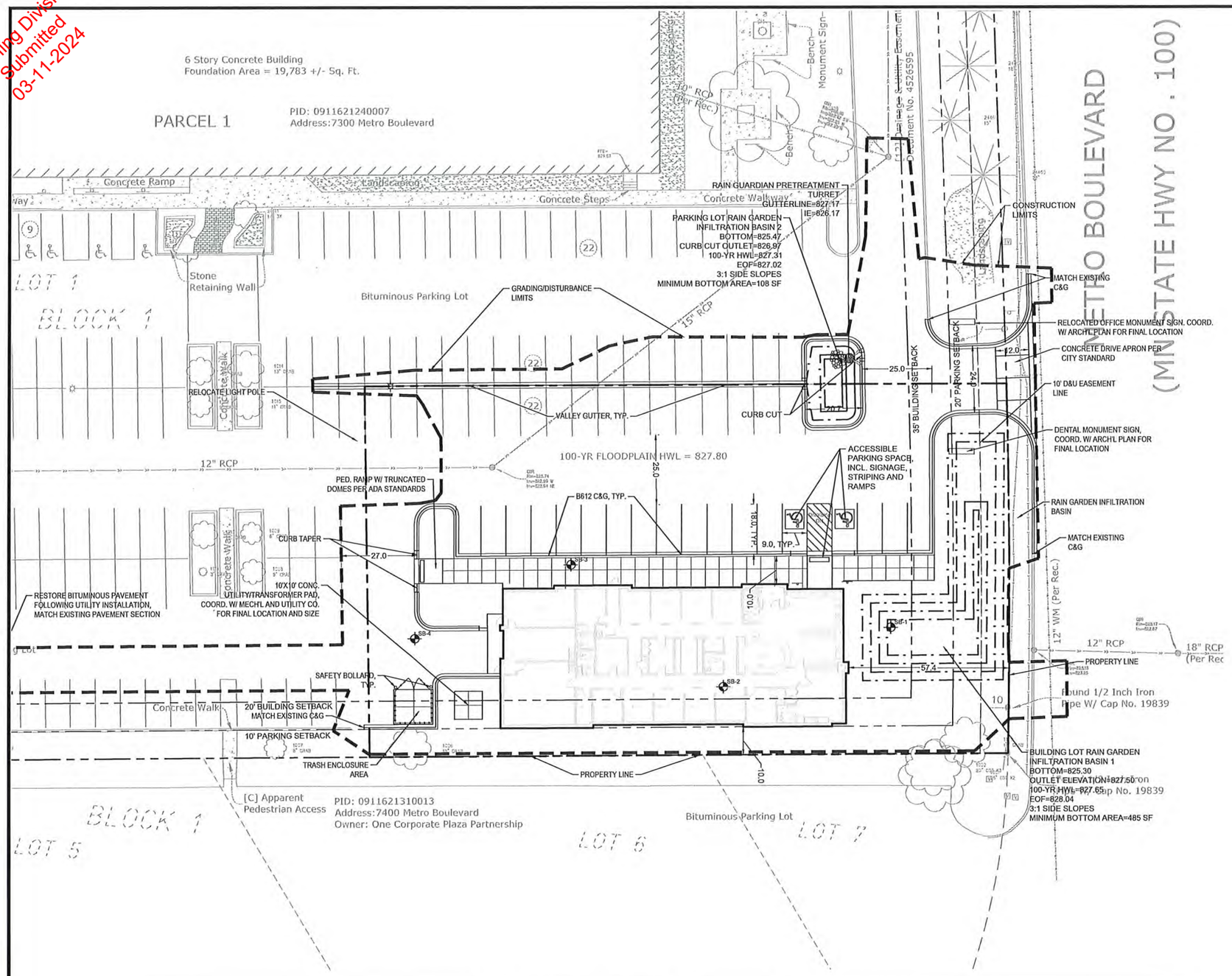
Submitted
03-11-2024



CITY OF EDINA SITE SPECIFIC NOTES:

1. RESERVED FOR CITY SPECIFIC SITE NOTES.

OWNER INFORMATION

HEMPEL REAL ESTATE
10050 CROSSTOWN CIRCLE, SUITE 600
EDEN PRAIRIE, MN 55334
BRENT JACOBSON
612-355-2609
BJACOBSON@HEMPELCOMPANIES.COM

OPERATIONAL NOTES

SNOW REMOVAL	ALL SNOW SHALL BE STORED ON-SITE OUTSIDE PARKING LOT. WHEN FULL, REMOVAL CO. SHALL REMOVE EXCESS OFF-SITE.
TRASH REMOVAL	TRASH SHALL BE PLACED IN EXTERIOR TRASH AREA AND REMOVED BY COMMERCIAL CO. WEEKLY.
DELIVERIES	DELIVERIES SHALL OCCUR AT THE FRONT DOOR VIA STANDARD COMMERCIAL DELIVERY VEHICLES (UPS, FED-EX, USPS).

SEE SHEET C0.1 FOR GENERAL SITE NOTES

SITE AREA CALCULATIONS

	EXISTING CONDITION			PROPOSED CONDITION		
IMPERVIOUS SURFACES						
BUILDING COVERAGE	19,792 SF	8.3%		25,919 SF	10.9%	
PAVEMENT	182,650 SF	76.6%		173,577 SF	72.8%	
TOTAL	202,442 SF	84.9%	4.6 AC	199,496 SF	83.7%	4.6 AC
PERVIOUS SURFACES						
TOTAL	35,878 SF	15.1%	0.8 AC	38,824 SF	16.3%	0.9 AC
TOTAL SITE AREA	238,320 SF	100.0%	5.5 AC	238,320 SF	100.0%	5.5 AC
DIFFERENCE (EX. VS PROP.)	-2,946 SF	-1.2%				
FULLY RECONSTRUCTED IMPERVI	28,845 SF					
DISTURBED AREA	40,403 SF	0.9 AC				

SITE DATA

ZONING SUMMARY		
EXISTING ZONING	POD-1 - PLANNED OFFICE	
PROPOSED ZONING		
	REQUIRED	PROVIDED
BUILDING SETBACKS		
FRONT STREET	35'	
REAR YARD	20'	
SIDE STREET	35'	
INTERIOR SIDE YARD	20'	
PARKING SETBACKS		
FRONT - (FROM ROW)	20'	
REAR	10'	
SIDE	10'	
	REQUIRED	PROVIDED
PARKING SUMMARY		
PARKING STALL DIMENSIONS	8.5' X 18'	
COMPACT PARKING STALL DIMENSIONS	7.5' X 16'	
DRIVE AISLE DIMENSIONS	24'	
FIRE ACCESS ROAD DIMENSIONS		
	REQUIRED	PROVIDED
PARKING STALLS - SURFACE	23	
PARKING STALLS - GARAGE	N/A	

SITE PLAN LEGEND:

- LIGHT DUTY BITUMINOUS PAVEMENT (IF APPLICABLE). SEE GEOTECHNICAL REPORT FOR AGGREGATE BASE & WEAR COURSE DEPTH, SEE DETAIL.
- HEAVY DUTY BITUMINOUS PAVEMENT (IF APPLICABLE). SEE GEOTECHNICAL REPORT FOR AGGREGATE BASE & WEAR COURSE DEPTH, SEE DETAIL.
- CONCRETE PAVEMENT (IF APPLICABLE) AS SPECIFIED (PAD OR WALK) SEE GEOTECHNICAL REPORT FOR AGGREGATE BASE & CONCRETE DEPTHS, WITHIN ROW SEE CITY DETAIL, WITHIN PRIVATE PROPERTY SEE CSG DETAIL
- PROPERTY LINE
- CONSTRUCTION LIMITS
- CURB AND GUTTER-SEE NOTES (T.O.) TIP OUT GUTTER WHERE APPLICABLE-SEE PLAN
- TRAFFIC DIRECTIONAL ARROW PAVEMENT MARKINGS
- SIGN AND POST ASSEMBLY. SHOP DRAWINGS REQUIRED.
  HC = ACCESSIBLE SIGN
  NP = NO PARKING FIRE LANE
  ST = STOP
  CP = COMPACT CAR PARKING ONLY
- ACCESSIBILITY ROUTE ARROW (IF APPLICABLE) DO NOT PAINT.

Know what's below. Call before you dig.

1" = 20'-0"
10'-0" 0 20'-0"

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Civil Engineering • Surveying • Landscape Architecture
5000 Glenwood Avenue
Golden Valley, MN 55422
civilsitegroup.com 612-615-0060

PRELIMINARY: NOT FOR CONSTRUCTION

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EDINA DENTAL OFFICE
7300 METRO BOULEVARD, EDINA, MINNESOTA 55439

HEMPEL REAL ESTATE
10050 CROSSTOWN CIRCLE, SUITE 600, EDEN PRAIRIE, MN 55334

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Robert A. Latta
DATE 3/11/24 LICENSE NO. 59612

ISSUE/SUBMITTAL SUMMARY

DATE	DESCRIPTION
03/11/2024	CITY SUBMITTAL

DRAWN BY	JR
REVIEWED BY	RL
PROJECT MANAGER	RL
PROJECT NUMBER	23.155

REVISION SUMMARY

DATE	DESCRIPTION

SITE PLAN

C2.0

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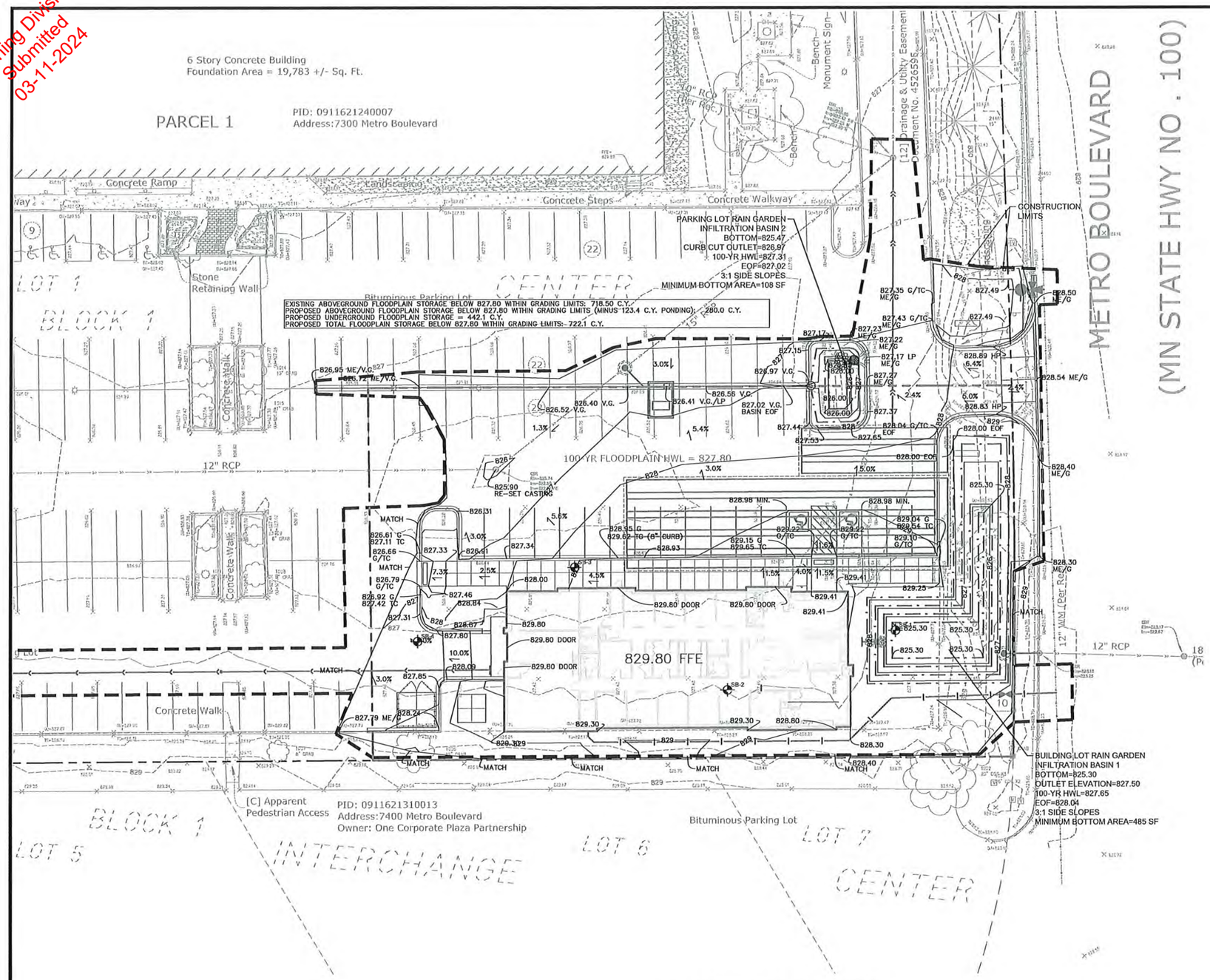
Submitted
03-11-2024



CITY OF EDINA GRADING NOTES:

1. RESERVED FOR CITY SPECIFIC GRADING NOTES.

EROSION CONTROL NOTES:

SEE SWPPP ON SHEETS SW1.0 - SW1.3

SEE SHEET C0.1 FOR GENERAL GRADING NOTES

GROUNDWATER ELEVATION PER BORING

BORING	GROUND WATER ELEVATION
B-1	
B-2	
B-3	
B-4	
B-5	
B-6	
B-7	
B-8	
B-9	
B-10	

PER GEOTECHNICAL REPORT COMPLETED BY XXX XXX, INC. ON XX/XX/XX.

GRADING PLAN LEGEND:

1125	EX. 1' CONTOUR ELEVATION INTERVAL
1137	1.0' CONTOUR ELEVATION INTERVAL
41.26	SPOT GRADE ELEVATION (GUTTER/FLOW LINE UNLESS OTHERWISE NOTED)
891.00 G	SPOT GRADE ELEVATION GUTTER
891.00 TC	SPOT GRADE ELEVATION TOP OF CURB
891.00 BS/TS	SPOT GRADE ELEVATION BOTTOM OF STAIRS/TOP OF STAIRS
891.00 ME	SPOT GRADE ELEVATION MATCH EXISTING
	GRADE BREAK - HIGH POINTS
GB	CURB AND GUTTER (T.O = TIP OUT)
EOF=1135.52	EMERGENCY OVERFLOW
	CONSTRUCTION LIMITS

Know what's below. Call before you dig.

1" = 20'-0"
10'-0" 0 20'-0"

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Golden Valley, MN 55422
civilsitegroup.com 612-615-0060

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EDINA DENTAL OFFICE
7300 METRO BOULEVARD, EDINA, MINNESOTA 55439

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10050 CROSSTOWN CIRCLE, SUITE 600, EDEN PRAIRIE, MN 55334

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Robert A. Lalla
DATE 3/11/24 LICENSE NO. 59612

ISSUE/SUBMITTAL SUMMARY

DATE	DESCRIPTION
03/11/2024	CITY SUBMITTAL

DRAWN BY	JR
REVIEWED BY	RL
PROJECT MANAGER	RL
PROJECT NUMBER	23365

REVISION SUMMARY

DATE	DESCRIPTION

GRADING PLAN

C3.0

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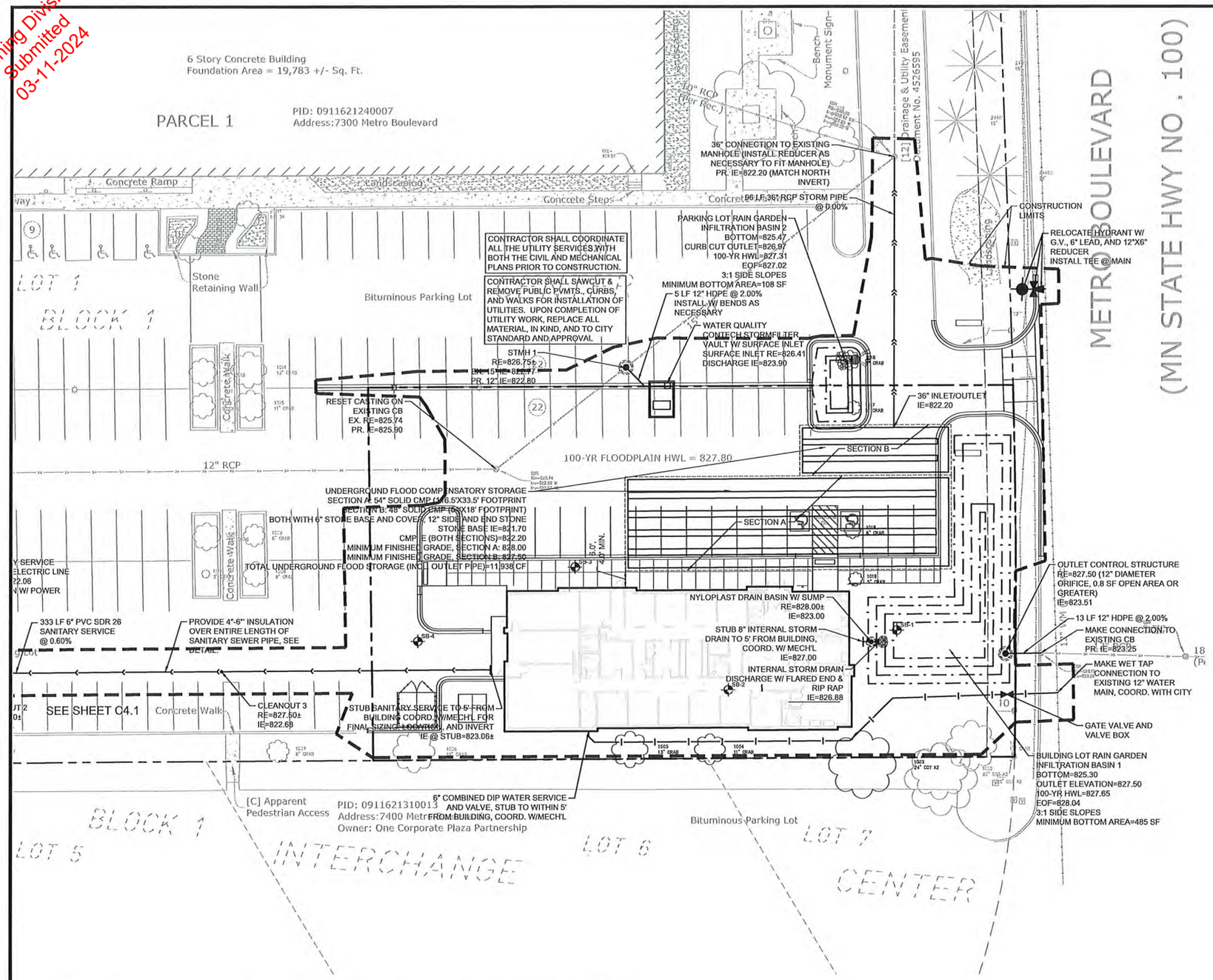

CITY OF EDINA UTILITY NOTES:

1. RESERVED FOR CITY SPECIFIC UTILITY NOTES.

SEE SHEET C0.1 FOR GENERAL UTILITY NOTES

UTILITY LEGEND:

- CATCH BASIN
- MANHOLE
- GATE VALVE AND VALVE BOX
- PROPOSED FIRE HYDRANT
- DT — DT — DRAINTILE
- WATER MAIN
- SANITARY SEWER
- STORM SEWER
- CONSTRUCTION LIMITS

Know what's below. Call before you dig.

1" = 20'-0"
10'-0" 0 20'-0"

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Civil Engineering • Surveying • Landscape Architecture
5000 Glenwood Avenue
Golden Valley, MN 55422
civilsitegroup.com 612-615-0060

PRELIMINARY: NOT FOR CONSTRUCTION

PROJECT
EDINA DENTAL OFFICE
7300 METRO BOULEVARD, EDINA, MINNESOTA 55439

HEMPEL REAL ESTATE
10050 CROSSTOWN CIRCLE, SUITE 600, EDEN PRAIRIE, MN 55334

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Robert A. Latta
DATE 3/11/24 LICENSE NO. 59612

ISSUE/SUBMITTAL SUMMARY

DATE	DESCRIPTION
03/11/2024	CITY SUBMITTAL

DRAWN BY	JR
REVIEWED BY	KL
PROJECT MANAGER	KL
PROJECT NUMBER	23263

REVISION SUMMARY

DATE	DESCRIPTION

UTILITY PLAN

C4.0

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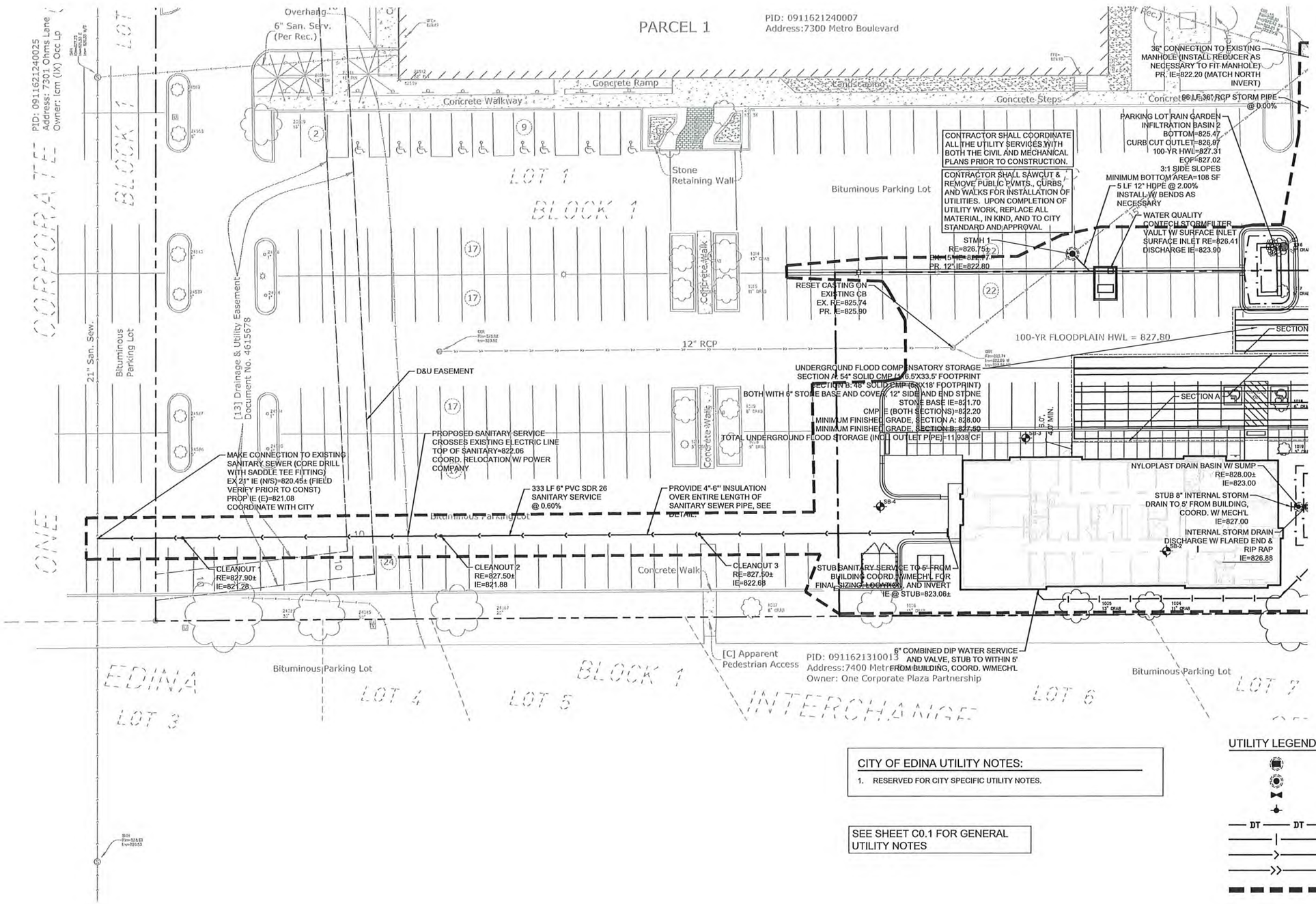
03-11-2024



CITY OF EDINA UTILITY NOTES:

1. RESERVED FOR CITY SPECIFIC UTILITY NOTES.

SEE SHEET C0.1 FOR GENERAL UTILITY NOTES

UTILITY LEGEND:

- CATCH BASIN
- MANHOLE
- GATE VALVE AND VALVE BOX
- PROPOSED FIRE HYDRANT
- DT — DT — DRAINTILE
- WATER MAIN
- SANITARY SEWER
- STORM SEWER
- CONSTRUCTION LIMITS

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Golden Valley, MN 55422
civilsitegroup.com 612-615-0060

PRELIMINARY: NOT FOR CONSTRUCTION

PROJECT
EDINA DENTAL OFFICE
7300 METRO BOULEVARD, EDINA, MINNESOTA 55439

HEMPEL REAL ESTATE
10050 CROSSTOWN CIRCLE, SUITE 600, EDEN PRAIRIE, MN 55334

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Robert A. Latta

DATE 3/11/24 LICENSE NO. 59612

ISSUE/SUBMITTAL SUMMARY

DATE	DESCRIPTION
03/11/2024	CITY SUBMITTAL

DRAWN BY	JR
REVIEWED BY	RL
PROJECT MANAGER	RL
PROJECT NUMBER	22365

REVISION SUMMARY

DATE	DESCRIPTION

UTILITY PLAN - SANITARY CONNECTION

C4.1

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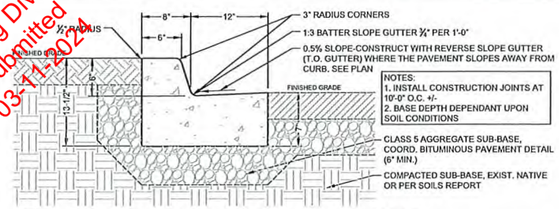
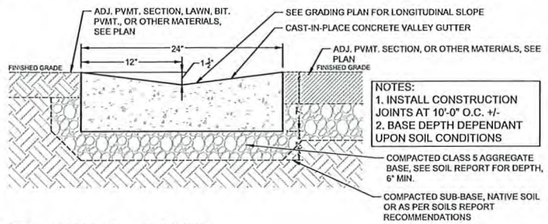
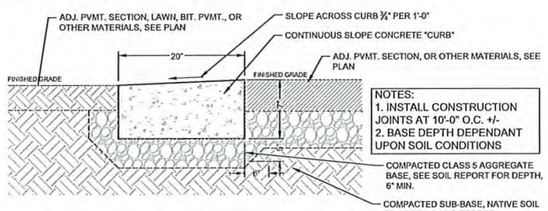


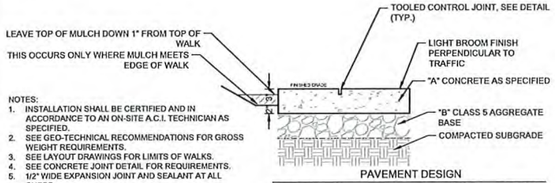
1 B-612 CONCRETE CURB AND GUTTER
NTS



2 VALLEY GUTTER
NTS



3 RIBBON CURB
NTS



NOTES:
1. INSTALLATION SHALL BE CERTIFIED AND IN ACCORDANCE TO AN ON-SITE A.C.I. TECHNICIAN AS SPECIFIED.
2. SEE GEO-TECHNICAL RECOMMENDATIONS FOR GROSS WEIGHT REQUIREMENTS.
3. SEE LAYOUT DRAWINGS FOR LIMITS OF WALKS.
4. SEE CONCRETE JOINT DETAIL FOR REQUIREMENTS.
5. 1/2" WIDE EXPANSION JOINT AND SEALANT AT ALL CURBS.
6. IF NO DESIGN IS DEFINED IN CHART, SEE GEOTECH REPORT FOR FINAL PAVEMENT SECTION.
7. IF DESIGN IS DEFINED IN CHART, IT SHOULD BE CONSIDERED FOR BIDDING PURPOSES ONLY. REFER TO GEOTECH FOR FINAL PAVEMENT SECTION.

PAVEMENT DESIGN

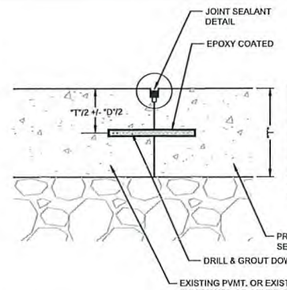
TYPE	CONC. (A)	AGG.(B)
WALKS	5"	6"
CONC. PVMT.	X"	X"

1. CURING:
   - 1.1. APPLY CURING COMPOUNDS IMMEDIATELY UPON FINISHING OF CONC. SURFACE.
   - 1.2. ALL CONC. SURFACES SHALL HAVE CURING COMPOUNDS ALLIED PER ASTM C-309. AT 200 S.F. PER GAL.
   - 1.3. ALL CURING COMPOUNDS SHALL BE TYPE 1 (CLEAR) AND CONTAIN ACRYLIC BASED CLASS B RESTRICTED RESIN SOLIDS.

4 CONCRETE PVMT./WALK/PAD
NTS (PRIVATE PROPERTY)

GENERAL DETAILS NOTES:

CONTRACTOR SHALL REVIEW ALL CIVIL AND ARCHITECTURAL DETAILS PRIOR TO WORK. ANY REDUNDANT, CONTRADICTORY OR CONFLICTING INFORMATION PRESENTED IN ANY PLAN OR DETAIL THROUGHOUT THE CONTRACT DOCUMENTS SHALL BE BROUGHT TO THE ATTENTION OF THE PROJECT ARCHITECT AND CIVIL ENGINEER AND RESOLVED PRIOR TO WORK COMMENCING.



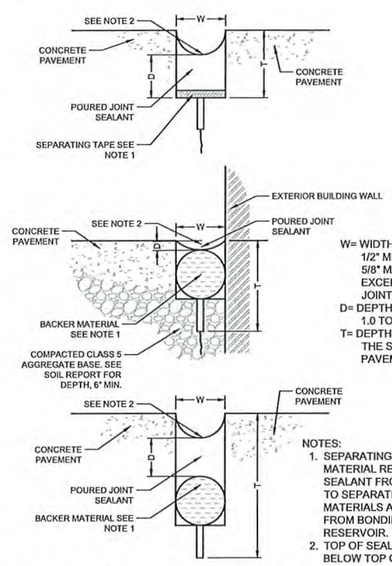
NOTES:
1. EITHER ONE PIECE OR THREADED SPLIT-TYPE DOWEL MAY BE USED.
2. USED THICKENED EDGE ON NEW PAVEMENT WHEN EXISTING PAVEMENT HAS THICKENED EDGE.

DOWEL SCHEDULE

DOWEL SIZE	DIA.	LENGTH	SPACING
6" OR LESS	X"	15"	12" O.C.
9" - 11"	1"	16"	12" O.C.
12"-15"	1-1/4"	20"	15" O.C.
16" OR MORE	1-1/2"	20"	15" O.C.
18" OR MORE	1-1/2"	22"	21" O.C.

"D" - DENOTES DOWEL DIAMETER
"T" - DENOTES PAVEMENT THICKNESS

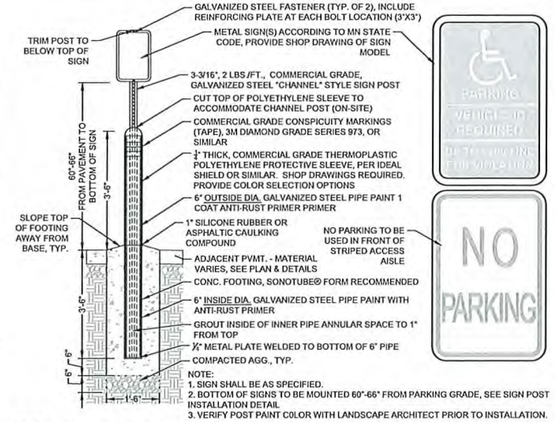
5 DOWELED JOINT BETWEEN TWO PAVEMENTS
NTS



W= WIDTH OF SEALANT RESERVOIR
1/2" MINIMUM
5/8" MAXIMUM
EXCEPT FOR 3/4" EXPANSION JOINTS
D= DEPTH OF SEALANT
1.0 TO 1.5 TIMES W
T= DEPTH OF INTIAL SAWCUT, 1/4 OF THE SLAB THINKNESS FOR PAVEMENT LESS THAN 12"

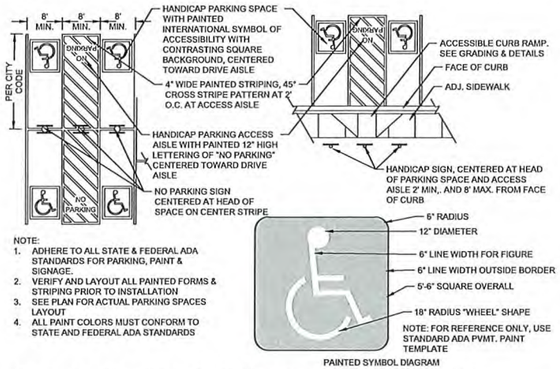
NOTES:
1. SEPARATING TAPE OR BACKER MATERIAL REQUIRED TO PREVENT JOINT SEALANT FROM FLOWING INTO SAWCUT, TO SEPARATE NONCOMPATIBLE MATERIALS AND TO PREVENT SEALANT FROM BONDING TO BOTTOM OF RESERVOIR.
2. TOP OF SEALANT WILL BE 1/8" TO 1/4" BELOW TOP OF PAVEMENT.

6 EXPANSION JOINT SEALANT DETAILS
NTS



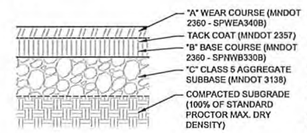
NOTE:
1. SIGN SHALL BE AS SPECIFIED.
2. BOTTOM OF SIGNS TO BE MOUNTED 60"-66" FROM PARKING GRADE, SEE SIGN POST INSTALLATION DETAIL
3. VERIFY POST PAINT COLOR WITH LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.

7 ACCESSIBLE SIGN & CHANNEL POST - LOT LOCATION
NTS



NOTE:
1. ADHERE TO ALL STATE & FEDERAL ADA STANDARDS FOR PARKING, PAINT & SIGNAGE.
2. VERIFY AND LAYOUT ALL PAINTED FORMS & STRIPING PRIOR TO INSTALLATION
3. SEE PLAN FOR ACTUAL PARKING SPACES LAYOUT
4. ALL PAINT COLORS MUST CONFORM TO STATE AND FEDERAL ADA STANDARDS

8 ACCESSIBLE PARKING PAVEMENT MARKING
NTS

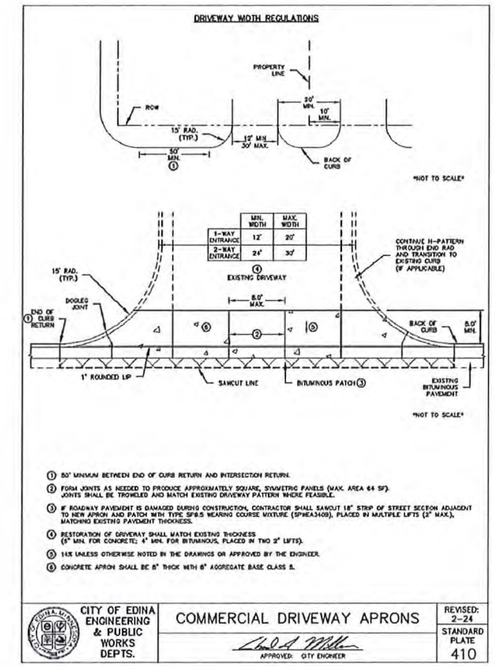


PAVEMENT DESIGN

TYPE	WEAR (A)	BASE (B)	AGG.(C)
LIGHT DUTY	X"	X"	X"
HEAVY DUTY	X"	X"	X"

NOTE:
IF DESIGN IS DEFINED IN ABOVE CHART, IT SHOULD BE CONSIDERED FOR BIDDING PURPOSES ONLY. REFER TO GEOTECHNICAL ENGINEER FOR RECOMMENDED PAVEMENT SECTION.

9 BITUMINOUS PAVEMENT - ALL TYPES
NTS



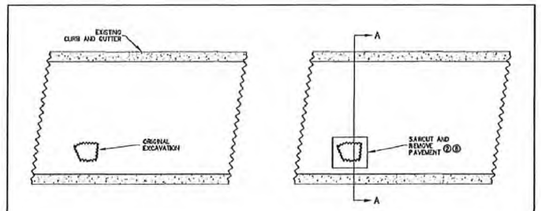
1. 50' MINIMUM BETWEEN END OF CURB RETURN AND INTERSECTION RETURN.
2. FORM JOINTS AS NEEDED TO PRODUCE APPROXIMATELY SQUARE, SYMMETRIC PANELS (MAX. AREA 64 SF). JOINTS SHALL BE TROWELED AND MATCH EXISTING DRIVEWAY PATTERN WHERE FEASIBLE.
3. IF ROADWAY PAVEMENT IS DAMAGED DURING CONSTRUCTION, CONTRACTOR SHALL SAWCUT 18" STRIP OF STREET SECTION ADJACENT TO NEW APRON AND PATCH WITH TYPE SP9.5 WEARING COURSE MIXTURE (SPWEA340B), PLACED IN MULTIPLE LIFTS (2" MAX.), MATCHING EXISTING PAVEMENT THICKNESS.
4. RESTORATION OF DRIVEWAY SHALL MATCH EXISTING THICKNESS (8" MIN. FOR CONCRETE; 4" MIN. FOR BITUMINOUS, PLACED IN TWO 2" LIFTS).
5. 1:6% UNLESS OTHERWISE NOTED IN THE DRAWINGS OR APPROVED BY THE ENGINEER.
6. CONCRETE APRON SHALL BE 6" THICK WITH 6" AGGREGATE BASE CLASS 5.

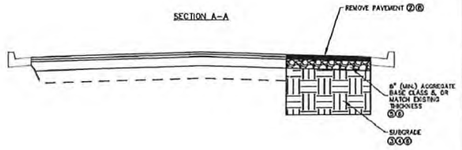
CITY OF EDINA ENGINEERING & PUBLIC WORKS DEPTS.

COMMERCIAL DRIVEWAY APRONS

APPROVED: CITY ENGINEER

REVISED: 2-24
STANDARD PLATE 410





NOTES:
1. PERMANENT OR TEMPORARY REPAIR SHALL BE COMPLETED WITHIN 48 HOURS OF ORIGINAL EXCAVATION.
2. SAWCUT AND REMOVE EXISTING PAVEMENT PER EDINA STANDARD PLATES 541-545, AS APPLICABLE. FOR TEMPORARY OR COLD WEATHER PATCHES, SAWCUT 1' (MIN.) BEYOND ORIGINAL EXCAVATION TO PRODUCE RECTILINEAR PATCH.
3. SUBGRADE SHALL BE BACKFILLED WITH SUITABLE MATERIAL AND COMPACTED IN MULTIPLE LIFTS NOT TO EXCEED 12". UNSUITABLE MATERIAL INCLUDES, BUT IS NOT LIMITED TO, FROZEN MATERIAL, PEAT, RUBBLE, HEAVY CLAY, AND BOULDERS.
4. EXCAVATED MATERIAL MAY BE USED FOR BACKFILL IF SUITABLE. ALL UNSUITABLE MATERIAL SHALL BE REMOVED FROM THE SITE.
5. EXCAVATION SHALL BE FILLED WITH 8" (MIN.) AGGREGATE BASE CLASS 5 FOR ROADWAY SUBBASE, OR MATCH EXISTING THICKNESS.
6. THE METHOD OF COMPACTION FOR SUBGRADE AND AGGREGATE BASE SHALL BE THE QUALITY COMPACTION METHOD PER MNDOT SPEC. 2015.3F.2. FLOODING OR FLUSHING THE EXCAVATION WITH WATER SHALL NOT BE PERMITTED.
7. INSPECTION AND ACCEPTANCE OF REPAIR BY CITY ENGINEER REQUIRED.
8. FOLLOWING COMPLETION OF REPAIR, BITUMINOUS ROADWAYS SHALL BE PATCHED PER EDINA STANDARD PLATE 541-544, AS APPLICABLE. CONCRETE ROADWAYS SHALL BE PATCHED PER EDINA STANDARD PLATE 545.

CITY OF EDINA ENGINEERING & PUBLIC WORKS DEPTS.

ROADWAY REMOVAL AND SUBGRADE REPAIR

APPROVED: CITY ENGINEER

REVISED: 01-17
STANDARD PLATE 540

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PRELIMINARY: NOT FOR CONSTRUCTION

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HEMPEL REAL ESTATE
10050 CROSSTOWN CIRCLE, SUITE 600, EDEN PRAIRIE, MN 55334

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

Robert A. Latta
DATE 3/11/24 LICENSE NO. 59612

ISSUE/SUBMITTAL SUMMARY

DATE	DESCRIPTION
03/11/2024	CITY SUBMITTAL

DRAWN BY	JR
REVIEWED BY	RL
PROJECT MANAGER	RL
PROJECT NUMBER	23365

REVISION SUMMARY

DATE	DESCRIPTION

CIVIL DETAILS

C5.0

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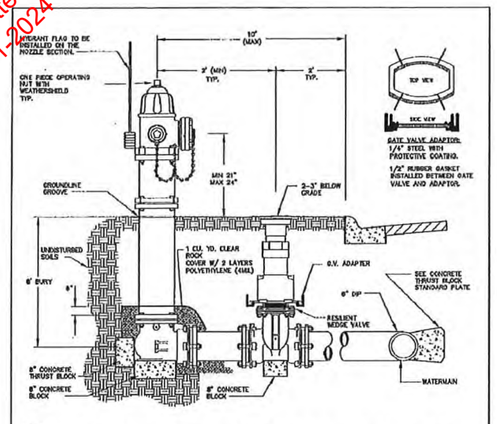
Planning Division
Submitted
03-11-2024



NOTES:
HYDRANTS SHALL BE WATEROUS CLASSIC PACER MODEL W857-250 WITH ONE (1)-FIVE AND ONE-QUARTER INCH (5 1/4") STORZ PUMPER NOZZLE WITH 16" BREAK OFF AND TWO (2)-TWO AND ONE-HALF INCH (2 1/2") THREADED NOZZLES.

ALL HYDRANTS AND GATE VALVES SHALL HAVE RESTRAINT COUPLING, RESTRAINT JOINT OR MECHANICAL JOINT CONNECTIONS. ALL VALVES TO OPEN COUNTER-CLOCKWISE.

HYDRANTS TO CONFORM WITH DIMENSIONAL REQUIREMENTS FOR NONTHREADED CONNECTIONS IN ACCORDANCE WITH NFPA 1963 STANDARDS FOR FIRE HOSE CONNECTIONS.

ALL BACKFILL SHALL BE PROPERLY COMPACTED.

2 CANDYCANE STYLE FLAGS PER HYDRANT (ONE INSTALLED, ONE TO EDINA PUBLIC WORKS).

A ONE-PIECE HEAVY DUTY OPERATING VALVE ROD IS REQUIRED ON HYDRANTS THAT ARE EXTENDED 24" OR MORE.

WHEN INSTALLING A HYDRANT ROD EXTENSION, THE NONBREAKABLE COUPLING SLEEVES GO ON THE BOTTOM AND THE BREAKABLE SLEEVES GO ON THE TOP OF THE EXTENSION ROD.

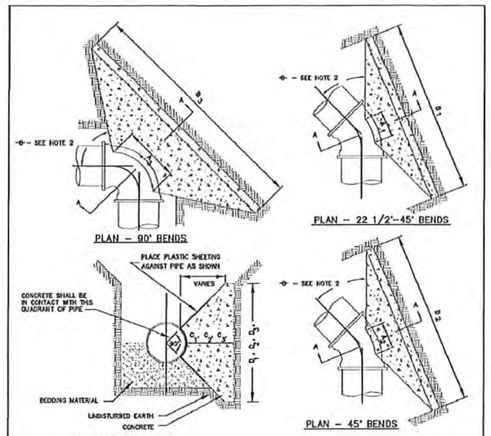
FIRE HYDRANTS SHALL BE PAINTED RED AT THE FACTORY.

POLYWRAP ALL PIPE AND FITTINGS, MEGA LUG ALL FITTINGS, INCLUDE CORROSION PROTECTION AT EACH JOINT.

ALL HYDRANTS SHALL BE INSTALLED WITH STORZ NOZZLE PERPENDICULAR TO ROADWAY.

WEEP HOLE TO REMAIN OPEN.

CITY OF EDINA ENGINEERING & PUBLIC WORKS DEPTS.	HYDRANT AND GATE VALVE APPROVED: CITY ENGINEER	REVISED: 1-19 STANDARD PLATE 100

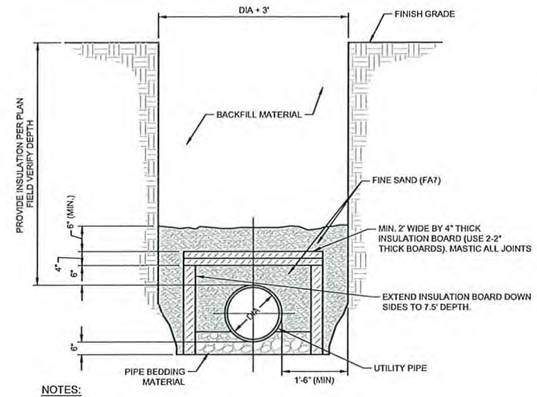


BUTTRESS DIMENSIONS

PIPE SIZE	22 1/2° BEND		45° BEND		TEES/90° BEND	
	B_1	D_1	B_2	D_2	B_3	D_3
6"	1'-5"	1'-5"	1'-5"	1'-5"	2'-1"	1'-8"
8"	1'-5"	1'-5"	2'-1"	1'-8"	2'-8"	2'-0"
12"	1'-10"	1'-10"	3'-4"	2'-0"	4'-9"	2'-8"
16"	3'-0"	2'-0"	3'-10"	3'-0"	6'-2"	3'-4"
20"	3'-8"	2'-8"	5'-6"	3'-4"	8'-4"	4'-0"
24"	4'-4"	3'-0"	6'-10"	3'-10"	9'-8"	5'-0"

NOTES:
1. DIMENSIONS IN TABLE ARE BASED ON A WATER PRESSURE OF 150 PSI AND AN EARTH RESISTANCE OF 1 TON PER SQ. FT.
2. DIMENSION C1,C2,C3 SHOULD BE AS LARGE ENOUGH TO MAKE ANGLE > 45 DEGREES.
3. DIMENSION A1,A2,A3 SHOULD BE AS LARGE AS POSSIBLE WITHOUT INTERFERING WITH M.J. BOLTS.
4. SHAPE OF BACK OF BUTTRESS MAY VARY AS LONG AS POUR IS AGAINST FIRM UNDISTURBED EARTH.
5. MEGA LUGS MAY BE USED IN CONJUNCTION WITH MIXED CONCRETE THRUST BLOCKING.
6. READY MIX CONCRETE ONLY MAY BE USED (MNDOT SPEC. 3461, GRADE 'B')

CITY OF EDINA ENGINEERING & PUBLIC WORKS DEPTS.	CONCRETE THRUST BLOCK APPROVED: CITY ENGINEER	REVISED: 04-09 STANDARD PLATE 120



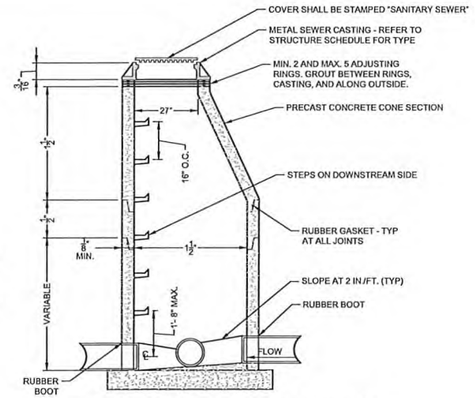
NOTES:

1. INSULATION BOARD TO BE CLOSED CELL. EXTRUDED POLYSTYRENE FOAM MEETING ASTM 578, TYPE VI, 40PSI COMPRESSING STRENGTH (ASTM D1621) 0.1%MAX. WATER ABSORPTION (ASTM C272).

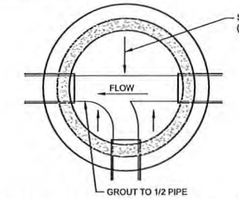
2. BACKFILL MATERIAL AROUND INSULATION MUST BE FINE SAND FREE FROM ROOT, ORGANIC MATERIAL, OR OTHER INJURIOUS MATERIALS.

3. OVERLAP ALL INSULATION BOARD JOINTS.

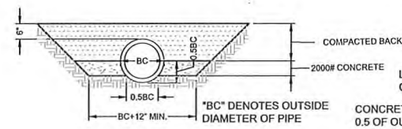
2 UTILITY PIPE INSULATION DETAIL
NTS



BASES SHALL BE 8" STANDARD PRECAST WITH 2" LEAN GROUT, OR POURED 8" SLAB REINFORCED WITH 6" x 6" 10/10 MESH



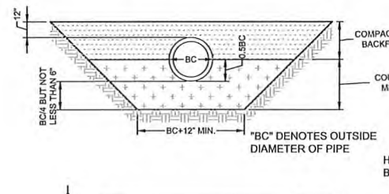
4 SANITARY SEWER MANHOLE
NTS



"BC" DENOTES OUTSIDE DIAMETER OF PIPE

LOAD FACTOR 2.3
CLASS A

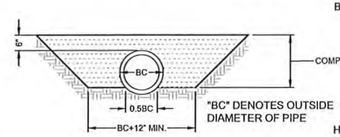
CONCRETE BACKFILL TO 0.5 OF OUTSIDE DIAMETER WITH SHAPED BEDDING.



"BC" DENOTES OUTSIDE DIAMETER OF PIPE

LOAD FACTOR 1.9
CLASS B

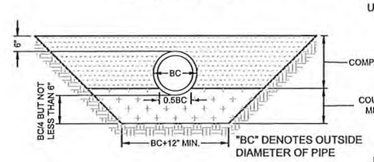
HAND SHAPED FROM ANGULAR BEDDING MATERIAL



"BC" DENOTES OUTSIDE DIAMETER OF PIPE

LOAD FACTOR 1.5
CLASS C-1

HAND SHAPED FROM FIRST UNDISTURBED SOIL

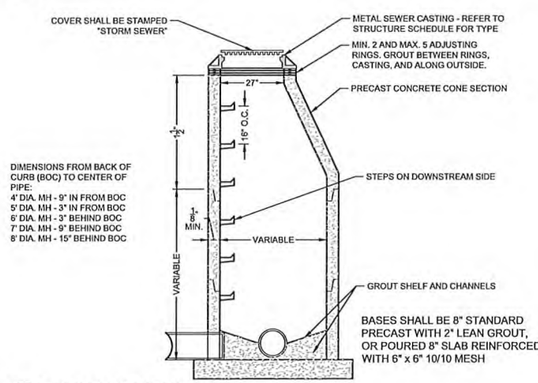


"BC" DENOTES OUTSIDE DIAMETER OF PIPE

LOAD FACTOR 1.5
CLASS C-2

HAND SHAPED FROM ANGULAR BEDDING MATERIAL

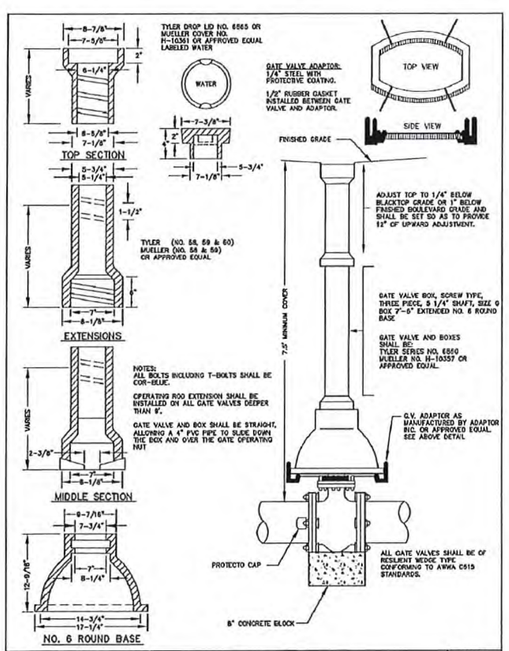
3 PIPE BEDDING - RCP & DIP
NTS



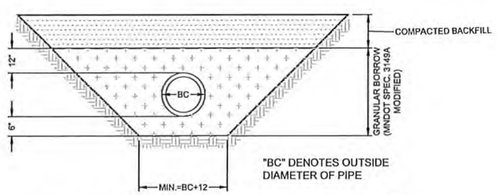
DIMENSIONS FROM BACK OF CURB (BOC) TO CENTER OF PIPE:
4' DIA. MH - 9" IN FROM BOC
5' DIA. MH - 3" IN FROM BOC
6' DIA. MH - 3" BEHIND BOC
7' DIA. MH - 9" BEHIND BOC
8' DIA. MH - 15" BEHIND BOC

BASES SHALL BE 8" STANDARD PRECAST WITH 2" LEAN GROUT, OR POURED 8" SLAB REINFORCED WITH 6" x 6" 10/10 MESH

5 STORM MANHOLE
NTS

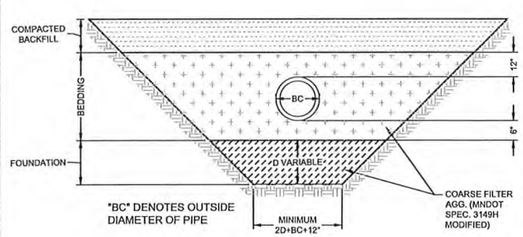


CITY OF EDINA ENGINEERING & PUBLIC WORKS DEPTS.	GATE VALVE AND BOX APPROVED: CITY ENGINEER	REVISED: 04-09 STANDARD PLATE 105



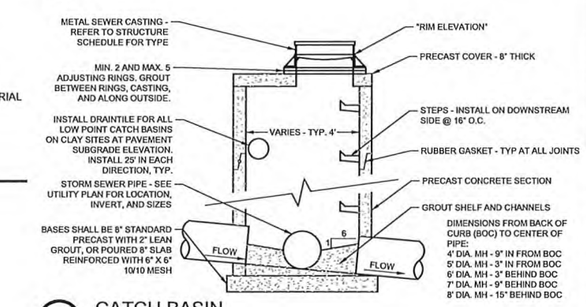
"BC" DENOTES OUTSIDE DIAMETER OF PIPE

PIPE FOUNDATION & BEDDING IN GOOD SOILS



PIPE FOUNDATION & BEDDING IN GOOD SOILS

1 PIPE BEDDING - PVC
NTS



BASES SHALL BE 8" STANDARD PRECAST WITH 2" LEAN GROUT, OR POURED 8" SLAB REINFORCED WITH 6" X 6" 10/10 MESH

DIMENSIONS FROM BACK OF CURB (BOC) TO CENTER OF PIPE:
4' DIA. MH - 9" IN FROM BOC
5' DIA. MH - 3" IN FROM BOC
6' DIA. MH - 3" BEHIND BOC
7' DIA. MH - 9" BEHIND BOC
8' DIA. MH - 15" BEHIND BOC

6 CATCH BASIN
NTS

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PRELIMINARY: NOT FOR CONSTRUCTION

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7300 METRO BOULEVARD, EDINA, MINNESOTA 55439

HEMPEL REAL ESTATE
10050 CROSSTOWN CIRCLE, SUITE 600, EDEN PRAIRIE, MN 55334

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Robert A. Latta
DATE 3/11/24 LICENSE NO. 59612

ISSUE/SUBMITTAL SUMMARY

DATE	DESCRIPTION
03/11/2024	CITY SUBMITTAL

DRAWN BY	JR
REVIEWED BY	RL
PROJECT MANAGER	RL
PROJECT NUMBER	23265

REVISION SUMMARY

DATE	DESCRIPTION

CIVIL DETAILS

C5.1

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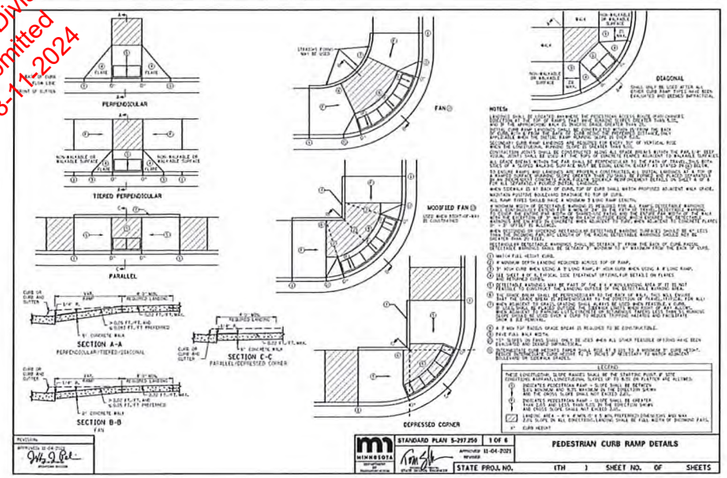
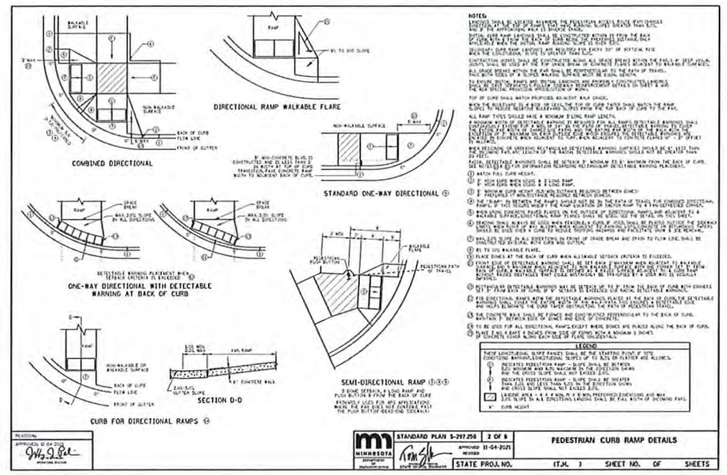
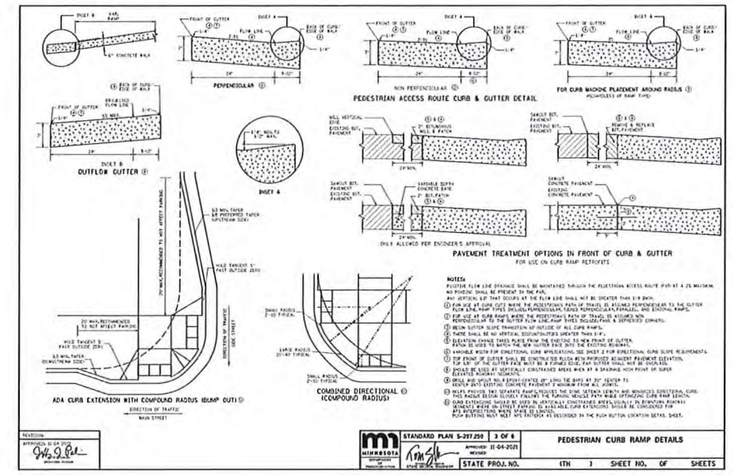
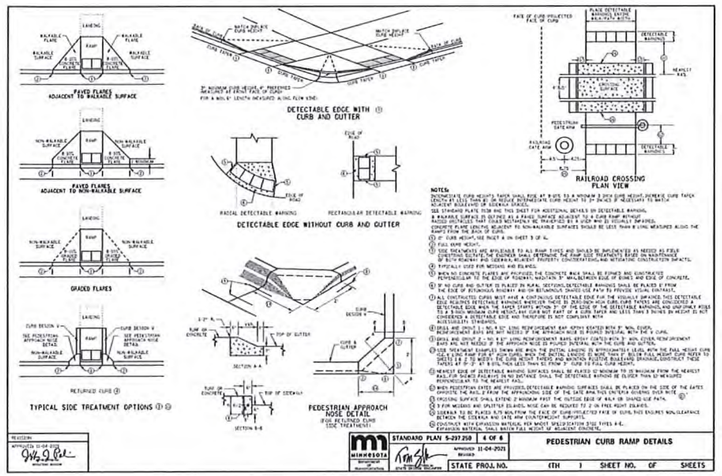
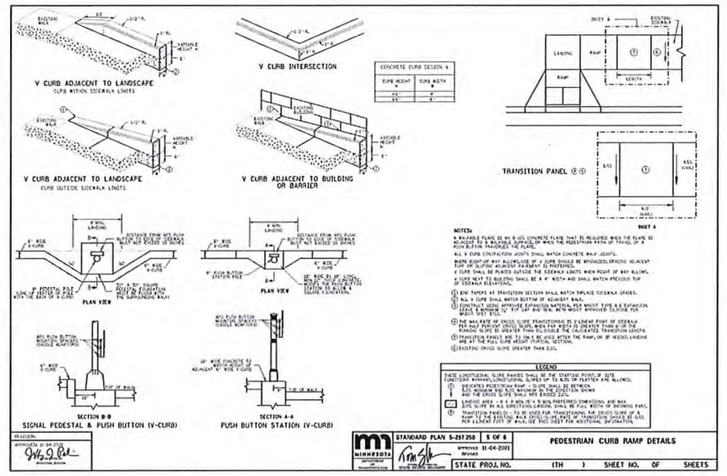
Planning Division
Submitted
03-11-2024

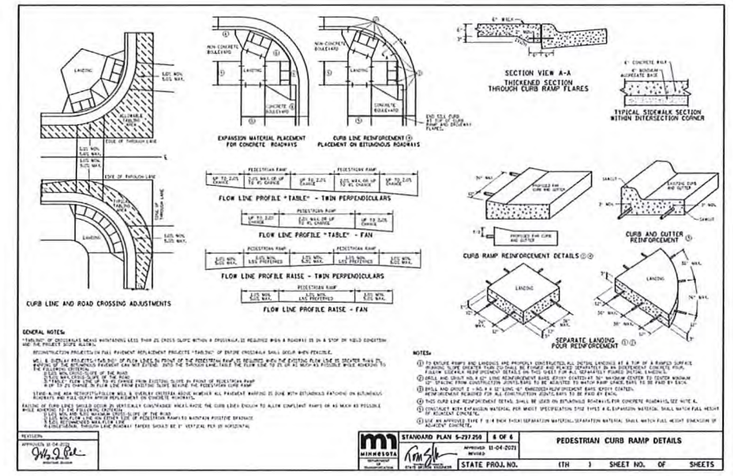


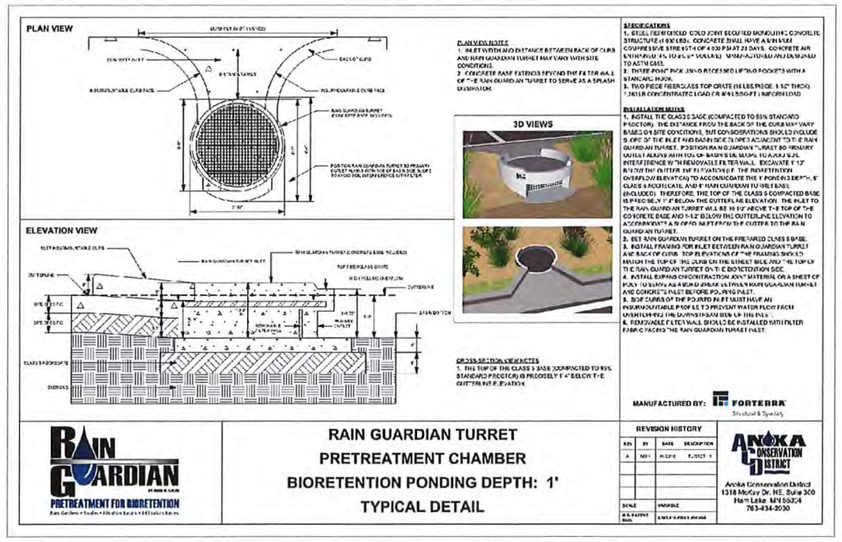












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Golden Valley, MN 55422
civilsitegroup.com 612-615-0060

PRELIMINARY: NOT FOR CONSTRUCTION

PROJECT
EDINA DENTAL OFFICE
7300 METRO BOULEVARD, EDINA, MINNESOTA 55439

HEMPEL REAL ESTATE
10050 CROSSTOWN CIRCLE, SUITE 600, EDEN PRAIRIE, MN 55344

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Robert A. Latta
DATE 3/11/24 LICENSE NO. 59612

ISSUE/SUBMITTAL SUMMARY

DATE	DESCRIPTION
03/11/2024	CITY SUBMITTAL

DRAWN BY JR
REVIEWED BY RL
PROJECT MANAGER RL
PROJECT NUMBER 23365

REVISION SUMMARY

DATE	DESCRIPTION

CIVIL DETAILS

C5.2

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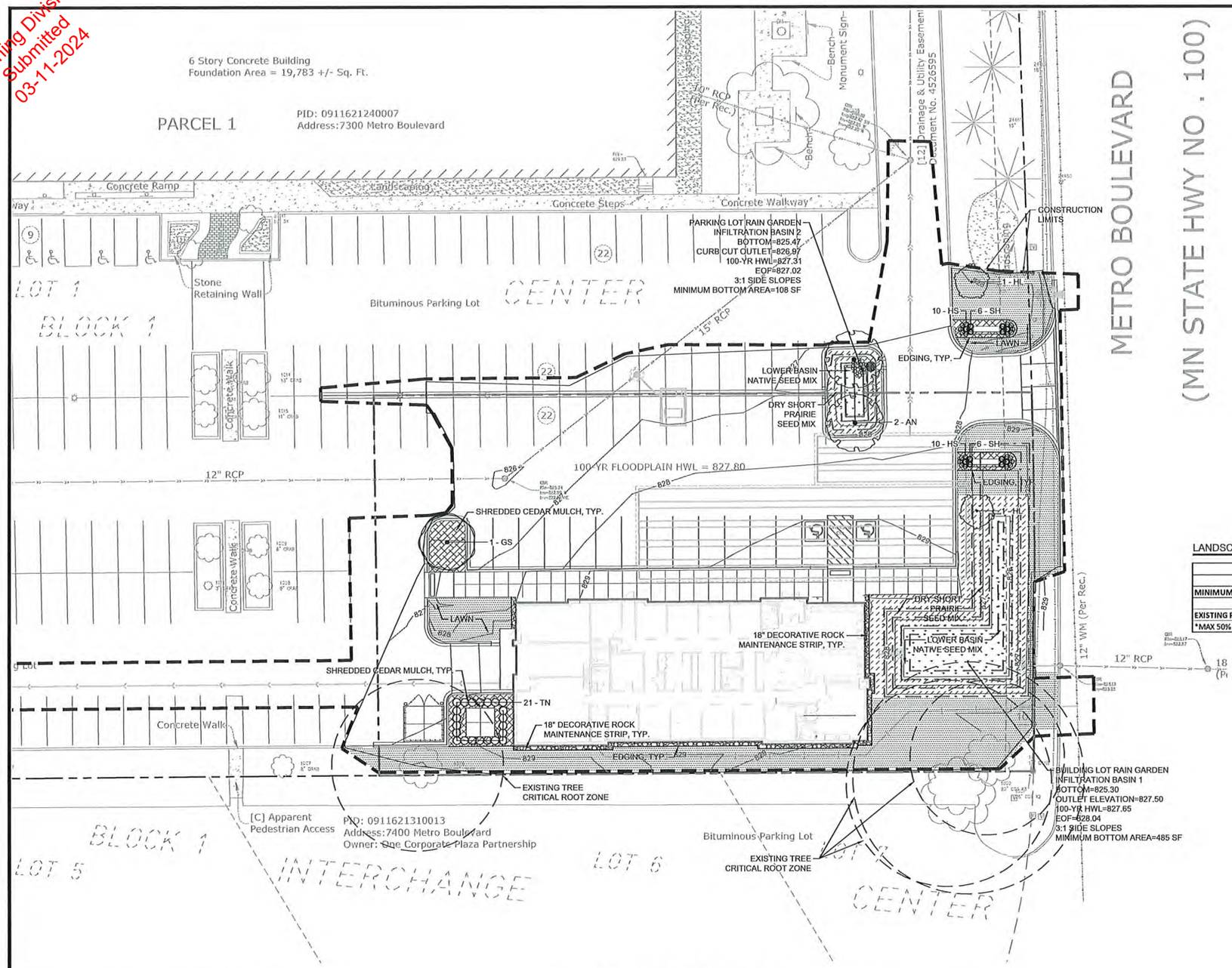
Planning Division
Submitted
03-11-2024



PLANT SCHEDULE

CODE	COMMON / BOTANICAL NAME	QTY	CONT
DECIDUOUS TREES			
AN	Northwood Red Maple / Acer rubrum 'Northwood'	3	3.5" CAL. B&B
GS	Skyline Honey Locust / Gleditsia triacanthos inermis 'Skyline'	1	3.5" CAL. B&B
ORNAMENTAL TREES			
HL	Limelight Panicle Hydrangea / Hydrangea paniculata 'Limelight'	2	1.5" CAL. B&B
CODE	COMMON / BOTANICAL NAME	QTY	SIZE
SHRUBS			
TN	Nova Japanese Yew / Taxus cuspidata 'Nova'	22	#3 CONT
GRASSES			
SH	Prairie Dropseed / Sporobolus heterolepis	12	#1 CONT
PERENNIALS			
HS	Stella Supreme Daylily / Hemerocallis x 'Stella Supreme'	20	#1 CONT

LANDSCAPE REQUIREMENTS:

OVERSTORY TREE REQUIREMENTS				
	SITE PERIMETER (LINEAR FT)	FT		MINIMUM REQ. TREES
MINIMUM OVERSTORY TREES REQUIRED	1,950.0	40		49
EXISTING PRESERVED TREES TO COUNT TOWARD MINIMUM OVERSTORY TREES REQUIRED				64
*MAX 50% OF ANY ONE SPECIES				

LEGEND

GROUND COVERS

- Lower Basin Native Seed Mix
- Rock Maintanence Strip / Rock Maintanence Strip
- Shredded Cedar Mulch / Shredded Hardwood Mulch
- Blue Grass Based / Sod

PROPOSED CANOPY TREE SYMBOLS - SEE PLANT SCHEDULE AND PLAN FOR SPECIES AND PLANTING SIZES

PROPOSED EVERGREEN TREE SYMBOLS - SEE PLANT SCHEDULE AND PLAN FOR SPECIES AND PLANTING SIZES

PROPOSED ORNAMENTAL TREE SYMBOLS - SEE PLANT SCHEDULE AND PLAN FOR SPECIES AND PLANTING SIZES

PROPOSED DECIDUOUS AND EVERGREEN SHRUB SYMBOLS - SEE PLANT SCHEDULE AND PLAN FOR SPECIES AND PLANTING SIZES

PROPOSED PERENNIAL PLANT SYMBOLS - SEE PLANT SCHEDULE AND PLAN FOR SPECIES AND PLANTING SIZES

EDGING - SHALL BE COMMERCIAL GRADE, 4" DEPTH ALUMINUM, BLACK OR DARK GREEN IN COLOR, INCLUDE ALL CONNECTORS, STAKES, & ALL APPURTENANCES PER MANUF. INSTALL PER MANUF. INSTRUC./SPECS.

DECORATIVE BOULDERS (ROUNDED & BLOCK STYLE), 18"-30" DIA.

CONSTRUCTION LIMITS

MULCH SCHEDULE

AREA	MULCH TYPE	EDGING	FABRIC	REMARKS
TREE RINGS	4" DEPTH, SHREDDED CEDAR	YES	NO	SEE DETAIL SHT. L1.1
PLANTING BEDS	4" DEPTH, SHREDDED CEDAR	YES	NO	
MAINT. STRIP AT BUILDING FOUNDATION	NA	NA	NA	
PLAYGROUND MULCH	12" DEPTH ENGINEERED PLAYGROUND MULCH, SEE REMARKS. DEPTH AS SPECIFIED BY PLAYGROUND EQUIPMENT MANUFACTURER.	SEE SITE PLAN	YES	ORGANIC MULCH DESIGNED SPECIFICALLY FOR USE ON PLAYGROUNDS(CERTIFIED BY ANSI/ASTM FOR PLAYGROUND USE. (I.E. SYLVA SOFTSTEP, SET,INC. PLAYGROUND, OR EQUIVALENT)
DOG PARK MULCH	4" DEPTH, SHREDDED WOOD, SEE REMARKS	YES	YES	ORGANIC MULCH SPECIFICALLY FOR USE IN DOG PARKS OR PLAYGROUNDS. SHALL KNIT TOGETHER TO FORM MATTED, ACCESSIBLE SURFACE. (I.E. SYLVA SOFTSTEP, SET, INC. PLAYGROUND, OR EQUIVALENT), PROVIDE SHOP DRAWINGS/DATA SHEETS FOR REVIEW.
NATIVE SEED AREAS	NA	NA	NA	
NOTE: COORDINATE ALL MULCH AND PLANTING BED MATERIAL PRIOR TO INSTALLATION, PROVIDE SAMPLES AND SHOP DRAWINGS/PHOTOS/DATA SHEETS OF ALL MATERIALS				

POLLINATOR SAFE PLANT MATERIAL:

1. THE CONTRACTOR SHALL PROVIDE ONLY PLANT MATERIAL FREE OF NEONICOTINOID BASED INSECTICIDES AND/OR TREATMENTS OF ANY KIND, INCLUDING BY NOT LIMITED TO IMIDACLOPRID (CONFIDOR, ADMIRE, GAUCHO, ADVOCATE), THIAMETHOXAM (ACTARA, PLATINUM, CRUISER), CLOTHIANIDIN (PONCHO, DANTOSU, DANTOP), ACETAMIPRID (MOSPILAN, ASSAIL, CHIPCOTRISTAR), THIACLOPRID (CALYPSO), DINOTEFURAN (STARKLE, SAFARI, VENOM), AND NITENPYRAM (CAPSTAR, GUARDIAN).
2. CONTRACTOR SHALL CERTIFY, THROUGH SUPPLIERS POLICY STATEMENT OR AFFIDAVIT, THAT NO NEONICOTINOID BASED INSECTICIDES HAVE BEEN USED ON SITE OR DIRECTLY ADJACENT TO THE GROWING OR STORAGE PLOTS OF THE SUPPLIED PLANT MATERIAL, INCLUDING THE PLANTING OF AGRICULTURAL (OR OTHER) SEED TREATED WITH NEONICS.

PLANTING SEASON SCHEDULE

SEASON	CONIFEROUS	DECIDUOUS	REMARKS
SPRING PLANTING	APRIL 15 - JUNE 15	APRIL 15 - JUNE 15	
FALL PLANTING	AUGUST 21 - SEPTEMBER 30	AUGUST 15 - NOVEMBER 15	
NOTE: ADJUSTMENTS TO PLANTING DATES MUST BE APPROVED IN WRITING BY THE LANDSCAPE ARCHITECT.			

SEE SHEET L1.1 FOR GENERAL LANDSCAPE NOTES

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1" = 20'-0"
10'-0" 0 20'-0"

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Patrick J. Sarver
DATE 3/11/24 LICENSE NO. 24904

ISSUE/SUBMITTAL SUMMARY

DATE	DESCRIPTION
03/11/2024	CITY SUBMITTAL

DRAWN BY	JR
REVIEWED BY	KL
PROJECT MANAGER	KL
PROJECT NUMBER	22069

REVISION SUMMARY

DATE	DESCRIPTION

LANDSCAPE PLAN

L1.0

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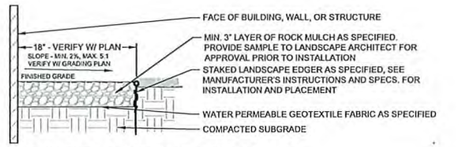
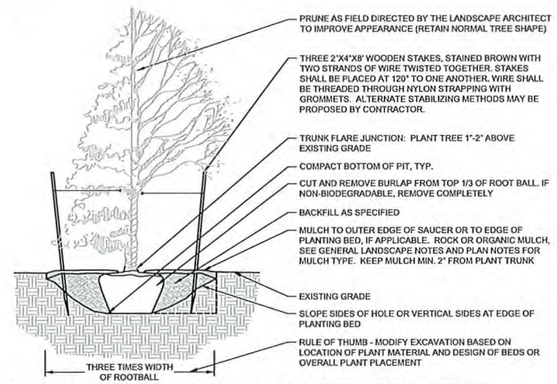
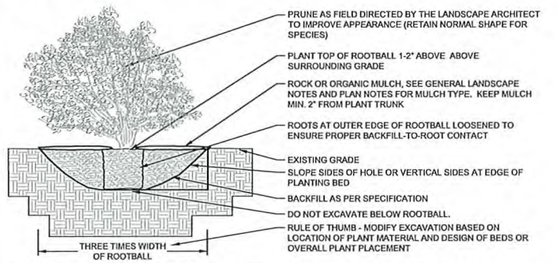
Submitted
03-11-2024



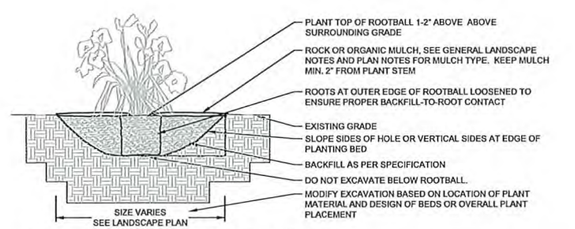
1 AGGREGATE MAINTANENCE STRIP
NTS



2 DECIDUOUS & CONIFEROUS TREE PLANTING
NTS



3 DECIDUOUS & CONIFEROUS SHRUB PLANTING
NTS



4 PERENNIAL BED PLANTING
NTS

IRRIGATION NOTES:

1. ENTIRE SITE SHALL BE FULLY IRRIGATED. THE CONTRACTOR SHALL SUBMIT IRRIGATION SHOP DRAWINGS FOR REVIEW AND APPROVAL BY THE LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.
2. PROVIDE SITE WIDE IRRIGATION SYSTEM DESIGN AND INSTALLATION. SYSTEM SHALL BE FULLY PROGRAMMABLE AND CAPABLE OF ALTERNATE DATE WATERING. THE SYSTEM SHALL PROVIDE HEAD TO HEAD OR DRIP COVERAGE AND BE CAPABLE OF DELIVERING ONE INCH OF PRECIPITATION PER WEEK. SYSTEM SHALL EXTEND INTO THE PUBLIC RIGHT-OF-WAY TO THE EDGE OF PAVEMENT/BACK OF CURB.
3. CONTRACTOR SHALL SECURE APPROVAL OF PROPOSED IRRIGATION SYSTEM INLCUDING PRICING FROM OWNER, PRIOR TO INSTALLATION.
4. SEE MECHANICAL AND ELECTRICAL PLANS AND SPECIFICATIONS FOR IRRIGATION WATER, METER, AND POWER CONNECTIONS.
5. CONTRACTOR TO VERIFY LOCATION OF ALL UNDERGROUND/ABOVE GROUND FACILITIES PRIOR TO ANY EXCAVATION/INSTALLATION. ANY DAMAGE TO UNDERGROUND/ABOVE GROUND FACILITIES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND COSTS ASSOCIATED WITH CORRECTING DAMAGES SHALL BE BORNE ENTIRELY BY THE CONTRACTOR.
6. SERVICE EQUIPMENT AND INSTALLATION SHALL BE PER LOCAL UTILITY COMPANY STANDARDS AND SHALL BE PER NATIONAL AND LOCAL CODES. EXACT LOCATION OF SERVICE EQUIPMENT SHALL BE COORDINATED WITH THE LANDSCAPE ARCHITECT OR EQUIVALENT AT THE JOB SITE.
7. CONTRACTOR SHALL COORDINATE WITH LOCAL UTILITY COMPANY FOR THE PROPOSED ELECTRICAL SERVICE AND METERING FACILITIES.
8. IRRIGATION WATER LINE CONNECTION SIZE IS 1-½" AT BUILDING. VERIFY WITH MECHANICAL PLANS.
9. ALL MAIN LINES SHALL BE 18" BELOW FINISHED GRADE.
10. ALL LATERAL LINES SHALL BE 12" BELLOW FINISHED GRADE.
11. ALL EXPOSED PVC RISERS, IF ANY, SHALL BE GRAY IN COLOR.
12. CONTRACTOR SHALL LAY ALL SLEEVES AND CONDUIT AT 2'-0" BELOW THE FINISHED GRADE OF THE TOP OF PAVEMENT. EXTEND SLEEVES TO 2'-0" BEYOND PAVEMENT.
13. CONTRACTOR SHALL MARK THE LOCATION OF ALL SLEEVES AND CONDUIT WITH THE SLEEVING MATERIAL "ELLED" TO 2'-0" ABOVE FINISHED GRADE AND CAPPED.
14. FABRICATE ALL PIPE TO MANUFACTURE'S SPECIFICATIONS WITH CLEAN AND SQUARE CUT JOINTS. USE QUALITY GRADE PRIMER AND SOLVENT CEMENT FORMULATED FOR INTENDED TYPE OF CONNECTION.
15. BACKFILL ALL TRENCHES WITH SOIL FREE OF SHARP OBJECTS AND DEBRIS.
16. ALL VALVE BOXES AND COVERS SHALL BE BLACK IN COLOR.
17. GROUP VALVE BOXES TOGETHER FOR EASE WHEN SERVICE IS REQUIRED. LOCATE IN PLANT BED AREAS WHENEVER POSSIBLE.
18. IRRIGATION CONTROLLER LOCATION SHALL BE VERIFIED ON-SITE WITH OWNER'S REPRESENTATIVE.
19. CONTROL WIRES: 14 GAUGE DIRECT BURIAL, SOLID COPPER IRRIGATION WIRE. RUN UNDER MAIN LINE. USE MOISTURE-PROOF SPLICES AND SPLICE ONLY AT VALVES OR PULL BOXES. RUN SEPARATE HOT AND COMMON WIRE TO EACH VALVE AND ONE (1) SPARE WIRE AND GROUND TO FURTHEST VALVE FROM CONTROLLER. LABEL OR COLOR CODE ALL WIRES.
20. AVOID OVER SPRAY ON BUILDINGS, PAVEMENT, WALLS AND ROADWAYS BY INDIVIDUALLY ADJUSTING RADIUS OR ARC ON SPRINKLER HEADS AND FLOW CONTROL ON AUTOMATIC VALVE.
21. ADJUST PRESSURE REGULATING VALVES FOR OPTIMUM PRESSURE ON SITE.
22. USE SCREENS ON ALL HEADS.
23. A SET OF AS-BUILT DRAWINGS SHALL BE MAINTAINED ON-SITE AT ALL TIMES IN AN UPDATED CONDITION.
24. ALL PIPE 3" AND OVER SHALL HAVE THRUST BLOCKING AT EACH TURN.
25. ALL AUTOMATIC REMOTE CONTROL VALVES WILL HAVE 3" MINIMUM DEPTH OF 3/4" WASHED GRAVEL UNDERNEATH VALVE AND VALVE BOX. GRAVEL SHALL EXTENT 3" BEYOND PERIMETER OF VALVE BOX.
26. THERE SHALL BE 3" MINIMUM SPACE BETWEEN BOTTOM OF VALVE BOX COVER AND TOP OF VALVE STRUCTURE.

LANDSCAPE NOTES:

1. ALL EXISTING UTILITY LOCATIONS SHOWN ARE APPROXIMATE. CONTACT "GOPHER STATE ONE CALL" (651-454-0002 OR 800-252-1166) FOR UTILITY LOCATIONS, 48 HOURS PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL REPAIR OR REPLACE ANY UTILITIES THAT ARE DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.
2. WHERE SHOWN, SHRUB & PERENNIAL BEDS SHALL BE MULCHED WITH 4" DEPTH (MINIMUM AFTER INSTALLATION AND/OR TOP DRESSING OPERATIONS) OF SHREDDED CEDAR MULCH.
3. ALL TREES SHALL BE MULCHED WITH SHREDDED CEDAR MULCH TO OUTER EDGE OF SAUCER OR TO EDGE OF PLANTING BED, IF APPLICABLE. ALL MULCH SHALL BE KEPT WITHIN A MINIMUM OF 2" FROM TREE TRUNK.
4. IF SHOWN ON PLAN, RANDOM SIZED LIMESTONE BOULDERS COLOR AND SIZE TO COMPLIMENT NEW LANDSCAPING. OWNER TO APPROVE BOULDER SAMPLES PRIOR TO INSTALLATION.
5. PLANT MATERIALS SHALL CONFORM WITH THE AMERICAN ASSOCIATION OF NURSERYMEN STANDARDS AND SHALL BE OF HARDY STOCK, FREE FROM DISEASE, DAMAGE AND DISFIGURATION. CONTRACTOR IS RESPONSIBLE FOR MAINTAINING PLUMPNESS OF PLANT MATERIAL FOR DURATION OF ACCEPTANCE PERIOD.
6. UPON DISCOVERY OF A DISCREPANCY BETWEEN THE QUANTITY OF PLANTS SHOWN ON THE SCHEDULE AND THE QUANTITY SHOWN ON THE PLAN, THE PLAN SHALL GOVERN.
7. CONDITION OF VEGETATION SHALL BE MONITORED BY THE LANDSCAPE ARCHITECT THROUGHOUT THE DURATION OF THE CONTRACT. LANDSCAPE MATERIALS PART OF THE CONTRACT SHALL BE WARRANTED FOR TWO (2) FULL GROWING SEASONS FROM SUBSTANTIAL COMPLETION DATE.
8. ALL AREAS DISTURBED BY CONSTRUCTION ACTIVITIES SHALL RECEIVE 6" LAYER TOPSOIL AND SOD AS SPECIFIED UNLESS OTHERWISE NOTED ON THE DRAWINGS.
9. COORDINATE LOCATION OF VEGETATION WITH UNDERGROUND AND OVERHEAD UTILITIES, LIGHTING FIXTURES, DOORS AND WINDOWS. CONTRACTOR SHALL STAKE IN THE FIELD FINAL LOCATION OF TREES AND SHRUBS FOR REVIEW AND APPROVAL BY THE LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.
10. ALL PLANT MATERIALS SHALL BE WATERED AND MAINTAINED UNTIL ACCEPTANCE.
11. REPAIR AT NO COST TO OWNER ALL DAMAGE RESULTING FROM LANDSCAPE CONTRACTOR'S ACTIVITIES.
12. SWEEP AND MAINTAIN ALL PAVED SURFACES FREE OF DEBRIS GENERATED FROM LANDSCAPE CONTRACTOR'S ACTIVITIES.
13. PROVIDE SITE WIDE IRRIGATION SYSTEM DESIGN AND INSTALLATION. SYSTEM SHALL BE FULLY PROGRAMMABLE AND CAPABLE OF ALTERNATE DATE WATERING. THE SYSTEM SHALL PROVIDE HEAD TO HEAD OR DRIP COVERAGE AND BE CAPABLE OF DELIVERING ONE INCH OF PRECIPITATION PER WEEK. SYSTEM SHALL EXTEND INTO THE PUBLIC RIGHT-OF-WAY TO THE EDGE OF PAVEMENT/BACK OF CURB.
14. CONTRACTOR SHALL SECURE APPROVAL OF PROPOSED IRRIGATION SYSTEM INLCUDING PRICING FROM OWNER, PRIOR TO INSTALLATION.

SEE SHEET L1.0 FOR LEGEND

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Golden Valley, MN 55422
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PRELIMINARY: NOT FOR CONSTRUCTION

PROJECT
EDINA DENTAL OFFICE
7300 METRO BOULEVARD, EDINA, MINNESOTA 55439

HEMPEL REAL ESTATE
10050 CROSSTOWN CIRCLE, SUITE 600, EDEN PRAIRIE, MN 55344

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Patrick J. Sarver
DATE 3/11/24 LICENSE NO. 24904

ISSUE/SUBMITTAL SUMMARY

DATE	DESCRIPTION
03/11/2024	CITY SUBMITTAL

DRAWN BY	JR
REVIEWED BY	RL
PROJECT MANAGER	RL
PROJECT NUMBER	23365

REVISION SUMMARY

DATE	DESCRIPTION

LANDSCAPE PLAN NOTES & DETAILS

L1.1

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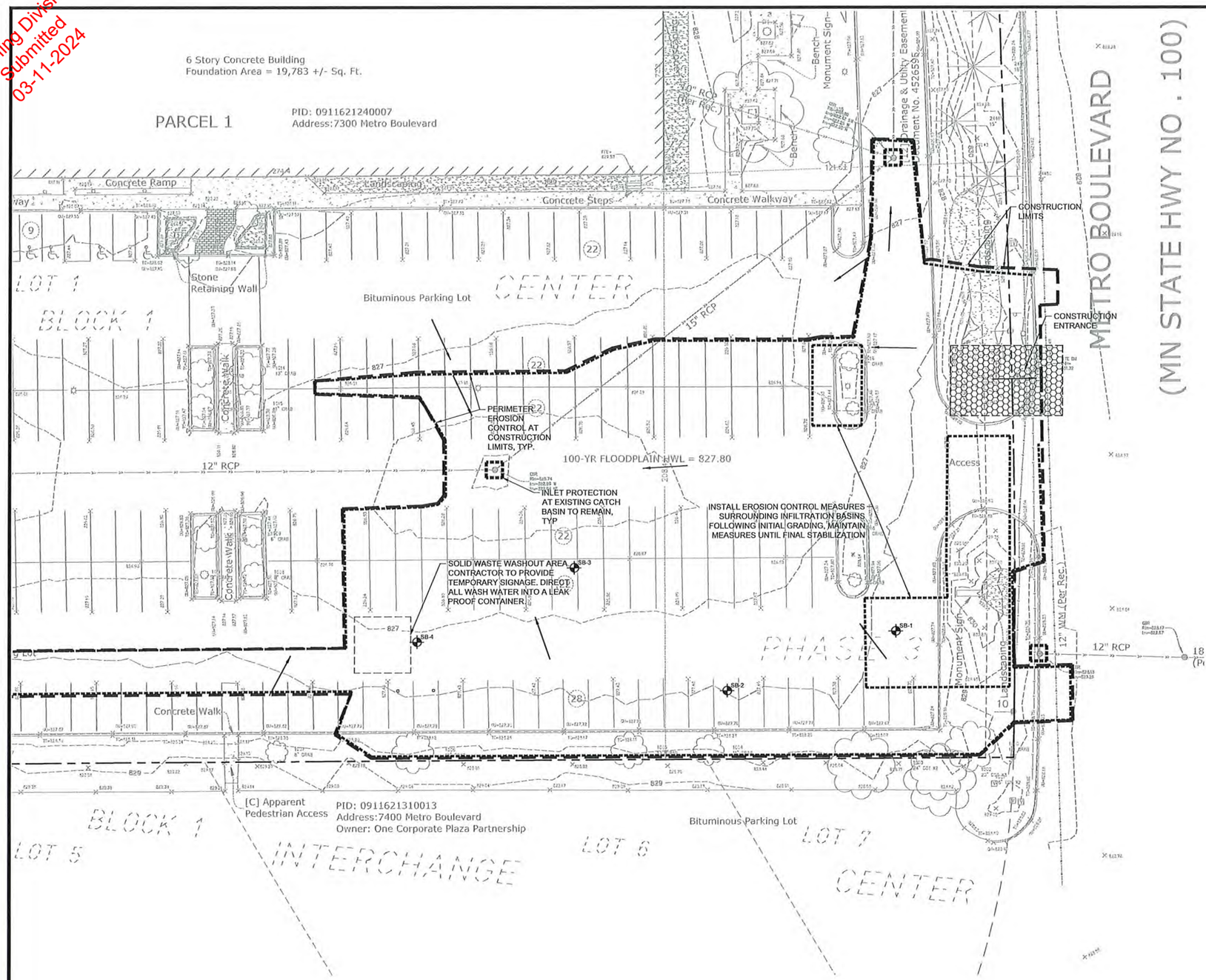
Planning Division
Submitted
03-11-2024



CITY OF EDINA EROSION CONTROL NOTES:

1. RESERVED FOR CITY SPECIFIC EROSION CONTROL NOTES.

ALL SPECIFIED EROSION AND SEDIMENT CONTROL PRACTICES, AND MEASURES CONTAINED IN THIS SWPPP ARE THE MINIMUM REQUIREMENTS. ADDITIONAL PRACTICES MAY BE REQUIRED DURING THE COURSE OF CONSTRUCTION.

SWPPP NOTES:

1. ALL EXISTING UTILITY LOCATIONS SHOWN ARE APPROXIMATE. CONTACT "GOPHER STATE ONE CALL" (651-454-0002 OR 800-252-1166) FOR UTILITY LOCATIONS, 48 HOURS PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL REPAIR OR REPLACE ANY UTILITIES THAT ARE DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.
2. THIS PROJECT IS LESS THAN ONE ACRE AND WILL NOT REQUIRE AN MPCA NPDES PERMIT. CONTRACTOR IS RESPONSIBLE FOR OBTAINING ANY EROSION CONTROL PERMITS REQUIRED BY THE CITY.
3. SEE SHEETS SW1.0 - SW1.5 FOR ALL EROSION CONTROL NOTES, DESCRIPTIONS, AND PRACTICES.
4. SEE GRADING PLAN FOR ADDITIONAL GRADING AND EROSION CONTROL NOTES.
5. CONTRACTOR IS RESPONSIBLE FOR SWPPP IMPLEMENTATION, INSPECTIONS, AND COMPLIANCE WITH NPDES PERMIT.

LEGEND:

--------- 1125 ---------	EX. 1' CONTOUR ELEVATION INTERVAL
——→	DRAINAGE ARROW
----------	SILT FENCE / BIOROLL - GRADING LIMIT
	INLET PROTECTION
	STABILIZED CONSTRUCTION ENTRANCE

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10'-0" 0 20'-0"

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PRELIMINARY: NOT FOR CONSTRUCTION

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7300 METRO BOULEVARD, EDINA, MINNESOTA 55439

HEMPEL REAL ESTATE
10050 CROSSTOWN CIRCLE, SUITE 600, EDEN PRAIRIE, MN 55334

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Robert A. Latta
DATE 3/11/24 LICENSE NO. 59612

ISSUE/SUBMITTAL SUMMARY

DATE	DESCRIPTION
03/11/2024	CITY SUBMITTAL

DRAWN BY	JR
REVIEWED BY	RL
PROJECT MANAGER	RL
PROJECT NUMBER	23365

REVISION SUMMARY

DATE	DESCRIPTION

SWPPP - EXISTING CONDITIONS

SW1.0

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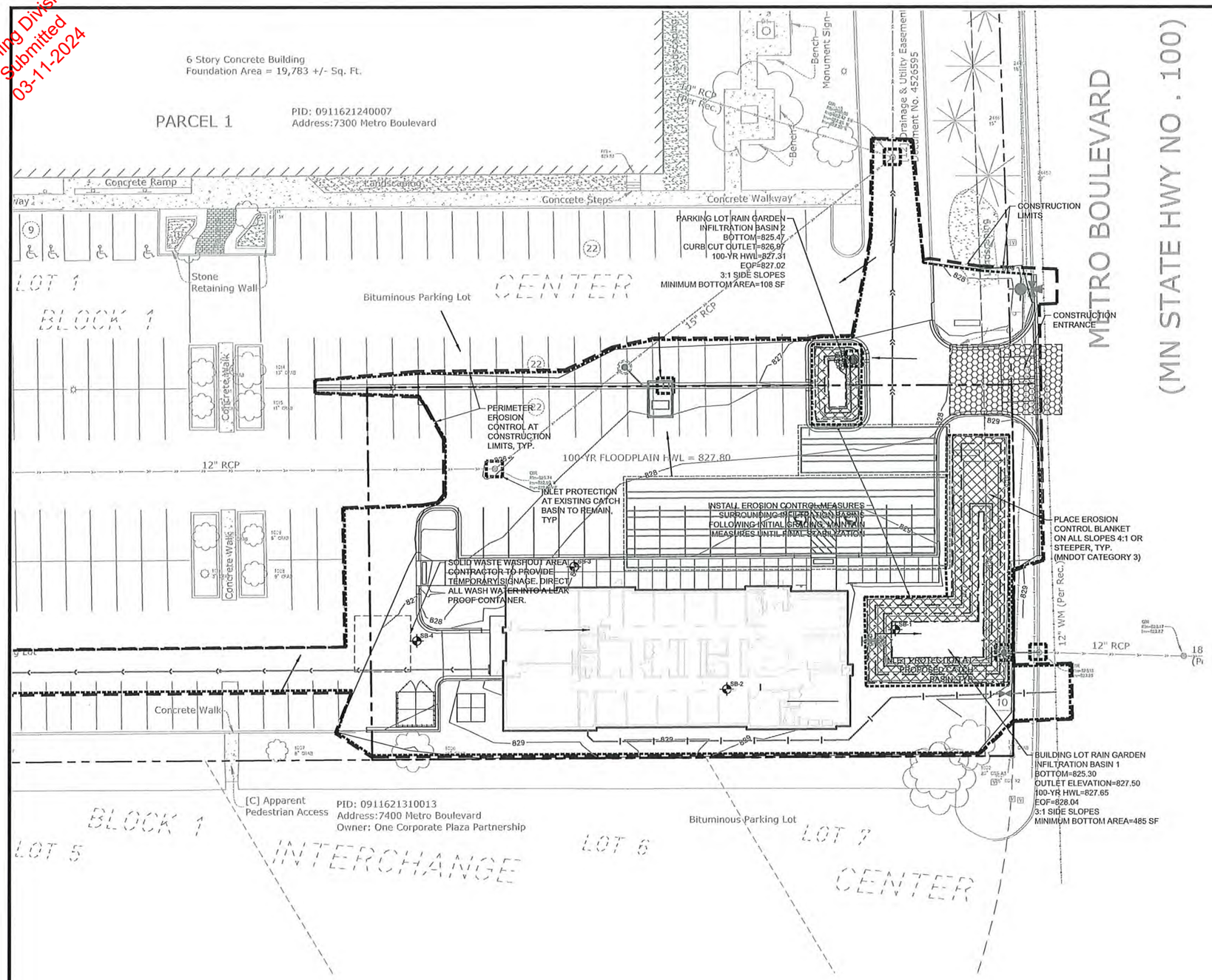
Planning Division
Submitted
03-11-2024



CITY OF EDINA EROSION CONTROL NOTES:

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LEGEND:

1125	EX. 1' CONTOUR ELEVATION INTERVAL
1137	1.0' CONTOUR ELEVATION INTERVAL
	DRAINAGE ARROW
	SILT FENCE / BIOROLL - GRADING LIMIT
	INLET PROTECTION
	STABILIZED CONSTRUCTION ENTRANCE
	EROSION CONTROL BLANKET

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Robert A. Latta
DATE 3/11/24 LICENSE NO. 59612

ISSUE/SUBMITTAL SUMMARY

DATE	DESCRIPTION
03/11/2024	CITY SUBMITTAL

DRAWN BY	JR
REVIEWED BY	RL
PROJECT MANAGER	RL
PROJECT NUMBER	23355

REVISION SUMMARY

DATE	DESCRIPTION

SWPPP - PROPOSED CONDITIONS

SW1.1

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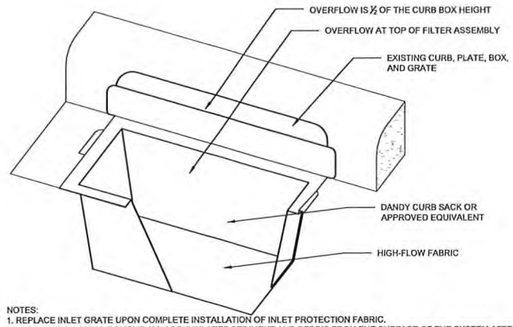
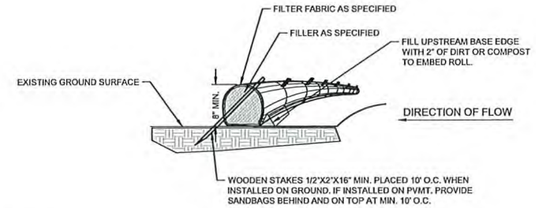
Planning Division
Submitted
03-11-2024



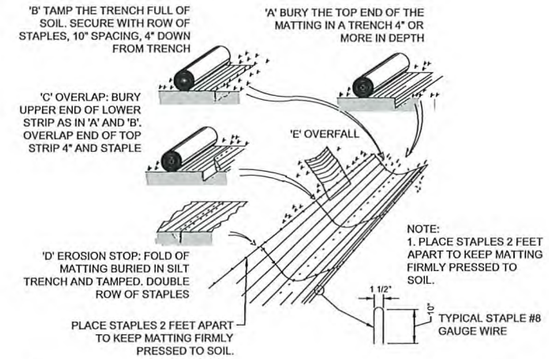
NOTES:
1. REPLACE INLET GRATE UPON COMPLETE INSTALLATION OF INLET PROTECTION FABRIC.
2. CONTRACTOR SHALL REMOVE ALL ACCUMULATED SEDIMENT AND DEBRIS FROM THE SURFACE OF THE SYSTEM AFTER EACH STORM EVENT AND AT THE COMPLETION OF THE CONTRACT.
3. CONTRACTOR SHALL USE DANDY SACK OR APPROVED EQUIVALENT FOR STRUCTURES WITHOUT CURBING.

1 CURB INLET SACK
NTS



NOTE:
1. COMPOST FILTER LOGS (BIO ROLLS) SHALL BE FILTREXX EROSION CONTROL SOXX OR APPROVED EQUAL.
2. COMPOST FILLER TO BE MADE FROM A COMPOST BLEND 30%-40% GRADE 2 (SPEC 3890) AND 60%-70% PARTIALLY DECOMPOSED WOOD CHIPS, PER MNDOT SPEC 3897.
3. FILTER FABRIC SHALL BE GEOTEXTILE KNITTED MATERIAL WITH MAX. OPENINGS OF 3/8".
4. IF MULTIPLE ROLLS NEEDED, OVERLAP BY MIN. 12" AT ENDS AND STAKE.
5. SILT SHALL BE REMOVED ONCE IT REACHES 80% OF THE HEIGHT OF THE ROLL OR AS DEEMED NECESSARY BY SITE CONTRACTOR TO MAINTAIN PROPER FUNCTION.

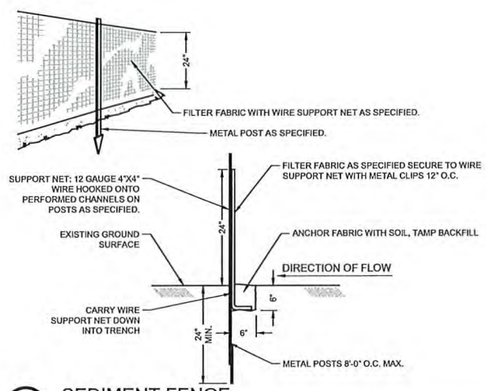
3 SEDIMENT BIO-ROLL / COMPOST FILTER LOG
NTS



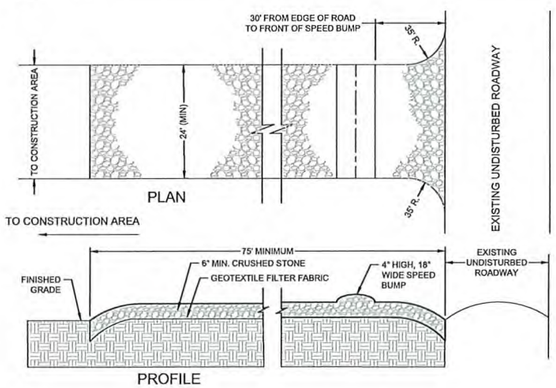
NOTE:
1. PLACE STAPLES 2 FEET APART TO KEEP MATTING FIRMLY PRESSED TO SOIL.

NOTE:
ANY USE OF EROSION CONTROL BLANKET MUST UTILIZE A FULLY BIO-DEGRADABLE EROSION CONTROL BLANKET (NO PLASTIC NETTING) WITH LOOSE WEAVE NETTING (OR NETLESS). SEE NMCWD RULE 5.3.1B.

5 EROSION BLANKET
NTS



2 SEDIMENT FENCE
NTS



NOTES:
1. PROVIDE APPROPRIATE TRANSITION BETWEEN STABILIZED CONSTRUCTION ENTRANCE AND UNDISTURBED ROADWAY.
2. THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO UNDISTURBED ROADWAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE OR ADDING STONE TO THE LENGTH OF THE ENTRANCE.
3. REPAIR AND CLEANOUT MEASURES USED TO TRAP SEDIMENT.
4. ALL SEDIMENT SPILLED, DROPPED, WASHED, OR TRACKED ONTO UNDISTURBED ROADWAY SHALL BE REMOVED AS DIRECTED BY THE ENGINEER.
5. FINAL LOCATION AND INSTALLATION SHALL BE COORDINATED WITH THE CITY PRIOR TO CONSTRUCTION ACTIVITIES.
6. CRUSHED STONE SHALL BE 1-1/2" DIA. CLOSE GRADED, AND IN ACCORDANCE TO MNDOT SECTION 2118.

4 STABILIZED CONSTRUCTION ACCESS
NTS

CivilSite GROUP
Civil Engineering • Surveying • Landscape Architecture
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Golden Valley, MN 55422
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PRELIMINARY: NOT FOR CONSTRUCTION

PROJECT
EDINA DENTAL OFFICE
7300 METRO BOULEVARD, EDINA, MINNESOTA 55439

HEMPEL REAL ESTATE
10050 CROSSTOWN CIRCLE, SUITE 600, EDEN PRAIRIE, MN 55334

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

Robert A. Latta
DATE 3/11/24 LICENSE NO. 59612

ISSUE/SUBMITTAL SUMMARY

DATE	DESCRIPTION
03/11/2024	CITY SUBMITTAL

DRAWN BY	JR
REVIEWED BY	RL
PROJECT MANAGER	RL
PROJECT NUMBER	23365

REVISION SUMMARY

DATE	DESCRIPTION

SWPPP - DETAILS

SW1.2

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Planning Division Submission 03-07-2024

THE CONTRACTOR AND ALL SUBCONTRACTORS INVOLVED WITH A CONSTRUCTION ACTIVITY THAT DISTURBS SITE SOIL OR WHO IMPLEMENT A POLLUTANT CONTROL MEASURE IDENTIFIED IN THE STORM WATER POLLUTION PREVENTION PLAN (SWPPP) MUST COMPLY WITH THE REQUIREMENTS OF THE NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM (NPDES) GENERAL PERMIT (DATED AUGUST 1, 2018 # MNR100001) AND ANY LOCAL GOVERNING AGENCY HAVING JURISDICTION CONCERNING EROSION AND SEDIMENTATION CONTROL.

STORMWATER DISCHARGE DESIGN REQUIREMENTS

SWPPP

THE NATURE OF THIS PROJECT WILL BE CONSISTENT WITH WHAT IS REPRESENTED IN THIS SET OF CONSTRUCTION PLANS AND SPECIFICATIONS. SEE THE SWPPP PLAN SHEETS AND SWPPP NARRATIVE (ATTACHMENT A: CONSTRUCTION SWPPP TEMPLATE) FOR ADDITIONAL SITE SPECIFIC SWPPP INFORMATION. THE PLANS SHOW LOCATIONS AND TYPES OF ALL TEMPORARY AND PERMANENT EROSION PREVENTION AND SEDIMENT CONTROL BMP'S. STANDARD DETAILS ARE ATTACHED TO THIS SWPPP DOCUMENT.

THE INTENDED SEQUENCING OF MAJOR CONSTRUCTION ACTIVITIES IS AS FOLLOWS:
1. INSTALL STABILIZED ROCK CONSTRUCTION ENTRANCE
2. INSTALLATION OF SILT FENCE AROUND SITE
3. INSTALL ORANGE CONSTRUCTION FENCING AROUND INFILTRATION AREAS
4. INSTALL INLET PROTECTION AT ALL ADJACENT AND DOWNSTREAM CATCH BASINS
5. CLEAR AND GRUB FOR TEMPORARY SEDIMENT BASIN / POND INSTALL
6. CONSTRUCT TEMPORARY SEDIMENT BASIN / POND (SECTION 14)
7. CLEAR AND GRUB REMAINDER OF SITE
8. STRIP AND STOCKPILE TOPSOIL
9. ROUGH GRADING OF SITE
10. STABILIZE DENUDED AREAS AND STOCKPILES
11. INSTALL SANITARY SEWER, WATER MAIN STORM SEWER AND SERVICES
12. INSTALL SILT FENCE / INLET PROTECTION AROUND CB'S
13. INSTALL STREET SECTION
14. INSTALL CURB AND GUTTER
15. BITUMINOUS ON STREETS
16. FINAL GRADE BOULEVARD, INSTALL SEED AND MULCH
17. REMOVE ACCUMULATED SEDIMENT FROM BASIN / POND
18. FINAL GRADE POND / INFILTRATION BASINS (DO NOT COMPACT SOILS IN INFILTRATION AREAS.)
19. WHEN ALL CONSTRUCTION ACTIVITY IS COMPLETE AND THE SITE IS STABILIZED BY EITHER SEED OR SOD/LANDSCAPING, REMOVE SILT FENCE AND RESEED ANY AREAS DISTURBED BY THE REMOVAL.

RECORDS RETENTION:

THE SWPPP (ORIGINAL OR COPIES) INCLUDING, ALL CHANGES TO IT, AND INSPECTIONS AND MAINTENANCE RECORDS MUST BE KEPT AT THE SITE DURING CONSTRUCTION BY THE PERMITTEE WHO HAS OPERATIONAL CONTROL OF THAT PORTION OF THE SITE. THE SWPPP CAN BE KEPT IN EITHER THE FIELD OFFICE OR IN AN ON SITE VEHICLE DURING NORMAL WORKING HOURS.

ALL OWNER(S) MUST KEEP THE SWPPP, ALONG WITH THE FOLLOWING ADDITIONAL RECORDS, ON FILE FOR THREE (3) YEARS AFTER SUBMITTAL OF THE NOT AS OUTLINED IN SECTION 4. THIS DOES NOT INCLUDE ANY RECORDS AFTER SUBMITTAL OF THE NOT.

1. THE FINAL SWPPP;
2. ANY OTHER STORMWATER RELATED PERMITS REQUIRED FOR THE PROJECT;
3. RECORDS OF ALL INSPECTION AND MAINTENANCE CONDUCTED DURING CONSTRUCTION (SEE SECTION 11, INSPECTIONS AND MAINTENANCE);
4. ALL PERMANENT OPERATION AND MAINTENANCE AGREEMENTS THAT HAVE BEEN IMPLEMENTED, INCLUDING ALL RIGHT OF WAY, CONTRACTS, COVENANTS AND OTHER BINDING REQUIREMENTS REGARDING PERPETUAL MAINTENANCE; AND
5. ALL REQUIRED CALCULATIONS FOR DESIGN OF THE TEMPORARY AND PERMANENT STORMWATER MANAGEMENT SYSTEMS.

SWPPP IMPLEMENTATION RESPONSIBILITIES:

1. THE OWNER AND CONTRACTOR ARE PERMITTEE(S) AS IDENTIFIED BY THE NPDES PERMIT.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL IMPLEMENTATION OF THE SWPPP, INCLUDING THE ACTIVITIES OF ALL OF THE CONTRACTOR'S SUBCONTRACTORS.
3. CONTRACTOR SHALL PROVIDE A PERSON(S) KNOWLEDGEABLE AND EXPERIENCED IN THE APPLICATION OF EROSION PREVENTION AND SEDIMENT CONTROL BMPS TO OVERSEE ALL INSTALLATION AND MAINTENANCE OF BMPS AND IMPLEMENTATION OF THE SWPPP.
4. CONTRACTOR SHALL PROVIDE PERSON(S) MEETING THE TRAINING REQUIREMENTS OF THE NPDES PERMIT TO CONDUCT INSPECTION AND MAINTENANCE OF ALL EROSION PREVENTION AND SEDIMENT CONTROL BMPS IN ACCORDANCE WITH THE REQUIREMENTS OF THE PERMIT. ONE OF THESE INDIVIDUAL(S) MUST BE AVAILABLE FOR AN ONSITE INSPECTION WITHIN 72 HOURS UPON REQUEST BY MPCA. CONTRACTOR SHALL PROVIDE TRAINING DOCUMENTATION FOR THESE INDIVIDUAL(S) AS REQUIRED BY THE NPDES PERMIT. THIS TRAINING DOCUMENTATION SHALL BE RECORDED IN OR WITH THE SWPPP BEFORE THE START OF CONSTRUCTION OR AS SOON AS THE PERSONNEL FOR THE PROJECT HAVE BEEN DETERMINED. DOCUMENTATION SHALL INCLUDE:
   - 4.1. NAMES OF THE PERSONNEL ASSOCIATED WITH THE PROJECT THAT ARE REQUIRED TO BE TRAINED PER SECTION 21 OF THE PERMIT.
   - 4.2. DATES OF TRAINING AND NAME OF INSTRUCTOR AND ENTITY PROVIDING TRAINING.
   - 4.3. CONTENT OF TRAINING COURSE OR WORKSHOP INCLUDING THE NUMBER OF HOURS OF TRAINING.
5. FOLLOWING FINAL STABILIZATION AND THE TERMINATION OF COVERAGE FOR THE NPDES PERMIT, THE OWNER IS EXPECTED TO FURNISH LONG TERM OPERATION AND MAINTENANCE (O & M) OF THE PERMANENT STORM WATER MANAGEMENT SYSTEM.

CONSTRUCTION ACTIVITY REQUIREMENTS

SWPPP AMENDMENTS (SECTION 6):

1. ONE OF THE INDIVIDUALS DESCRIBED IN ITEM 21.2.A OR ITEM 21.2.B OR ANOTHER QUALIFIED INDIVIDUAL MUST COMPLETE ALL SWPPP CHANGES. CHANGES INVOLVING THE USE OF A LESS STRINGENT BMP MUST INCLUDE A JUSTIFICATION DESCRIBING HOW THE REPLACEMENT BMP IS EFFECTIVE FOR THE SITE CHARACTERISTICS.
2. PERMITTEES MUST AMEND THE SWPPP TO INCLUDE ADDITIONAL OR MODIFIED BMPS AS NECESSARY TO CORRECT PROBLEMS IDENTIFIED OR ADDRESS SITUATIONS WHENEVER THERE IS A CHANGE IN DESIGN, CONSTRUCTION, OPERATION, MAINTENANCE, WEATHER OR SEASONAL CONDITIONS HAVING A SIGNIFICANT EFFECT ON THE DISCHARGE OF POLLUTANTS TO SURFACE WATERS OR GROUNDWATER.
3. PERMITTEES MUST AMEND THE SWPPP TO INCLUDE ADDITIONAL OR MODIFIED BMPS AS NECESSARY TO CORRECT PROBLEMS IDENTIFIED OR ADDRESS SITUATIONS WHENEVER INSPECTIONS OR INVESTIGATIONS BY THE SITE OWNER OR OPERATOR, USEPA OR MPCA OFFICIALS INDICATE THE SWPPP IS NOT EFFECTIVE IN ELIMINATING OR SIGNIFICANTLY MINIMIZING THE DISCHARGE OF POLLUTANTS TO SURFACE WATERS OR GROUNDWATER OR THE DISCHARGES ARE CAUSING WATER QUALITY STANDARD EXCEEDANCES (E.G., NUISANCE CONDITIONS AS DEFINED IN MINN. R. 7050.0210, SUBP. 2) OR THE SWPPP IS NOT CONSISTENT WITH THE OBJECTIVES OF A USEPA APPROVED TMDL.

BMP SELECTION AND INSTALLATION (SECTION 7):

1. PERMITTEES MUST SELECT, INSTALL, AND MAINTAIN THE BMPS IDENTIFIED IN THE SWPPP AND IN THIS PERMIT IN AN APPROPRIATE AND FUNCTIONAL MANNER AND IN ACCORDANCE WITH RELEVANT MANUFACTURER SPECIFICATIONS AND ACCEPTED ENGINEERING PRACTICES.

EROSION PREVENTION (SECTION 8):

1. BEFORE WORK BEGINS, PERMITTEES MUST DELINEATE THE LOCATION OF AREAS NOT TO BE DISTURBED.
2. PERMITTEES MUST MINIMIZE THE NEED FOR DISTURBANCE OF PORTIONS OF THE PROJECT WITH STEEP SLOPES. WHEN STEEP SLOPES MUST BE DISTURBED, PERMITTEES MUST USE TECHNIQUES SUCH AS PHASING AND STABILIZATION PRACTICES DESIGNED FOR STEEP SLOPES (E.G., SLOPE DRAINING AND TERRACING).
3. PERMITTEES MUST STABILIZE ALL EXPOSED SOIL AREAS, INCLUDING STOCKPILES. STABILIZATION MUST BE INITIATED IMMEDIATELY TO LIMIT SOIL EROSION WHEN CONSTRUCTION ACTIVITY HAS PERMANENTLY OR TEMPORARILY CEASED ON ANY PORTION OF THE SITE AND WILL NOT RESUME FOR A PERIOD EXCEEDING 14 CALENDAR DAYS. STABILIZATION MUST BE COMPLETED NO LATER THAN 14 CALENDAR DAYS AFTER THE CONSTRUCTION ACTIVITY HAS CEASED. STABILIZATION IS NOT REQUIRED ON CONSTRUCTED BASE COMPONENTS OF ROADS, PARKING LOTS AND SIMILAR SURFACES. STABILIZATION IS NOT REQUIRED ON TEMPORARY STOCKPILES WITHOUT SIGNIFICANT SILT, CLAY OR ORGANIC COMPONENTS (E.G., CLEAN AGGREGATE STOCKPILES, DEMOLITION CONCRETE STOCKPILES, SAND STOCKPILES) BUT PERMITTEES MUST PROVIDE SEDIMENT CONTROLS AT THE BASE OF THE STOCKPILE.
4. FOR PUBLIC WATERS THAT THE MINNESOTA DNR HAS PROMULGATED "WORK IN WATER RESTRICTIONS" DURING SPECIFIED FISH SPAWNING TIME FRAMES, PERMITTEES MUST COMPLETE STABILIZATION OF ALL EXPOSED SOIL AREAS WITHIN 200 FEET OF THE WATER'S EDGE, AND THAT DRAIN TO THESE WATERS, WITHIN 24 HOURS DURING THE RESTRICTION PERIOD.
5. PERMITTEES MUST STABILIZE THE NORMAL WETTED PERIMETER OF THE LAST 200 LINEAR FEET OF TEMPORARY OR PERMANENT DRAINAGE DITCHES OR SWALES THAT DRAIN WATER FROM THE SITE WITHIN 24 HOURS AFTER CONNECTING TO A SURFACE WATER OR PROPERTY EDGE. PERMITTEES MUST COMPLETE STABILIZATION OF REMAINING PORTIONS OF TEMPORARY OR PERMANENT DITCHES OR SWALES WITHIN 14 CALENDAR DAYS AFTER CONNECTING TO A SURFACE WATER OR PROPERTY EDGE AND CONSTRUCTION IN THAT PORTION OF THE DITCH TEMPORARILY OR PERMANENTLY CEASES.
6. TEMPORARY OR PERMANENT DITCHES OR SWALES BEING USED AS A SEDIMENT CONTAINMENT SYSTEM DURING CONSTRUCTION (WITH PROPERLY DESIGNED ROCK-DITCH CHECKS, BIO ROLLS, SILT DIKES, ETC.) DO NOT NEED TO BE STABILIZED. PERMITTEES MUST STABILIZE THESE AREAS WITHIN 24 HOURS AFTER THEIR USE AS A SEDIMENT CONTAINMENT SYSTEM CEASES
7. PERMITTEES MUST NOT USE MULCH, HYDROMULCH, TACKIFIER, POLYACRYLAMIDE OR SIMILAR EROSION PREVENTION PRACTICES WITHIN ANY PORTION OF THE NORMAL WETTED PERIMETER OF A TEMPORARY OR PERMANENT DRAINAGE DITCH OR SWALE SECTION WITH A CONTINUOUS SLOPE OF GREATER THAN 2 PERCENT.
8. PERMITTEES MUST PROVIDE TEMPORARY OR PERMANENT ENERGY DISSIPATION AT ALL PIPE OUTLETS WITHIN 24 HOURS AFTER CONNECTION TO A SURFACE WATER OR PERMANENT STORMWATER TREATMENT SYSTEM.
9. PERMITTEES MUST NOT DISTURB MORE LAND (I.E., PHASING) THAN CAN BE EFFECTIVELY INSPECTED AND MAINTAINED IN ACCORDANCE WITH SECTION 11.

SEDIMENT CONTROL (SECTION 9):

1. PERMITTEES MUST ESTABLISH SEDIMENT CONTROL BMPS ON ALL DOWNGRADIENT PERIMETERS OF THE SITE AND DOWNGRADIENT AREAS OF THE SITE THAT DRAIN TO ANY SURFACE WATER, INCLUDING CURB AND GUTTER SYSTEMS. PERMITTEES MUST LOCATE SEDIMENT CONTROL PRACTICES UPGRADIENT OF ANY BUFFER ZONES. PERMITTEES MUST INSTALL SEDIMENT CONTROL PRACTICES BEFORE ANY UPGRADIENT LAND-DISTURBING ACTIVITIES BEGIN AND MUST KEEP THE SEDIMENT CONTROL PRACTICES IN PLACE UNTIL THEY ESTABLISH PERMANENT COVER.
2. IF DOWNGRADIENT SEDIMENT CONTROLS ARE OVERLOADED, BASED ON FREQUENT FAILURE OR EXCESSIVE MAINTENANCE REQUIREMENTS, PERMITTEES MUST INSTALL ADDITIONAL UPGRADIENT SEDIMENT CONTROL PRACTICES OR REDUNDANT BMPS TO ELIMINATE THE OVERLOADING AND AMEND THE SWPPP TO IDENTIFY THESE ADDITIONAL PRACTICES AS REQUIRED IN ITEM 6.3.
3. TEMPORARY OR PERMANENT DRAINAGE DITCHES AND SEDIMENT BASINS DESIGNED AS PART OF A SEDIMENT CONTAINMENT SYSTEM (E.G., DITCHES WITH ROCK-CHECK DAMS) REQUIRE SEDIMENT CONTROL PRACTICES ONLY AS APPROPRIATE FOR SITE CONDITIONS.
4. A FLOATING SILT CURTAIN PLACED IN THE WATER IS NOT A SEDIMENT CONTROL BMP TO SATISFY ITEM 9.2 EXCEPT WHEN WORKING ON A SHORELINE OR BELOW THE WATERLINE. IMMEDIATELY AFTER THE SHORT TERM CONSTRUCTION ACTIVITY (E.G., INSTALLATION OF RIP RAP ALONG THE SHORELINE) IN THAT AREA IS COMPLETE, PERMITTEES MUST INSTALL AN UPLAND PERIMETER CONTROL PRACTICE IF EXPOSED SOILS STILL DRAIN TO A SURFACE WATER.
5. PERMITTEES MUST RE-INSTALL ALL SEDIMENT CONTROL PRACTICES ADJUSTED OR REMOVED TO ACCOMMODATE SHORT-TERM ACTIVITIES SUCH AS CLEARING OR GRUBBING, OR PASSAGE OF VEHICLES, IMMEDIATELY AFTER THE SHORT-TERM ACTIVITY IS COMPLETED. PERMITTEES MUST RE-INSTALL SEDIMENT CONTROL PRACTICES BEFORE THE NEXT PRECIPITATION EVENT EVEN IF THE SHORT-TERM ACTIVITY IS NOT COMPLETE.
6. PERMITTEES MUST PROTECT ALL STORM DRAIN INLETS USING APPROPRIATE BMPS DURING CONSTRUCTION UNTIL THEY ESTABLISH PERMANENT COVER ON ALL AREAS WITH POTENTIAL FOR DISCHARGING TO THE INLET.
7. PERMITTEES MAY REMOVE INLET PROTECTION FOR A PARTICULAR INLET IF A SPECIFIC SAFETY CONCERN (E.G. STREET FLOODING/FREEZING) IS IDENTIFIED BY THE PERMITTEES OR THE JURISDICTIONAL AUTHORITY (E.G., CITY/COUNTY/TOWNSHIP/MINNESOTA DEPARTMENT OF TRANSPORTATION ENGINEER). PERMITTEES MUST DOCUMENT THE NEED FOR REMOVAL IN THE SWPPP.
8. PERMITTEES MUST PROVIDE SILT FENCE OR OTHER EFFECTIVE SEDIMENT CONTROLS AT THE BASE OF STOCKPILES ON THE DOWNGRADIENT PERIMETER.
9. PERMITTEES MUST LOCATE STOCKPILES OUTSIDE OF NATURAL BUFFERS OR SURFACE WATERS, INCLUDING STORMWATER CONVEYANCES SUCH AS CURB AND GUTTER SYSTEMS UNLESS THERE IS A BYPASS IN PLACE FOR THE STORMWATER.
10. PERMITTEES MUST INSTALL A VEHICLE TRACKING BMP TO MINIMIZE THE TRACK OUT OF SEDIMENT FROM THE CONSTRUCTION SITE OR ONTO PAVED ROADS WITHIN THE SITE.
11. PERMITTEES MUST USE STREET SWEEPING IF VEHICLE TRACKING BMPS ARE NOT ADEQUATE TO PREVENT SEDIMENT TRACKING ONTO THE STREET.
12. PERMITTEES MUST INSTALL TEMPORARY SEDIMENT BASINS AS REQUIRED IN SECTION 14.
13. IN ANY AREAS OF THE SITE WHERE FINAL VEGETATIVE STABILIZATION WILL OCCUR, PERMITTEES MUST RESTRICT VEHICLE AND EQUIPMENT USE TO MINIMIZE SOIL COMPACTION.
14. PERMITTEES MUST PRESERVE TOPSOIL ON THE SITE, UNLESS INFEASIBLE.
15. PERMITTEES MUST DIRECT DISCHARGES FROM BMPS TO VEGETATED AREAS UNLESS INFEASIBLE.
16. PERMITTEES MUST PRESERVE A 50 FOOT NATURAL BUFFER OR, IF A BUFFER IS INFEASIBLE ON THE SITE, PROVIDE REDUNDANT (DOUBLE) PERIMETER SEDIMENT CONTROLS WHEN A SURFACE WATER IS LOCATED WITHIN 50 FEET OF THE PROJECT'S EARTH DISTURBANCES AND STORMWATER FLOWS TO THE SURFACE WATER. PERMITTEES MUST INSTALL PERIMETER SEDIMENT CONTROLS AT LEAST 5 FEET APART UNLESS LIMITED BY LACK OF AVAILABLE SPACE. NATURAL BUFFERS ARE NOT REQUIRED ADJACENT TO ROAD DITCHES, JUDICIAL DITCHES, COUNTY DITCHES, STORMWATER CONVEYANCE CHANNELS, STORM DRAIN INLETS, AND SEDIMENT BASINS. IF PRESERVING THE BUFFER IS INFEASIBLE, PERMITTEES MUST DOCUMENT THE REASONS IN THE SWPPP. SHEET PILING IS A REDUNDANT PERIMETER CONTROL IF INSTALLED IN A MANNER THAT RETAINS ALL STORMWATER.
17. PERMITTEES MUST USE POLYMERS, FLOCCULANTS, OR OTHER SEDIMENTATION TREATMENT CHEMICALS IN ACCORDANCE WITH ACCEPTED ENGINEERING PRACTICES, DOSING SPECIFICATIONS AND SEDIMENT REMOVAL DESIGN SPECIFICATIONS PROVIDED BY THE MANUFACTURER OR SUPPLIER. THE PERMITTEES MUST USE CONVENTIONAL EROSION AND SEDIMENT CONTROLS PRIOR TO CHEMICAL ADDITION AND MUST DIRECT TREATED STORMWATER TO A SEDIMENT CONTROL SYSTEM FOR FILTRATION OR SETTLEMENT OF THE FLOC PRIOR TO DISCHARGE.

DEWATERING AND BASIN DRAINING (SECTION 10):

1. PERMITTEES MUST DISCHARGE TURBID OR SEDIMENT-LADEN WATERS RELATED TO DEWATERING OR BASIN DRAINING (E.G., PUMPED DISCHARGES, TRENCH/DITCH CUTS FOR DRAINAGE) TO A TEMPORARY OR PERMANENT SEDIMENT BASIN ON THE PROJECT SITE UNLESS INFEASIBLE. PERMITTEES MAY DEWATER TO SURFACE WATERS IF THEY VISUALLY CHECK TO ENSURE ADEQUATE TREATMENT HAS BEEN OBTAINED AND NUISANCE CONDITIONS (SEE MINN. R. 7050.0210, SUBP. 2) WILL NOT RESULT FROM THE DISCHARGE. IF PERMITTEES CANNOT DISCHARGE THE WATER TO A SEDIMENTATION BASIN PRIOR TO ENTERING A SURFACE WATER, PERMITTEES MUST TREAT IT WITH APPROPRIATE BMPS SUCH THAT THE DISCHARGE DOES NOT ADVERSELY AFFECT THE SURFACE WATER OR DOWNSTREAM PROPERTIES.
2. IF PERMITTEES MUST DISCHARGE WATER CONTAINING OIL OR GREASE, THEY MUST USE AN OIL-WATER SEPARATOR OR SUITABLE FILTRATION DEVICE (E.G., CARTRIDGE FILTERS, ABSORBENTS PADS) PRIOR TO DISCHARGE.
3. PERMITTEES MUST DISCHARGE ALL WATER FROM DEWATERING OR BASIN-DRAINING ACTIVITIES IN A MANNER THAT DOES NOT CAUSE EROSION OR SCOUR IN THE IMMEDIATE VICINITY OF DISCHARGE POINTS OR INUNDATION OF WETLANDS IN THE IMMEDIATE VICINITY OF DISCHARGE POINTS THAT CAUSES SIGNIFICANT ADVERSE IMPACT TO THE WETLAND.
4. IF PERMITTEES USE FILTERS WITH BACKWASH WATER, THEY MUST HAUL THE BACKWASH WATER AWAY FOR DISPOSAL, RETURN THE BACKWASH WATER TO THE BEGINNING OF THE TREATMENT PROCESS, OR INCORPORATE THE BACKWASH WATER INTO THE SITE IN A MANNER THAT DOES NOT CAUSE EROSION.

INSPECTIONS AND MAINTENANCE (SECTION 11):

1. PERMITTEES MUST ENSURE A TRAINED PERSON, AS IDENTIFIED IN ITEM 21.2.B, WILL INSPECT THE ENTIRE CONSTRUCTION SITE AT LEAST ONCE EVERY SEVEN (7) DAYS DURING ACTIVE CONSTRUCTION AND WITHIN 24 HOURS AFTER A RAINFALL EVENT GREATER THAN 1/2 INCH IN 24 HOURS.
2. PERMITTEES MUST INSPECT AND MAINTAIN ALL PERMANENT STORMWATER TREATMENT BMPS.
3. PERMITTEES MUST INSPECT ALL EROSION PREVENTION AND SEDIMENT CONTROL BMPS AND POLLUTION PREVENTION MANAGEMENT MEASURES TO ENSURE INTEGRITY AND EFFECTIVENESS. PERMITTEES MUST REPAIR, REPLACE OR SUPPLEMENT ALL NONFUNCTIONAL BMPS WITH FUNCTIONAL BMPS BY THE END OF THE NEXT BUSINESS DAY AFTER DISCOVERY UNLESS ANOTHER TIME FRAME IS SPECIFIED IN ITEM 11.5 OR 11.6. PERMITTEES MAY TAKE ADDITIONAL TIME IF FIELD CONDITIONS PREVENT ACCESS TO THE AREA.
4. DURING EACH INSPECTION, PERMITTEES MUST INSPECT SURFACE WATERS, INCLUDING DRAINAGE DITCHES AND CONVEYANCE SYSTEMS BUT NOT CURB AND GUTTER SYSTEMS, FOR EVIDENCE OF EROSION AND SEDIMENT DEPOSITION. PERMITTEES MUST REMOVE ALL DELTAS AND SEDIMENT DEPOSITED IN SURFACE WATERS, INCLUDING DRAINAGE WAYS, CATCH BASINS, AND OTHER DRAINAGE SYSTEMS AND RESTABILIZE THE AREAS WHERE SEDIMENT REMOVAL RESULTS IN EXPOSED SOIL. PERMITTEES MUST COMPLETE REMOVAL AND STABILIZATION WITHIN SEVEN (7) CALENDAR DAYS OF DISCOVERY UNLESS PRECLUDED BY LEGAL, REGULATORY, OR PHYSICAL ACCESS CONSTRAINTS. PERMITTEES MUST USE ALL REASONABLE EFFORTS TO OBTAIN ACCESS. IF PRECLUDED, REMOVAL AND STABILIZATION MUST TAKE PLACE WITHIN SEVEN (7) DAYS OF OBTAINING ACCESS. PERMITTEES ARE RESPONSIBLE FOR CONTACTING ALL LOCAL, REGIONAL, STATE AND FEDERAL AUTHORITIES AND RECEIVING ANY APPLICABLE PERMITS, PRIOR TO CONDUCTING ANY WORK IN SURFACE WATERS.
5. PERMITTEES MUST INSPECT CONSTRUCTION SITE VEHICLE EXIT LOCATIONS, STREETS AND CURB AND GUTTER SYSTEMS WITHIN AND ADJACENT TO THE PROJECT FOR SEDIMENTATION FROM EROSION OR TRACKED SEDIMENT FROM VEHICLES. PERMITTEES MUST REMOVE SEDIMENT FROM ALL PAVED SURFACES WITHIN ONE (1) CALENDAR DAY OF DISCOVERY OR, IF APPLICABLE, WITHIN A SHORTER TIME TO AVOID A SAFETY HAZARD TO USERS OF PUBLIC STREETS.
6. PERMITTEES MUST REPAIR, REPLACE OR SUPPLEMENT ALL PERIMETER CONTROL DEVICES WHEN THEY BECOME NONFUNCTIONAL OR THE SEDIMENT REACHES 1/2 OF THE HEIGHT OF THE DEVICE.
7. PERMITTEES MUST DRAIN TEMPORARY AND PERMANENT SEDIMENTATION BASINS AND REMOVE THE SEDIMENT WHEN THE DEPTH OF SEDIMENT COLLECTED IN THE BASIN REACHES 1/2 THE STORAGE VOLUME.
8. PERMITTEES MUST ENSURE THAT AT LEAST ONE INDIVIDUAL PRESENT ON THE SITE (OR AVAILABLE TO THE PROJECT SITE IN THREE (3) CALENDAR DAYS) IS TRAINED IN THE JOB DUTIES DESCRIBED IN ITEM 21.2.B.
9. PERMITTEES MAY ADJUST THE INSPECTION SCHEDULE DESCRIBED IN ITEM 11.2 AS FOLLOWS:
   - a. INSPECTIONS OF AREAS WITH PERMANENT COVER CAN BE REDUCED TO ONCE PER MONTH, EVEN IF CONSTRUCTION ACTIVITY CONTINUES ON OTHER PORTIONS OF THE SITE; OR
   - b. WHERE SITES HAVE PERMANENT COVER ON ALL EXPOSED SOIL AND NO CONSTRUCTION ACTIVITY IS OCCURRING ANYWHERE ON THE SITE, INSPECTIONS CAN BE REDUCED TO ONCE PER MONTH AND, AFTER 12 MONTHS, MAY BE SUSPENDED COMPLETELY UNTIL CONSTRUCTION ACTIVITY RESUMES. THE MPCA MAY REQUIRE INSPECTIONS TO RESUME IF CONDITIONS WARRANT; OR
   - c. WHERE CONSTRUCTION ACTIVITY HAS BEEN SUSPENDED DUE TO FROZEN GROUND CONDITIONS, INSPECTIONS MAY BE SUSPENDED. INSPECTIONS MUST RESUME WITHIN 24 HOURS OF RUNOFF OCCURRING, OR UPON RESUMING CONSTRUCTION, WHICHEVER COMES FIRST.
10. PERMITTEES MUST RECORD ALL INSPECTIONS AND MAINTENANCE ACTIVITIES WITHIN 24 HOURS OF BEING CONDUCTED AND THESE RECORDS MUST BE RETAINED WITH THE SWPPP. THESE RECORDS MUST INCLUDE:
   - a. DATE AND TIME OF INSPECTIONS; AND
   - b. NAME OF PERSONS CONDUCTING INSPECTIONS; AND
   - c. ACCURATE FINDINGS OF INSPECTIONS, INCLUDING THE SPECIFIC LOCATION WHERE CORRECTIVE ACTIONS ARE NEEDED; AND
   - d. CORRECTIVE ACTIONS TAKEN (INCLUDING DATES, TIMES, AND PARTY COMPLETING MAINTENANCE ACTIVITIES); AND
   - e. DATE OF ALL RAINFALL EVENTS GREATER THAN 1/2 INCHES IN 24 HOURS, AND THE AMOUNT OF RAINFALL FOR EACH EVENT. PERMITTEES MUST OBTAIN RAINFALL AMOUNTS BY EITHER A PROPERLY MAINTAINED RAIN GAUGE INSTALLED ONSITE, A WEATHER STATION THAT IS WITHIN ONE (1) MILE OF YOUR LOCATION, OR A WEATHER REPORTING SYSTEM THAT PROVIDES SITE SPECIFIC RAINFALL DATA FROM RADAR SUMMARIES; AND
   - f. IF PERMITTEES OBSERVE A DISCHARGE DURING THE INSPECTION, THEY MUST RECORD AND SHOULD PHOTOGRAPH AND DESCRIBE THE LOCATION OF THE DISCHARGE (I.E., COLOR, ODOR, SETTLED OR SUSPENDED SOLIDS, OIL SHEEN, AND OTHER OBVIOUS INDICATORS OF POLLUTANTS); AND
   - g. ANY AMENDMENTS TO THE SWPPP PROPOSED AS A RESULT OF THE INSPECTION MUST BE DOCUMENTED AS REQUIRED IN SECTION 6 WITHIN SEVEN (7) CALENDAR DAYS.

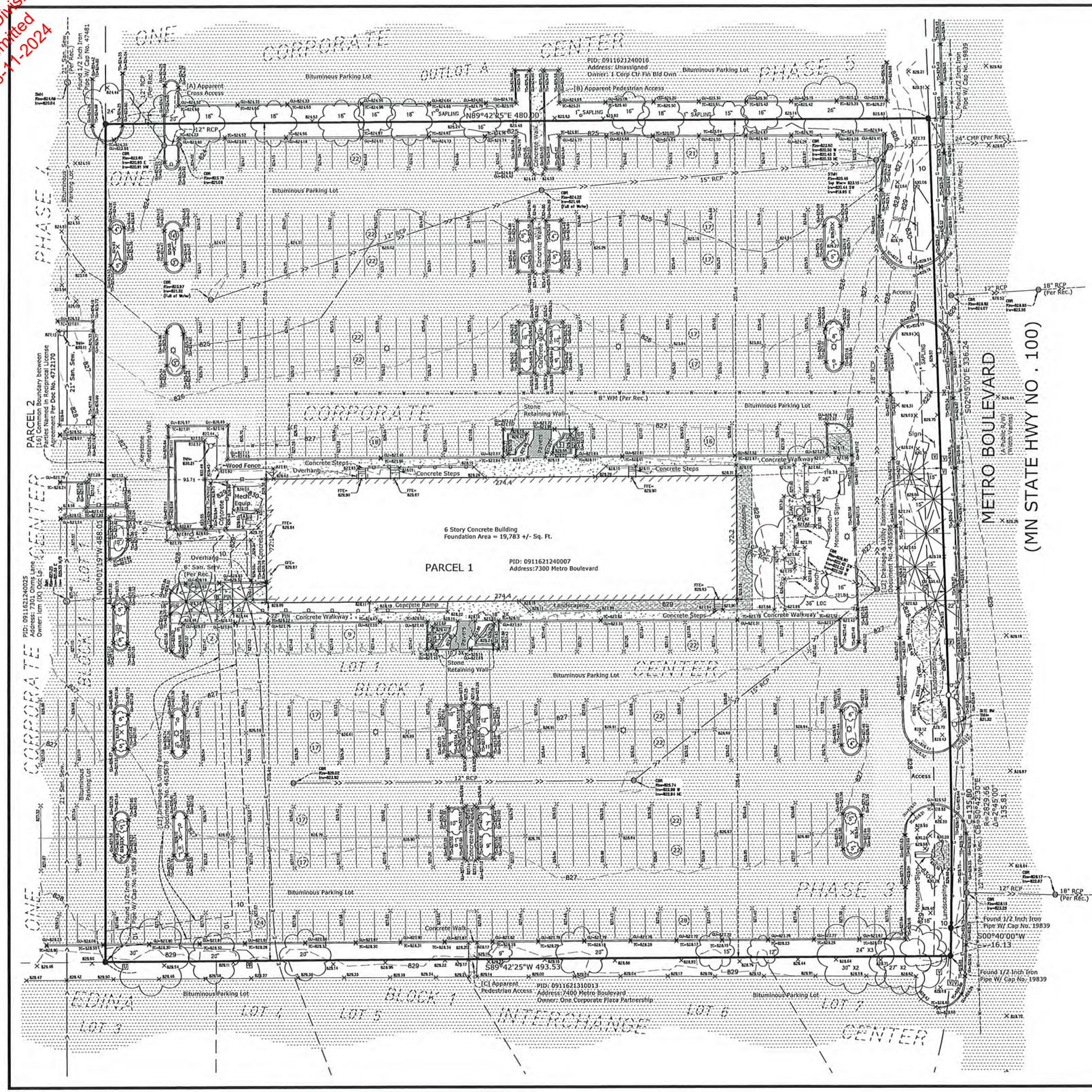
POLLUTION PREVENTION MANAGEMENT (SECTION 12):

1. PERMITTEES MUST PLACE BUILDING PRODUCTS AND LANDSCAPE MATERIALS UNDER COVER (E.G., PLASTIC SHEETING OR TEMPORARY ROOFS) OR PROTECT THEM BY SIMILARLY EFFECTIVE MEANS DESIGNED TO MINIMIZE CONTACT WITH STORMWATER. PERMITTEES ARE NOT REQUIRED TO COVER OR PROTECT PRODUCTS WHICH ARE EITHER NOT A SOURCE OF CONTAMINATION TO STORMWATER OR ARE DESIGNED TO BE EXPOSED TO STORMWATER.
2. PERMITTEES MUST PLACE PESTICIDES, FERTILIZERS AND TREATMENT CHEMICALS UNDER COVER (E.G., PLASTIC SHEETING OR TEMPORARY ROOFS) OR PROTECT THEM BY SIMILARLY EFFECTIVE MEANS DESIGNED TO MINIMIZE CONTACT WITH STORMWATER.
3. PERMITTEES MUST STORE HAZARDOUS MATERIALS AND TOXIC WASTE, (INCLUDING OIL, DIESEL FUEL, GASOLINE, HYDRAULIC FLUIDS, PAINT SOLVENTS, PETROLEUM-BASED PRODUCTS, WOOD PRESERVATIVES, ADDITIVES, CURING COMPOUNDS, AND ACIDS) IN SEALED CONTAINERS TO PREVENT SPILLS, LEAKS OR OTHER DISCHARGE. STORAGE AND DISPOSAL OF HAZARDOUS WASTE MATERIALS MUST BE IN COMPLIANCE WITH MINN. R. CH. 7045 INCLUDING SECONDARY CONTAINMENT AS APPLICABLE.
4. PERMITTEES MUST PROPERLY STORE, COLLECT AND DISPOSE SOLID WASTE IN COMPLIANCE WITH MINN. R. CH. 7035.
5. PERMITTEES MUST POSITION PORTABLE TOILETS SO THEY ARE SECURE AND WILL NOT TIP OR BE KNOCKED OVER. PERMITTEES MUST PROPERLY DISPOSE SANITARY WASTE IN ACCORDANCE WITH MINN. R. CH. 7041.
6. PERMITTEES MUST TAKE REASONABLE STEPS TO PREVENT THE DISCHARGE OF SPILLED OR LEAKED CHEMICALS, INCLUDING FUEL, FROM ANY AREA WHERE CHEMICALS OR FUEL WILL BE LOADED OR UNLOADED INCLUDING THE USE OF DRIP PANS OR ABSORBENTS UNLESS INFEASIBLE. PERMITTEES MUST ENSURE ADEQUATE SUPPLIES ARE AVAILABLE AT ALL TIMES TO CLEAN UP DISCHARGED MATERIALS AND THAT AN APPROPRIATE DISPOSAL METHOD IS AVAILABLE FOR RECOVERED SPILLED MATERIALS. PERMITTEES MUST REPORT AND CLEAN UP SPILLS IMMEDIATELY AS REQUIRED BY MINN. STAT. 115.061, USING DRY CLEAN UP MEASURES WHERE POSSIBLE.
7. PERMITTEES MUST LIMIT VEHICLE EXTERIOR WASHING AND EQUIPMENT TO A DEFINED AREA OF THE SITE. PERMITTEES MUST CONTAIN RUNOFF FROM THE WASHING AREA IN A SEDIMENT BASIN OR OTHER SIMILARLY EFFECTIVE CONTROLS AND MUST DISPOSE WASTE FROM THE WASHING ACTIVITY PROPERLY. PERMITTEES MUST PROPERLY USE AND STORE SOAPS, DETERGENTS, OR SOLVENTS.
8. PERMITTEES MUST PROVIDE EFFECTIVE CONTAINMENT FOR ALL LIQUID AND SOLID WASTES GENERATED BY WASHOUT OPERATIONS (E.G., CONCRETE, STUCCO, PAINT, FORM RELEASE OILS, CURING COMPOUNDS AND OTHER CONSTRUCTION MATERIALS) RELATED TO THE CONSTRUCTION ACTIVITY. PERMITTEES MUST PREVENT LIQUID AND SOLID WASHOUT WASTES FROM CONTACTING THE GROUND AND MUST DESIGN THE CONTAINMENT SO IT DOES NOT RESULT IN RUNOFF FROM THE WASHOUT OPERATIONS OR AREAS. PERMITTEES MUST PROPERLY DISPOSE LIQUID AND SOLID WASTES IN COMPLIANCE WITH MPCA RULES. PERMITTEES MUST INSTALL A SIGN INDICATING THE LOCATION OF THE WASHOUT FACILITY.

PERMIT TERMINATION (SECTION 4 AND SECTION 13):

1. PERMITTEES MUST SUBMIT A NOT WITHIN 30 DAYS AFTER ALL TERMINATION CONDITIONS LISTED IN SECTION 13 ARE COMPLETE.
2. PERMITTEES MUST SUBMIT A NOT WITHIN 30 DAYS AFTER SELLING OR OTHERWISE LEGALLY TRANSFERRING THE ENTIRE SITE, INCLUDING PERMIT RESPONSIBILITY FOR ROADS (E.G., STREET SWEEPING) AND STORMWATER INFRASTRUCTURE FINAL CLEAN OUT, OR TRANSFERRING PORTIONS OF A SITE TO ANOTHER PARTY. THE PERMITTEES' COVERAGE UNDER THIS PERMIT TERMINATES AT MIDNIGHT ON THE SUBMISSION DATE OF THE NOT.
3. PERMITTEES MUST COMPLETE ALL CONSTRUCTION ACTIVITY AND MUST INSTALL PERMANENT COVER OVER ALL AREAS PRIOR TO SUBMITTING THE NOT. VEGETATIVE COVER MUST CONSIST OF A UNIFORM PERENNIAL VEGETATION WITH A DENSITY OF 70 PERCENT OF ITS EXPECTED FINAL GROWTH. VEGETATION IS NOT REQUIRED WHERE THE FUNCTION OF A SPECIFIC AREA DICTATES NO VEGETATION, SUCH AS IMPERVIOUS SURFACES OR THE BASE OF A SAND FILTER.
4. PERMITTEES MUST CLEAN THE PERMANENT STORMWATER TREATMENT SYSTEM OF ANY ACCUMULATED SEDIMENT AND MUST ENSURE THE SYSTEM MEETS ALL APPLICABLE REQUIREM

Planning Division
Submitted
03-11-2024



DESCRIPTION OF PROPERTY SURVEYED

Parcel 1:

Lot 1, Block 1, Corporate Center Phase 3, according to the recorded plat thereof, Hennepin County, Minnesota.

Parcel 2:

Reciprocal License Agreement dated April 21, 1982, recorded April 21, 1982, as Document No. 4712170, in the Office of the County Recorder, Hennepin County, Minnesota.

(Abstract Property)

GENERAL SURVEY NOTES

1. Bearings are based on the Hennepin County County Coordinate System (1986 Adjustment).
2. Elevations are based on the NGVD 29 Datum. Site Benchmark is the top nut of the fire hydrant located on the west side of Metro Blvd, approximately 135 feet north of the southeast corner of the subject property, as shown hereon. Elevation = 831.32
3. We have shown the location of utilities to the best of our ability based on observed evidence together with evidence from the following sources: plans obtained from utility companies, plans provided by client, markings by utility companies and other appropriate sources. We have used this information to develop a view of the underground utilities for this site. However, lacking excavation, the exact location of underground features cannot be accurately, completely and reliably depicted. Where additional or more detailed information is required, the client is advised that excavation may be necessary. Also, please note that seasonal conditions may inhibit our ability to visibly observe all the utilities located on the subject property.

ALTA/NSPS LAND TITLE SURVEY NOTES

(numbered per Table A)

1. Monuments placed and/or found at all major corners of the boundary of the surveyed property as shown hereon.
2. Site Address: 7300 Metro Boulevard, Edina, Minnesota 55439.
3. This property is contained in Zone X (area determined to be outside the 0.2% annual chance floodplain) per Flood Insurance Rate Map, Community Panel No. 27053C0451F, effective date of November 4, 2016.
4. The Gross land area is 238,321 +/- square feet or 5.471 +/- acres.
7. (a) Exterior dimensions of buildings at ground level as shown hereon.
8. Substantial features observed in the process of conducting the fieldwork as shown hereon. Please note that seasonal conditions may inhibit our ability to visibly observed all site features located on the subject property.
9. The number of striped parking stalls on this site are as follows: 465 Regular + 9 Handicap = 474 Total Parking Stalls.
13. The names of the adjoining owners of the platted lands, as shown hereon, are based on information obtained from the Hennepin County Interactive Property Map.

SURVEY REPORT

1. This map and report was prepared with the benefit of a Commitment for Title Insurance issued by First American Title Insurance Company, File No. NCS-1192508-MPLS, dated September 17, 2023. We note the following with regards to Schedule B II Exceptions of the herein referenced Title Commitment:
   a. Item no.'s 1-11, 14 and 17-21 are not survey related.
   b. The following are numbered per the referenced Title Commitment:
      [12]. Drainage and utility easements as shown on the recorded plat of One Corporate Center Phase 3 recorded December 4, 1979, as Document No. 4526595. ***Easement as shown hereon along the easterly 10 feet of the subject property.***
      [13]. Easement for drainage and utility purposes in favor of the City of Edina, a Minnesota municipal corporation and the terms and provisions as contained in Easement dated December 12, 1980, recorded January 6, 1981 as Document No. 4615678. ***10 foot wide easement is depicted heron in the southwest portion of the subject property.***
      [15]. Easement for the provision of cable communications and other services and access rights in favor of the Minneapolis Division of Time Warner Cable Inc., doing business as Time Warner Cable and the terms, conditions and provisions as contained in Easement For Cable/Telecommunications Services dated January 21, 2004, recorded February 15, 2006, as Document No. 8749497. ***The Easement affects the subject property. The Easement is blanket in nature and cannot be depicted hereon.***
      [16]. Terms, conditions, provisions, covenants and agreements as contained in Reciprocal License Agreement dated April 21, 1982, recorded April 21, 1982, as Document No. 4712170. ***Agreement allows for free and unrestricted flow of vehicular or pedestrian traffic over the common boundary line between the subject property and the adjacent property to the west.***
2. Conflicts such as (but not limited to): encroachments, protrusions, access, occupation, and easements and/or servitudes:
   [A]. There is an apparent cross access between the subject property and the adjacent property to the north, as shown hereon near the northwest corner of the subject property. We have not been provided with documentation for said cross access.
   [B]. There is an apparent pedestrian access across the north line of the subject property, as shown hereon. We have not been provided with documentation for said pedestrian access.
   [C]. There is an apparent pedestrian access across the south line of the subject property, as shown hereon. We have not been provided with documentation for said pedestrian access.

ALTA CERTIFICATION

To: Won Huh LLC, a Minnesota limited liability company; Revitalize OCC, LLC, a Minnesota limited liability company; Emerald City Capital, LLC, a Wyoming limited liability company; and First American Title Insurance Company:

This is to certify that this map or plat and the survey on which it is based were made in accordance with the 2021 Minimum Standard Detail Requirements for ALTA/NSPS Land Title Surveys, jointly established and adopted by ALTA and NSPS, and includes Items 1, 2, 3, 4, 7(a), 8, 9, and 13 of Table A thereof.

The fieldwork was completed on 10-20-2023.

Dated this 27th day of October, 2023.

Rory L. Synstelien — Minnesota License No. 44565
rory@civilsitegroup.com

N

30 15 0 15 30 60

SCALE IN FEET

Linetype & Symbol Legend

E	ELECTRIC LINE
E E	ELECTRIC LINE (RECORD)
F	FIBER/COMM. LINE
F F	FIBER/COMM. LINE (RECORD)
G	GASMAIN
G G	GASMAIN (RECORD)
OH	OVERHEAD UTILITIES
>	SANITARY SEWER
> >	SANITARY SEWER (RECORD)
>>	STORM SEWER
>> >>	STORM SEWER (RECORD)
T	TELEPHONE LINE
T T	TELEPHONE LINE (RECORD)
W	WATERMAIN
W W	WATERMAIN (RECORD)
x	CHAINLINK FENCELINE
	WOODEN FENCELINE
o o	GUARDRAIL
Δ	ACCESS RESTRICTION
	CONCRETE SURFACE
	PAVER SURFACE
	BITUMINOUS SURFACE
	GRAVEL/LANDSCAPE SURFACE
A	AIR CONDITIONER
S	CABLE TV BOX
E	ELECTRIC MANHOLE
E	ELECTRIC TRANSFORMER
E	ELECTRICAL METER
F	FIBER/COMM. MANHOLE
	POWER POLE
	GUY WIRE
G	GAS METER
G	GAS MANHOLE
G	GAS VALVE
R	ROOF DRAIN
S	SEWER CLEAN OUT
S	SANITARY MANHOLE
ST	STORM MANHOLE
	CATCH BASIN
	FLARED END SECTION
T	TELEPHONE BOX
T	TELEPHONE MANHOLE
T	TRAFFIC SIGNAL
	HYDRANT
	FIRE CONNECTION
P	POST INDICATOR VALVE
W	WATER MANHOLE
W	WATER VALVE
W	WELL
V	UTILITY VAULT
U	UTILITY MANHOLE
E	ELECTRICAL OUTLET
H	HAND HOLE
	BOLLARD
	FLAG POLE
FT	FUEL TANK
	HANDICAP SYMBOL
	LIGHT POLE
M	MAIL BOX
	SIGN
	CONIFEROUS TREE
	DECIDUOUS TREE
SB	SOIL BORING
	FOUND IRON MONUMENT
	SET OR TO BE SET IRON MONUMENT
	CAST IRON MONUMENT

CivilSite GROUP
Civil Engineering • Surveying • Landscape Architecture
5000 Glenwood Avenue
Golden Valley, MN 55422
civilsitegroup.com 612-615-0060

PROJECT
Edina Dental Office
7300 Metro Boulevard, Edina, Minnesota 55439

CLIENT
DJR Architecture, Inc.
333 Washington Avenue N, #210, Minneapolis, Minnesota 55401

I HEREBY CERTIFY THAT THIS SURVEY, PLAN, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED LAND SURVEYOR UNDER THE LAWS OF THE STATE OF MINNESOTA.

RORY L. SYNSTELIEN
DATE 10-27-2023 LICENSE NO. 44565

QA/QC

FIELD CREW	DH/HS
DRAWN BY	DS
REVIEWED BY	CJ
UPDATED BY	.

VICINITY MAP

REVISION SUMMARY

DATE	DESCRIPTION

PROJECT NO.: 23365.00

ALTA/NSPS LAND TITLE SURVEY

V1.0

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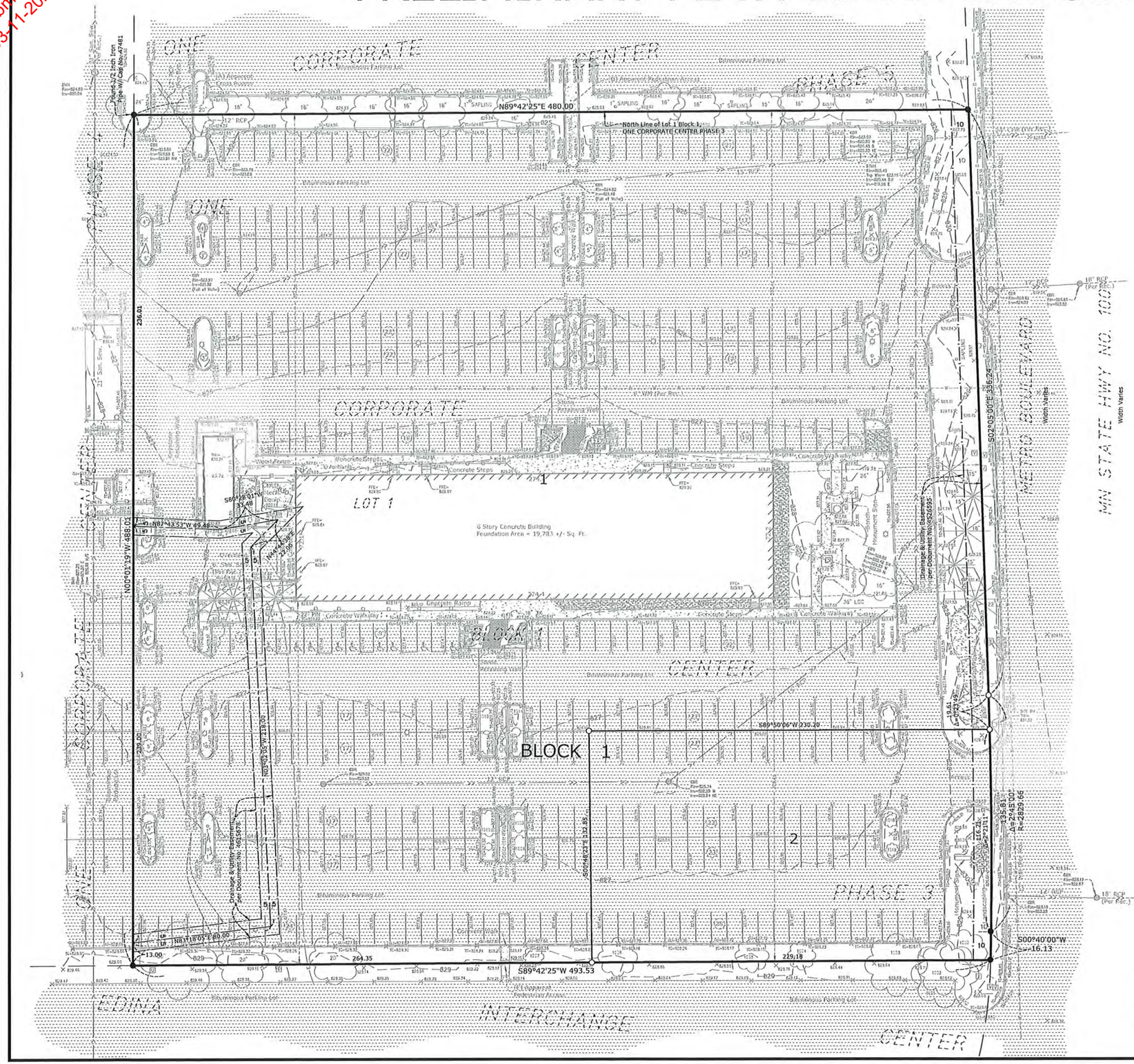
Planning Division
Submitted
03-11-2024

PRELIMINARY PLAT: EDINA ENDODONTICS



PRELIMINARY PLAT GENERAL NOTES

PROPERTY DESCRIPTION:
Lot 1, Block 1, Corporate Center Phase 3, according to the recorded plat thereof, Hennepin County, Minnesota. (Abstract Property)

This preliminary plat was prepared with the benefit of a Commitment for Title Insurance issued by First American Title Insurance Company, File No. NCS-1192508-MPLS, dated September 17, 2023.

DATE OF PREPARATION:
03-05-2024
Please note background survey information is per a survey by Civil Site Group dated 10-27-2023 (Revised 2/15-2024)

APPLICANT:
Hempel Real Estate
Brent Jacobson
612-355-2609
bjacobson@hempelcompanies.com
1005 Crosstown Cir, Suite 600, Eden Prairie, MN 55344

BENCHMARKS:
The Benchmark is the top nut of the fire hydrant located on the west side of Metro Blvd, approximately 135 feet north of the southeast corner of the subject property, as shown hereon.
Elevation = 831.32 Feet (NGVD29)

AREAS:
Proposed Lot 1 = 207,802 Sq. Ft. or 4.770 Acres
Proposed Lot 2 = 30,519 Sq. Ft. or 0.701 Acres
Total Property Area = 238,321 Sq. Ft. or 5.471 Acres

FLOOD ZONE:
This property is contained in Zone X (area determined to be outside the 0.2% annual chance floodplain) per Flood Insurance Rate Map, Community Panel No. 27053C0451F, effective date of November 4, 2016.

Rory L. Synstelien — Minnesota License No. 44565
rory@civilsitegroup.com

N
30 15 0 15 30 60
SCALE IN FEET

Linetype & Symbol Legend

- ELECTRIC LINE
- ELECTRIC LINE (RECORD)
- FIBER/COMM. LINE
- FIBER/COMM. LINE (RECORD)
- GASMAIN
- GASMAIN (RECORD)
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- SANITARY SEWER (RECORD)
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- STORM SEWER (RECORD)
- TELEPHONE LINE
- TELEPHONE LINE (RECORD)
- WATERMAIN
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- CHAINLINK FENCELINE
- WOODEN FENCELINE
- GUARDRAIL
- ACCESS RESTRICTION
- CONCRETE SURFACE
- PAVER SURFACE
- BITUMINOUS SURFACE
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- ELECTRIC TRANSFORMER
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- UTILITY VAULT
- UTILITY MANHOLE
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- HANDICAP SYMBOL
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- CONIFEROUS TREE
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- SB SOIL BORING
- FOUND IRON MONUMENT
- SET OR TO BE SET IRON MONUMENT
- CAST IRON MONUMENT

CivilSite GROUP
Civil Engineering • Surveying • Landscape Architecture
5000 Glenwood Avenue
Golden Valley, MN 55422
civilsitegroup.com 612-615-0060

PROJECT: EDINA ENDODONTICS
7300 Metro Boulevard, Edina, Minnesota 55439

CLIENT: Hempel Real Estate
1005 Crosstown Cir, Suite 600, Eden Prairie, MN 55344

I HEREBY CERTIFY THAT THIS SURVEY, PLAN, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED LAND SURVEYOR UNDER THE LAWS OF THE STATE OF MINNESOTA.

RORY L. SYNSTELIEN
DATE 3-11-2024 LICENSE NO. 44565

Q/A/QC

FIELD CREW	.
DRAWN BY	DP
REVIEWED BY	CJ
UPDATED BY	.

VICINITY MAP

REVISION SUMMARY

DATE	DESCRIPTION
.	.
.	.
.	.
.	.

PROJECT NO.: 23365

PRELIMINARY PLAT

V2.0

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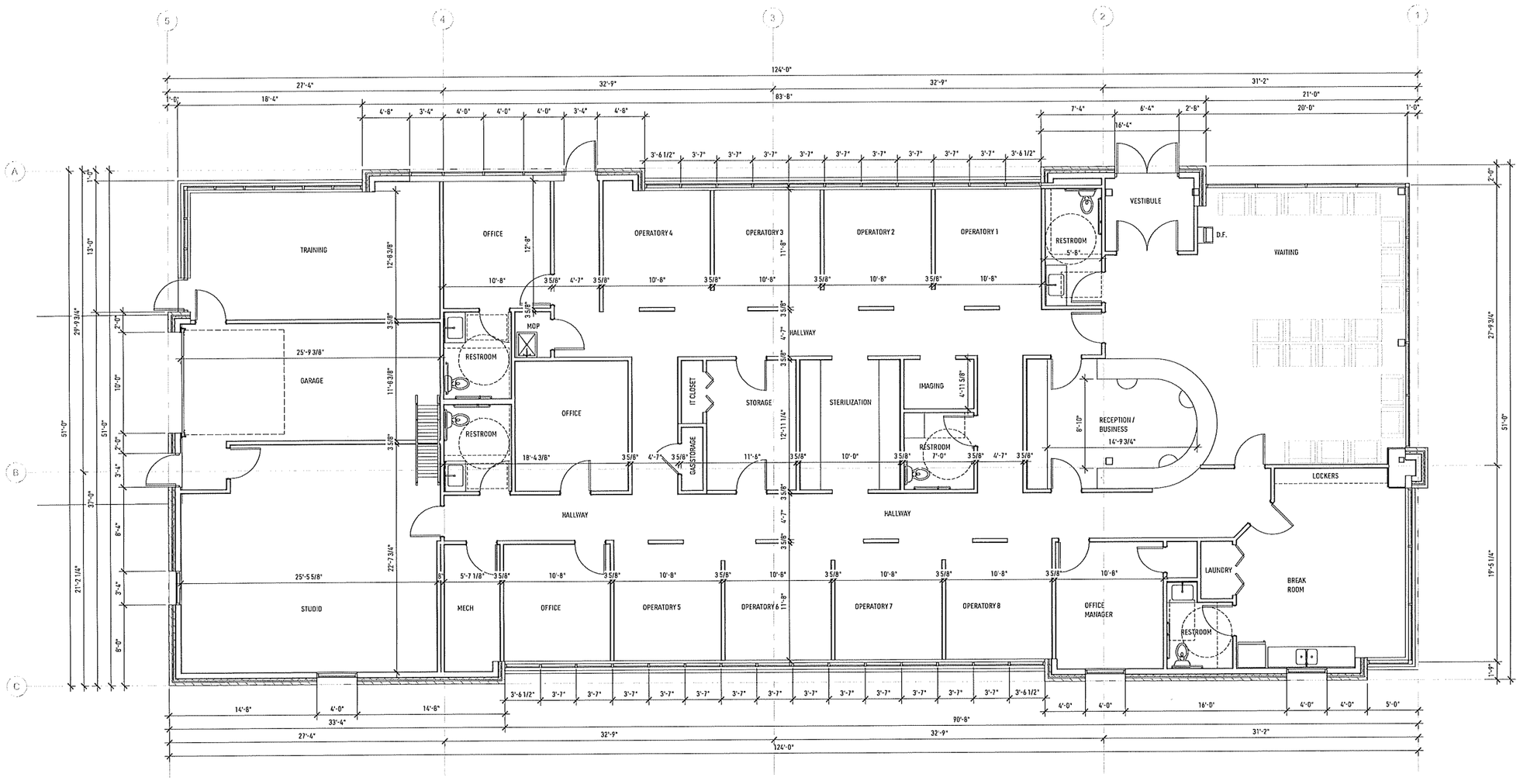
Planning Division
Submitted
03-11-2024

DJR



A1 FLOOR PLAN - LEVEL 1
3/16" = 1'-0"

CONCEPT DESIGN
3/11/24

EDINA
ENDODONTIC
EDINA, MN
23-147.00

SCHEMATIC PLAN

A1.0

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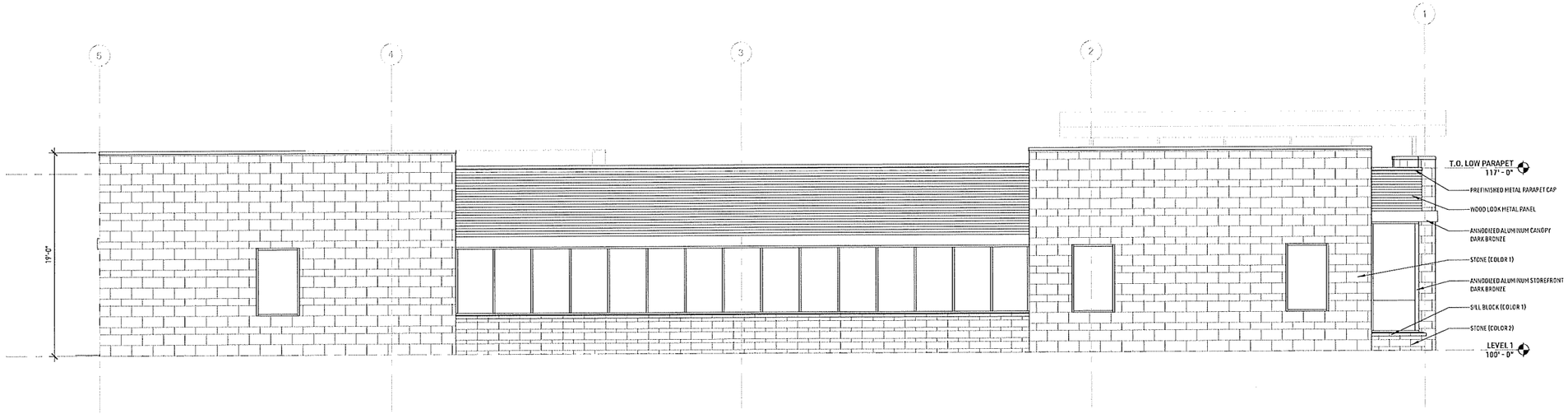
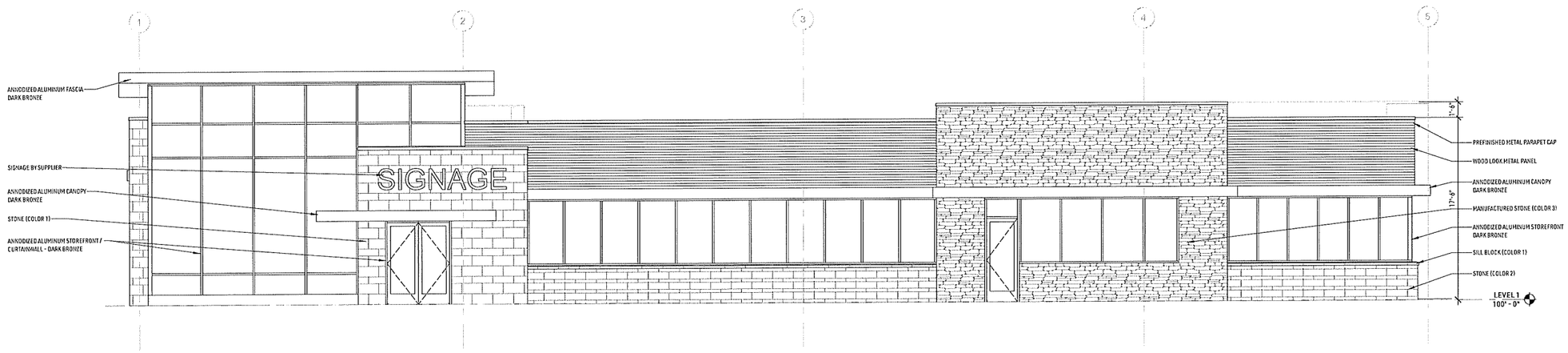
Planning Division
Submitted
03-11-2024



A3 SOUTH EXTERIOR ELEVATION
3/16" = 1'-0"



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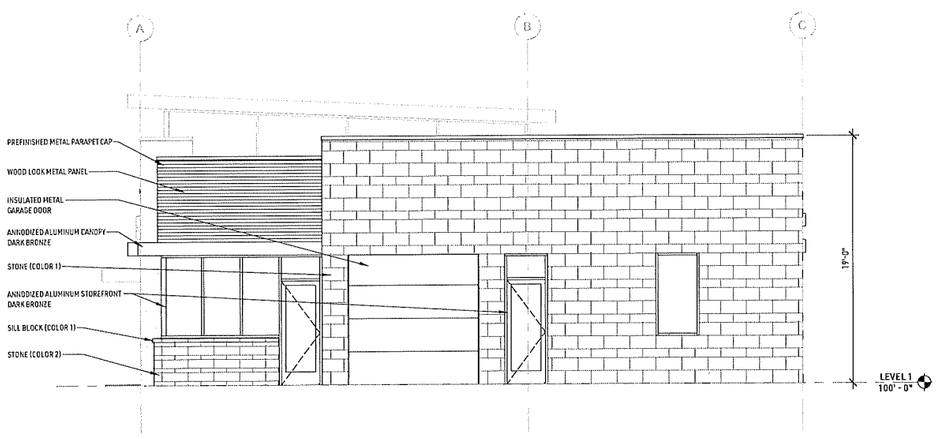

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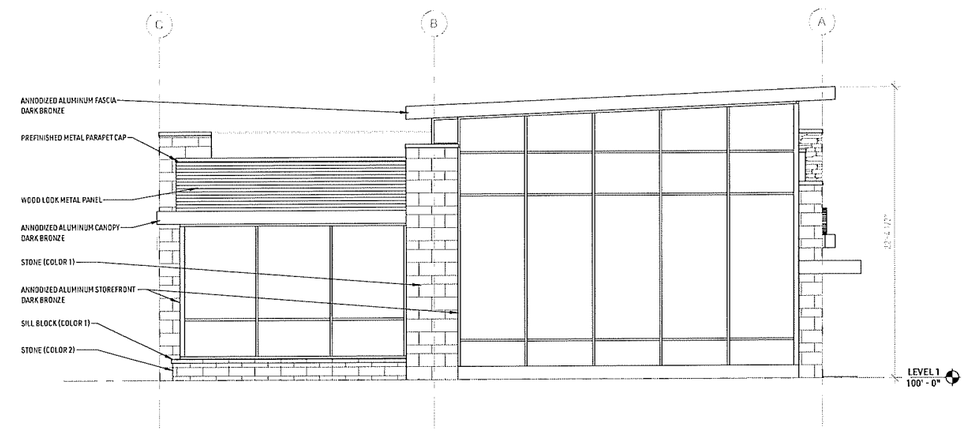

A1 EAST EXTERIOR ELEVATION
3/16" = 1'-0"

DJR

CONCEPT DESIGN
3/11/24

EDINA
ENDODONTIC
EDINA, MN
23-147.00

EXTERIOR ELEVATIONS

A2.0

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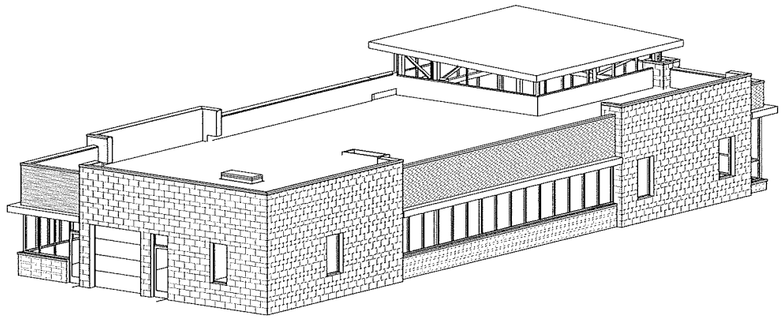
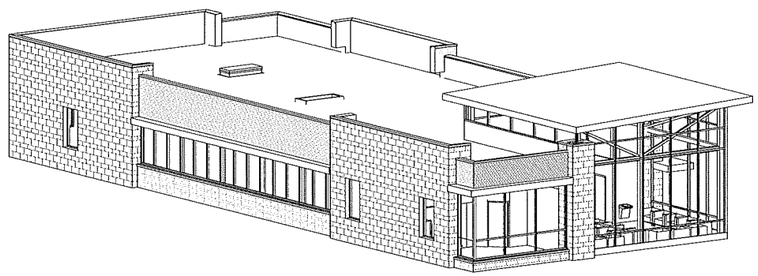
Submitted
03-11-2024



8 Axon SW



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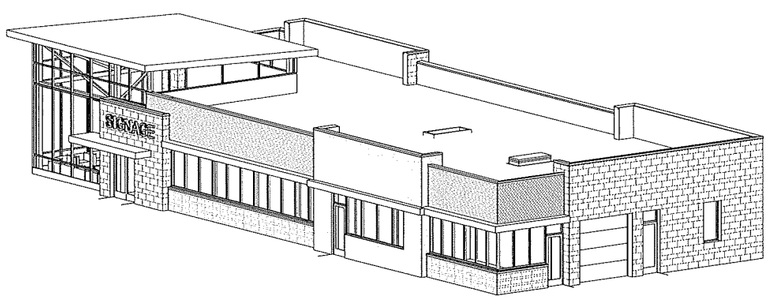

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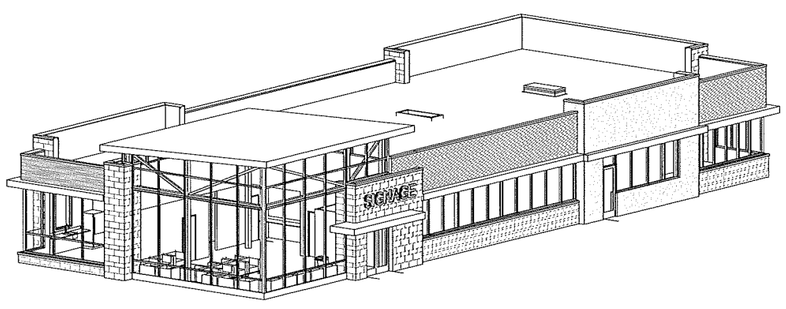

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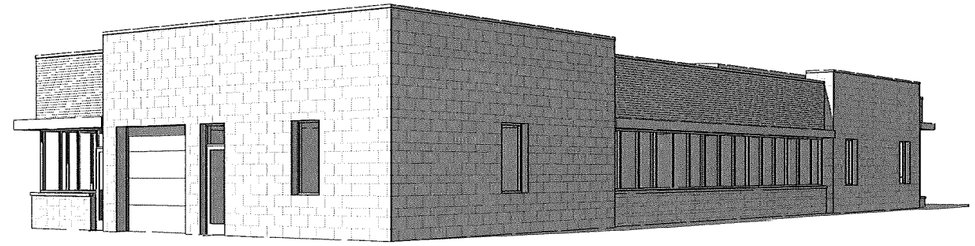
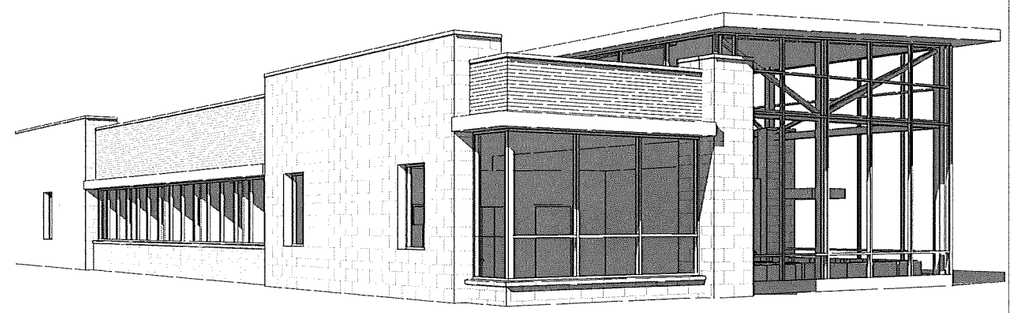
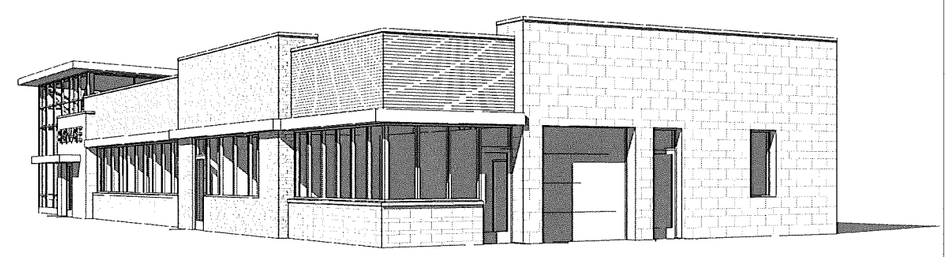


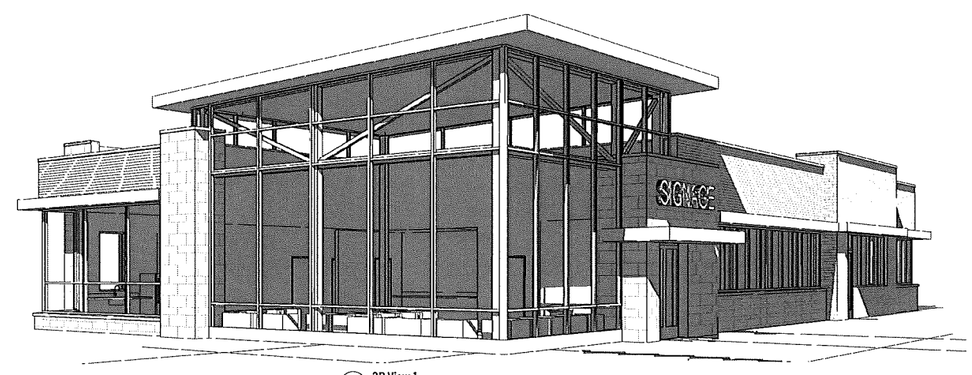
4 3D View 4



3 3D View 3



2 3D View 2



1 3D View 1

DJR

CONCEPT DESIGN
3/11/2024

EDINA
ENDODONTIC
EDINA, MN
23-147.00

BUILDING EXTERIOR

A2.1

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Planning Division
Submitted
03-11-2024

DJR

EXTERIOR RENDERING

3.11.2024

Site Plan Submittal - Edina Endodontic

7300 Metro Boulevard, Edina, MN 55439

23-147.00

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Planning Division
Submitted
03-11-2024

EXTERIOR RENDERING

3.11.2024

Site Plan Submittal - Edina Endodontic

7300 Metro Boulevard, Edina, MN 55439

23-147.00

DJR

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Planning Division
Submitted
03-11-2024

WOOD LOOK METAL PANEL - Longboard - Dark National Walnut

STOREFRONT / CURTAINWALL / CANOPIES
- Annodized Aluminum - Dark Bronze

STONE (COLOR 1) - US Stone - Plaza Grey

STONE (COLOR 2) - US Stone - Cottonwood

MANUFACTURED STONE (COLOR 3) - Coronado Stone - Black Forest

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Planning Division
Submitted
03-11-2024

DJR

SITE PHOTOS
3.11.2024

Site Plan Submittal - Edina Endodontic
7300 Metro Boulevard, Edina, MN 55439
23-147.00

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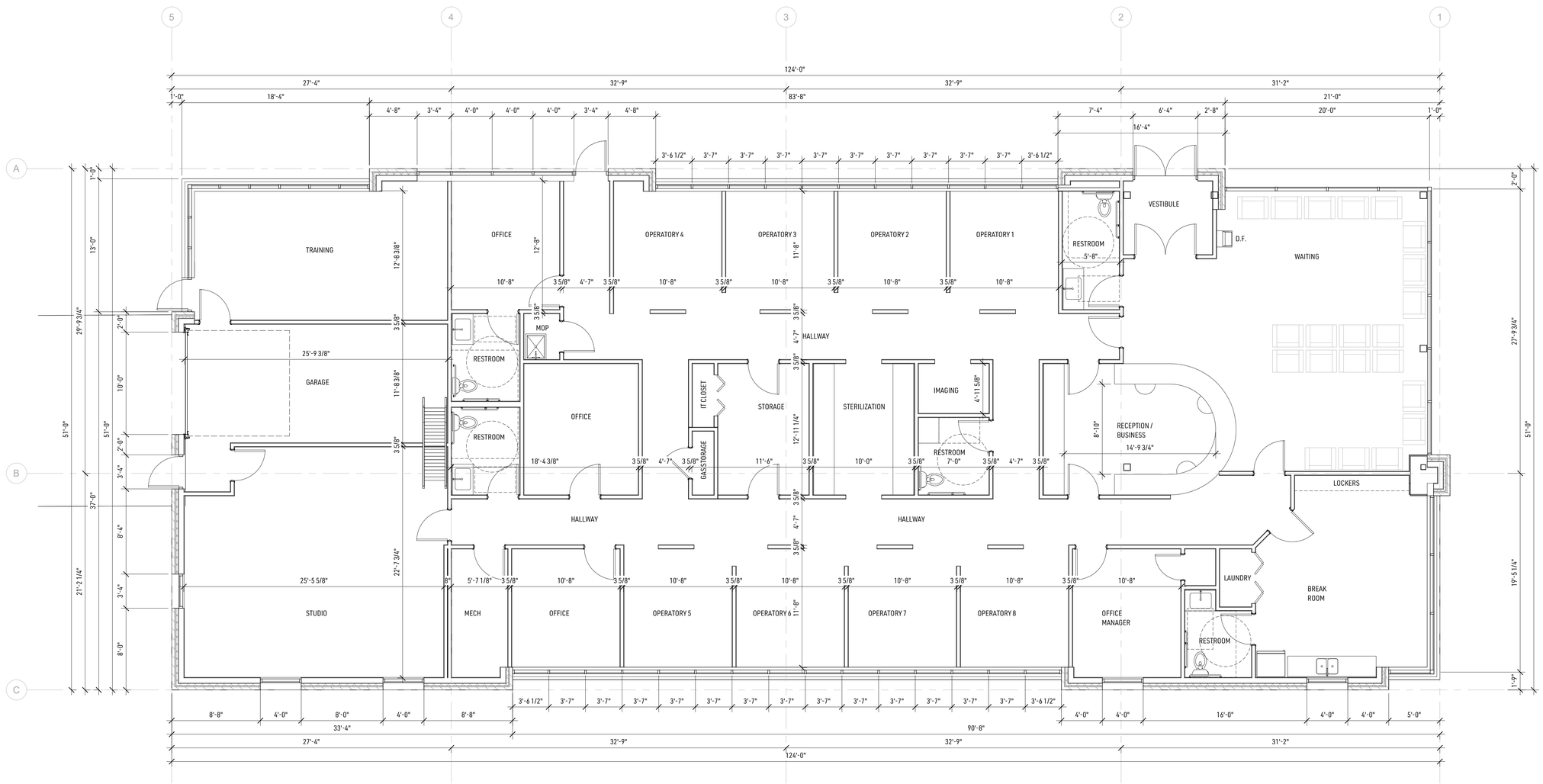

A1 FLOOR PLAN - LEVEL 1
3/16" = 1'-0"

N

CONCEPT DESIGN
3/11/24

EDINA ENDODONTIC

EDINA, MN
23-147.00

SCHEMATIC PLAN

A1.0

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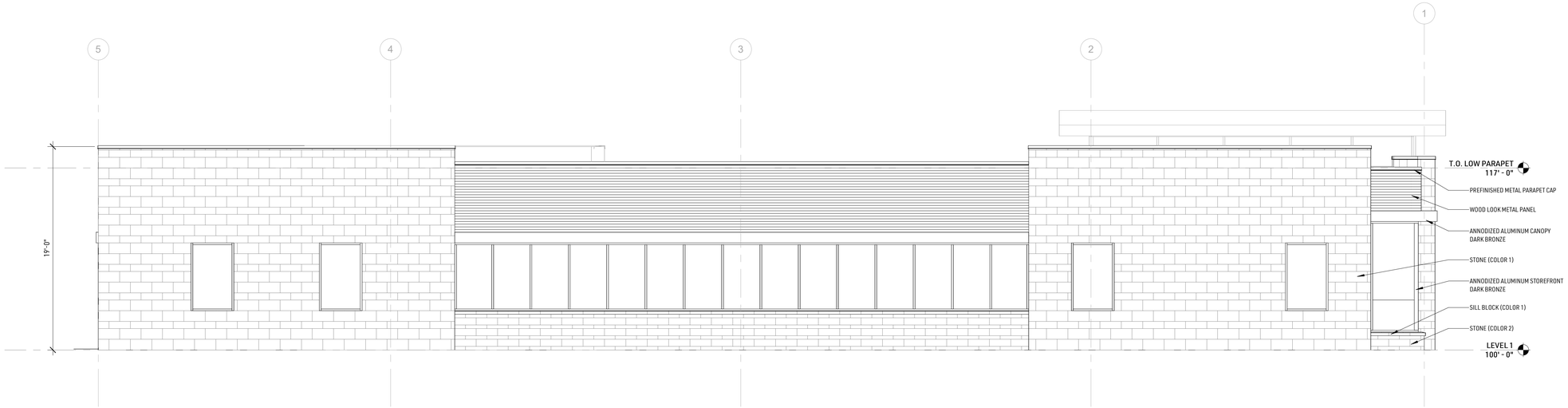
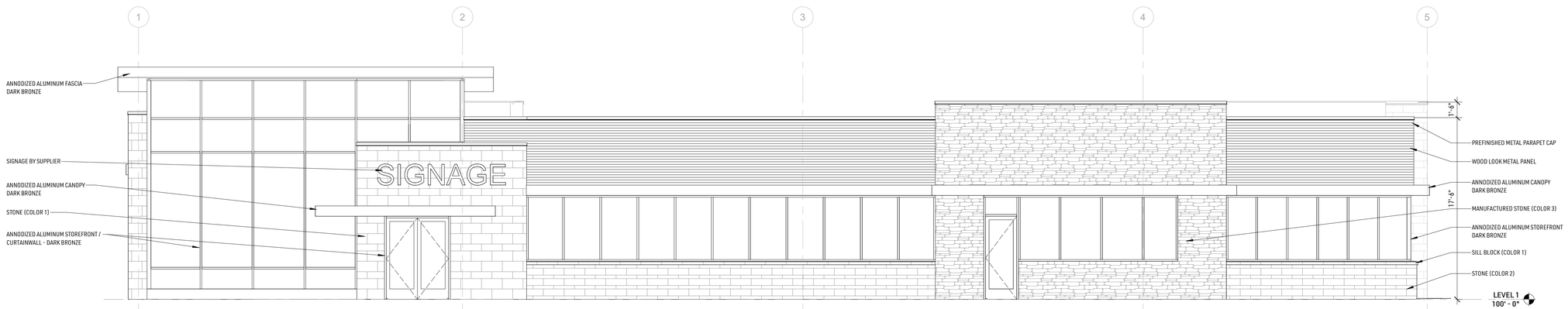


A3 **SOUTH EXTERIOR ELEVATION**
3/16" = 1'-0"



A2 **NORTH EXTERIOR ELEVATION**
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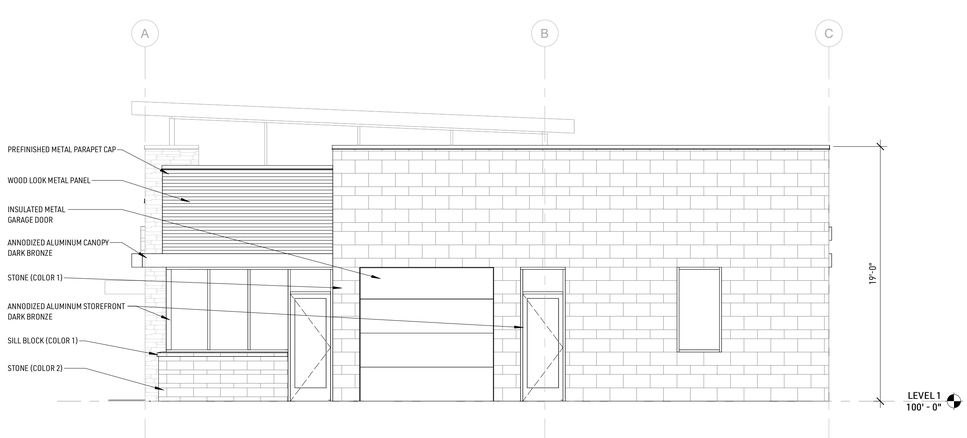

B1 **WEST EXTERIOR ELEVATION**
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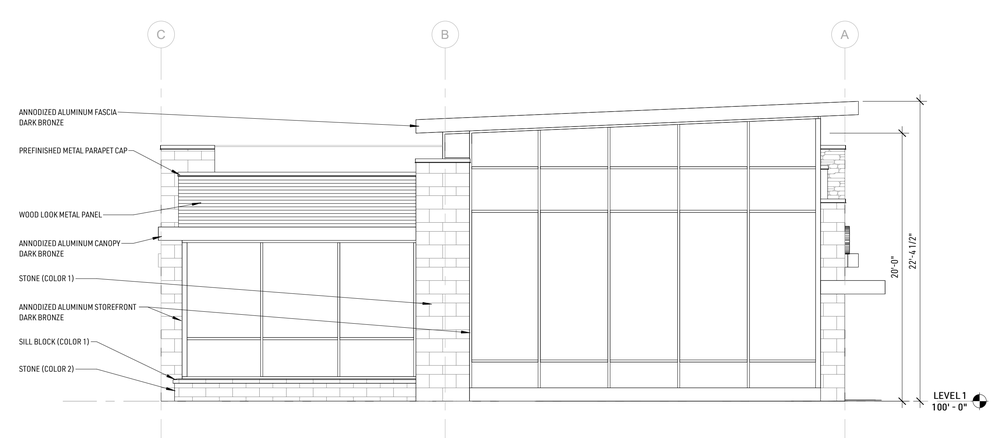

A1 **EAST EXTERIOR ELEVATION**
3/16" = 1'-0"

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CONCEPT DESIGN
3/11/24

EDINA ENDODONTIC

EDINA, MN
23-147.00

EXTERIOR ELEVATIONS

A2.0

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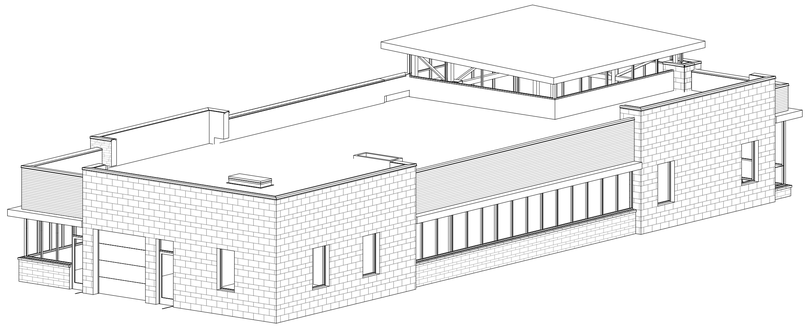
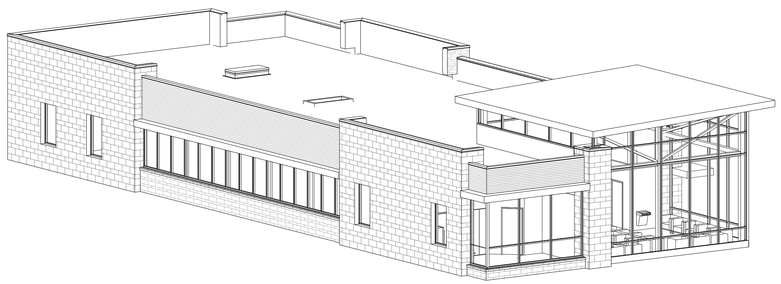


8 Axon SW



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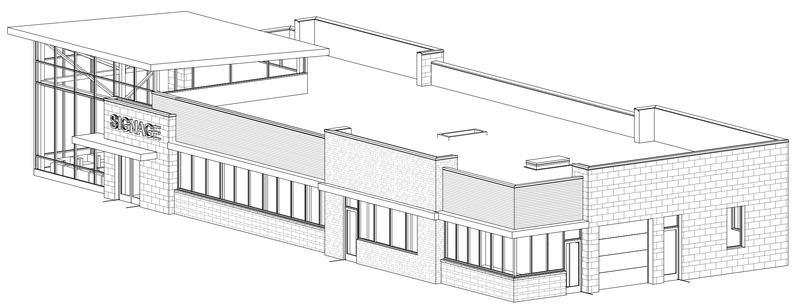

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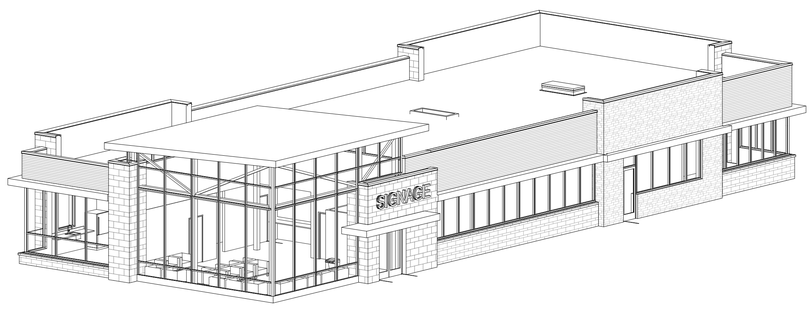

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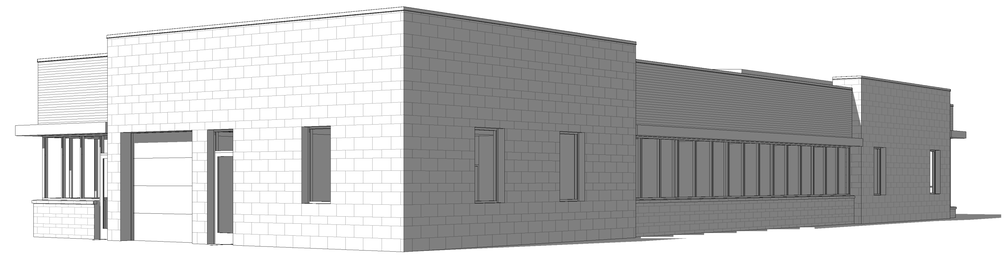

4 3D View 4

3 3D View 3

2 3D View 2

1 3D View 1

DJR

CONCEPT DESIGN
3/11/2024

EDINA
ENDODONTIC
EDINA, MN
23-147.00

BUILDING EXTERIOR

A2.1

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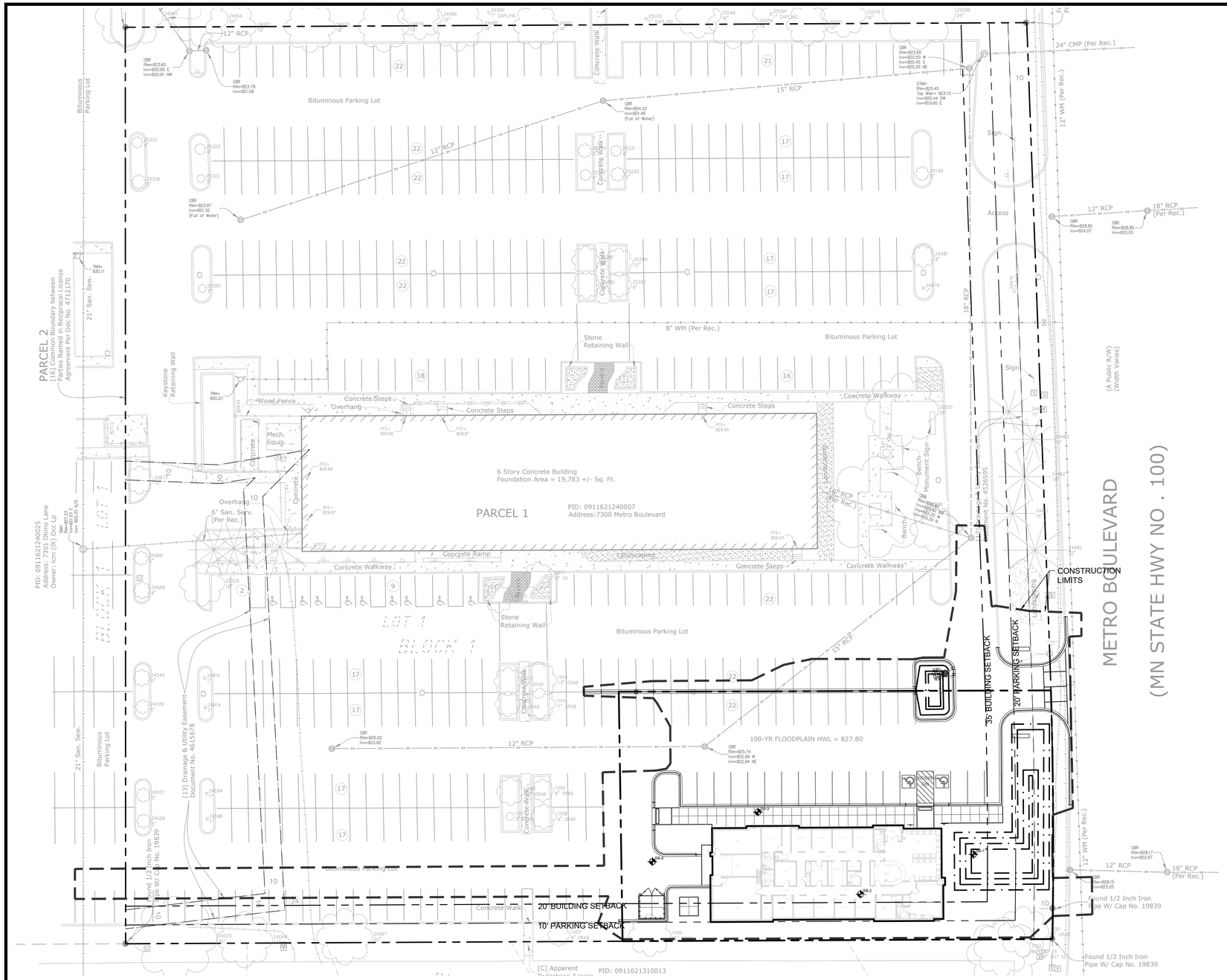

CITY OF EDINA SITE SPECIFIC NOTES:

1. RESERVED FOR CITY SPECIFIC SITE NOTES.

OWNER INFORMATION

HEMPEL REAL ESTATE
10050 CROSSTOWN CIRCLE, SUITE 600
EDEN PRAIRIE, MN 55334
BRENT JACOBSON
612-355-2609
BJACOBSON@HEMPELCOMPANIES.COM

OPERATIONAL NOTES

SNOW REMOVAL	ALL SNOW SHALL BE STORED ON-SITE OUTSIDE PARKING LOT. WHEN FULL, REMOVAL CO. SHALL REMOVE EXCESS OFF-SITE.
TRASH REMOVAL	TRASH SHALL BE PLACED IN EXTERIOR TRASH AREA AND REMOVED BY COMMERCIAL CO. WEEKLY.
DELIVERIES	DELIVERIES SHALL OCCUR AT THE FRONT DOOR VIA STANDARD COMMERCIAL DELIVERY VEHICLES (UPS, FED-EX, USPS).

SEE SHEET C0.1 FOR GENERAL SITE NOTES

SITE AREA CALCULATIONS	EXISTING CONDITION			PROPOSED CONDITION		
IMPERVIOUS SURFACES						
BUILDING COVERAGE	19,792 SF	8.3%		25,919 SF	10.9%	
PAVEMENT	182,650 SF	76.6%		173,577 SF	72.8%	
TOTAL	202,442 SF	84.9%	4.6 AC	199,496 SF	83.7%	4.6 AC
PERVIOUS SURFACES						
TOTAL	35,878 SF	15.1%	0.8 AC	38,824 SF	16.3%	0.9 AC
TOTAL SITE AREA	238,320 SF	100.0%	5.5 AC	238,320 SF	100.0%	5.5 AC
DIFFERENCE (EX. VS PROP.)	-2,946 SF	-1.2%				
FULLY RECONSTRUCTED IMPERVI	28,845 SF					
DISTURBED AREA	40,403 SF	0.9 AC				

SITE DATA

ZONING SUMMARY		
EXISTING ZONING	POD-1 - PLANNED OFFICE	
PROPOSED ZONING		
	REQUIRED	PROVIDED
BUILDING SETBACKS		
FRONT STREET	35'	
REAR YARD	20'	
SIDE STREET	35'	
INTERIOR SIDE YARD	20'	
PARKING SETBACKS		
FRONT - (FROM ROW)	20'	
REAR	10'	
SIDE	10'	
	REQUIRED	PROVIDED
PARKING SUMMARY		
PARKING STALL DIMENSIONS	8.5' X 18'	
COMPACT PARKING STALL DIMENSIONS	7.5' X 16'	
DRIVE AISLE DIMENSIONS	24'	
FIRE ACCESS ROAD DIMENSIONS		
	REQUIRED	PROVIDED
PARKING STALLS - SURFACE	23	
PARKING STALLS - GARAGE	N/A	

SITE PLAN LEGEND:

- LIGHT DUTY BITUMINOUS PAVEMENT (IF APPLICABLE). SEE GEOTECHNICAL REPORT FOR AGGREGATE BASE & WEAR COURSE DEPTH, SEE DETAIL.
- HEAVY DUTY BITUMINOUS PAVEMENT (IF APPLICABLE). SEE GEOTECHNICAL REPORT FOR AGGREGATE BASE & WEAR COURSE DEPTH, SEE DETAIL.
- CONCRETE PAVEMENT (IF APPLICABLE) AS SPECIFIED (PAD OR WALK) SEE GEOTECHNICAL REPORT FOR AGGREGATE BASE & CONCRETE DEPTHS, WITHIN ROW SEE CITY DETAIL, WITHIN PRIVATE PROPERTY SEE CSG DETAIL
- PROPERTY LINE
- CONSTRUCTION LIMITS
- CURB AND GUTTER-SEE NOTES (T.O.) TIP OUT GUTTER WHERE APPLICABLE-SEE PLAN
- TRAFFIC DIRECTIONAL ARROW PAVEMENT MARKINGS
- SIGN AND POST ASSEMBLY. SHOP DRAWINGS REQUIRED.
  HC = ACCESSIBLE SIGN
  NP = NO PARKING FIRE LANE
  ST = STOP
  CP = COMPACT CAR PARKING ONLY
- ACCESSIBILITY ROUTE ARROW (IF APPLICABLE) DO NOT PAINT.

Know what's **below.** Call before you dig.

1" = 20'-0"
10'-0" 0 20'-0"

CivilSite GROUP
Civil Engineering • Surveying • Landscape Architecture
5000 Glenwood Avenue
Golden Valley, MN 55422
civilsitegroup.com 612-615-0060

PRELIMINARY: NOT FOR CONSTRUCTION

PROJECT

EDINA DENTAL OFFICE

7300 METRO BOULEVARD, EDINA, MINNESOTA 55439

HEMPEL REAL ESTATE
10050 CROSSTOWN CIRCLE, SUITE 600, EDEN PRAIRIE, MN 55334

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

Robert A. Latta

DATE 3/11/24 LICENSE NO. 59612

ISSUE/SUBMITTAL SUMMARY

DATE	DESCRIPTION
03/11/2024	CITY SUBMITTAL

DRAWN BY	JR
REVIEWED BY	RL
PROJECT MANAGER	RL
PROJECT NUMBER	23365

REVISION SUMMARY

DATE	DESCRIPTION

SITE PLAN - OVERALL

C2.0

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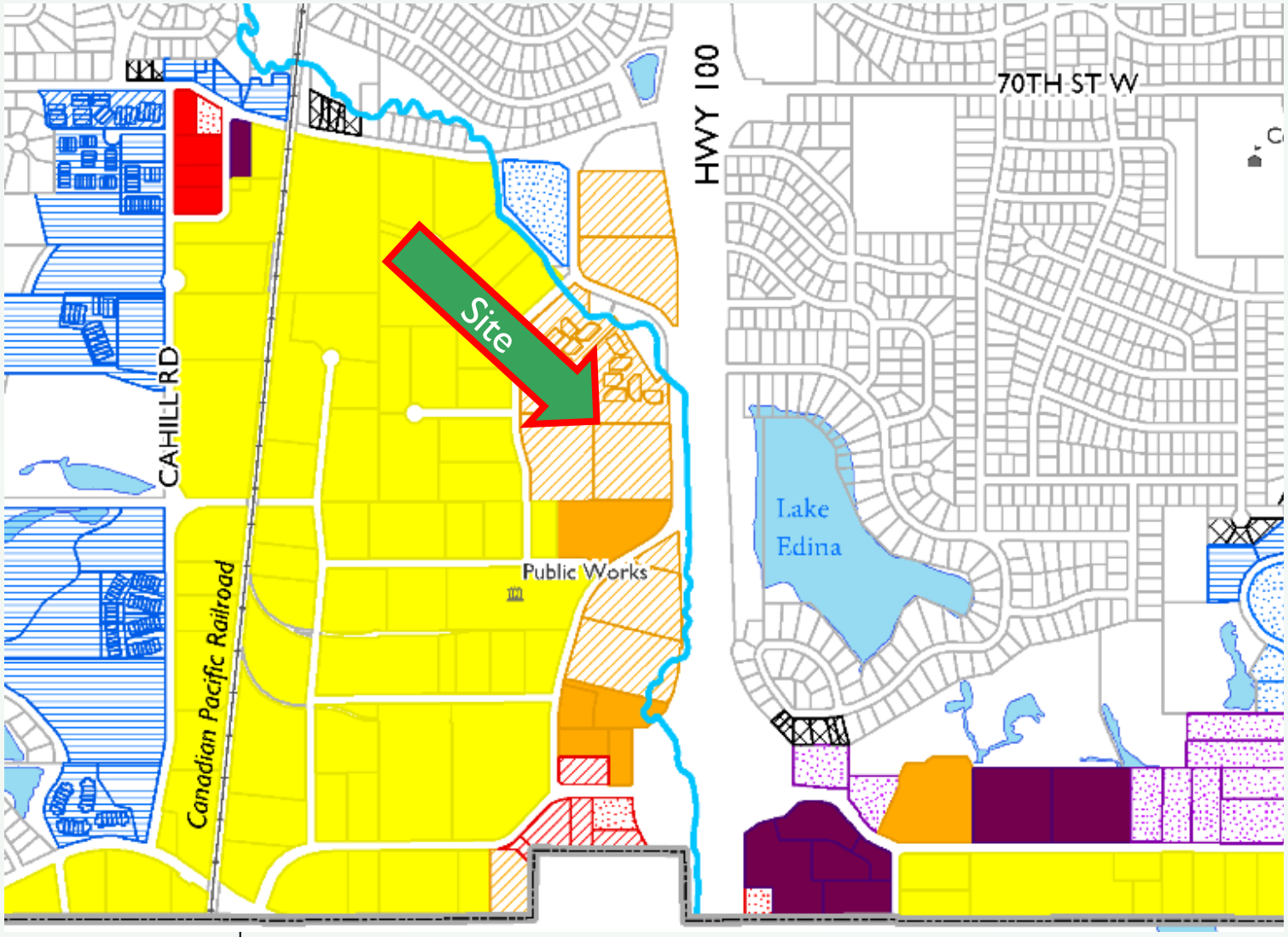


Zoning Map

Legend

- Lakes
- Creeks
- Edina Boundary

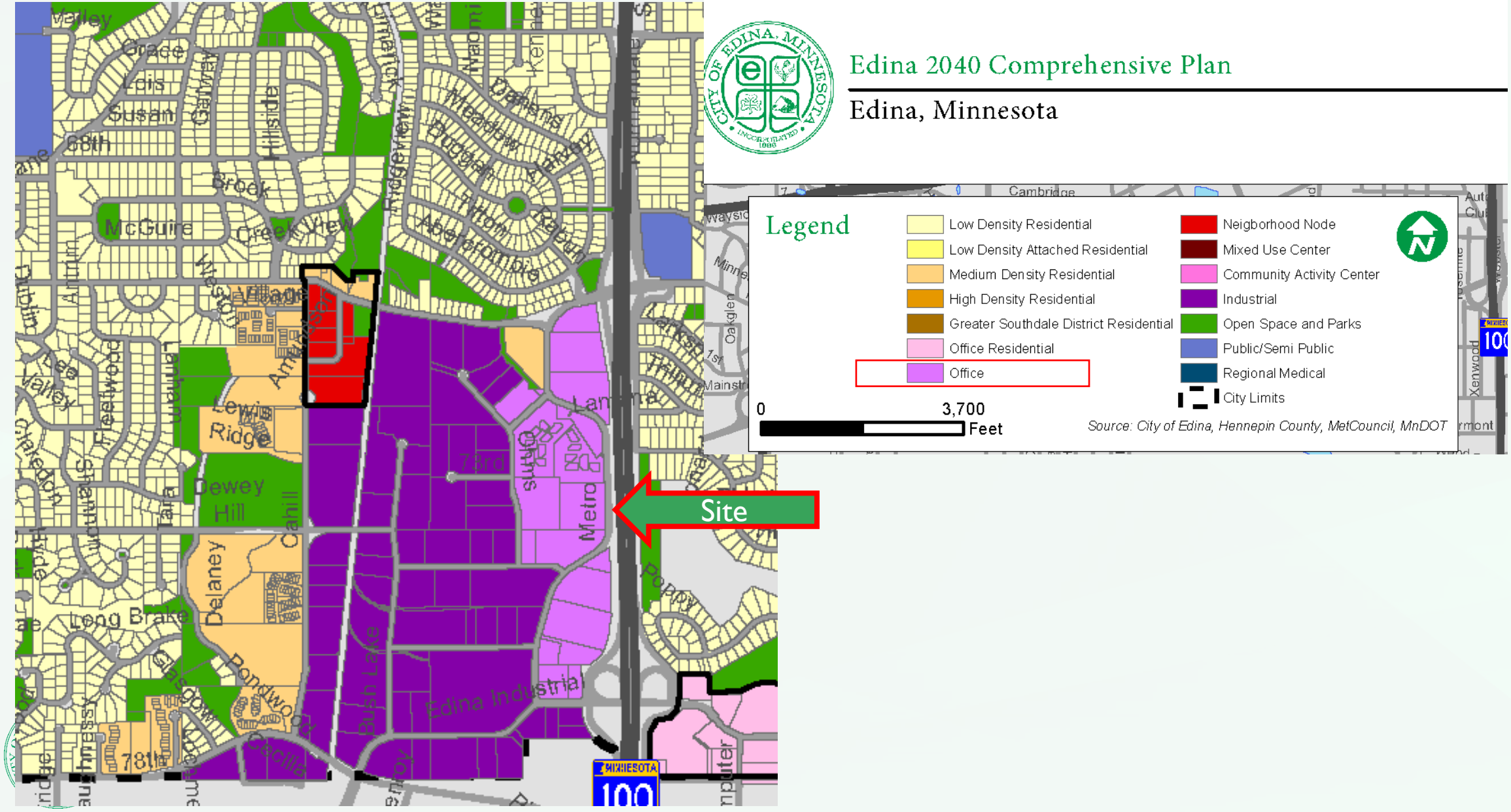
Zoning

- R-1 - Single Dwelling Unit
- R-2 - Double Dwelling Unit
- PRD-1 - Planned Residence
- PRD-2 - Planned Residence
- PRD-3 - Planned Residence
- PRD-4 - Planned Residence
- PRD-5 - Planned Residence
- PCD-1 - Planned Commercial
- PCD-2 - Planned Commercial
- PCD-3 - Planned Commercial
- PCD-4 - Planned Commercial
- POD-1 - Planned Office
- POD-2 - Planned Office
- RMD - Regional Medical
- PID - Planned Industrial
- PUD - Planned Unit Development
- APD - Automotive Parking
- PSR-4 - Planned Residence
- MDD-4 - Mixed Development
- MDD-5 - Mixed Development
- MDD-6 - Mixed Development

The CITY of EDINA

Edina 2040 Comprehensive Plan

Edina, Minnesota



Existing Site

The CITY of EDINA

File #193806824
March 19, 2024

DRAFT

Traffic and Parking Study for the Edina Endodontic Building in Edina, MN

Prepared for:
City of Edina

4801 W. 50th Street
Edina, MN 55424

Prepared by:

Stantec Consulting Services Inc.
One Carlson Pkwy, #100
Plymouth, MN 55447
Phone: 7963-479-4200
Fax: 763-479-4242

Table of Contents

TABLE OF CONTENTS ..... **I**

1.0 EXECUTIVE SUMMARY **1-1**

2.0 PURPOSE AND BACKGROUND **2-1**

3.0 EXISTING CONDITIONS **3-1**

4.0 TRAFFIC FORECASTS **4-1**

5.0 TRAFFIC ANALYSIS **5-1**

6.0 PARKING ANALYSIS **6-1**

7.0 CONCLUSIONS AND RECOMMENDATIONS **7-1**

8.0 APPENDIX **8-1**

FIGURES

FIGURE 1 PROJECT LOCATION 2-2
FIGURE 2 SITE PLAN 2-3
FIGURE 3 EXISTING CONDITIONS 3-2
FIGURE 4 WEEKDAY A.M. AND P.M. PEAK HOUR VOLUMES 4-3

I hereby certify that this report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

_____ DATE: March 19, 2024
Edward F. Terhaar
License No. 24441

DRAFT

1.0 Executive Summary

The purpose of this Traffic and Parking Study is to evaluate the impacts of the proposed Edina Endodontic dental office located at 7300 Metro Boulevard in Edina, MN. The proposed project location is currently part of the surface parking lot for the 7300 Metro Boulevard office building.

This study examined weekday a.m. and p.m. peak hour traffic impacts of the proposed development at the following intersections:

- W. 70th Street/Metro Boulevard
- W. 74th Street/Metro Boulevard
- Edina Industrial Boulevard/Metro Boulevard
- Metro Boulevard/parking lot access

The proposed project will involve construction of a new 6,300 square foot building for the dental clinic. The project includes 18 parking spaces immediately adjacent to the building. Existing parking for the 7300 Metro Boulevard office building will also be available. As shown in the site plan, the primary access will be provided at the existing parking lot access on Metro Boulevard. The project is expected to be completed in 2025.

The conclusions drawn from the information and analyses presented in this report are as follows:

- The proposed development is expected to add 20 trips during the a.m. peak hour, 25 trips during the p.m. peak hour, and 227 trips daily.

- The trips added to the roadway system by the proposed development are expected to have minimal impact on traffic operations on the surrounding street system. All movements and intersections operate at acceptable levels of service under all scenarios. No improvements are needed at the subject intersections to accommodate the proposed project.

- Data provided in the ITE publication *Parking Generation*, 6th Edition, for Medical-Dental Office most closely matches the proposed facility. Based on the ITE data, the peak weekday parking demand is 17 spaces. Edina City code requires one space per 300 square feet of building space. This equates to 21 spaces.

- Existing parking usage in the 7300 Metro Boulevard parking lot was recorded on three days, with the number of spaces available ranging from 177 to 196. The majority of the spaces used were located close to the office building, with minimal usage near the proposed building. The 18 parking spaces immediately adjacent to the building combined with available spaces in the remainder of the lot will accommodate the expected peak parking demand and the City code requirement.

- Future plans for this area include a sidewalk on the east side of Metro Boulevard and a standard bike lane on Metro Boulevard south of W. 74th Street. The proposed project will benefit from the existing and proposed sidewalk and bicycle facilities in this area.

- The project owner is encouraged to provide bicycle parking spaces to promote bicycle use. Outside racks for short-term parking are recommended for employees and facility users.

- Per City requirements, a Travel Demand Management (TDM) plan is required for this project. The goal of the TDM plan is to reduce vehicular trips during peak hours and carbon emissions from vehicles. TDM strategies for this site include:

    - Providing maps that show the area bus routes and schedules.
    - Providing maps of bicycle and pedestrian facilities.
    - Providing information on starting and joining commuter programs.
    - Providing bicycle parking spaces for employees and facility users.

2.0 Purpose and Background

The purpose of this Traffic and Parking Study is to evaluate the impacts of the proposed Edina Endodontic dental office located at 7300 Metro Boulevard in Edina, MN. The proposed project location is currently part of the surface parking lot for the 7300 Metro Boulevard office building. The project location is shown in **Figure 1**.

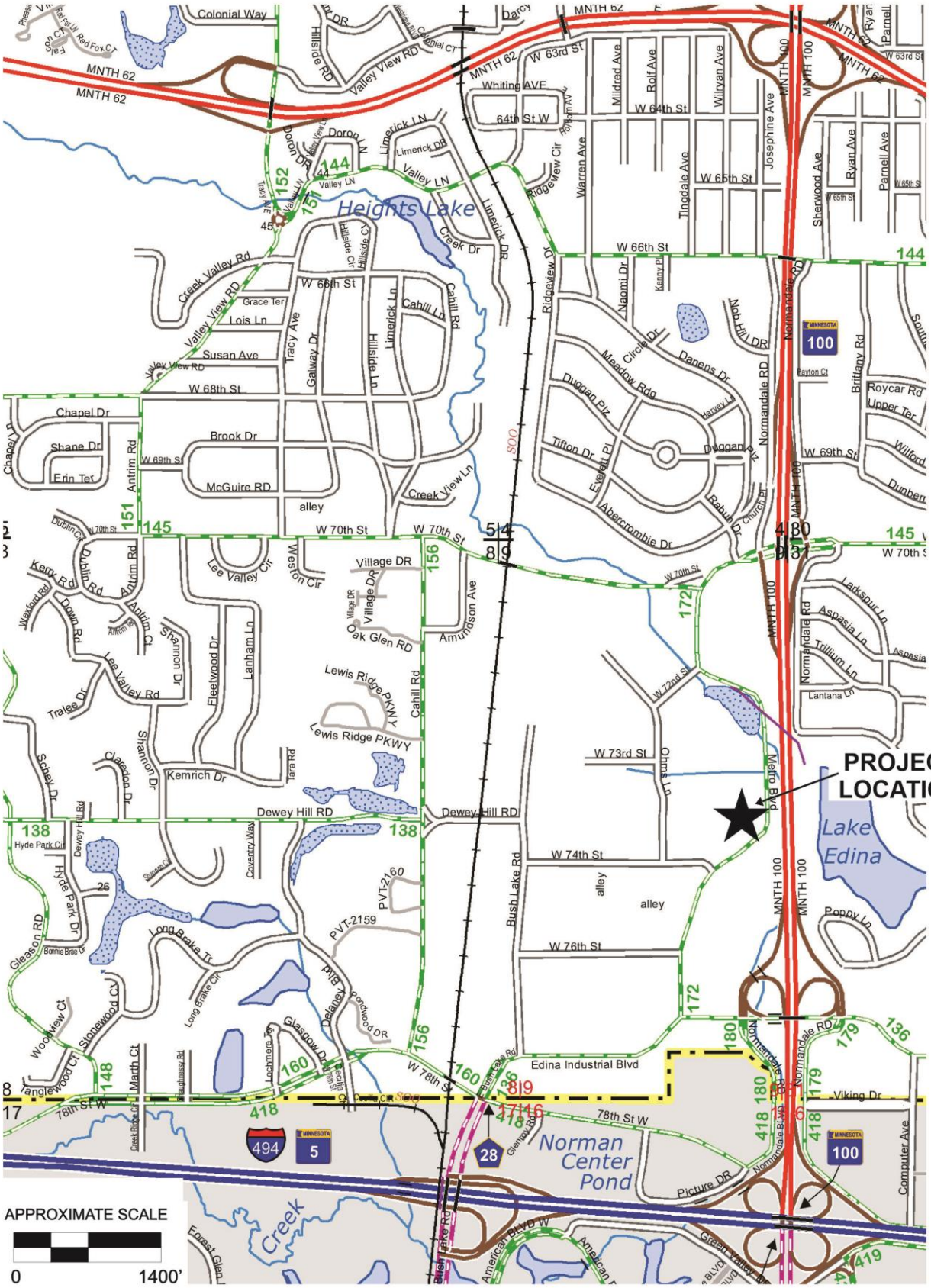
This study examined weekday a.m. and p.m. peak hour traffic impacts of the proposed development at the following intersections:

- W. 70th Street/Metro Boulevard
- W. 74th Street/Metro Boulevard
- Edina Industrial Blvd/Metro Boulevard
- Metro Boulevard/parking lot access

Proposed Development Characteristics

The proposed project will involve construction of a new 6,300 square foot building for the dental clinic. The project includes 18 parking spaces immediately adjacent to the building. Existing parking for the 7300 Metro Boulevard office building will also be available. As shown in the site plan, the primary access will be provided at the existing parking lot access on Metro Boulevard.

The project is expected to be completed in 2025. The current site plan is shown in **Figure 2**.



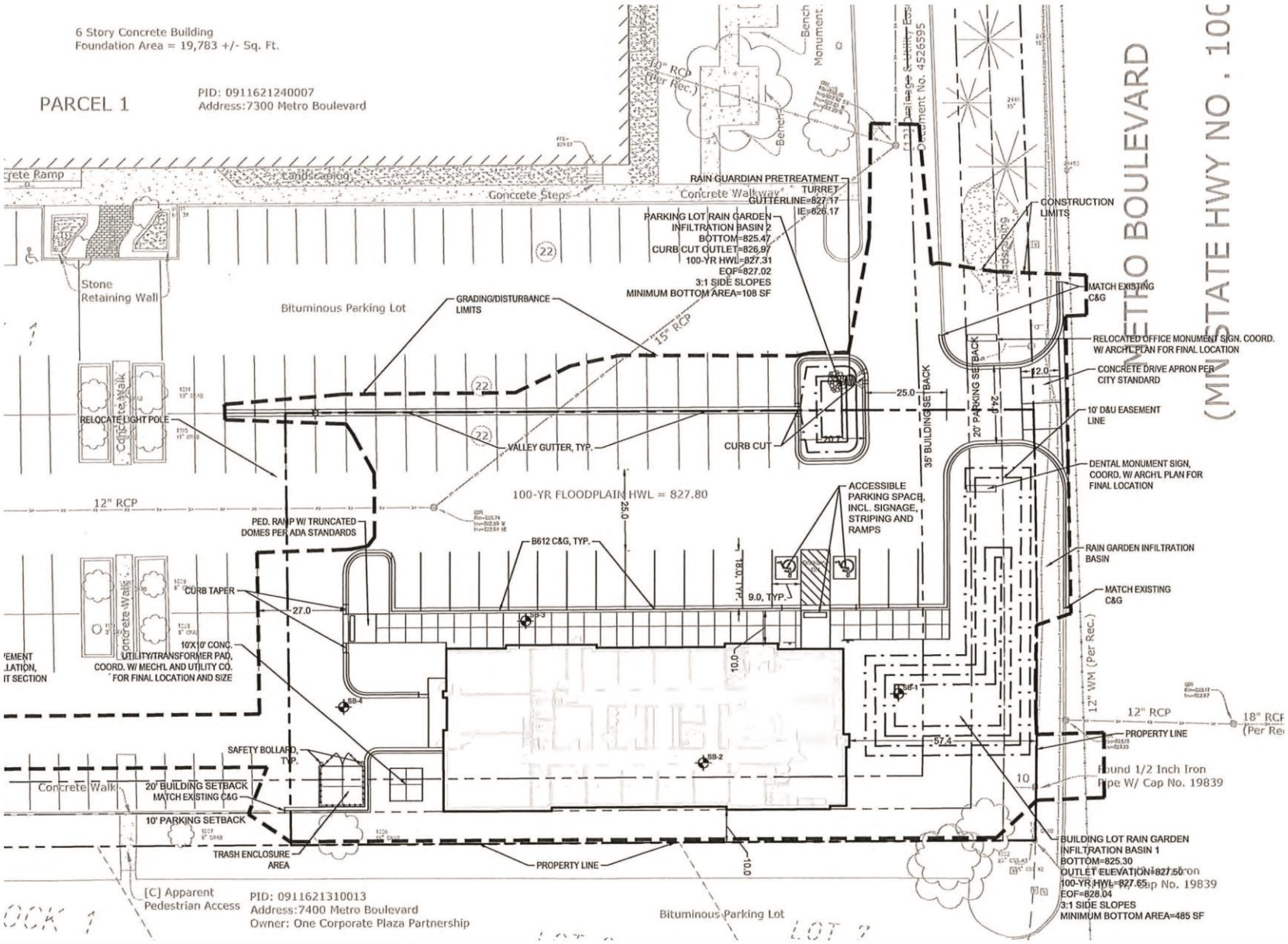
Stantec

TRAFFIC AND PARKING STUDY
FOR DENTAL OFFICE
IN EDINA, MN

FIGURE 1

PROJECT LOCATION

DRAFT



DRAFT

Stantec

TRAFFIC AND PARKING STUDY
FOR DENTAL OFFICE
IN EDINA, MN

FIGURE 2

SITE PLAN

3.0 Existing Conditions

The proposed project will be located in the southeast corner of the existing parking lot for the 7300 Metro Boulevard office building. The site is bounded by Metro Boulevard on the east and existing office uses and parking on the west, north, and south.

Near the site location, Metro Boulevard is a two lane undivided roadway. Existing conditions at the proposed project location are shown in **Figure 3** and described below.

W. 70th Street/Metro Boulevard

This four-way intersection is controlled with a traffic signal. The eastbound approach provides one left turn lane and one left turn/through/right turn lane. The westbound approach provides one left turn/through/right turn lane. The northbound approach provides one left turn/through lane and one through/right turn lane. The southbound approach provides one left turn/through lane and one right turn lane.

Metro Boulevard/parking lot access

This three-way intersection is controlled with a stop sign on the eastbound approach. The eastbound approach provides one left turn/right turn lane. The southbound approach provides one through/right turn lane. The northbound approach provides one left turn/through lane.

W. 74th Street/Metro Boulevard

This three-way intersection is controlled with a stop sign on the eastbound approach. The eastbound approach provides one left turn/right turn lane. The southbound approach provides through/right turn lane. The northbound approach provides one left turn/through lane.

Edina Industrial Boulevard /Metro Boulevard

This four-way intersection is controlled with a traffic signal. The eastbound and westbound approaches provide one left turn/through lane and one through/right turn lane. The northbound approach provides one left turn/through/right turn lane. The southbound approach provides one left turn lane and one through/right turn lane.

Traffic Volume Data

Weekday traffic volume data was recorded at the existing intersections in January, 2024. Existing traffic volume data is presented later in this report.

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TRAFFIC AND PARKING STUDY
FOR DENTAL OFFICE
IN EDINA, MN

FIGURE 3

EXISTING CONDITIONS

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4.0 Traffic Forecasts

Traffic Forecast Scenarios

To adequately address the impacts of the proposed project, forecasts and analyses were completed for the year 2025. Specifically, weekday a.m. and p.m. peak hour traffic forecasts were completed for the following scenarios:

- *2024 Existing*. Existing volumes were determined through traffic counts at the subject intersections. The existing volume information includes trips generated by the uses near the project site.

- *2025 No-Build*. Existing volumes at the subject intersections were increased by 1.0 percent per year to determine 2025 No-Build volumes. The 1.0 percent per year growth rate was calculated based on both recent growth experienced near the site and projected growth due to additional development in the area.

- *2025 Build*. Trips generated by the proposed development were added to the 2025 No-Build volumes to determine 2025 Build volumes.

Trip Generation for Proposed Project

Weekday a.m. and p.m. peak hour trip generation for the proposed development and existing uses to be removed were calculated based on data presented in the eleventh edition of Trip Generation, published by the Institute of Transportation Engineers (ITE). The resultant trip generation estimates are shown in **Table 4-1**.

Table 4-1
Trip Generation for Proposed Project

Use	Size	Weekday AM Peak Hour			Weekday PM Peak Hour			Weekday Daily
		In	Out	Total	In	Out	Total	Total
Medical Office Building	6,300 SF	15	5	20	7	18	25	227

Note: SF=square feet

As shown, the project adds 20 trips during the a.m. peak hour, 25 trips during the p.m. peak hour, and 227 trips daily.

Trip Distribution Percentages

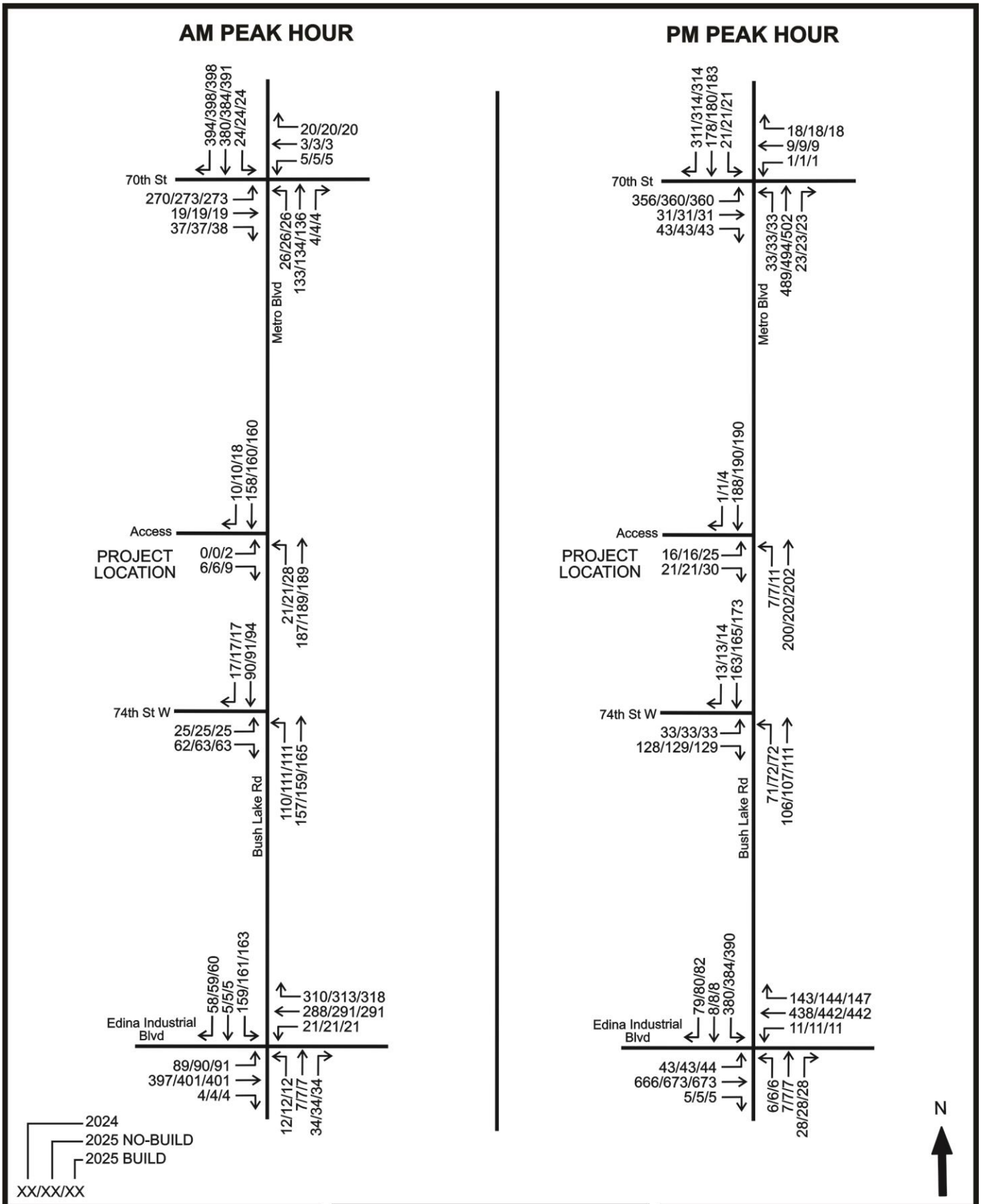
Trip distribution percentages for the subject development trips were established based on the nearby roadway network, existing and expected future traffic patterns, and location of the subject development in relation to major attractions and population concentrations.

The distribution percentages for trips generated by the proposed development are as follows:

- 45 percent to/from the north on Metro Boulevard
- 5 percent to/from the west on W. 70th Street
- 5 percent to/from the west on W. 74th Street
- 10 percent to/from the west on Edina Industrial Boulevard
- 35 percent to/from the east on Edina Industrial Boulevard

Traffic Volumes

Development trips were assigned to the surrounding roadway network using the preceding trip distribution percentages. Traffic volumes were established for all the forecasting scenarios described earlier during the weekday a.m. and p.m. peak hours. The resultant traffic volumes are presented in **Figure 4.**



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TRAFFIC AND PARKING STUDY
FOR DENTAL OFFICE
IN EDINA, MN

FIGURE 4

WEEKDAY A.M. AND P.M. PEAK HOUR VOLUMES

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5.0 Traffic Analysis

Intersection Level of Service Analysis

Traffic analyses were completed for the subject intersections for all scenarios described earlier during the weekday a.m. and p.m. peak hours using Synchro/SimTraffic software. Initial analysis was completed using existing geometrics and intersection control.

Capacity analysis results are presented in terms of level of service (LOS), which is defined in terms of traffic delay at the intersection. LOS ranges from A to F. LOS A represents the best intersection operation, with little delay for each vehicle using the intersection. LOS F represents the worst intersection operation with excessive delay. The following is a detailed description of the conditions described by each LOS designation:

- Level of service A corresponds to a free flow condition with motorists virtually unaffected by the intersection control mechanism. For a signalized or an unsignalized intersection, the average delay per vehicle would be approximately 10 seconds or less.

- Level of service B represents stable flow with a high degree of freedom, but with some influence from the intersection control device and the traffic volumes. For a signalized intersection, the average delay ranges from 10 to 20 seconds. An unsignalized intersection would have delays ranging from 10 to 15 seconds for this level.

- Level of service C depicts a restricted flow which remains stable, but with significant influence from the intersection control device and the traffic volumes. The general level of comfort and convenience changes noticeably at this level. The delay ranges from 20 to 35 seconds for a signalized intersection and from 15 to 25 seconds for an unsignalized intersection at this level.

- Level of service D corresponds to high-density flow in which speed and freedom are significantly restricted. Though traffic flow remains stable, reductions in comfort and convenience are experienced. The control delay for this level is 35 to 55 seconds for a signalized intersection and 25 to 35 seconds for an unsignalized intersection.

- Level of service E represents unstable flow of traffic at or near the capacity of the intersection with poor levels of comfort and convenience. The delay ranges from 55 to 80 seconds for a signalized intersection and from 35 to 50 seconds for an unsignalized intersection at this level.

- Level of service F represents forced flow in which the volume of traffic approaching the intersection exceeds the volume that can be served. Characteristics often experienced include long queues, stop-and-go waves, poor travel times, low comfort and convenience, and increased accident exposure. Delays over 80 seconds for a signalized intersection and over 50 seconds for an unsignalized intersection correspond to this level of service.

The LOS results for the study intersections are discussed below.

2024 Existing

Weekday A.M. and P.M. Peak Hour LOS Results

Intersection	Traffic Control	AM Peak Hour LOS	PM Peak Hour LOS
W. 70th Street/Metro Boulevard	Signal	B/C	C/D
Metro Boulevard/parking lot access	EB stop	A/A	A/B
W. 74th Street/Metro Boulevard	EB stop	A/B	A/B
Edina Industrial Blvd/Metro Blvd	Signal	A/C	B/C

Note: Level of service results presented with overall intersection LOS followed by worst movement LOS.

During the a.m. peak hour, all intersections operate at LOS B or better and movements operate at LOS C or better. During the p.m. peak hour, all intersections operate at LOS C or better and movements operate at LOS D or better.

2025 No Build

Weekday A.M. and P.M. Peak Hour LOS Results

Intersection	Traffic Control	AM Peak Hour LOS	PM Peak Hour LOS
W. 70th Street/Metro Boulevard	Signal	B/C	C/D
Metro Boulevard/parking lot access	EB stop	A/A	A/B
W. 74th Street/Metro Boulevard	EB stop	A/B	A/B
Edina Industrial Blvd/Metro Blvd	Signal	A/C	B/C

Note: Level of service results presented with overall intersection LOS followed by worst movement LOS.

During the a.m. peak hour, all intersections operate at LOS B or better and movements operate at LOS C or better. During the p.m. peak hour, all intersections operate at LOS C or better and movements operate at LOS D or better.

2025 Build

Weekday A.M. and P.M. Peak Hour LOS Results

Intersection	Traffic Control	AM Peak Hour LOS	PM Peak Hour LOS
W. 70th Street/Metro Boulevard	Signal	B/C	C/D
Metro Boulevard/parking lot access	EB stop	A/A	A/B
W. 74th Street/Metro Boulevard	EB stop	A/B	A/B
Edina Industrial Blvd/Metro Blvd	Signal	A/C	B/C

Note: Level of service results presented with overall intersection LOS followed by worst movement LOS.

During the a.m. peak hour, all intersections operate at LOS B or better and movements operate at LOS C or better. During the p.m. peak hour, all intersections operate at LOS C or better and movements operate at LOS D or better.

Overall Traffic Impact

The trips added to the roadway system by the proposed development are expected to have minimal impact on traffic operations on the surrounding street system. All movements and intersections operate at acceptable levels of service under all scenarios. No improvements are needed at the subject intersections to accommodate the proposed project.

Bicycle and Pedestrian Facilities

Under existing conditions, no sidewalk is provided on either side of Metro Boulevard. Striped shoulders are provided on both sides of Metro Boulevard. The Nine Mile Creek Regional Trail is located immediately north of the site with a bridge connection over TH 100. Bicycles are allowed on all the surrounding streets.

Future plans for this area include a sidewalk on the east side of Metro Boulevard and a standard bike lane on Metro Boulevard south of W. 74th Street. The proposed project will benefit from the existing and proposed sidewalk and bicycle facilities in this area.

The project owner is encouraged to provide bicycle parking spaces to promote bicycle use. Outside racks for short-term parking are recommended for employees and facility users.

Transit Facilities

The subject site presently is served by the Metro Transit bus routes 6 and 540. Bus stops exist on Metro Boulevard and W. 74th Street.

Travel Demand Management Plan (TDM)

Per City requirements, a Travel Demand Management (TDM) plan is required for this project. The goal of the TDM plan is to reduce vehicular trips during peak hours and carbon emissions from vehicles. TDM strategies for this site include:

- Providing maps that show the area bus routes and schedules.
- Providing maps of bicycle and pedestrian facilities.
- Providing information on starting and joining commuter programs.
- Providing bicycle parking spaces for employees and facility users.

The goal of the TDM plan is a 10 percent reduction in single occupant vehicle trips. The TDM plan strategies should be implemented at the time the project is complete and fully operational.

6.0 Parking Analysis

The project includes 18 parking spaces immediately adjacent to the building. Existing parking for the 7300 Metro Boulevard office building will also be available.

Parking data from the Institute of Transportation Engineers (ITE) was used to determine the expected parking demand for the proposed land use. Data provided in the ITE publication *Parking Generation*, 6th Edition, for Medical-Dental Office most closely matches the proposed facility. Based on the ITE data, the peak weekday parking demand is 17 spaces.

Edina City code requires one space per 300 square feet of building space. This equates to 21 spaces.

The existing parking lot on the south side of the 7300 Metro Boulevard office building has a total 237 spaces available. Parking usage was recorded on multiple days to determine the number of spaces that are currently used. The results are shown in Table 6-1.

Table 6-1
Parking Usage for Lot on South Side of 7300 Metro Boulevard Building

Date and Time	Total Spaces	Spaces Use	Spaces Available
Wednesday 2/14/24 9:20 a.m.	237	41	196
Thursday 2/22/24 1:50 p.m.	237	60	177
Thursday 3/14/24 11:20 a.m.	237	54	183

As shown in the table, the number of spaces available ranged from 177 to 196. The majority of the spaces used were located close to the office building, with minimal usage near the proposed building. The 18 parking spaces immediately adjacent to the building combined with available spaces in the remainder of the lot will accommodate the expected peak parking demand and the City code requirement.

7.0 Conclusions and Recommendations

The conclusions drawn from the information and analyses presented in this report are as follows:

- The proposed development is expected to add 20 trips during the a.m. peak hour, 25 trips during the p.m. peak hour, and 227 trips daily.

- The trips added to the roadway system by the proposed development are expected to have minimal impact on traffic operations on the surrounding street system. All movements and intersections operate at acceptable levels of service under all scenarios. No improvements are needed at the subject intersections to accommodate the proposed project.

- Data provided in the ITE publication *Parking Generation*, 6th Edition, for Medical-Dental Office most closely matches the proposed facility. Based on the ITE data, the peak weekday parking demand is 17 spaces. Edina City code requires one space per 300 square feet of building space. This equates to 21 spaces.

- Existing parking usage in the 7300 Metro Boulevard parking lot was recorded on three days, with the number of spaces available ranging from 177 to 196. The majority of the spaces used were located close to the office building, with minimal usage near the proposed building. The 18 parking spaces immediately adjacent to the building combined with available spaces in the remainder of the lot will accommodate the expected peak parking demand and the City code requirement.

- Future plans for this area include a sidewalk on the east side of Metro Boulevard and a standard bike lane on Metro Boulevard south of W. 74th Street. The proposed project will benefit from the existing and proposed sidewalk and bicycle facilities in this area.

- The project owner is encouraged to provide bicycle parking spaces to promote bicycle use. Outside racks for short-term parking are recommended for employees and facility users.

- Per City requirements, a Travel Demand Management (TDM) plan is required for this project. The goal of the TDM plan is to reduce vehicular trips during peak hours and carbon emissions from vehicles. TDM strategies for this site include:

    - Providing maps that show the area bus routes and schedules.
    - Providing maps of bicycle and pedestrian facilities.
    - Providing information on starting and joining commuter programs.
    - Providing bicycle parking spaces for employees and facility users.

8.0 Appendix

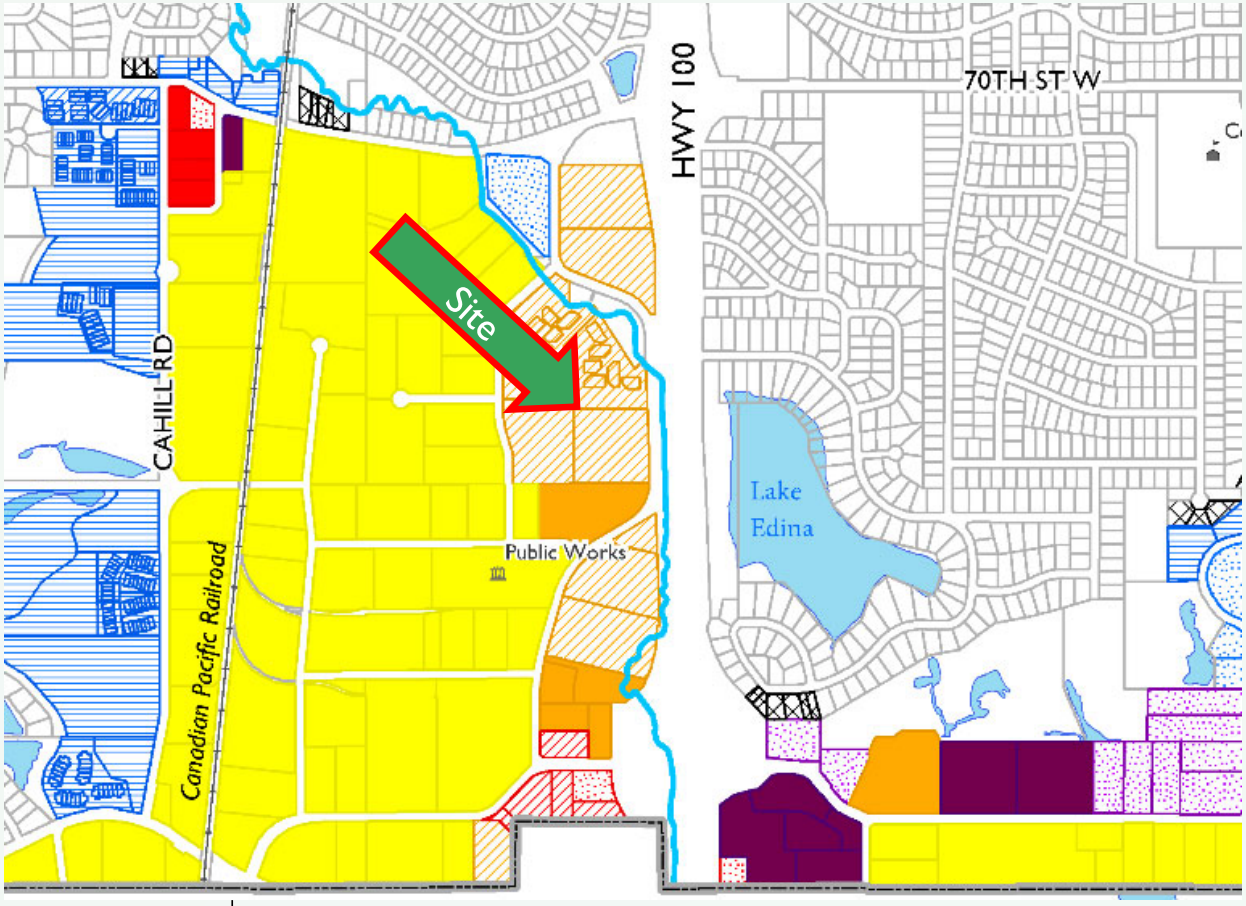
- Level of Service Worksheets

The CITY of EDINA

Site Plan with Variances and Subdivision

7300 Metro Boulevard – Dental Office

EdinaMN.gov



Zoning Map

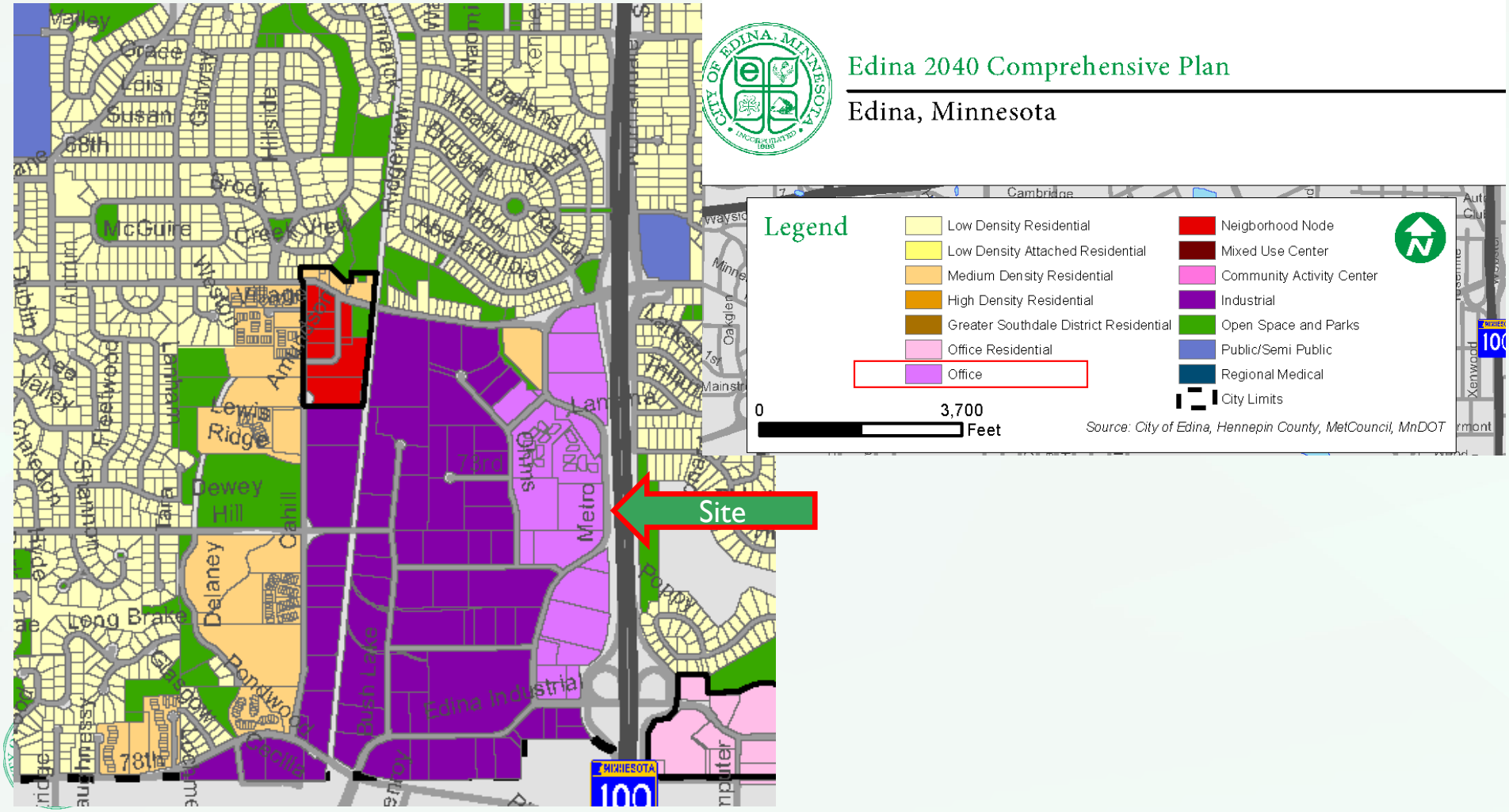
Legend

- Lakes
- Creeks
- Edina Boundary

Zoning

- R-1 - Single Dwelling Unit
- R-2 - Double Dwelling Unit
- PRD-1 - Planned Residence
- PRD-2 - Planned Residence
- PRD-3 - Planned Residence
- PRD-4 - Planned Residence
- PRD-5 - Planned Residence
- PCD-1 - Planned Commercial
- PCD-2 - Planned Commercial
- PCD-3 - Planned Commercial
- PCD-4 - Planned Commercial
- POD-1 - Planned Office
- POD-2 - Planned Office
- RMD - Regional Medical
- PID - Planned Industrial
- PUD - Planned Unit Development
- APD - Automotive Parking
- PSR-4 - Planned Residence
- MDD-4 - Mixed Development
- MDD-5 - Mixed Development
- MDD-6 - Mixed Development

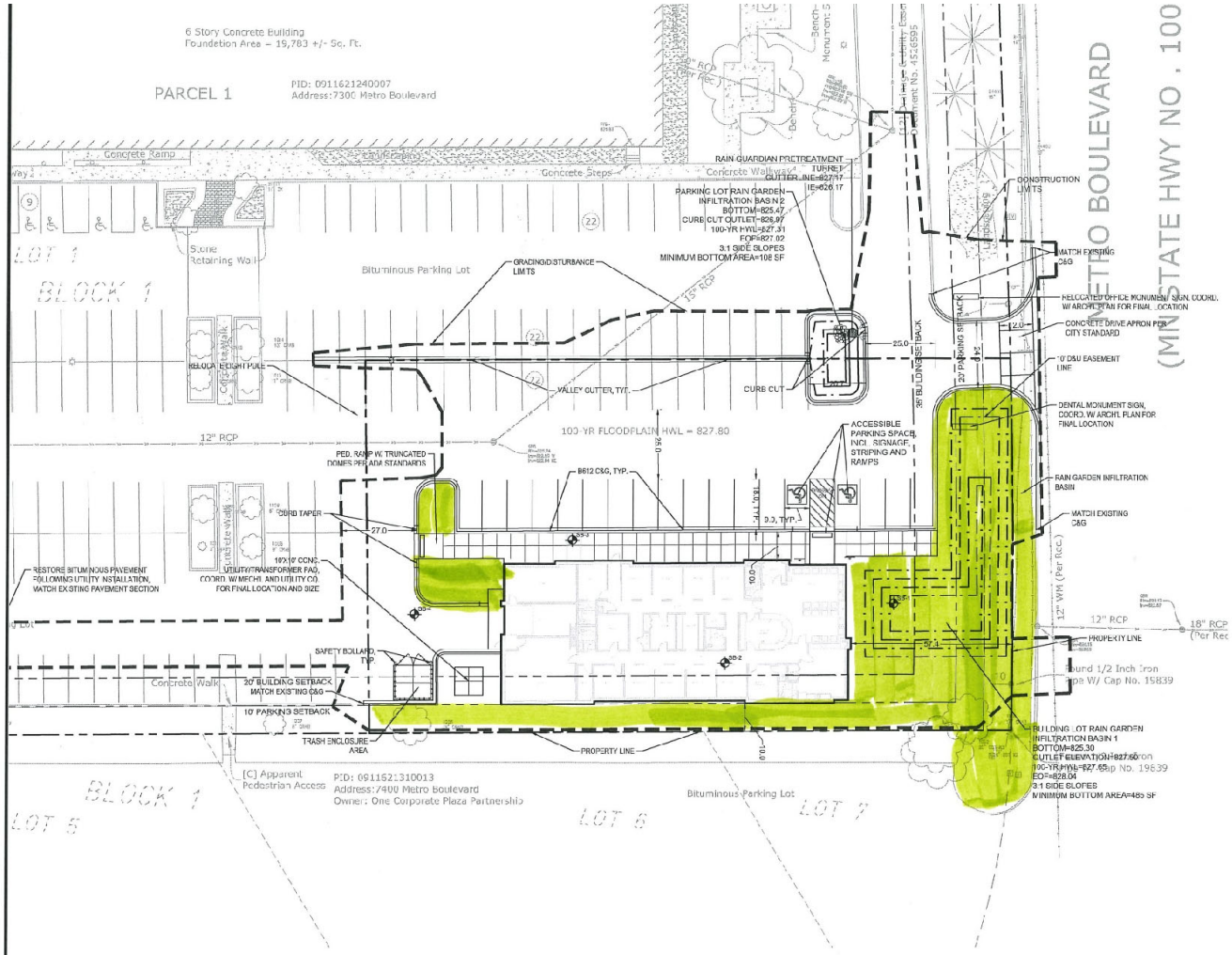
The CITY of EDINA


Edina 2040 Comprehensive Plan
Edina, Minnesota
Legend
Low Density Residential
Low Density Attached Residential
Medium Density Residential
High Density Residential
Greater Southdale District Residential
Office Residential
Office
Neigborhood Node
Mixed Use Center
Community Activity Center
Industrial
Open Space and Parks
Public/Semi Public
Regional Medical
City Limits
0
3,700
Feet
Source: City of Edina, Hennepin County, MetCouncil, MnDOT
Site
McGuire
Lewis Ridge
Dewey Hill
Cahill
Delaney
Long Brake
Edina Industrial
Metro
Lamplighter
Bush Lake
Bryant
Xerwood
Cambridge
Oakglen
Mainstr
Minne
Valley
Grace
Leis
Susan
68th
Brook
Creek View
Ridge View
Naomi
Kenwood
Dahlena
Wheatfield
Duggan
Brown
Abercrombie
Raleigh
Lancaster
Lincoln
Arthur
Dublin
Lee
Deerwood
Lanham
Tara
Clareden
Hyde
Westwood
Glasgow
Bondwood
Cascade
78th
77th
Xerwood
Auto Club
MINNESOTA
100

Existing Site

The CITY of EDINA

METRO BLVD DEVELOPMENT



SITE DATA		
ZONING SUMMARY		
EXISTING ZONING	POD-1 - PLANNED OFFICE	
PROPOSED ZONING		
	REQUIRED	PROVIDED
BUILDING SETBACKS		
FRONT STREET	30'	
REAR YARD	20'	
SIDE STREET	30'	
INTERIOR SIDE YARD	20'	
PARKING SETBACKS		
FRONT - (FROM ROW)	20'	
REAR	10'	
SIDE	10'	
	REQUIRED	PROVIDED
PARKING SUMMARY		
PARKING STALL DIMENSIONS	8.5' X 18'	
COMPACT PARKING STALL DIMENSIONS	7.5' X 16'	
DRIVE AISLE DIMENSIONS	24'	
FIRE ACCESS ROAD DIMENSIONS		
	REQUIRED	PROVIDED
PARKING STALLS - SURFACE	23	
PARKING STALLS - GARAGE	N/A	

SITE PLAN LEGEND:

- LIGHT DUTY BITUMINOUS PAVEMENT (IF APPLICABLE). SEE GEOTECHNICAL REPORT FOR AGGREGATE BASE & WEAR COURSE DEPTH, SEE DETAIL.
- HEAVY DUTY BITUMINOUS PAVEMENT (IF APPLICABLE). SEE GEOTECHNICAL REPORT FOR AGGREGATE BASE & WEAR COURSE DEPTH, SEE DETAIL.
- CONCRETE PAVEMENT (IF APPLICABLE) AS SPECIFIED (PAD OR WALK) SEE GEOTECHNICAL REPORT FOR AGGREGATE BASE & CONCRETE DEPTHS. WITHIN ROW SEE CITY DETAIL. WITHIN PRIVATE PROPERTY SEE C&G DETAIL
- PROPERTY LINE
- CONSTRUCTION LIMITS
- CURB AND GUTTER-SEE NOTES (T.C.) TIP CUT GUTTER WHERE APPLICABLE-SEE PLAN
- TRAFFIC DIRECTIONAL ARROW PAVEMENT MARKINGS
- SIGN AND POST ASSEMBLY. SHOP DRAWINGS REQUIRED. HC = ACCESSIBLE SIGN NP = NO PARKING FIRE LANE ST = STOP CP = COMPACT CAR PARKING ONLY
- ACCESSIBILITY ROUTE ARROW (IF APPLICABLE) DO NOT PAINT.

CITY OF EDINA
MAR 11 2024
PLANNING DEPARTMENT

CITY OF EDINA SITE SPECIFIC NOTES:

1. RESERVED FOR CITY SPECIFIC SITE NOTES.

OWNER INFORMATION

HEMPEL REAL ESTATE
10850 CROSSTOWN CIRCLE, SUITE 600
EDEN PRAIRIE, MN 55344
BRENT JACOBSON
612-355-3609
BJACOBSON@HEMPELCOMPANIES.COM

OPERATIONAL NOTES

SNOW REMOVAL	ALL SNOW SHALL BE STORED ON-SITE OUTSIDE PARKING LOT. WHEN FULL, REMOVAL CO. SHALL REMOVE EXCESS OFF-SITE.
TRASH REMOVAL	TRASH SHALL BE PLACED IN EXTERIOR TRASH AREA AND REMOVED BY COMMERCIAL CO. WEEKLY.
DELIVERIES	DELIVERIES SHALL OCCUR AT THE FRONT DOOR VIA STANDARD COMMERCIAL DELIVERY VEHICLES (UPS, FED-EX, USPS).

SEE SHEET CC.1 FOR GENERAL

SITE AREA CALCULATIONS	EXISTING CONDITION			PROPOSED CONDITION		
IMPERVIOUS SURFACES						
BUILDING COVERAGE	15,792 SF	8.3%		25,919 SF	10.9%	
PAVEMENT	182,650 SF	76.6%		173,577 SF	72.8%	
TOTAL	202,442 SF	84.9%	4.6 AC	199,496 SF	83.7%	4.6 AC
PERVIOUS SURFACES						
TOTAL	35,878 SF	15.1%	0.8 AC	38,824 SF	16.3%	0.9 AC
TOTAL SITE AREA	238,320 SF	100.0%	5.5 AC	238,320 SF	100.0%	5.5 AC
DIFFERENCE (EX. VS PROP.)	-2,946 SF	-1.2%				
FULLY RECONSTRUCTED IMPERVI	28,845 SF					

CivilSite GROUP
Civil Engineering • Surveying • Landscape Architecture
5500 Glenwood Avenue
Golden Valley, MN 55422
civilsitegroup.com 612-615-0060

PRELIMINARY: NOT FOR CONSTRUCTION

PROJECT
EDINA DENTAL OFFICE
7300 METRO BOULEVARD, EDINA, MINNESOTA 55439

HEMPEL REAL ESTATE
10850 CROSSTOWN CIRCLE, SUITE 600, EDEN PRAIRIE, MN 55344

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

Robert A. Latta
DATE 5/11/24 LICENSE NO. 58612

ISSUE/SUBMITTAL SUMMARY

DATE	DESCRIPTION
03/11/2024	CITY SUBMITTAL

DRAWN BY JR
REVIEWED BY RL
PROJECT MANAGER RL
PROJECT NUMBER 2339

REVISION SUMMARY

DATE	DESCRIPTION

SITE PLAN

CC.0

Planning
Submi
03-11-202

EXTERIOR RENDERING

3.11.2024

Site Plan Submittal - Edina Endodontic

7300 Metro Boulevard, Edina, MN 55439

Planning Division
Submitted
03-11-2024

EXTERIOR RENDERING
3.11.2024

DJR

Site Plan Submittal - Edina Endodontic
7300 Metro Boulevard, Edina, MN 55439
23-147.00

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Request Requires

- Site Plan Review.
- Variances: First floor ceiling height from 20 to 10 feet and side yard setback from 20 to 10 feet.
- Subdivision.

The CITY of EDINA

Site Plan

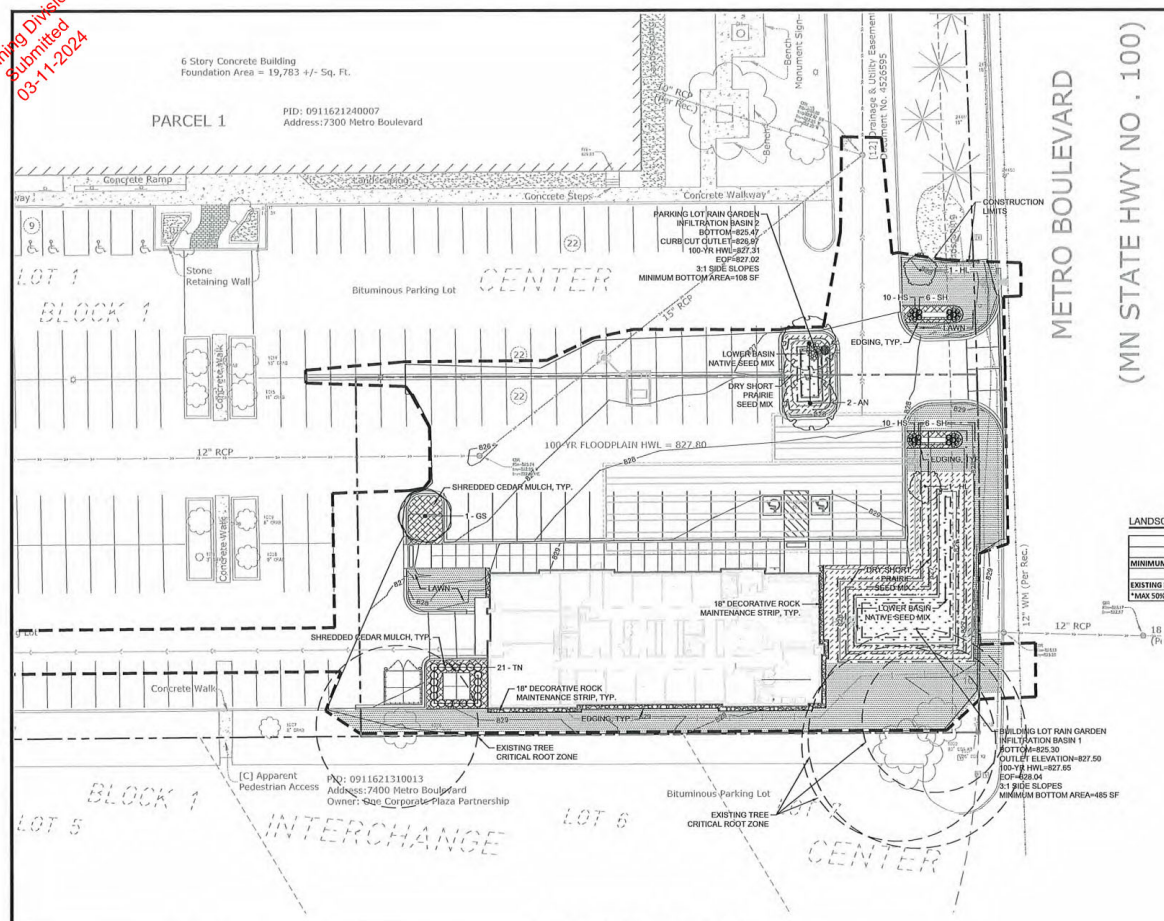
The CITY of EDINA

Compliance Table

	City Standard (POD-2) (Setback measured to the property line)	Proposed (Setback measured to the property line)
Building Setbacks Front – Metro Boulevard Side – North Side – South Rear	 35 feet 20 feet 20 feet 20 feet	 90 feet 200+ feet ***10 feet**** 21 feet
Building height	8 stories & 96 feet	1 story
First floor ceiling height	20 feet	***10 feet****
Floor area ratio (FAR)	.50% 110,790 s.f. existing building plus 6,000 s.f. proposed = 116,790 s.f. total Site area = 238,321s.f.	.50%
Parking	Dental = 1 space/300 s.f. = 20 spaces required Existing building & dental office = 389 spaces	31 spaces proposed 412 spaces total existing and proposed spaces

****Variance Requested***

Planning Division
Submitted
03-11-2024



PLANT SCHEDULE

CODE	COMMON / BOTANICAL NAME	QTY	CONT
DECIDUOUS TREES			
AN	Northwood Red Maple / Acer rubrum 'Northwood'	3	3.5" CAL. B&B
GS	Skyline Honey Locust / Gleditsia triacanthos inermis 'Skyline'	1	3.5" CAL. B&B
ORNAMENTAL TREES			
HL	Limelight Panicle Hydrangea / Hydrangea paniculata 'Limelight'	2	1.5" CAL. B&B
CODE	COMMON / BOTANICAL NAME	QTY	SIZE
SHRUBS			
TN	Nova Japanese Yew / Taxus cuspidata 'Nova'	22	#3 CONT
GRASSES			
SH	Prairie Dropseed / Sporobolus heterolepis	12	#1 CONT
PERENNIALS			
HS	Stella Supreme Daylily / Hemerocallis x 'Stella Supreme'	20	#1 CONT

LANDSCAPE REQUIREMENTS:

OVERSTORY TREE REQUIREMENTS			
	SITE PERIMETER (LINEAR FT)	FT	MINIMUM REQ. TREES
MINIMUM OVERSTORY TREES REQUIRED	1,950.0	40	49
EXISTING PRESERVED TREES TO COUNT TOWARD MINIMUM OVERSTORY TREES REQUIRED			64
*MAX 50% OF ANY ONE SPECIES			

LEGEND

GROUND COVERS

- Lower Basin Native Seed Mix
- Rock Maintanence Strip / Rock Maintanence Strip
- Shredded Cedar Mulch / Shredded Hardwood Mulch
- Blue Grass Based / Sod

PROPOSED CANOPY TREE SYMBOLS - SEE PLANT SCHEDULE AND PLAN FOR SPECIES AND PLANTING SIZES

PROPOSED EVERGREEN TREE SYMBOLS - SEE PLANT SCHEDULE AND PLAN FOR SPECIES AND PLANTING SIZES

PROPOSED ORNAMENTAL TREE SYMBOLS - SEE PLANT SCHEDULE AND PLAN FOR SPECIES AND PLANTING SIZES

PROPOSED DECIDUOUS AND EVERGREEN SHRUB SYMBOLS - SEE PLANT SCHEDULE AND PLAN FOR SPECIES AND PLANTING SIZES

PROPOSED PERENNIAL PLANT SYMBOLS - SEE PLANT SCHEDULE AND PLAN FOR SPECIES AND PLANTING SIZES

EDGING - SHALL BE COMMERCIAL GRADE, 4" DEPTH ALUMINUM, BLACK OR DARK GREEN IN COLOR, INCLUDE ALL CONNECTORS, STAKES, & ALL APPURTENANCES PER MANUF. INSTALL PER MANUF. INSTRUC./SPECS.
DECORATIVE BOULDERS (ROUNDED & BLOCK STYLE), 18"-30" DIA.

CONSTRUCTION LIMITS

MULCH SCHEDULE

AREA	MULCH TYPE	EDGING	FABRIC	REMARKS
TREE RINGS	4" DEPTH, SHREDDED CEDAR	YES	NO	SEE DETAIL SHT. L1.1
PLANTING BEDS	4" DEPTH, SHREDDED CEDAR	YES	NO	
MAINT. STRIP AT BUILDING FOUNDATION	NA	NA	NA	
PLAYGROUND MULCH	12" DEPTH ENGINEERED PLAYGROUND MULCH, SEE REMARKS. DEPTH AS SPECIFIED BY PLAYGROUND EQUIPMENT MANUFACTURER.	SEE SITE PLAN	YES	ORGANIC MULCH DESIGNED SPECIFICALLY FOR USE ON PLAYGROUNDS/CERTIFIED BY ANSI/ASTM FOR PLAYGROUND USE. (I.E. SYLVA SOFTSTEP, SET,INC. PLAYGROUND, OR EQUIVALENT)
DOG PARK MULCH	4" DEPTH, SHREDDED WOOD, SEE REMARKS	YES	YES	ORGANIC MULCH SPECIFICALLY FOR USE IN DOG PARKS OR PLAYGROUNDS. SHALL KNIT TOGETHER TO FORM MATTED, ACCESSIBLE SURFACE. (I.E. SYLVA SOFTSTEP, SET,INC. PLAYGROUND, OR EQUIVALENT), PROVIDE SHOP DRAWINGS/DATA SHEETS FOR REVIEW.
NATIVE SEED AREAS	NA	NA	NA	
NOTE: COORDINATE ALL MULCH AND PLANTING BED MATERIAL PRIOR TO INSTALLATION, PROVIDE SAMPLES AND SHOP DRAWINGS/PHOTOS/DATA SHEETS OF ALL MATERIALS				

POLLINATOR SAFE PLANT MATERIAL:

1. THE CONTRACTOR SHALL PROVIDE ONLY PLANT MATERIAL FREE OF NEONICOTINOID BASED INSECTICIDES AND/OR TREATMENTS OF ANY KIND, INCLUDING BY NOT LIMITED TO IMIDACLOPRID (CONFIDOR, ADMIRE, GAUCHO, ADVOCATE), THIAMETHOXAM (ACTARA, PLATINUM, CRUISER), CLOTHIANIDIN (PONCHO, DANTOSU, DANTOP), ACETAMIPRID (MOSPILAN, ASSAIL, CHIPCOTRISTAR), THIACLOPRID (CALYPSO), DINOTEFURAN (STARKLE, SAFARI, VENOM), AND NITENPYRAM (CAPSTAR, GUARDIAN).
2. CONTRACTOR SHALL CERTIFY, THROUGH SUPPLIERS POLICY STATEMENT OR AFFIDAVIT, THAT NO NEONICOTINOID BASED INSECTICIDES HAVE BEEN USED ON SITE OR DIRECTLY ADJACENT TO THE GROWING OR STORAGE PLOTS OF THE SUPPLIED PLANT MATERIAL, INCLUDING THE PLANTING OF AGRICULTURAL (OR OTHER) SEED TREATED WITH NEONICS.

PLANTING SEASON SCHEDULE

SEASON	CONIFEROUS	DECIDUOUS	REMARKS
SPRING PLANTING	APRIL 15 - JUNE 15	APRIL 15 - JUNE 15	
FALL PLANTING	AUGUST 21 - SEPTEMBER 30	AUGUST 15 - NOVEMBER 15	
NOTE: ADJUSTMENTS TO PLANTING DATES MUST BE APPROVED IN WRITING BY THE LANDSCAPE ARCHITECT.			

SEE SHEET L1.1 FOR GENERAL LANDSCAPE NOTES

Know what's below. Call before you dig.

1" = 20'-0"
10'-0" 0 20'-0"

CivilSite GROUP
Civil Engineering • Surveying • Landscape Architecture
5000 Glenwood Avenue
Golden Valley, MN 55422
civilsitegroup.com 612-615-0060

PRELIMINARY: NOT FOR CONSTRUCTION

PROJECT
EDINA DENTAL OFFICE
7300 METRO BOULEVARD, EDINA, MINNESOTA 55439

HEMPEL REAL ESTATE
10550 CROSSTOWN CIRCLE, SUITE 600, EDEN PRAIRIE, MN 55344

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED LANDSCAPE ARCHITECT UNDER THE LAWS OF THE STATE OF MINNESOTA.

Patrick J. Sarver
DATE 3/11/24 LICENSE NO. 24904

ISSUE/SUBMITTAL SUMMARY

DATE	DESCRIPTION
03/11/2024	CITY SUBMITTAL

DRAWN BY JR
REVIEWED BY PL
PROJECT MANAGER PL
PROJECT NUMBER 2205

REVISION SUMMARY

DATE	DESCRIPTION

LANDSCAPE PLAN

L1.0

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Planning Division
Submitted
03-11-2024

EXTERIOR RENDERING
3.11.2024

DJR

Site Plan Submittal - Edina Endodontic
7300 Metro Boulevard, Edina, MN 55439
23-147.00

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Planning Division
Submitted
03-11-2024

WOOD LOOK METAL PANEL - Longboard - Dark National Walnut

STOREFRONT / CURTAINWALL / CANOPIES
- Annodized Aluminum - Dark Bronze

STONE (COLOR 1) - US Stone - Plaza Grey

STONE (COLOR 2) - US Stone - Cottonwood

MANUFACTURED STONE (COLOR 3) - Coronado Stone - Black Forest

DJR

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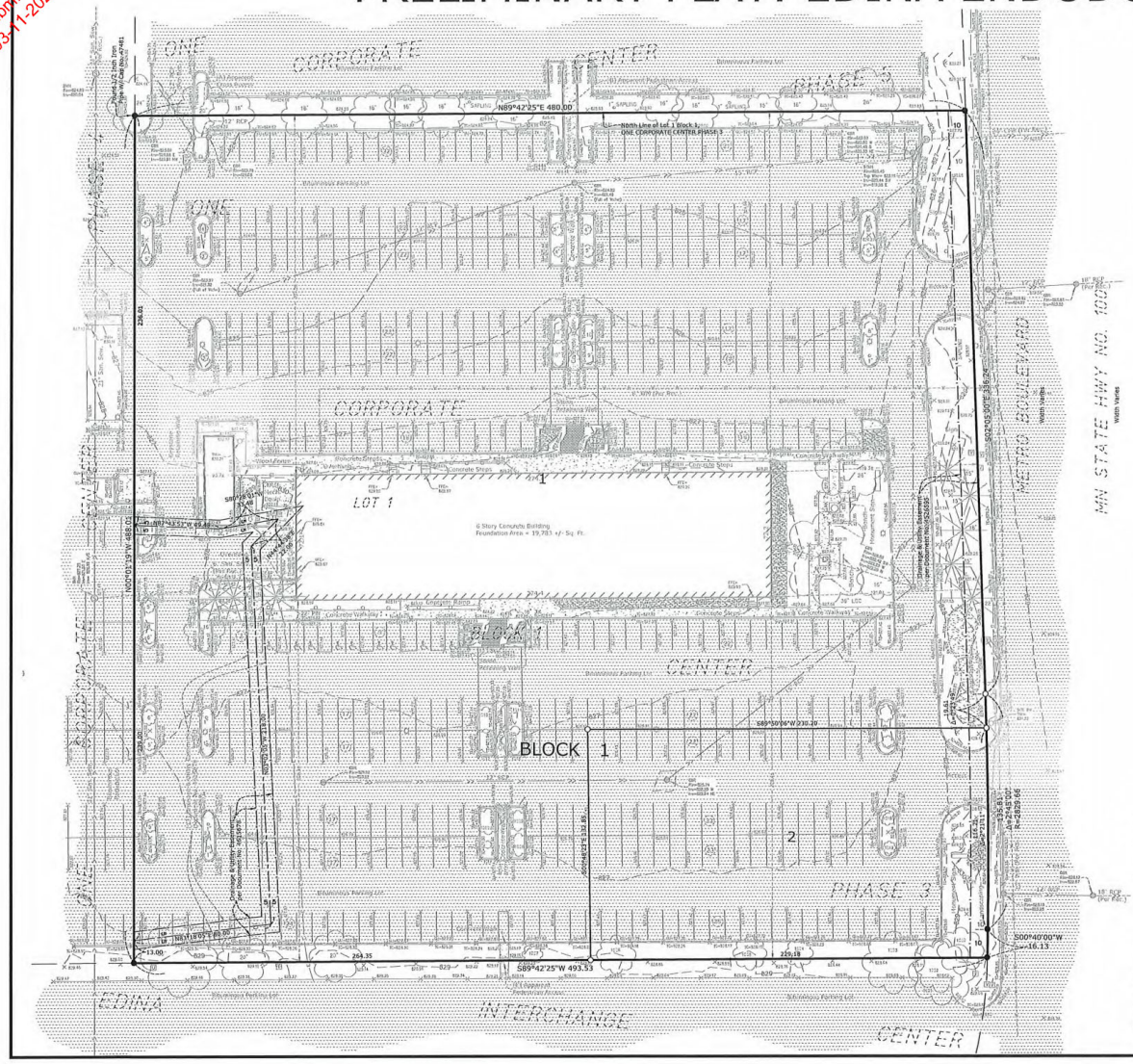
MATERIALS
3.11.2024

Site Plan Submittal - Edina Endodontic
7300 Metro Boulevard, Edina, MN 55439
23-147.00

Planning Division
Submitted
03-11-2024

PRELIMINARY PLAT: EDINA ENDODONTICS


ONE CORPORATE CENTER
PHASE 3
N89°42'25"E 480.00
North Line of Lot 1 Block 1
ONE CORPORATE CENTER PHASE 3
CORPORATE
LOT 1
6 Story Concrete Building
Foundation Area = 19,783 +/- Sq. Ft.
BLOCK 1
CENTER
S89°50'06"W 230.20
BLOCK 1
2
PHASE 3
S00°40'00"W 16.13
S89°42'25"W 493.53
264.35
229.18
EDINA INTERCHANGE
CENTER
MN STATE HWY NO. 100
METRO BOULEVARD
Width Varies
S02°03'30"E 386.27
N00°01'13"W 489.00
236.80
230.00
N00°01'13"W 489.00
S00°01'13"E 489.00
N00°01'13"W 489.00
Drainage & Utility Easement per Document No. 4623455
Drainage & Utility Easement per Document No. 4623455
Concrete Walkways
Concrete Steps
Bituminous Parking Lot
Bituminous Walkways
Concrete Curb
Concrete Wall Plug
Bituminous Pedestrian Ramp
Bituminous Parking Lot
Bituminous Pedestrian Ramp
Bituminous Parking Lot
Bituminous Pedestrian Ramp
Bituminous Parking Lot

Primary Issue

- **Are the Variances for first floor height and side setback justified?**

The CITY of EDINA

Primary Issue

- **Variance Criteria**

1. ***The variances would be in harmony with the general purposes and intent of the ordinance.***

2. ***The variance would be consistent with the Comprehensive Plan.***

3. ***There are practical difficulties in complying with the ordinance. The term "practical difficulties" means the following:***

   i. ***The property owner proposes to use the property in a reasonable manner not permitted by the Zoning Ordinance.***

   ii. ***The plight of the landowner is due to circumstances unique to the property not created by the landowner.***

   iii. ***The variance, if granted, will not alter the essential character of the locality.***

The CITY of EDINA

Primary Issue

- **Variance Criteria**

1. The requested variances meet the variance criteria.

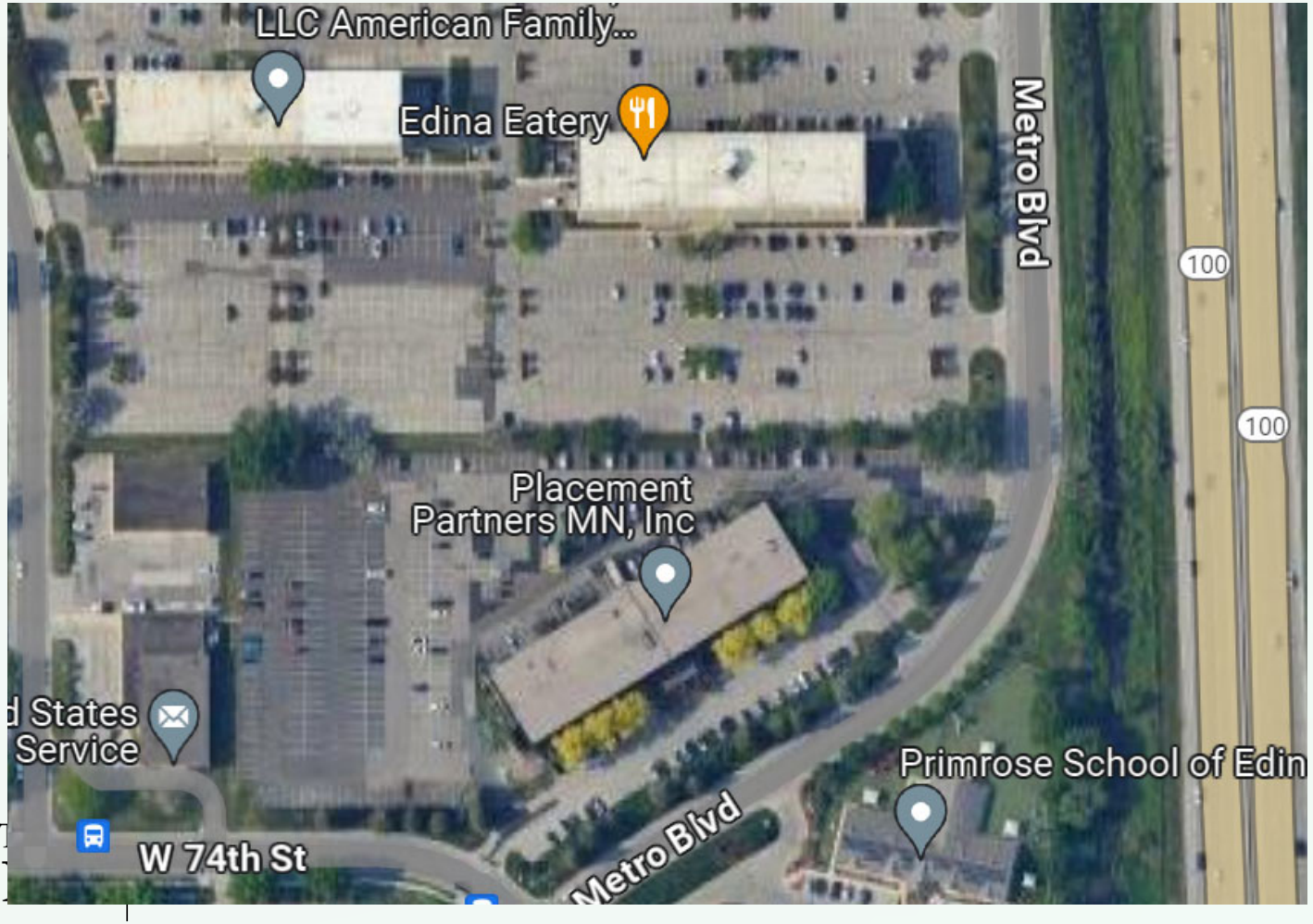
2. The intent of the side yard setback is to provide adequate spacing between buildings. The proposed dental office would allow for adequate space from the nearest building. The building to the south is setback further on the lot, 165 feet back from the street and is 60 feet back from the side lot line. The rear of the proposed dental office would be over 70 feet away.

3. These variances are reasonable given the distance separation from the closest building at over 70 feet. The reason for the request is that the building could be moved up closer to the street, as recommended in the sketch plan. With a closer setback to the side lot line, they are able to provide parking along the side of the building and not disturb the location of the existing drive-aisle.

4. The proposed use of the building is consistent with the Comprehensive Plan.

5. There are no buildings in this area with 20-foot first floor ceiling height.

6. Stantec conducted a traffic and parking study and concluded that the project would have minimal impact on existing roads and there would be adequate parking.


LLC American Family...
Edina Eatery
Metro Blvd
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Placement
Partners MN, Inc
d States
Service
Primrose School of Edin
W 74th St
Metro Blvd

The CIT
EDI

Primary Issue

- **Variance Criteria**

1. The requested variances meet the variance criteria.

2. The intent of the side yard setback is to provide adequate spacing between buildings. The proposed dental office would allow for adequate space from the nearest building. The building to the south is setback further on the lot, 165 feet back from the street and is 60 feet back from the side lot line. The rear of the proposed dental office would be over 70 feet away.

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4. The proposed use of the building is consistent with the Comprehensive Plan.

5. There are no buildings in this area with 20-foot first floor ceiling height.

6. Stantec conducted a traffic and parking study and concluded that the project would have minimal impact on existing roads and there would be adequate parking.

Recommendation

The CITY of EDINA

PEDESTRIAN NETWORK

The goal of the City's pedestrian network is to provide safe movement for all ages and abilities and to encourage active lifestyles. It should provide network continuity with broad geographic coverage and without notable gaps. Figure 3.2 below indicates locations of existing and future proposed pedestrian facilities. Refer to "Pedestrian Facilities" design guidelines in Chapter 6 for specific guidance regarding the application of these facilities.

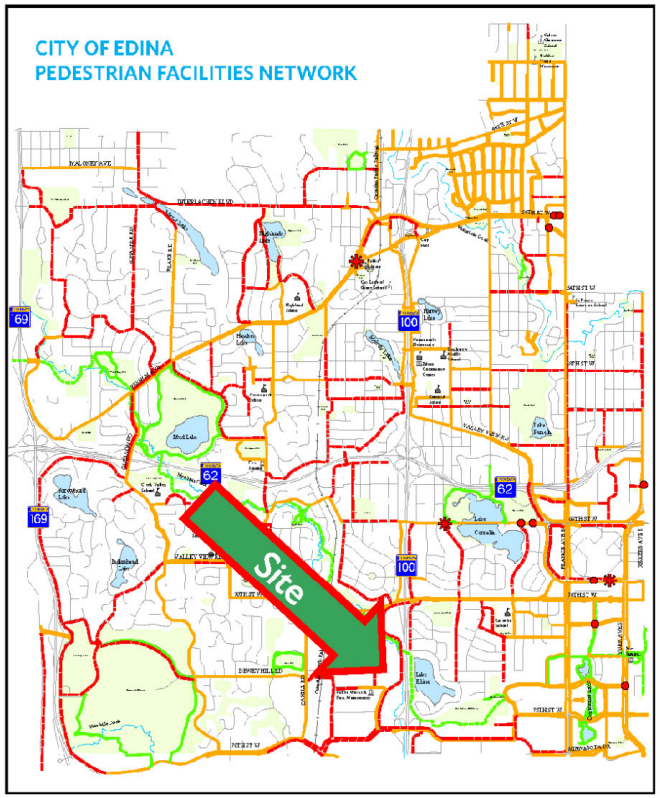


Figure 3.2. Edina Living Streets Classification Map

BICYCLE NETWORK

Edina's Living Streets should provide safe, convenient and comfortable access for bicyclists throughout the city. Edina's network of Living Streets shall accommodate all types, levels, and ages of bicyclists. Figure 3.3 below indicates locations of existing and future proposed bicycle facilities. Refer to "Bicycle Facilities" design guidelines in Chapter 6 for specific guidance regarding the application of these facilities.

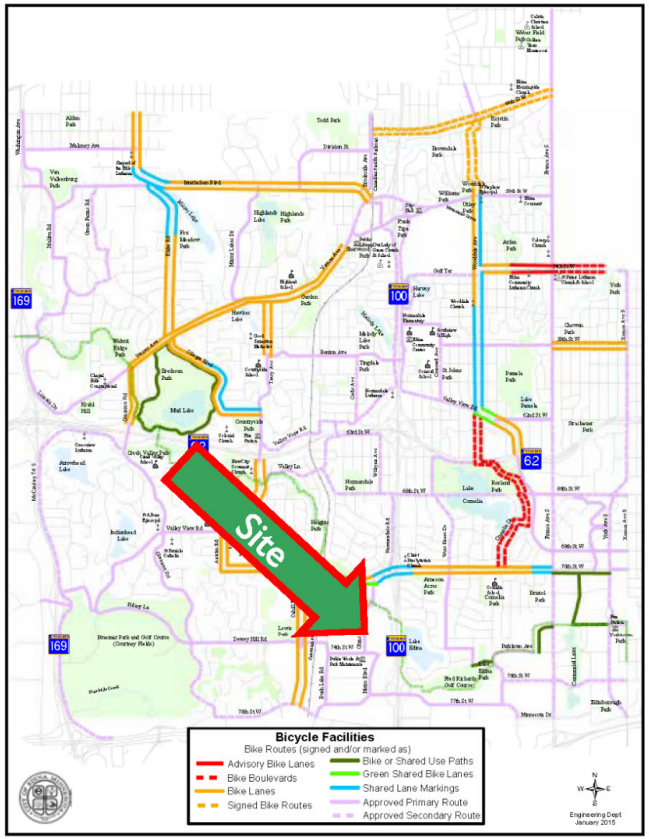
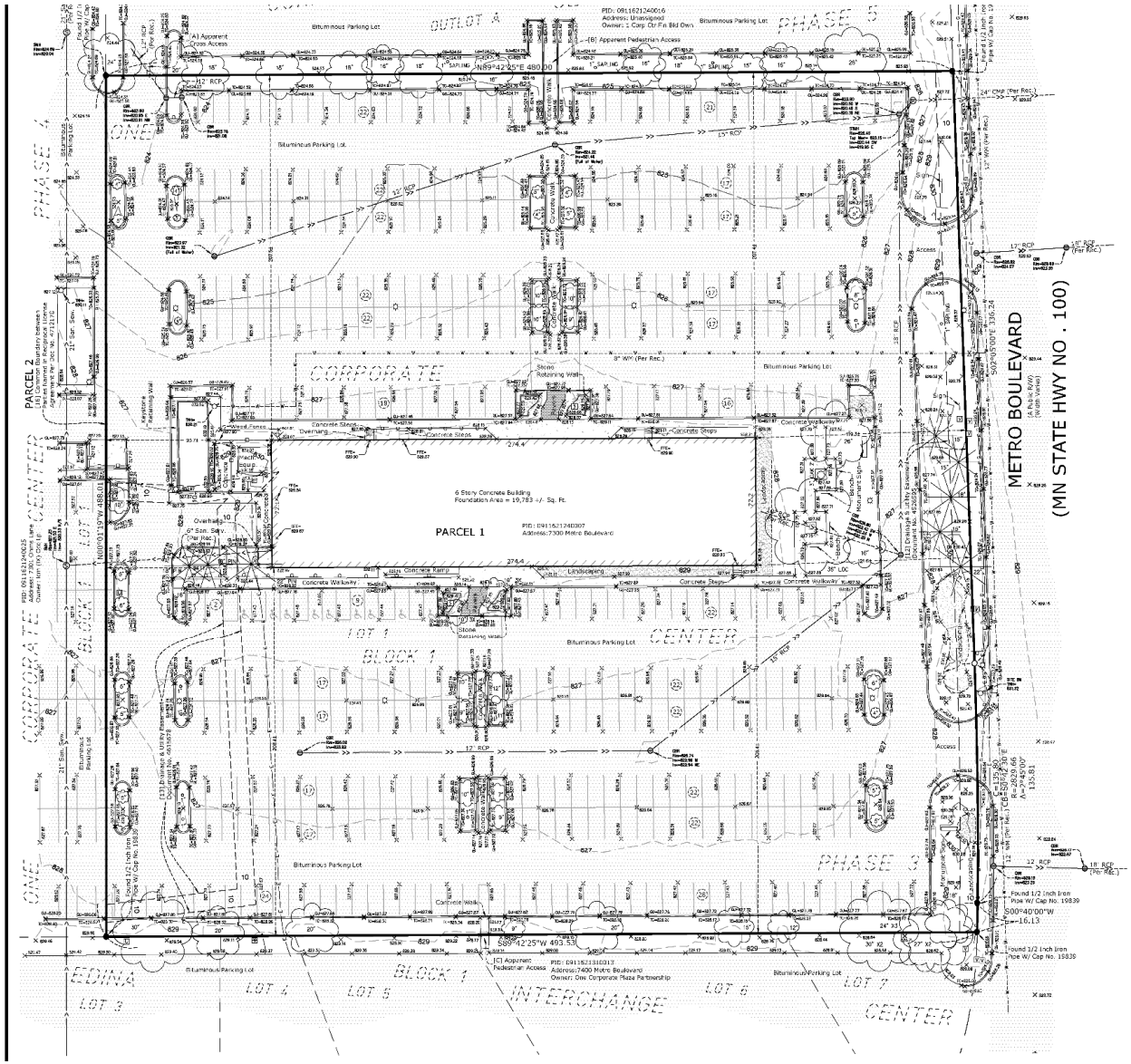


Figure 3.3. Bicycle Facilities Network



Parcel 2:

Reciprocal License Agreement dated April 21, 1982, recorded April 21, 1982, as Document No. 4712170, in the Office of the County Recorder, Hennepin County, Minnesota.

(Abstract Property)

GENERAL SURVEY NOTES

1. Bearings are based on the Hennepin County County Coordinate System (1986 Adjustment).
2. Elevations are based on the NGVD 29 Datum. Site Benchmark is the top nut of the fire hydrant located on the west side of Metro Blvd. approximately 135 feet north of the southeast corner of the subject property, as shown hereon. Elevation = 831.32
3. We have shown the location of utilities to the best of our ability based on observed evidence together with evidence from the following sources: plans obtained from utility companies, plans provided by client, markings by utility companies and other appropriate sources. We have used this information to develop a view of the underground utilities for this site. However, lacking excavation, the exact location of underground features cannot be accurately, completely and reliably depicted. Where additional or more detailed information is required, the client is advised that excavation may be necessary. Also, please note that seasonal conditions may inhibit our ability to visibly observe all the utilities located on the subject property.

ALTA/NSPS LAND TITLE SURVEY NOTES

(numbered per Table A)

1. Monuments placed and/or found at all major corners of the boundary of the surveyed property as shown hereon.
2. Site Address: 7300 Metro Boulevard, Edina, Minnesota 55439.
3. This property is contained in Zone X (area determined to be outside the 0.2% annual chance floodplain) per Flood Insurance Rate Map, Community Panel No. 27053C0451F, effective date of November 4, 2016.
4. The Gross land area is 238,321 +/- square feet or 5.471 +/- acres.
7. (a) Exterior dimensions of buildings at ground level as shown hereon.
8. Substantial features observed in the process of conducting the fieldwork as shown hereon. Please note that seasonal conditions may inhibit our ability to visibly observed all site features located on the subject property.
9. The number of striped parking stalls on this site are as follows: 465 Regular + 9 Handicap = 474 Total Parking Stalls.
13. The names of the adjoining owners of the platted lands, as shown hereon, are based on information obtained from the Hennepin County Interactive Property Map.

SURVEY REPORT

1. This map and report was prepared with the benefit of a Commitment for Title Insurance issued by First American Title Insurance Company, File No. NCS-1192508-MPLS, dated September 27, 2023. We note the following with regards to Schedule B II Exceptions of the herein referenced Title Commitment:
   a. Item no.'s 1-11, 14 and 17-21 are not survey related.
   b. The following are numbered per the referenced Title Commitment:
      [12]. Drainage and utility easements as shown on the recorded plat of One Corporate Center Phase 3 recorded December 2, 1979, as Document No. 4582595. ***Easement as shown hereon along the easterly 10 feet of the subject property.***
      [13]. Easement for drainage and utility purposes in favor of the City of Edina, a Minnesota municipal corporation and the terms and provisions as contained in Easement dated December 12, 1980, recorded January 6, 1981 as Document No. 4613676. ***10 foot wide easement is depicted heron in the southwest portion of the subject property.***
      [15]. Easement for the provision of cable communications and other services and access rights in favor of the Minneapolis Division of Time Warner Cable Inc., doing business as Time Warner Cable and the terms, conditions and provisions as contained in Easement for Cable/Telecommunications Services dated January 21, 2004, recorded February 15, 2006, as Document No. 8740407. ***The Easement affects the subject property. The Easement is blanket in nature and cannot be depicted hereon.***
      [16]. Terms, conditions, provisions, covenants and agreements as contained in Reciprocal License Agreement dated April 21, 1982, recorded April 21, 1982, as Document No. 4712170. ***Agreement allows for free and unrestricted flow of vehicular or pedestrian traffic over the common boundary line between the subject property and the adjacent property to the west.***
2. Conflicts such as (but not limited to): encroachments, protrusions, access, occupation, and easements and/or servitudes:
   [A]. There is an apparent cross access between the subject property and the adjacent property to the north, as shown hereon near the northwest corner of the subject property. We have not been provided with documentation for said cross access.
   [B]. There is an apparent pedestrian access across the north line of the subject property, as shown hereon. We have not been provided with documentation for said pedestrian access.
   [C]. There is an apparent pedestrian access across the south line of the subject property, as shown hereon. We have not been provided with documentation for said pedestrian access.

ALTA CERTIFICATION

To: Wan Hub LLC, a Minnesota limited liability company; Revitalize OCC, LLC, a Minnesota limited liability company; Emerald City Capital, LLC, a Wyoming limited liability company; and First American Title Insurance Company:

This is to certify that this map or plat and the survey on which it is based were made in accordance with the 2021 Minimum Standard Detail Requirements for ALTA/NSPS Land Title Surveys, jointly established and adopted by ALTA and NSPS, and includes items 1, 2, 3, 4, 7(a), 8, 9, and 13 of table A thereof.

The fieldwork was completed on 10-03-2023.

Dated this 27th day of October, 2023.

Rory L. Synstelien Minnesota License No. 44565
rory@civilengroup.com

N

30 15 0 15 30 60
SCALE IN FEET

Linetype & Symbol Legend

ELECTRIC LINE	AIR CONDITIONER
ELECTRIC LINE (RECORD)	CABLE TV BOX
FIBER/COMM. LINE	ELECTRIC MANHOLE
FIBER/COMM. LINE (RECORD)	ELECTRIC TRANSFORMER
GASMAIN	ELECTRICAL METER
GASMAIN (RECORD)	FIBER/COMM. MANHOLE
OVERHEAD UTILITIES	POWER POLE
SANITARY SEWER	GUY WIRE
SANITARY SEWER (RECORD)	GAS METER
STORM SEWER	GAS MANHOLE
STORM SEWER (RECORD)	GAS VALVE
TELEPHONE LINE	ROOF DRAIN
TELEPHONE LINE (RECORD)	SEWER CLEAN OUT
WATERMAIN	SANITARY MANHOLE
WATERMAIN (RECORD)	STORM MANHOLE
CHAINLINK FENCELINE	CATCH BASIN
WOODEN FENCELINE	FLARED END SECTION
GUARDRAIL	TELEPHONE BOX
ACCESS RESTRICTION	TELEPHONE MANHOLE
CONCRETE SURFACE	TRAFFIC SIGNAL
PAVER SURFACE	HYDRANT
BITUMINOUS SURFACE	FIRE CONNECTION
GRAVEL/LANDSCAPE SURFACE	POST INDICATOR VALVE
	WATER MANHOLE
	WATER VALVE
	WELL
	UTILITY VAULT
	UTILITY MANHOLE
	ELECTRICAL OUTLET
	HANDHOLE
	BOLLARD
	FLAG POLE
	FUEL TANK
	HANDICAP SYMBOL
	LIGHT POLE
	MAIL BOX
	SIGN
	CONIFEROUS TREE
	DECIDUOUS TREE
	SOIL BORING
	FOUND IRON MONUMENT
	SET OR TO BE SET IRON MONUMENT
	CAST IRON MONUMENT

Civil Engineering · Surveying · Land Development · Planning
5800 Glenwood Avenue
Golden Valley, MN 55422
civilengroup.com 612 615 0060

PROJECT
Edina Dental Office
7300 Metro Boulevard, Edina, Minnesota 55439

CLIENT
DJR Architecture, Inc.
333 Washington Avenue N. #210, Minneapolis, Minnesota 55401

I HEREBY CERTIFY THAT THIS SURVEY, PLAN, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED LAND SURVEYOR UNDER THE LAWS OF THE STATE OF MINNESOTA.

RORY L. SYNSTELIEN
DATE 10-27-2023 LICENSE NO. 44565

QA/QC

FIELD CREW	DHHS
DRAWN BY	DB
REVIEWED BY	CJ
UPDATED BY	

VICINITY MAP

REVISION SUMMARY

DATE	DESCRIPTION

PROJECT NO. 23089-00

ALTA/NSPS LAND TITLE SURVEY

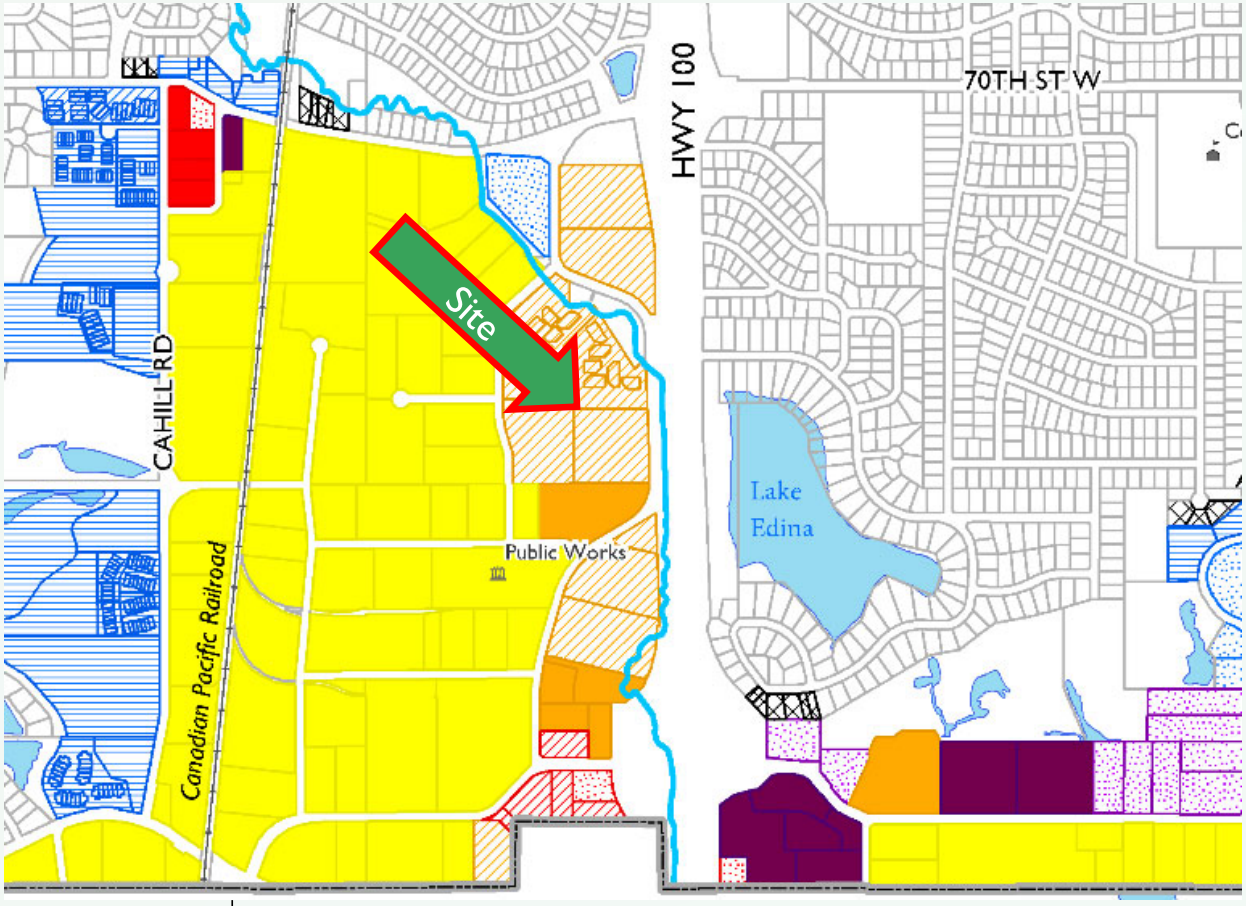
V1.0

The CITY of EDINA

Site Plan with Variances and Subdivision

7300 Metro Boulevard – Dental Office

EdinaMN.gov



Zoning Map

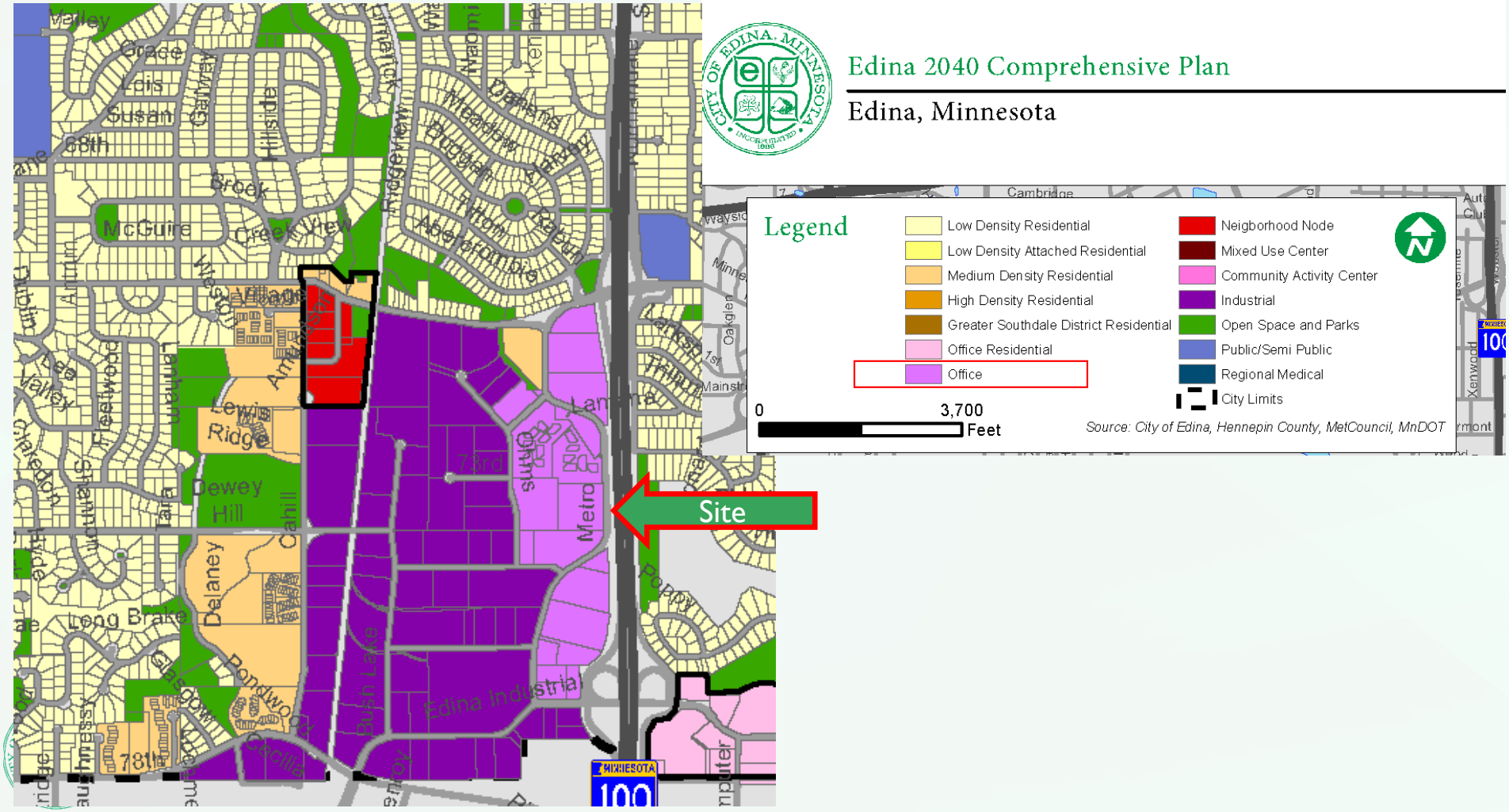
Legend

- Lakes
- Creeks
- Edina Boundary

Zoning

- R-1 - Single Dwelling Unit
- R-2 - Double Dwelling Unit
- PRD-1 - Planned Residence
- PRD-2 - Planned Residence
- PRD-3 - Planned Residence
- PRD-4 - Planned Residence
- PRD-5 - Planned Residence
- PCD-1 - Planned Commercial
- PCD-2 - Planned Commercial
- PCD-3 - Planned Commercial
- PCD-4 - Planned Commercial
- POD-1 - Planned Office
- POD-2 - Planned Office
- RMD - Regional Medical
- PID - Planned Industrial
- PUD - Planned Unit Development
- APD - Automotive Parking
- PSR-4 - Planned Residence
- MDD-4 - Mixed Development
- MDD-5 - Mixed Development
- MDD-6 - Mixed Development

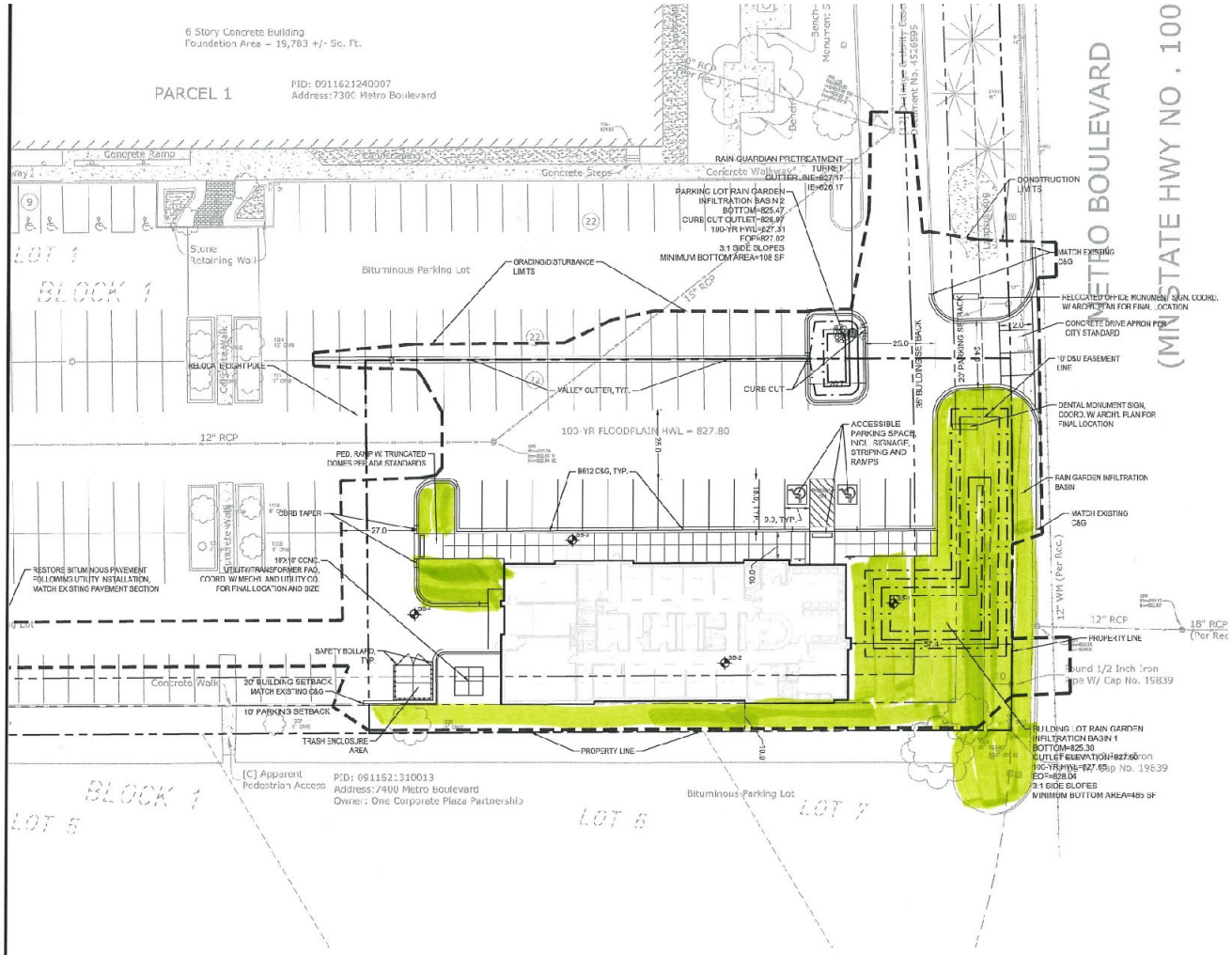
The CITY of EDINA


Edina 2040 Comprehensive Plan
Edina, Minnesota
Legend
Low Density Residential
Low Density Attached Residential
Medium Density Residential
High Density Residential
Greater Southdale District Residential
Office Residential
Office
Neigborhood Node
Mixed Use Center
Community Activity Center
Industrial
Open Space and Parks
Public/Semi Public
Regional Medical
City Limits
0
3,700
Feet
Source: City of Edina, Hennepin County, MetCouncil, MnDOT
Site
McGuire
Lewis Ridge
Dewey Hill
Cahill
Delaney
Long Brake
Edina Industrial
Metro
Lamplighter
Bush Lake
Bryant
Xerwood
Cambridge
Oakglen
Mainstr
Minne
Valley
Grace
Leis
Susan
68th
Brook
Creek View
Ridge View
Naomi
Kenwood
Dahlena
Wheatfield
Duggan
Brown
Abercrombie
Raleigh
Lancaster
Lincoln
Arthur
Dublin
Lee
Deerwood
Lanham
Tara
Clareden
Hyde
Westwood
Glasgow
Bondwood
Cascade
78th
77th
Xerwood
Auto Club
MINNESOTA
100

Existing Site

The CITY of EDINA

METRO BLVD DEVELOPMENT



SITE DATA		
ZONING SUMMARY		
EXISTING ZONING	POD-1 - PLANNED OFFICE	
PROPOSED ZONING		
	REQUIRED	PROVIDED
BUILDING SETBACKS		
FRONT STREET	30'	
REAR YARD	20'	
SIDE STREET	30'	
INTERIOR SIDE YARD	20'	
PARKING SETBACKS		
FRONT - (FROM ROW)	20'	
REAR	10'	
SIDE	10'	
	REQUIRED	PROVIDED
PARKING SUMMARY		
PARKING STALL DIMENSIONS	8.5' X 18'	
COMPACT PARKING STALL DIMENSIONS	7.5' X 16'	
DRIVE AISLE DIMENSIONS	24'	
FIRE ACCESS ROAD DIMENSIONS		
	REQUIRED	PROVIDED
PARKING STALLS - SURFACE	23	
PARKING STALLS - GARAGE	N/A	

SITE PLAN LEGEND:

- LIGHT DUTY BITUMINOUS PAVEMENT (IF APPLICABLE). SEE GEOTECHNICAL REPORT FOR AGGREGATE BASE & WEAR COURSE DEPTH, SEE DETAIL.
- HEAVY DUTY BITUMINOUS PAVEMENT (IF APPLICABLE). SEE GEOTECHNICAL REPORT FOR AGGREGATE BASE & WEAR COURSE DEPTH, SEE DETAIL.
- CONCRETE PAVEMENT (IF APPLICABLE) AS SPECIFIED (PAD OR WALK) SEE GEOTECHNICAL REPORT FOR AGGREGATE BASE & CONCRETE DEPTHS. WITHIN ROW SEE CITY DETAIL. WITHIN PRIVATE PROPERTY SEE C&G DETAIL
- PROPERTY LINE
- CONSTRUCTION LIMITS
- CURB AND GUTTER-SEE NOTES (T.C.) TIP CUT GUTTER WHERE APPLICABLE-SEE PLAN
- TRAFFIC DIRECTIONAL ARROW PAVEMENT MARKINGS
- SIGN AND POST ASSEMBLY. SHOP DRAWINGS REQUIRED. HC = ACCESSIBLE SIGN NP = NO PARKING FIRE LANE ST = STOP CP = COMPACT CAR PARKING ONLY
- ACCESSIBILITY ROUTE ARROW (IF APPLICABLE) DO NOT PAINT.

CITY OF EDINA
MAR 11 2024
PLANNING DEPARTMENT

CITY OF EDINA SITE SPECIFIC NOTES:

1. RESERVED FOR CITY SPECIFIC SITE NOTES.

OWNER INFORMATION

HEMPEL REAL ESTATE
10850 CROSSTOWN CIRCLE, SUITE 600
EDEN PRAIRIE, MN 55344
BRENT JACOBSON
612-355-3609
BJACOBSON@HEMPELCOMPANIES.COM

OPERATIONAL NOTES

SNOW REMOVAL	ALL SNOW SHALL BE STORED ON-SITE OUTSIDE PARKING LOT. WHEN FULL, REMOVAL CO. SHALL REMOVE EXCESS OFF-SITE.
TRASH REMOVAL	TRASH SHALL BE PLACED IN EXTERIOR TRASH AREA AND REMOVED BY COMMERCIAL CO. WEEKLY.
DELIVERIES	DELIVERIES SHALL OCCUR AT THE FRONT DOOR VIA STANDARD COMMERCIAL DELIVERY VEHICLES (UPS, FED-EX, USPS).

SEE SHEET CC.1 FOR GENERAL

SITE AREA CALCULATIONS	EXISTING CONDITION			PROPOSED CONDITION		
IMPERVIOUS SURFACES						
BUILDING COVERAGE	15,792 SF	8.3%		25,919 SF	10.9%	
PAVEMENT	182,650 SF	76.6%		173,577 SF	72.8%	
TOTAL	202,442 SF	84.9%	4.6 AC	199,496 SF	83.7%	4.6 AC
PERVIOUS SURFACES						
TOTAL	35,878 SF	15.1%	0.8 AC	38,824 SF	16.3%	0.9 AC
TOTAL SITE AREA	238,320 SF	100.0%	5.5 AC	238,320 SF	100.0%	5.5 AC
DIFFERENCE (EX. VS PROP.)	-2,946 SF	-1.2%				
FULLY RECONSTRUCTED IMPERVI	28,845 SF					

CivilSite GROUP
Civil Engineering • Surveying • Landscape Architecture
5500 Glenwood Avenue
Golden Valley, MN 55422
civilsitegroup.com 612-615-0060

PRELIMINARY: NOT FOR CONSTRUCTION

PROJECT
EDINA DENTAL OFFICE
7300 METRO BOULEVARD, EDINA, MINNESOTA 55439

HEMPEL REAL ESTATE
10850 CROSSTOWN CIRCLE, SUITE 600, EDEN PRAIRIE, MN 55344

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

Robert A. Latta
DATE 5/11/24 LICENSE NO. 58612

ISSUE/SUBMITTAL SUMMARY

DATE	DESCRIPTION
03/11/2024	CITY SUBMITTAL

DRAWN BY JR
REVIEWED BY RL
PROJECT MANAGER RL
PROJECT NUMBER 2339

REVISION SUMMARY

DATE	DESCRIPTION

SITE PLAN

CC.0

Planning
Submi
03-11-202

EXTERIOR RENDERING

3.11.2024

Site Plan Submittal - Edina Endodontic

7300 Metro Boulevard, Edina, MN 55439

Planning Division
Submitted
03-11-2024

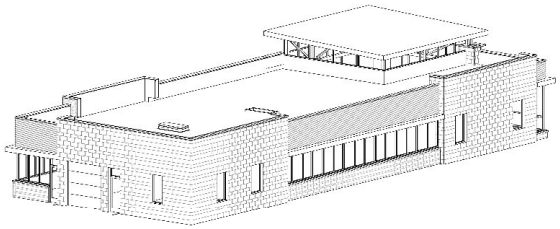
EXTERIOR RENDERING
3.11.2024

DJR

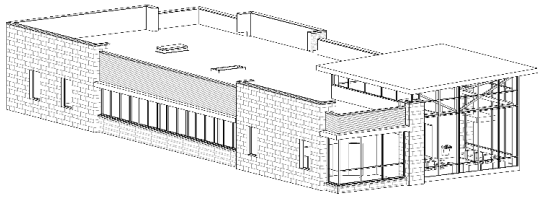
Site Plan Submittal - Edina Endodontic
7300 Metro Boulevard, Edina, MN 55439
23-147.00

©2024 DJR

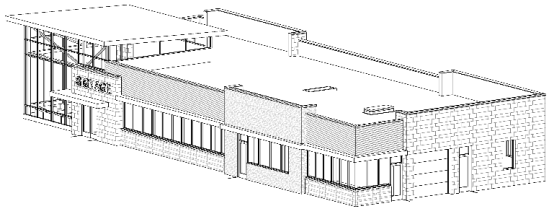
DJR



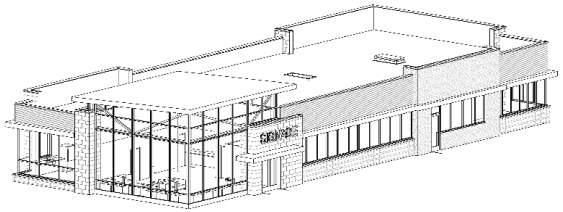
8 Axon SW



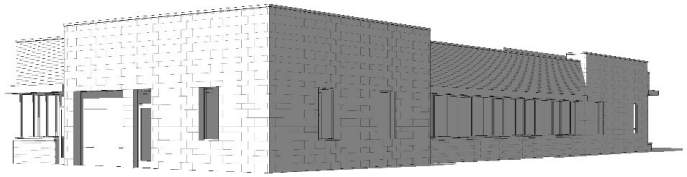
7 Axon SE



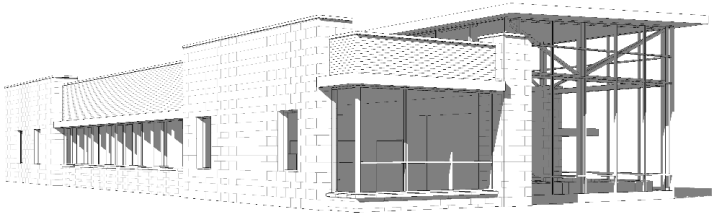
6 Axon NW



5 Axon NE

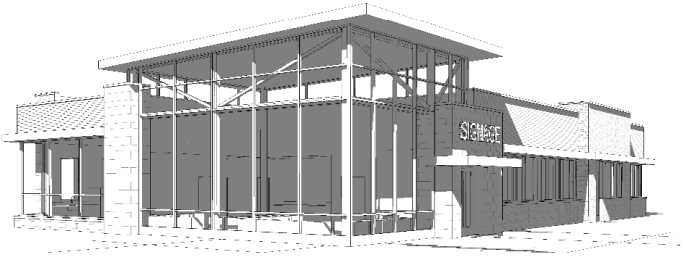


4 3D View 4



3 3D View 3

2 3D View 2



1 3D View 1

CONCEPT DESIGN

EDINA
ENDODONTIC

EDINA, MN

BUILDING EXTERIOR

A2.1

Request Requires

- Site Plan Review.
- Variances: First floor ceiling height from 20 to 10 feet and side yard setback from 20 to 10 feet.
- Subdivision.

The CITY of EDINA

Site Plan

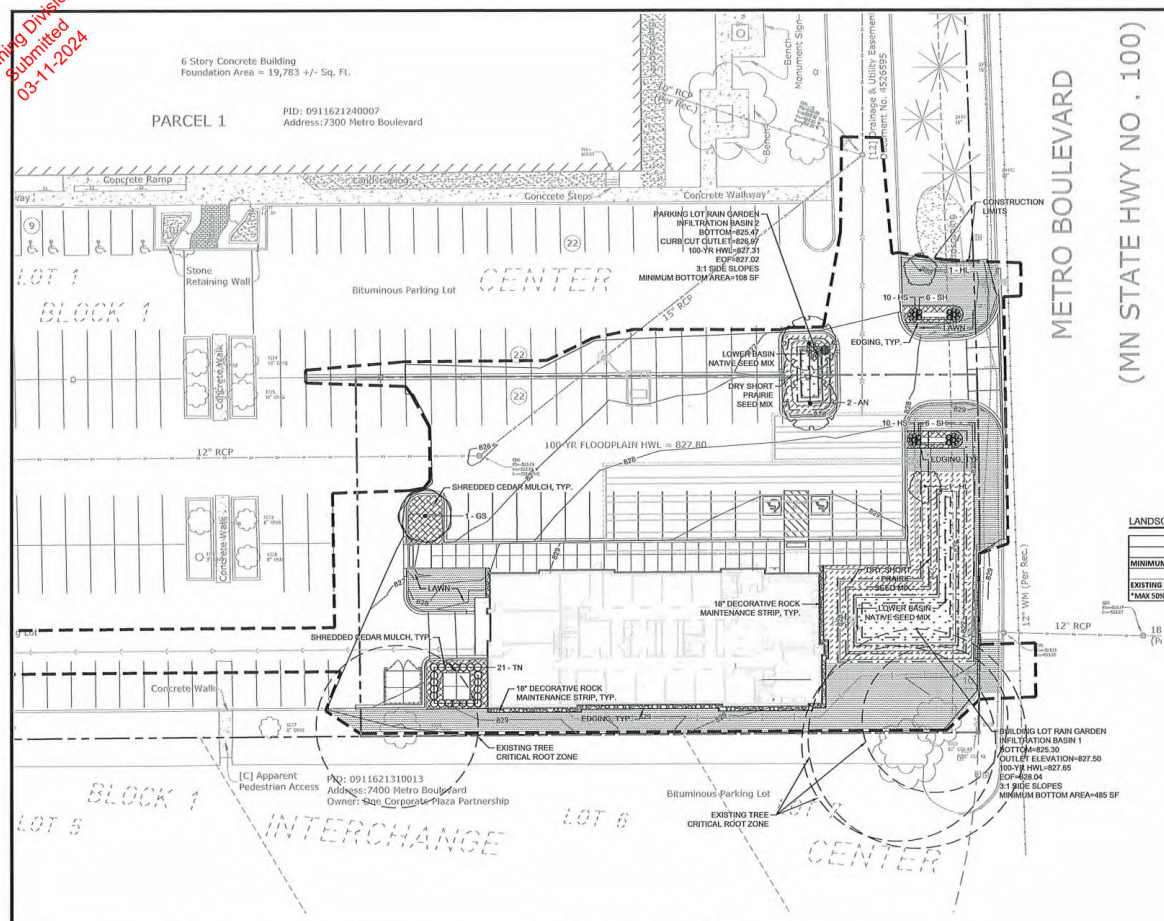
The CITY of EDINA

Compliance Table

	City Standard (POD-2) (Setback measured to the property line)	Proposed (Setback measured to the property line)
Building Setbacks Front – Metro Boulevard Side – North Side – South Rear	 35 feet 20 feet 20 feet 20 feet	 90 feet 200+ feet ***10 feet**** 21 feet
Building height	8 stories & 96 feet	1 story
First floor ceiling height	20 feet	***10 feet****
Floor area ratio (FAR)	.50% 110,790 s.f. existing building plus 6,000 s.f. proposed = 116,790 s.f. total Site area = 238,321s.f.	.50%
Parking	Dental = 1 space/300 s.f. = 20 spaces required Existing building & dental office = 389 spaces	31 spaces proposed 412 spaces total existing and proposed spaces

****Variance Requested***

Planning Division
Submitted
03-11-2024



PLANT SCHEDULE

CODE	COMMON / BOTANICAL NAME	QTY	CONT
DECIDUOUS TREES			
AN	Northwood Red Maple / Acer rubrum 'Northwood'	3	3.5" CAL. B&B
GS	Skyline Honey Locust / Gleditsia triacanthos inermis 'Skyline'	1	3.5" CAL. B&B
ORNAMENTAL TREES			
HL	Limelight Panicle Hydrangea / Hydrangea paniculata 'Limelight'	2	1.5" CAL. B&B
CODE	COMMON / BOTANICAL NAME	QTY	SIZE
SHRUBS			
TN	Nova Japanese Yew / Taxus cuspidata 'Nova'	22	#3 CONT
GRASSES			
SH	Prairie Dropseed / Sporobolus heterolepis	12	#1 CONT
PERENNIALS			
HS	Stella Supreme Daylily / Hemerocallis x 'Stella Supreme'	20	#1 CONT

LANDSCAPE REQUIREMENTS:

OVERSTORY TREE REQUIREMENTS			
	SITE PERIMETER (LINEAR FT)	FT	MINIMUM REQ. TREES
MINIMUM OVERSTORY TREES REQUIRED	1,950.0	40	49
EXISTING PRESERVED TREES TO COUNT TOWARD MINIMUM OVERSTORY TREES REQUIRED			64
*MAX 50% OF ANY ONE SPECIES			

LEGEND

GROUND COVERS

- Lower Basin Native Seed Mix
- Rock Maintanence Strip / Rock Maintanence Strip
- Shredded Cedar Mulch / Shredded Hardwood Mulch
- Blue Grass Based / Sod

PROPOSED CANOPY TREE SYMBOLS - SEE PLANT SCHEDULE AND PLAN FOR SPECIES AND PLANTING SIZES

PROPOSED EVERGREEN TREE SYMBOLS - SEE PLANT SCHEDULE AND PLAN FOR SPECIES AND PLANTING SIZES

PROPOSED ORNAMENTAL TREE SYMBOLS - SEE PLANT SCHEDULE AND PLAN FOR SPECIES AND PLANTING SIZES

PROPOSED DECIDUOUS AND EVERGREEN SHRUB SYMBOLS - SEE PLANT SCHEDULE AND PLAN FOR SPECIES AND PLANTING SIZES

PROPOSED PERENNIAL PLANT SYMBOLS - SEE PLANT SCHEDULE AND PLAN FOR SPECIES AND PLANTING SIZES

EDGING - SHALL BE COMMERCIAL GRADE, 4" DEPTH ALUMINUM, BLACK OR DARK GREEN IN COLOR, INCLUDE ALL CONNECTORS, STAKES, & ALL APPURTENANCES PER MANUF. INSTALL PER MANUF. INSTRUC./SPECS.
DECORATIVE BOULDERS (ROUNDED & BLOCK STYLE), 18"-30" DIA.

CONSTRUCTION LIMITS

MULCH SCHEDULE

AREA	MULCH TYPE	EDGING	FABRIC	REMARKS
TREE RINGS	4" DEPTH, SHREDDED CEDAR	YES	NO	SEE DETAIL SHT. L1.1
PLANTING BEDS	4" DEPTH, SHREDDED CEDAR	YES	NO	
MAINT. STRIP AT BUILDING FOUNDATION	NA	NA	NA	
PLAYGROUND MULCH	12" DEPTH ENGINEERED PLAYGROUND MULCH, SEE REMARKS. DEPTH AS SPECIFIED BY PLAYGROUND EQUIPMENT MANUFACTURER.	SEE SITE PLAN	YES	ORGANIC MULCH DESIGNED SPECIFICALLY FOR USE ON PLAYGROUNDS/CERTIFIED BY ANSI/ASTM FOR PLAYGROUND USE. (I.E. SYLVA SOFTSTEP, SET,INC. PLAYGROUND, OR EQUIVALENT)
DOG PARK MULCH	4" DEPTH, SHREDDED WOOD, SEE REMARKS	YES	YES	ORGANIC MULCH SPECIFICALLY FOR USE IN DOG PARKS OR PLAYGROUNDS. SHALL KNIT TOGETHER TO FORM MATTED, ACCESSIBLE SURFACE. (I.E. SYLVA SOFTSTEP, SET,INC. PLAYGROUND, OR EQUIVALENT), PROVIDE SHOP DRAWINGS/DATA SHEETS FOR REVIEW.
NATIVE SEED AREAS	NA	NA	NA	
NOTE: COORDINATE ALL MULCH AND PLANTING BED MATERIAL PRIOR TO INSTALLATION, PROVIDE SAMPLES AND SHOP DRAWINGS/PHOTOS/DATA SHEETS OF ALL MATERIALS				

POLLINATOR SAFE PLANT MATERIAL:

1. THE CONTRACTOR SHALL PROVIDE ONLY PLANT MATERIAL FREE OF NEONICOTINOID BASED INSECTICIDES AND/OR TREATMENTS OF ANY KIND, INCLUDING BY NOT LIMITED TO IMIDACLOPRID (CONFIDOR, ADMIRE, GAUCHO, ADVOCATE), THIAMETHOXAM (ACTARA, PLATINUM, CRUISER), CLOTHIANIDIN (PONCHO, DANTOSU, DANTOP), ACETAMIPRID (MOSPILAN, ASSAIL, CHIPCOTRISTAR), THIACLOPRID (CALYPSO), DINOTEFURAN (STARKLE, SAFARI, VENOM), AND NITENPYRAM (CAPSTAR, GUARDIAN).
2. CONTRACTOR SHALL CERTIFY, THROUGH SUPPLIERS POLICY STATEMENT OR AFFIDAVIT, THAT NO NEONICOTINOID BASED INSECTICIDES HAVE BEEN USED ON SITE OR DIRECTLY ADJACENT TO THE GROWING OR STORAGE PLOTS OF THE SUPPLIED PLANT MATERIAL, INCLUDING THE PLANTING OF AGRICULTURAL (OR OTHER) SEED TREATED WITH NEONICS.

PLANTING SEASON SCHEDULE

SEASON	CONIFEROUS	DECIDUOUS	REMARKS
SPRING PLANTING	APRIL 15 - JUNE 15	APRIL 15 - JUNE 15	
FALL PLANTING	AUGUST 21 - SEPTEMBER 30	AUGUST 15 - NOVEMBER 15	
NOTE: ADJUSTMENTS TO PLANTING DATES MUST BE APPROVED IN WRITING BY THE LANDSCAPE ARCHITECT.			

SEE SHEET L1.1 FOR GENERAL LANDSCAPE NOTES

Know what's below. Call before you dig.

1" = 20'-0"
10'-0" 0 20'-0"

CivilSite GROUP
Civil Engineering • Surveying • Landscape Architecture
5000 Glenwood Avenue
Golden Valley, MN 55422
civilsitegroup.com 612-615-0060

PRELIMINARY: NOT FOR CONSTRUCTION

PROJECT
EDINA DENTAL OFFICE
7300 METRO BOULEVARD, EDINA, MINNESOTA 55439

HEMPEL REAL ESTATE
10550 CROSSTOWN CIRCLE, SUITE 600, EDEN PRAIRIE, MN 55344

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED LANDSCAPE ARCHITECT UNDER THE LAWS OF THE STATE OF MINNESOTA.

Patrick J. Sarver
DATE 3/11/24 LICENSE NO. 24904

ISSUE/SUBMITTAL SUMMARY

DATE	DESCRIPTION
03/11/2024	CITY SUBMITTAL

DRAWN BY JR
REVIEWED BY PL
PROJECT MANAGER PL
PROJECT NUMBER 2205

REVISION SUMMARY

DATE	DESCRIPTION

LANDSCAPE PLAN

L1.0

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Planning Division
Submitted
03-11-2024

EXTERIOR RENDERING
3.11.2024

DJR

Site Plan Submittal - Edina Endodontic
7300 Metro Boulevard, Edina, MN 55439
23-147.00

©2024 DJR

Planning Division
Submitted
03-11-2024

WOOD LOOK METAL PANEL - Longboard - Dark National Walnut

STOREFRONT / CURTAINWALL / CANOPIES
- Annodized Aluminum - Dark Bronze

STONE (COLOR 1) - US Stone - Plaza Grey

STONE (COLOR 2) - US Stone - Cottonwood

MANUFACTURED STONE (COLOR 3) - Coronado Stone - Black Forest

DJR

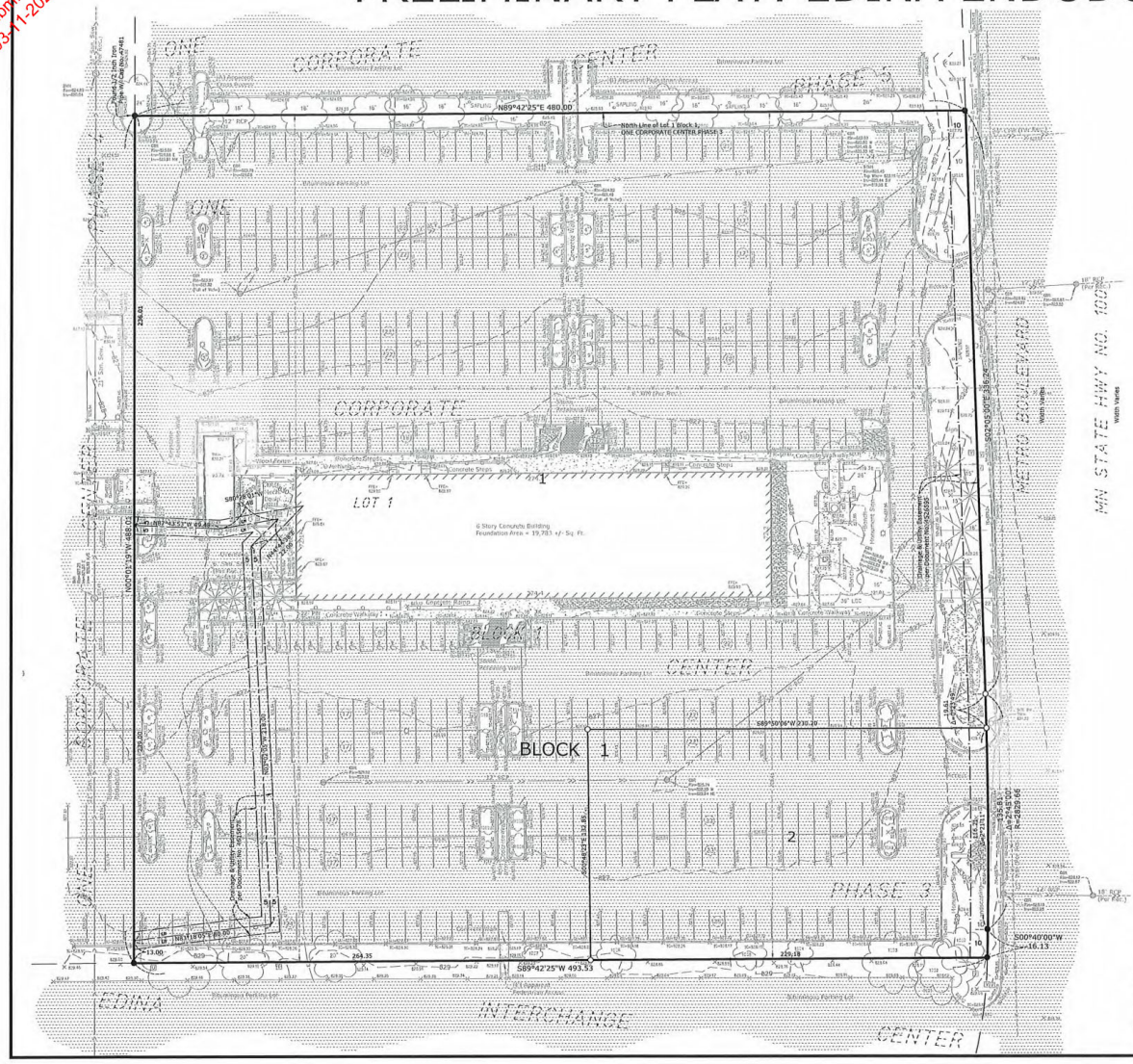
©2024 DJR

MATERIALS
3.11.2024

Site Plan Submittal - Edina Endodontic
7300 Metro Boulevard, Edina, MN 55439
23-147.00

Planning Division
Submitted
03-11-2024

PRELIMINARY PLAT: EDINA ENDODONTICS


ONE CORPORATE CENTER
PHASE 3
N89°42'25"E 480.00
North Line of Lot 1 Block 1
ONE CORPORATE CENTER PHASE 3
CORPORATE
LOT 1
6 Story Concrete Building
Foundation Area = 19,783 +/- Sq. Ft.
BLOCK 1
CENTER
S89°50'06"W 230.20
BLOCK 1
2
PHASE 3
S00°40'00"W 16.13
S89°42'25"W 493.53
264.35
229.18
EDINA INTERCHANGE
CENTER
MN STATE HWY NO. 100
METRO BOULEVARD
Width Varies
S02°03'30"E 386.27
N00°01'13"W 489.00
236.80
230.00
N00°01'13"W 489.00
S00°01'13"E 489.00
N00°01'13"W 489.00
Drainage & Utility Easement per Document No. 4623455
Drainage & Utility Easement per Document No. 4623455
Concrete Walkways
Concrete Steps
Bituminous Parking Lot
Bituminous Walkways
Concrete Curb
Concrete Wall Plug
Bituminous Pedestrian Ramp
Bituminous Parking Lot
Bituminous Pedestrian Ramp
Bituminous Parking Lot
Bituminous Pedestrian Ramp
Bituminous Parking Lot

Primary Issue

- **Are the Variances for first floor height and transparency justified?**

The CITY of EDINA

Primary Issue

- **Variance Criteria**

1. ***The variances would be in harmony with the general purposes and intent of the ordinance.***

2. ***The variance would be consistent with the Comprehensive Plan.***

3. ***There are practical difficulties in complying with the ordinance. The term "practical difficulties" means the following:***

   i. ***The property owner proposes to use the property in a reasonable manner not permitted by the Zoning Ordinance.***

   ii. ***The plight of the landowner is due to circumstances unique to the property not created by the landowner.***

   iii. ***The variance, if granted, will not alter the essential character of the locality.***

The CITY of EDINA

Primary Issue

- **Variance Criteria**

1. The requested variances meet the variance criteria.

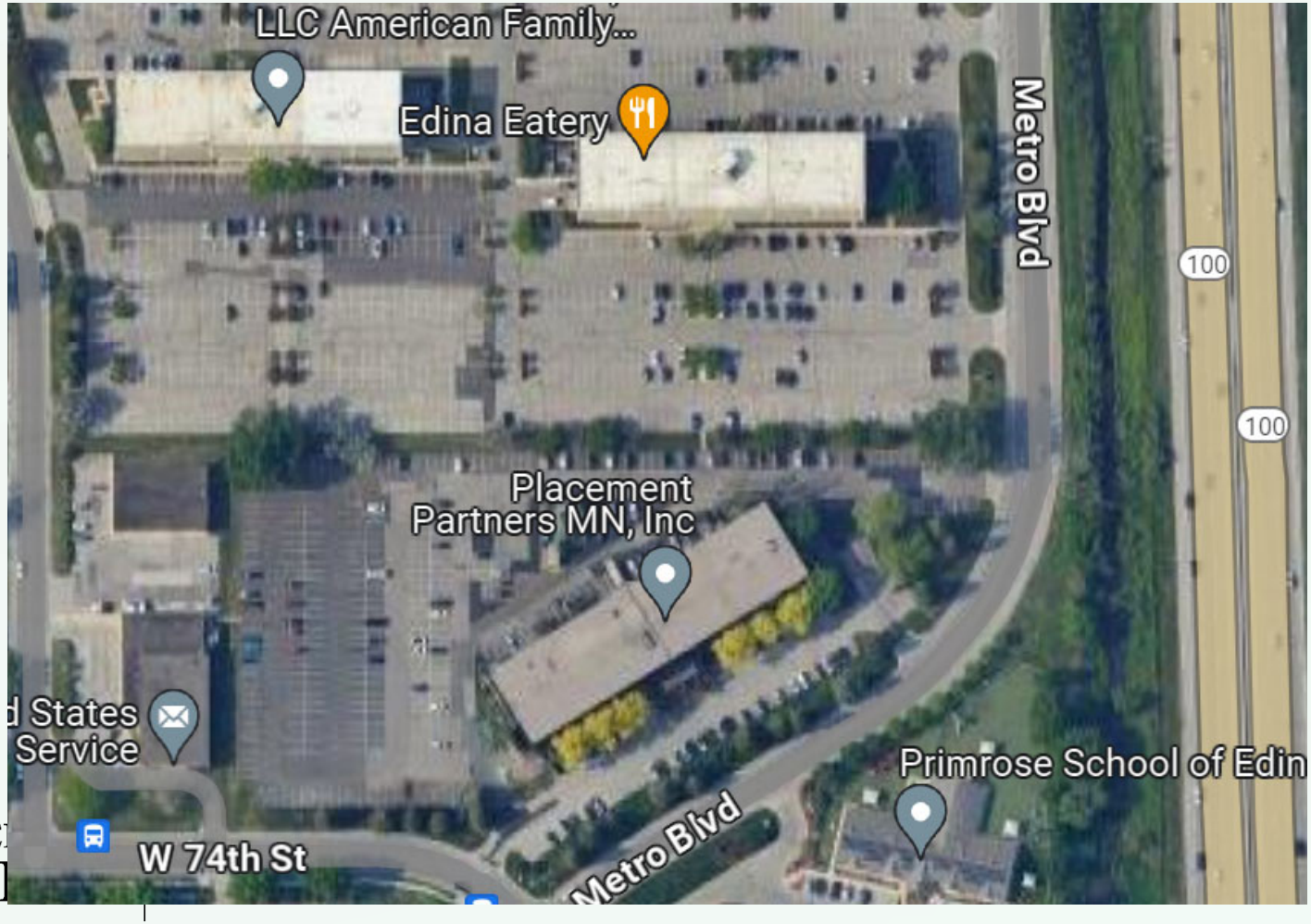
2. The intent of the side yard setback is to provide adequate spacing between buildings. The proposed dental office would allow for adequate space from the nearest building. The building to the south is setback further on the lot, 165 feet back from the street and is 60 feet back from the side lot line. The rear of the proposed dental office would be over 70 feet away.

3. These variances are reasonable given the distance separation from the closest building at over 70 feet. The reason for the request is that the building could be moved up closer to the street, as recommended in the sketch plan. With a closer setback to the side lot line, they are able to provide parking along the side of the building and not disturb the location of the existing drive-aisle.

4. The proposed use of the building is consistent with the Comprehensive Plan.

5. There are no buildings in this area with 20-foot first floor ceiling height.

6. Stantec conducted a traffic and parking study and concluded that the project would have minimal impact on existing roads and there would be adequate parking.


LLC American Family...
Edina Eatery
Metro Blvd
100
100
Placement
Partners MN, Inc
d States
Service
Primrose School of Edin
W 74th St
Metro Blvd

The C
EDI

Recommendation

The CITY of EDINA

PEDESTRIAN NETWORK

The goal of the City's pedestrian network is to provide safe movement for all ages and abilities and to encourage active lifestyles. It should provide network continuity with broad geographic coverage and without notable gaps. Figure 3.2 below indicates locations of existing and future proposed pedestrian facilities. Refer to "Pedestrian Facilities" design guidelines in Chapter 6 for specific guidance regarding the application of these facilities.

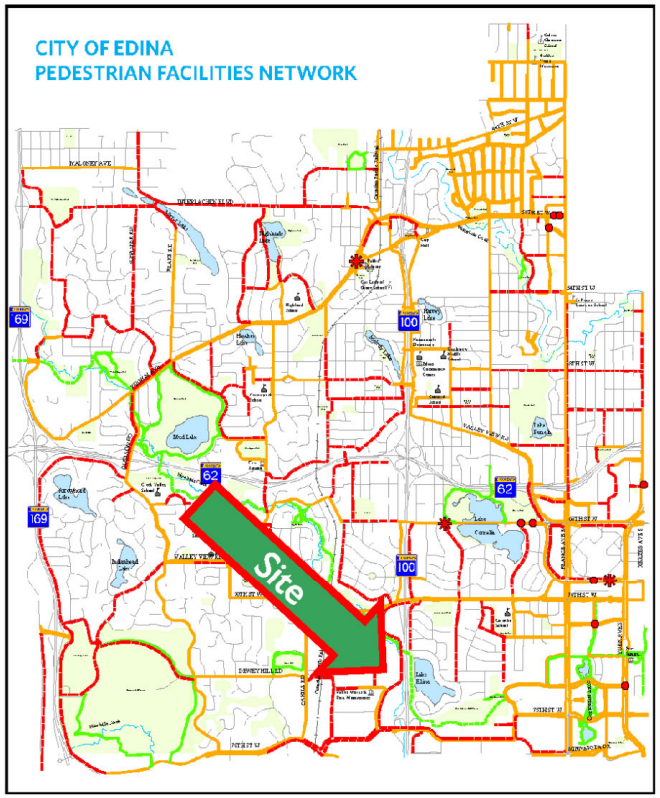


Figure 3.2. Edina Living Streets Classification Map

BICYCLE NETWORK

Edina's Living Streets should provide safe, convenient and comfortable access for bicyclists throughout the city. Edina's network of Living Streets shall accommodate all types, levels, and ages of bicyclists. Figure 3.3 below indicates locations of existing and future proposed bicycle facilities. Refer to "Bicycle Facilities" design guidelines in Chapter 6 for specific guidance regarding the application of these facilities.

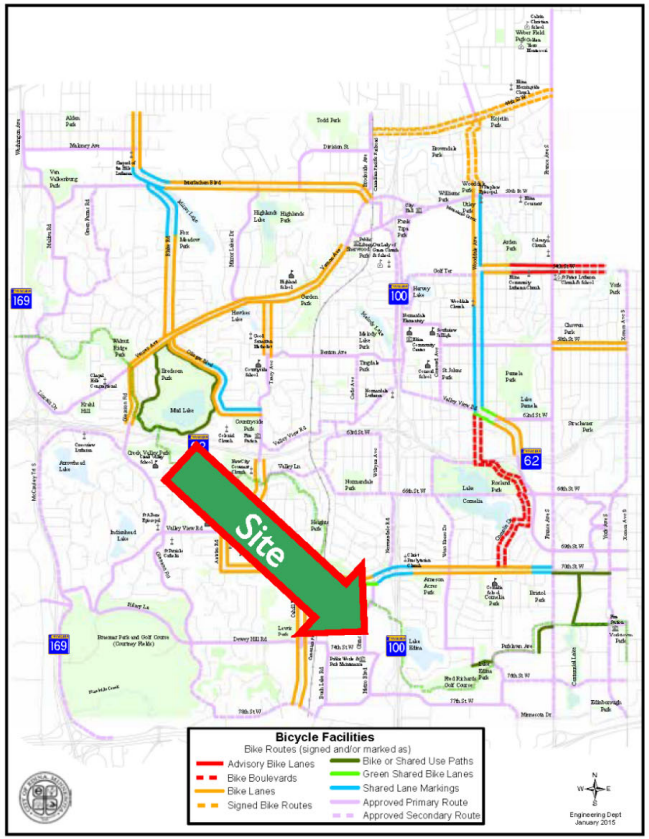
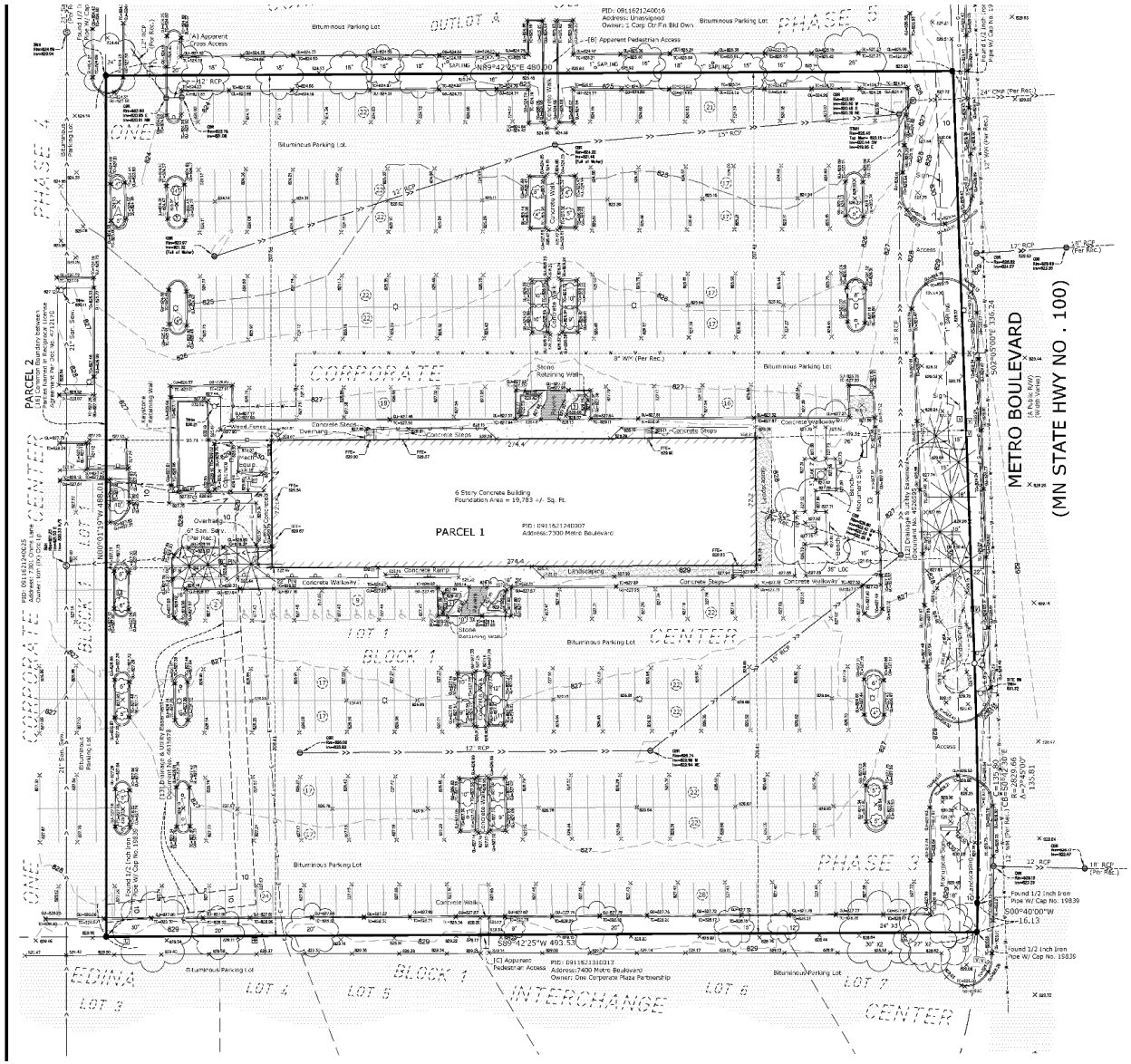


Figure 3.3. Bicycle Facilities Network



Parcel 2:

Reciprocal License Agreement dated April 21, 1982, recorded April 21, 1982, as Document No. 4712170, in the Office of the County Recorder, Hennepin County, Minnesota.

(Abstract Property)

GENERAL SURVEY NOTES

1. Bearings are based on the Hennepin County County Coordinate System (1986 Adjustment).
2. Elevations are based on the NGVD 29 Datum. Site Benchmark is the top nut of the fire hydrant located on the west side of Metro Blvd. approximately 135 feet north of the southeast corner of the subject property, as shown hereon. Elevation = 831.32
3. We have shown the location of utilities to the best of our ability based on observed evidence together with evidence from the following sources: plans obtained from utility companies, plans provided by client, markings by utility companies and other appropriate sources. We have used this information to develop a view of the underground utilities for this site. However, lacking excavation, the exact location of underground features cannot be accurately, completely and reliably depicted. Where additional or more detailed information is required, the client is advised that excavation may be necessary. Also, please note that seasonal conditions may inhibit our ability to visibly observe all the utilities located on the subject property.

ALTA/NSPS LAND TITLE SURVEY NOTES

(numbered per Table A)

1. Monuments placed and/or found at all major corners of the boundary of the surveyed property as shown hereon.
2. Site Address: 7300 Metro Boulevard, Edina, Minnesota 55439.
3. This property is contained in Zone X (area determined to be outside the 0.2% annual chance floodplain) per Flood Insurance Rate Map, Community Panel No. 27053C0451F, effective date of November 4, 2016.
4. The Gross land area is 238,321 +/- square feet or 5.471 +/- acres.
7. (a) Exterior dimensions of buildings at ground level as shown hereon.
8. Substantial features observed in the process of conducting the fieldwork as shown hereon. Please note that seasonal conditions may inhibit our ability to visibly observed all site features located on the subject property.
9. The number of striped parking stalls on this site are as follows: 465 Regular + 9 Handicap = 474 Total Parking Stalls.
13. The names of the adjoining owners of the platted lands, as shown hereon, are based on information obtained from the Hennepin County Interactive Property Map.

SURVEY REPORT

1. This map and report was prepared with the benefit of a Commitment for Title Insurance issued by First American Title Insurance Company, File No. NCS-1192508-MPLS, dated September 27, 2023. We note the following with regards to Schedule B II Exceptions of the herein referenced Title Commitment:
   a. Item no.'s 1-11, 14 and 17-21 are not survey related.
   b. The following are numbered per the referenced Title Commitment:
      [12]. Drainage and utility easements as shown on the recorded plat of One Corporate Center Phase 3 recorded December 2, 1979, as Document No. 4582595. ***Easement as shown hereon along the easterly 10 feet of the subject property.***
      [13]. Easement for drainage and utility purposes in favor of the City of Edina, a Minnesota municipal corporation and the terms and provisions as contained in Easement dated December 12, 1980, recorded January 6, 1981 as Document No. 4613676. ***10 foot wide easement is depicted heron in the southwest portion of the subject property.***
      [15]. Easement for the provision of cable communications and other services and access rights in favor of the Minneapolis Division of Time Warner Cable Inc., doing business as Time Warner Cable and the terms, conditions and provisions as contained in Easement for Cable/Telecommunications Services dated January 21, 2004, recorded February 15, 2006, as Document No. 8740407. ***The Easement affects the subject property. The Easement is blanket in nature and cannot be depicted hereon.***
      [16]. Terms, conditions, provisions, covenants and agreements as contained in Reciprocal License Agreement dated April 21, 1982, recorded April 21, 1982, as Document No. 4712170. ***Agreement allows for free and unrestricted flow of vehicular or pedestrian traffic over the common boundary line between the subject property and the adjacent property to the west.***
2. Conflicts such as (but not limited to): encroachments, protrusions, access, occupation, and easements and/or servitudes:
   [A]. There is an apparent cross access between the subject property and the adjacent property to the north, as shown hereon near the northwest corner of the subject property. We have not been provided with documentation for said cross access.
   [B]. There is an apparent pedestrian access across the north line of the subject property, as shown hereon. We have not been provided with documentation for said pedestrian access.
   [C]. There is an apparent pedestrian access across the south line of the subject property, as shown hereon. We have not been provided with documentation for said pedestrian access.

ALTA CERTIFICATION

To: Wan Hub LLC, a Minnesota limited liability company; Revitalize OCC, LLC, a Minnesota limited liability company; Emerald City Capital, LLC, a Wyoming limited liability company; and First American Title Insurance Company:

This is to certify that this map or plat and the survey on which it is based were made in accordance with the 2021 Minimum Standard Detail Requirements for ALTA/NSPS Land Title Surveys, jointly established and adopted by ALTA and NSPS, and includes items 1, 2, 3, 4, 7(a), 8, 9, and 13 of table A thereof.

The fieldwork was completed on 10-03-2023.

Dated this 27th day of October, 2023.

Rory L. Synstelien Minnesota License No. 44565
rory@civilengroup.com

N

30 15 0 15 30 60
SCALE IN FEET

Linetype & Symbol Legend

ELECTRIC LINE	AIR CONDITIONER
ELECTRIC LINE (RECORD)	CABLE TV BOX
FIBER/COMM. LINE	ELECTRIC MANHOLE
FIBER/COMM. LINE (RECORD)	ELECTRIC TRANSFORMER
GASMAIN	ELECTRICAL METER
GASMAIN (RECORD)	FIBER/COMM. MANHOLE
OVERHEAD UTILITIES	POWER POLE
SANITARY SEWER	GUY WIRE
SANITARY SEWER (RECORD)	GAS METER
STORM SEWER	GAS MANHOLE
STORM SEWER (RECORD)	GAS VALVE
TELEPHONE LINE	ROOF DRAIN
TELEPHONE LINE (RECORD)	SEWER CLEAN OUT
WATERMAIN	SANITARY MANHOLE
WATERMAIN (RECORD)	STORM MANHOLE
CHAINLINK FENCELINE	CATCH BASIN
WOODEN FENCELINE	FLARED END SECTION
GUARDRAIL	TELEPHONE BOX
ACCESS RESTRICTION	TELEPHONE MANHOLE
CONCRETE SURFACE	TRAFFIC SIGNAL
PAVER SURFACE	HYDRANT
BITUMINOUS SURFACE	FIRE CONNECTION
GRAVEL/LANDSCAPE SURFACE	POST INDICATOR VALVE
	WATER MANHOLE
	WATER VALVE
	WELL
	UTILITY VAULT
	UTILITY MANHOLE
	ELECTRICAL OUTLET
	HANDHOLE
	BOLLARD
	FLAG POLE
	FUEL TANK
	HANDICAP SYMBOL
	LIGHT POLE
	MAIL BOX
	SIGN
	CONIFEROUS TREE
	DECIDUOUS TREE
	SOIL BORING
	FOUND IRON MONUMENT
	SET OR TO BE SET IRON MONUMENT
	CAST IRON MONUMENT

Civil Engineering · Surveying · Land Development · Planning
5800 Glenwood Avenue
Golden Valley, MN 55422
civilengroup.com 612 615 0060

PROJECT
Edina Dental Office
7300 Metro Boulevard, Edina, Minnesota 55439

CLIENT
DJR Architecture, Inc.
333 Washington Avenue N. #210, Minneapolis, Minnesota 55401

I HEREBY CERTIFY THAT THIS SURVEY, PLAN, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED LAND SURVEYOR UNDER THE LAWS OF THE STATE OF MINNESOTA.

RORY L. SYNSTELIEN
DATE 10-27-2023 LICENSE NO. 44565

QA/QC

FIELD CREW	DHHS
DRAWN BY	DB
REVIEWED BY	CJ
UPDATED BY	

VICINITY MAP

REVISION SUMMARY

DATE	DESCRIPTION

PROJECT NO. 23089-00

ALTA/NSPS LAND TITLE SURVEY

V1.0