Agenda Planning Commission City Of Edina, Minnesota City Hall, Council Chambers

Tuesday, October 25, 2022 7:00 PM

Watch the meeting on cable TV or at EdinaMN.gov/LiveMeetings or Facebook.com/EdinaMN.

Participate in Community Comment and Public Hearings Call 786-496-5601 Enter Conference Pin 8550148#

Press *1 on your telephone keypad when you would like to get in the queue to speak An operator will introduce you when it is your turn

- I. Call To Order
- II. Roll Call
- III. Approval Of Meeting Agenda
- IV. Approval Of Meeting Minutes
 - A. Draft Minutes of Regular Meeting September 28, 2022
 - B. Draft Minutes of Regular Meeting October 12, 2022
- V. Community Comment

During "Community Comment," the Board/Commission will invite residents to share relevant issues or concerns. Individuals must limit their comments to three minutes. The Chair may limit the number of speakers on the same issue in the interest of time and topic. Generally speaking, items that are elsewhere on tonight's agenda may not be addressed during Community Comment. Individuals should not expect the Chair or Board/Commission Members to respond to their comments tonight. Instead, the Board/Commission might refer the matter to staff for consideration at a future meeting.

- VI. Public Hearings
 - A. Site Plan Review and Ordinance Amendment 4901 West 77th Street
 - B. Subdivision with Lot Area, Width and Depth Variances 6416 McCauley Circle
- VII. Chair And Member Comments

VIII. Staff Comments

IX. Adjournment

The City of Edina wants all residents to be comfortable being part of the public process. If you need assistance in the way of hearing amplification, an interpreter, large-print documents or something else, please call 952-927-8861 72 hours in advance of the meeting.



CITY OF EDINA

4801 West 50th Street Edina, MN 55424 www.edinamn.gov

Date:	October 25, 2022	Agenda Item #: IV.A.
To:	Planning Commission	Item Type:
From:	Liz Olson, Administrative Support Specialist	Minutes
Subject:	Draft Minutes of Regular Meeting September 28, 2022	Item Activity: Action

ACTION REQUESTED:

Approve the draft minutes from September 28, 2022.

INTRODUCTION:

ATTACHMENTS:

Draft Minutes Planning Commission 9-28-22



Minutes City Of Edina, Minnesota Planning Commission Edina City Hall Council Chambers September 28, 2022

I. Call To Order

Chair Agnew called the meeting to order at 7:00 PM.

II. <u>Roll Call</u>

Answering the roll call were: Commissioners Miranda, Strauss, Gandhi, Alkire, Olsen, Padilla, Smith, and Chair Agnew. Staff Present: Cary Teague, Community Development Director, Liz Olson, Administrative Support Specialist.

Absent from the roll call: Commissioner Bennett.

III. Approval Of Meeting Agenda

Commissioner Strauss moved to approve the September 28, 2022, agenda. Commissioner Padilla seconded the motion. Motion carried unanimously.

IV. <u>Approval Of Meeting Minutes</u> <u>A. Minutes: Planning Commission, September 14, 2022</u>

Commissioner Smith moved to approve the September 14, 2022, meeting minutes. Commissioner Alkire seconded the motion. Motion carried unanimously.

V. <u>Community Comment</u>

None.

VI. <u>Public Hearings</u> <u>A. Comprehensive Plan Amendment, Rezoning and Site Plan with Multiple Variances – 6106 Vernon Avenue</u>

Director Teague presented the request of 6106 Vernon Avenue for a Comprehensive Plan Amendment, Rezoning and Site Plan with Multiple Variances. Staff recommends denial of the Comprehensive Plan Amendment, Rezoning and Site Plan with Multiple Variances, as requested subject to the findings and conditions listed in the staff report.

Staff answered Commission questions.

Appearing for the Applicant

Mr. Chris Davis, 6637 Parkwood Road, Edina, Special X Properties addressed the Commission and answered questions of the Commission.

Mr. Marty Collins, Special X Properties, addressed the Commission on different possible entities for the property.

Public Hearing

Mr. Matthew Pepper, 6004 Eden Prairie Road, addressed the Commission and indicated he was against this proposal.

Ms. Tracy Pepper, 6004 Eden Prairie Road, addressed the Commission and indicated she was also against this proposal.

Mr. Jeff Knapp, Parkwood Condominiums, 6005 Eden Prairie Road #207, addressed the Commission and suggested another alternative use for the property.

Mr. Samuel Reed, 6012 Eden Prairie Road, addressed the Commission and stated he was not in favor of this proposed development.

Ms. Nancy Sheer, 6105 Eden Prairie Road, addressed the Commission and stated she was opposed to the proposed development.

Ms. Joanne Dudek, 6021 Vernon Avenue, addressed the Commission and stated was against the proposed development.

Ms. Sara Irwin, 6025 Kamar Drive, addressed the Commission and explained she was not in favor of this proposal.

Mr. John Wellborn, 6008 Eden Prairie Road, addressed the Commission and indicated he was against this proposed development.

Mr. Jeremie Kass, 6016 Kamar Drive, addressed the Commission and stated his family was against this proposal.

Mr. Todd Young, 5511 Highwood Drive W., addressed the Commission and indicated being an architect he was against this proposed development.

Mr. Scott Forbes, 6101 Saxony Road, addressed the Commission and explained he was fundamentally against this proposal.

Ms. Mary Kenealy-Bredice, 6020 Kamar Drive, addressed the Commission and indicated she was against the proposed development.

Mr. Marty Collins, 6104 Jeffrey Lane, addressed the Commission and explained he was in favor of this development. He reviewed some other types of businesses that could go onto the property.

Mr. Tom Kohlbry, 6004 Kamar Drive, addressed the Commission and indicated he was against this proposed development.

Ms. Sue Ettinger-Levy, 6102 Arctic Way, addressed the Commission and indicated she was in support of this proposed development.

Ms. Heidi Moon, 6009 Leslee Lane, addressed the Commission and explained she was in support of the development.

Mr. Bobby McConnell, 6029 Kamar Drive, addressed the Commission and stated he was in support of this proposed development.

Mr. Etienn Laubignat , 6028 Kamar Drive, addressed the Commission and indicated he was in support of this development.

Mr. Terry Brown, Blake Ridge Townhomes, addressed the Commission and explained he was adamantly opposed to this development.

Commissioner Miranda moved to close the public hearing. Commissioner Olson seconded the motion. Motion carried.

The Commission discussed the proposed development and asked staff questions.

- The number and degree of the variances suggested are unusual and are too many
- The Comprehensive Plan is old for this site but is not outdated
- The Comprehensive Plan is consistent with the land use next door
- Parking was supposed to reflect the maximum number of staff which seemed very low
- Concerned with parking, delivery issues, trash, and noise late at night with the bar
- Concerned about the traffic in the afternoon with children in the area
- The proposed restaurant development does not fit into the neighborhood
- No mention of sustainability in the plans
- Would like to separate dumpsters for recycling and trash
- The proposed development is too much for the site with all of the variances needed
- Site is too small for the idea presented

The Commission concurred on denial of the proposed development.

<u>Motion</u>

Commissioner Alkire moved that the Planning Commission recommend denial to the City Council of the Comprehensive Plan Amendment, Rezoning and Site Plan with Multiple Variances as outlined in the staff memo subject to the conditions and findings therein. Commissioner Padilla seconded the motion. Motion carried unanimously.

The Planning Commission meeting can be viewed online at the City website.

The Commission took a two-minute recess to clear the chamber.

VII. Chair and Member Comments

Received.

VIII. Staff Comments

Received.

IX. Adjournment

Commissioner Alkire moved to adjourn the September 28, 2022, Meeting of the Edina Planning Commission at 9:30 PM. Commissioner Olson seconded the motion. Motion carried unanimously.



CITY OF EDINA

4801 West 50th Street Edina, MN 55424 www.edinamn.gov

Date:	October 25, 2022	Agenda Item #: IV.B.
То:	Planning Commission	Item Type:
From:	Liz Olson, Administrative Support Specialist	Minutes Item Activity:
Subject:	Draft Minutes of Regular Meeting October 12, 2022	Action

ACTION REQUESTED: Approve the draft minutes from October 12, 2022.

INTRODUCTION:

ATTACHMENTS:

Draft Planning Commission Minutes 10-12-22



Minutes City Of Edina, Minnesota Planning Commission Edina City Hall Council Chambers October 12, 2022

I. Call To Order

Chair Agnew called the meeting to order at 7:00 PM.

II. <u>Roll Call</u>

Answering the roll call were: Commissioners Miranda, Strauss, Gandhi, Alkire, Bennett, Padilla, Olson, and Chair Agnew. Staff Present: Cary Teague, Community Development Director, Emily Bodeker, Assistant Planner and Liz Olson, Administrative Support Specialist.

Absent from the roll call: Commissioner Smith and Hu.

III. Approval Of Meeting Agenda

Commissioner Alkire moved to approve the October 12, 2022, agenda. Commissioner Padilla seconded the motion. Motion carried unanimously.

IV. Community Comment

None.

V. Public Hearings

A. B-22-18, A 10 Foot Height Variance for a Freestanding Sign – 7777 Washington Avenue S

Assistant Planner Bodeker presented the request of 7777 Washington Avenue South for a 10-foot height Variance for a freestanding sign at the subject property. Staff recommended approval of the 10-foot height variance for a freestanding sign, as requested, subject to the findings and conditions listed in the staff report.

Staff answered Commission questions.

Appearing for the Applicant

The applicant was at the meeting but did not speak.

Public Hearing

None.

Commissioner Miranda moved to close the public hearing. Commissioner Padilla seconded the motion. Motion carried.

The Commission discussed the proposed variance.

<u>Motion</u>

Commissioner Miranda moved that the Planning Commission recommend of the 10 Foot Height Variance for a Freestanding Sign at 7777 Washington Avenue South as outlined in the staff memo subject to the conditions and findings therein. Commissioner Padilla seconded the motion. Motion carried unanimously.

B. B-22-17, a Parking Variance - 7429 Bush Lake Road

Director Teague presented the request of 7429 Bush Lake Road for a parking variance. Staff recommends approval of the parking variance, as requested subject to the findings and conditions listed in the staff report.

Staff answered Commission questions.

Appearing for the Applicant

Mr. Sean Ewen, Co-Founder and President of Wooden Hill Brewing Company, addressed the Commission.

Public Hearing

None.

Commissioner Alkire moved to close the public hearing. Commissioner Olson seconded the motion. Motion carried unanimously.

The Commissioner discussed the proposed parking variance.

<u>Motion</u>

Commissioner Olson moved that the Planning Commission recommend approval of the parking variance at 7429 Bush Lake Road as outlined in the staff memo subject to the conditions and findings therein. Commissioner Strauss seconded the motion. Motion carried unanimously.

VII. <u>Reports/Recommendations</u>

A. Sketch Plan Review – 4212 and 4216 Valley View Road

Director Teague presented the request for a sketch plan review.

Staff answered Commission questions.

Appearing for the Applicant

Mr. Steve Behnke and Paul Donnay, 6605 Mohawk Trail, addressed the Commission.

The Commission asked questions of the applicant.

The Commission reviewed the sketch plan and offered the following comments:

- Does not like the design of the front-loaded garages
- Wish there was an alley
- Would be interested seeing six units rather than five to help with affordability and density
- Would like to see missing middle housing rather than all of the driveways

The entire meeting can be viewed on the City website.

B. Status Update for Commissioners on 2022 Work Plan Progress

Chair Agnew asked Commissioners for a status update on their progress for their 2022 work plan. The lead Commissioners reviewed their work plans.

VIII. Correspondence and Petitions

None.

IX. Chair and Member Comments

None.

X. Staff Comments

Received.

XI. Adjournment

Commissioner Padilla moved to adjourn the October 10, 2022, Meeting of the Edina Planning Commission at 8:38 PM. Commissioner Alkire seconded the motion. Motion carried unanimously.



CITY OF EDINA

4801 West 50th Street Edina, MN 55424 www.edinamn.gov

Date:	October 25, 2022	Agenda Item #: VI.A.
То:	Planning Commission	Item Type:
From:	Cary Teague, Community Development Director	Report and Recommendation
		Item Activity:
Subject:	Site Plan Review and Ordinance Amendment - 4901 West 77th Street	Action

ACTION REQUESTED:

Recommend the City Council approve the site plan and ordinance amendment regarding signage.

INTRODUCTION:

First Bank and Trust is proposing to build a 15,000 square foot office and bank with an interior drivethrough at the vacant site at 4901 77th Street West within Pentagon Village. The request requires a site plan review and ordinance amendment to allow an interior facing sign. (See attached plans and narrative.)

Pentagon Village is a 12.5-acre site that is zoned PUD-17. The subject property is Lot 4 within the overall master plan that called for a future 19,000 square foot office/retail building. The proposed building is generally within the same building pad. (See the attached approved overall master plan for Pentagon Village.)

The request requires the following:

- 1. Site Plan review; and
- 2. Ordinance amendment to allow interior facing signage.

ATTACHMENTS:

Staff Report Proposed Plans Applicant Narrative Applicant Narrative - Signs Original Pentagon Park Approval - Master Plan Traffic and Parking Study Draft Ordinance

Approved Master Plan for Pentagon Village

Better Together Public Hearing Comment Report

Staff Presentation

Applicant Presentation



Date: October 25, 2022

To: Planning Commission

From: Cary Teague, Community Development Director

Subject: Site Plan Review & Ordinance Amendment – 4901 West 77th Street

Information / Background:

First Bank and Trust is proposing to build a 15,000 square foot office and bank with an interior drive-through at the vacant site at 4901 77th Street West within Pentagon Village. The request requires a site plan review and ordinance amendment to allow an interior facing sign. (See attached plans and narrative.)

Pentagon Village is a 12.5-acre site that is zoned PUD-17. The subject property is Lot 4 within the overall master plan that called for a future 19,000 square foot office/retail building. The proposed building is generally within the same building pad. (See the attached approved overall master plan for Pentagon Village.)

The request requires the following:

- I. Site Plan review; and
- 2. Ordinance amendment to allow interior facing signage.

The proposed site plan has not changed from the original approval. The site has already been graded and is ready for construction of a building. Internal drive aisles/road and parking lots have not changed. The footprint of the building and building height are generally the same as the approved master plan.

SUPPORTING INFORMATION

Surrounding Land Uses

Northerly:	Multi-family residential and retail; zoned MDD-6, Mixed Development District and
	guided OR, Office Residential.
Easterly:	Office/light industrial uses; zoned PID, Planned Industrial District and guided OR,
	Office Residential.
Southerly:	City of Bloomington.
Westerly:	Highway 100.

Existing Site Features

The subject site within the Pentagon Village PUD, is vacant and relatively flat.

Planning

Guide Plan designation:	Office/Residential (20-75 residential units per acre)
Zoning:	PUD-17, Planned Unit Development District-17

Site Circulation/Traffic

The proposal would not change the vehicular or pedestrian circulation of the overall development plan for Pentagon Village. Vehicle access would be by the interior roadway. There would be sidewalks provided on all three sides of the building.

WSB conducted traffic studies for this development. The studies concluded that the existing roadways can be supported by the project, and no roadway improvements are necessary. (See attached traffic study.)

Parking

When the project was approved in 2019, a parking study was done by WSB. The study determined that the amount of parking proposed within this project is adequate. There are 1,422 parking stalls in the existing ramp and surface parking areas. The study concluded that 1,359 spaces is adequate for the overall development. (See attached study.) The ramp and adjacent surface parking lot would serve the site. The office proposed here is slightly smaller than the anticipated office/retail use.

Landscaping

The landscape plan is consistent with the previously approved landscape plan for the site. (See the approved overall development plan.) There would be overstory trees and a full complement of understory shrubs and bushes around the building. (See attached landscape plan.)

Grading/Drainage/Utilities

There have been no changes to the approved plans from 2019. The city engineer has reviewed the proposed plans and found them to be acceptable. The site has already been graded for the development of this site.

Building/Building Material

The building materials would be a combination of stone and metal panel. (See attached renderings and building materials and the previously approved plan.)

Mechanical Equipment/Trash Enclosures

Any rooftop and/or ground level equipment and trash enclosures would have to be screened if visible from adjacent property lines. The trash area would be located within the building.

	PUD-17	Proposed
Setbacks - Buildings 77 th Street Computer Avenue Normandale Boulevard Viking Drive	I 5 feet I 5 feet 50 feet I 0 Feet	l6 feet l5 feet NA NA
Building Height	12 stories & 144 feet per the height overlay district	2 stories
Parking lot and drive aisle setback	20 feet	20+ feet
Maximum Floor Area Ratio (FAR)	1.5%	1.5%

Compliance Table

Ordinance Amendment – Wall Signage

The applicant is requesting an amendment to the PUD to allow three wall signs. (See attached building elevations showing the wall signage.) The third wall sign is the symbol of the bank above the main entry; from some angles of the building appears to be part of the "First Bank & Trust" lettering as on the north elevation. Staff believes the request is reasonable, given the architecture of the building and its angles, and the visibility of this site within the PUD. The building is highly visible on all four sides, and the bank desires to have signage facing inward to the Pentagon Village development and parking areas as well as on the street.

STAFF RECOMMENDATION

Recommend the City Council approve the Site Plan request and Ordinance Amendment for 4901 77th Street West.

Approval is based on the following findings:

- I. The proposed land uses, and development meet all city code requirements.
- 2. The proposed plans are consistent with plans approved for this site in 2019.
- 3. The proposed signage is reasonable given the architecture of the building and visibility of the building on all four sides.

Approval is subject to the following Conditions:

- I. The Plan must be consistent with the Plans date stamped September 26, 2022.
- 2. The Final Landscape Plan must meet all minimum landscaping requirements per Chapter 36 of the Zoning Ordinance. A performance bond, letter-of-credit, or cash deposit must be submitted for one and one-half times the cost amount for completing the required landscaping, screening, or erosion control measures at the time of any building permit.
- 3. Provision of code compliant bike racks for each use near the building entrances.
- 4. The Final Lighting Plan must meet all minimum requirements per Section 36-1260 of the City Code.
- 5. Roof-top mechanical equipment and ground level equipment must be screened per Section 36-1459 of the City Code.
- 6. Submit a copy of the Nine Mile Creek Watershed District permit. The City may require revisions to the approved plans to meet the district's requirements.
- 7. Hours of construction must be consistent with City Code.

Staff Recommendation

Staff recommends approval of the request subject to the findings and conditions above.

Deadline for a city decision: December 20, 2022

PROJECT OWNER:

FIRST BANK & TRUST 1909 Highway 36 West Roseville, MN 55113 TEL: (651) 325-4160 CONTACT: SCOTT LAUGEN EMAIL: Scott.Laugen@bankeasy.com

PROPERTY OWNER:

JAY SCOTT SOLOMON REAL ESTATE GROUP TEL: 651-336-6060 EMAIL: JayScott@Solomon RE.com

ARCHITECT::

HTG ARCHITECTS 1010 MAIN STREET (SUITE 100) HOPKINS, MN 55343 TEL: (952) 204-3235 CONTACT: KYLE PEDERSON EMAIL: KPEDERSON@HTG-ARCHITECTS.COM

CIVIL ENGINEER::

TYLER MAXSON WESTWOOD 12701 WHITEWATER DRIVE, SUITE 300 MINNETONKA, MN 55343 TEL: 952-697-5748 EMAIL: tyler.maxson@westwoodps.com

LANDSCAPE DESIGN::

TYLER MAXSON WESTWOOD 12701 WHITEWATER DRIVE, SUITE 300 MINNETONKA, MN 55343 TEL: 952-697-5748 EMAIL: tyler.maxson@westwoodps.com

SHEET INDEX

TITLE SHEET

- TS-01 C0.0 C1.0 C2.0 COVER
- ALTA LAND TITLE SURVEY EXISTING CONDITIONS & REMOVALS PLAN
- CIVIL SITE PLAN
- C3.0 C4.0 GRADING PLAN C4.1 EROSION CONTROL PLAN
- C5.0 UTILITY PLAN
- LANDSCAPE PLAN L1.0
- A-01 ARCHITECTURAL SITE PLAN
- A-02 A-03 COMPARISON SITE PLAN ARCHITECTURAL FLOOR PLANS
- A-04 EXTERIOR ELEVATIONS
- A-05 PREVIOUSLY APPROVED ELEVATIONS
- A-06 A-07 **PROJECT LOCATION PHOTOGRAPHS PROJECT LOCATION PHOTOGRAPHS**
- A-08 EXTERIOR RENDERING
- A-09 EXTERIOR RENDERING
- A-10 EXTERIOR RENDERING
- A-11 EXTERIOR RENDERING A-12 EXTERIOR RENDERING

PROJECT DATA

DESCRIPTION:	A NEW 2-STORY FINANCIAL INSTITUTION FEATURING A CLIENT LOBBY, OFFICES, WORK STATIONS, A TRAINING FACILITY, FITNESS ROOM, SUPPLEMENTARY OFFICE SPACES, AND AN ENCLOSED DRIVE-UP LANE. THE MAIN LEVEL CONSISTS OF APPROXIMATELY 5,000 SF INTERIOR SPACES AND 5,600 SQUARE FEET OF COVERED PARKING AND DRIVE-UP SPACES. THE UPPER LEVEL IS APPROXIMATELY 10,000 SQUARE FEET. THE EXISTING PARKING LOT AND DRIVE LANES ARE ALREADY COMPLETED. THERE IS ROUGHLY 39,000 SF OF PAD READY AREA THAT THE NEW BUILDING WILL BE CONSTRUCTED ON.
PARCEL ID:	3102824330031
ZONING:	PUD - PENTAGON VILLAGE
PROPOSED USE:	FINANCIAL INSTITUTION (APPROVED USE)
CONDITIONAL USE:	BANK DRIVE-UP LANE (CONDITIONAL USE)
OCCUPANCY TYPE:	B - BUSINESS
BUILDING HEIGHT:	37'-0" +/-
BUILDING AREA:	20,600 TOTAL (15,000 FINANCIAL INSTITUTION / 5,600 ENCLOSED DRIVE LANE)

VICINITY MAP



SITE LOCATION

TITLE SHEET

TS-01

FIRST BANK & TRUST - PENTAGON VILLAGE

EDINA MINNESOTA 09/26/2022

FIRST BANK & TRUST - PENTAGON VILLAGE **NEW CONSTRUCTION 4901 WEST 77TH STREET** EDINA, MINNESOTA

EXTERIOR RENDERING







PRELIMINARY SITE **IMPROVEMENT PLANS** FOR FIRST BANK & TRUST **PENTAGON VILLAGE** EDINA, MN

PREPARED FOR: **HTG ARCHITECTS**

1010 MAINSTREET HOPKINS, MN 55343 CONTACT: SEAN RABOIN PHONE: 952.278.8880 EMAIL: SRABOIN@HTG-ARCHITECTS.COM

PREPARED BY:

Westwood

Phone Fax **Toll Free**

(952) 937-5150 12701 Whitewater Drive, Suite #300 (952) 937-5822 Minnetonka, MN 55343 (888) 937-5150 westwoodps.com Westwood Professional Services, Inc.

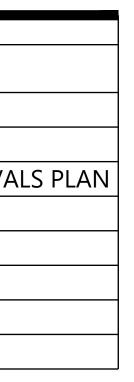
PROJECT NUMBER: 0037303.00 CONTACT: .

SHEET INDEX

	Sheet List Table
SHEET NUMBER	SHEET TITLE
C0.0	COVER
C1.0	ALTA NSPS LAND TITLE SURVEY
C2.0	EXISTING CONDITIONS & REMOVA
C3.0	CIVIL SITE PLAN
C4.0	GRADING PLAN
C4.1	EROSION CONTROL PLAN
C5.0	UTILITY PLAN
L1.0	LANDSCAPE PLAN



Vicinity Map

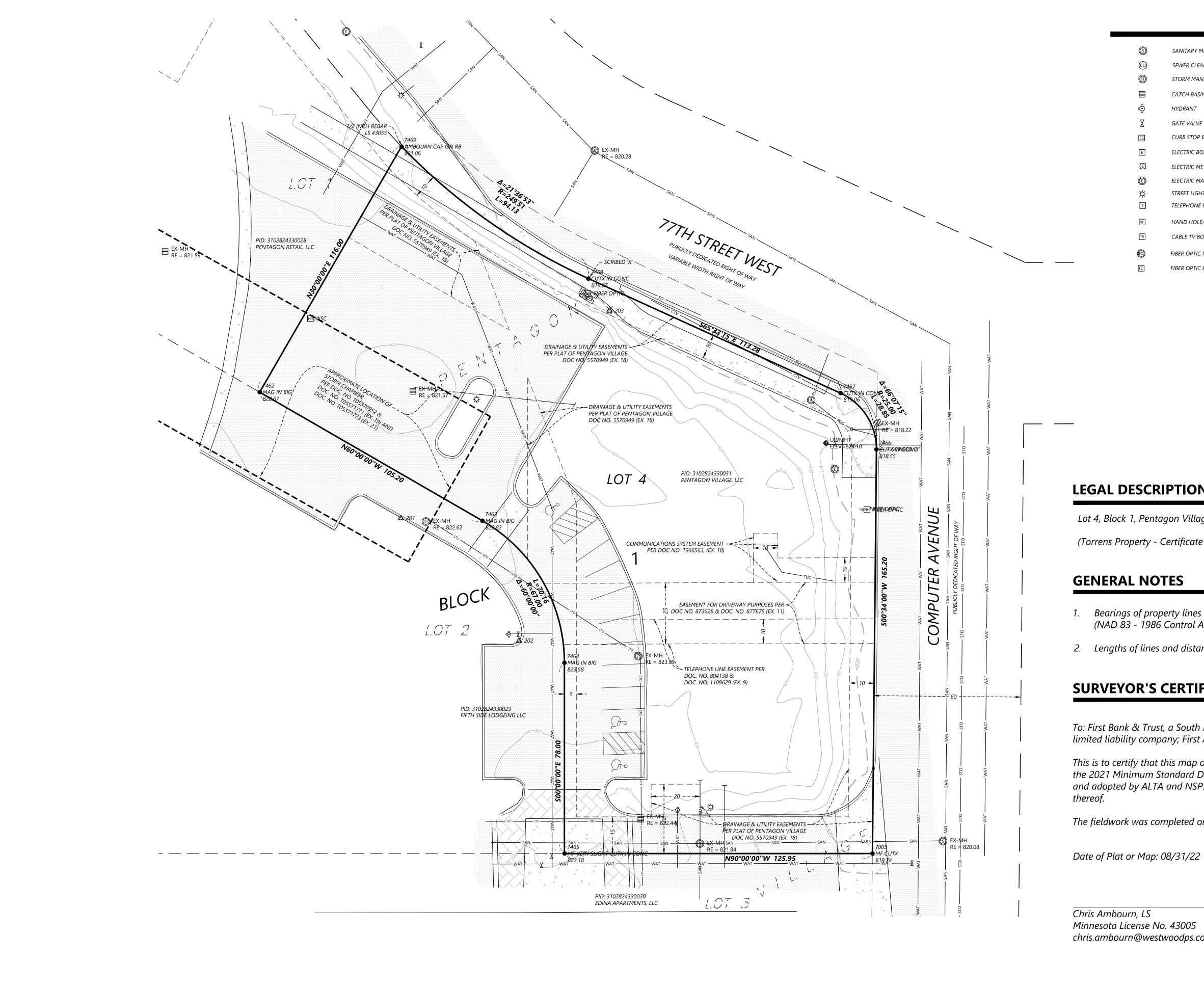


NO.	DATE	REVISION	SHEETS
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PRELIMINARY SITE IMPROVEMENT PLANS FOR

FIRST BANK & TRUST PENTAGON VILLAGE EDINA, MN

INITIAL SUBMITTAL DATE: 09/26/22 SHEET: C0.0 OF 8



Chris Ambourn, LS Minnesota License No. 43005 chris.ambourn@westwoodps.com

LEGEND

SANITARY MANHOLE	Ģ—−¤	STREET LIGHT
SEWER CLEANOUT	\otimes	STEEL/WOOD POST
STORM MANHOLE	.	SIGN
STORM MANHOLE		POWER UNDERGROUND
CATCH BASIN	PUG	POWER UNDERGROUND
HYDRANT	SAN	SANITARY SEWER
GATE VALVE	STO	STORM SEWER
CURB STOP BOX	GAS	GAS LINE
	—— РОН ———	POWER OVERHEAD
ELECTRIC BOX	WAT	WATERMAIN
ELECTRIC METER	TUC	
ELECTRIC MANHOLE	TUG	TELEPHONE UNDERGROU
STREET LIGHT	FO	FIBER OPTIC
TELEPHONE BOX	CTV	UNDERGROUND TELEVISIO
		CURB & GUTTER
HAND HOLE/JUNCTION BOX		CONCRETE SURFACE
CABLE TV BOX		BITUMINOUS SURFACE
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FIBER OPTIC PEDESTAL	FOUNI	D SCRIBED 'X'
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LEGAL DESCRIPTION (Per Commitment No. NCS-1141416-MPLS)

Lot 4, Block 1, Pentagon Village Hennepin County, Minnesota.

(Torrens Property - Certificate of Title No. 1474191)

1. Bearings of property lines shown hereon are based on the Hennepin County coordinate system, (NAD 83 - 1986 Control Adjustment).

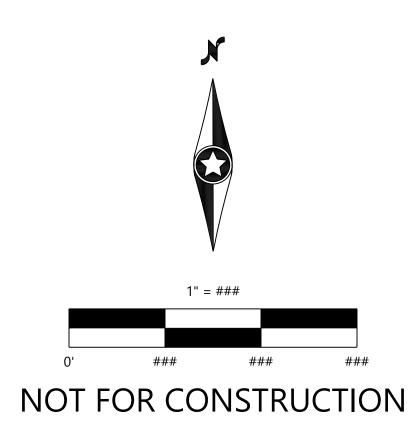
2. Lengths of lines and distances between features are measured in US-Survey Feet

SURVEYOR'S CERTIFICATE

To: First Bank & Trust, a South Dakota state banking corporation; Pentagon Village, LLC, a Minnesota limited liability company; First American Title Insurance Company:

This is to certify that this map or plat and the survey on which it is based were made in accordance with the 2021 Minimum Standard Detail Requirements for ALTA/NSPS Land Title Surveys, jointly established and adopted by ALTA and NSPS, and includes Items 1-4, 6(a)(b), 7(a), 8, 9, 11(a)(b), 13 and 16 of Table A

The fieldwork was completed on August 24, 2022.



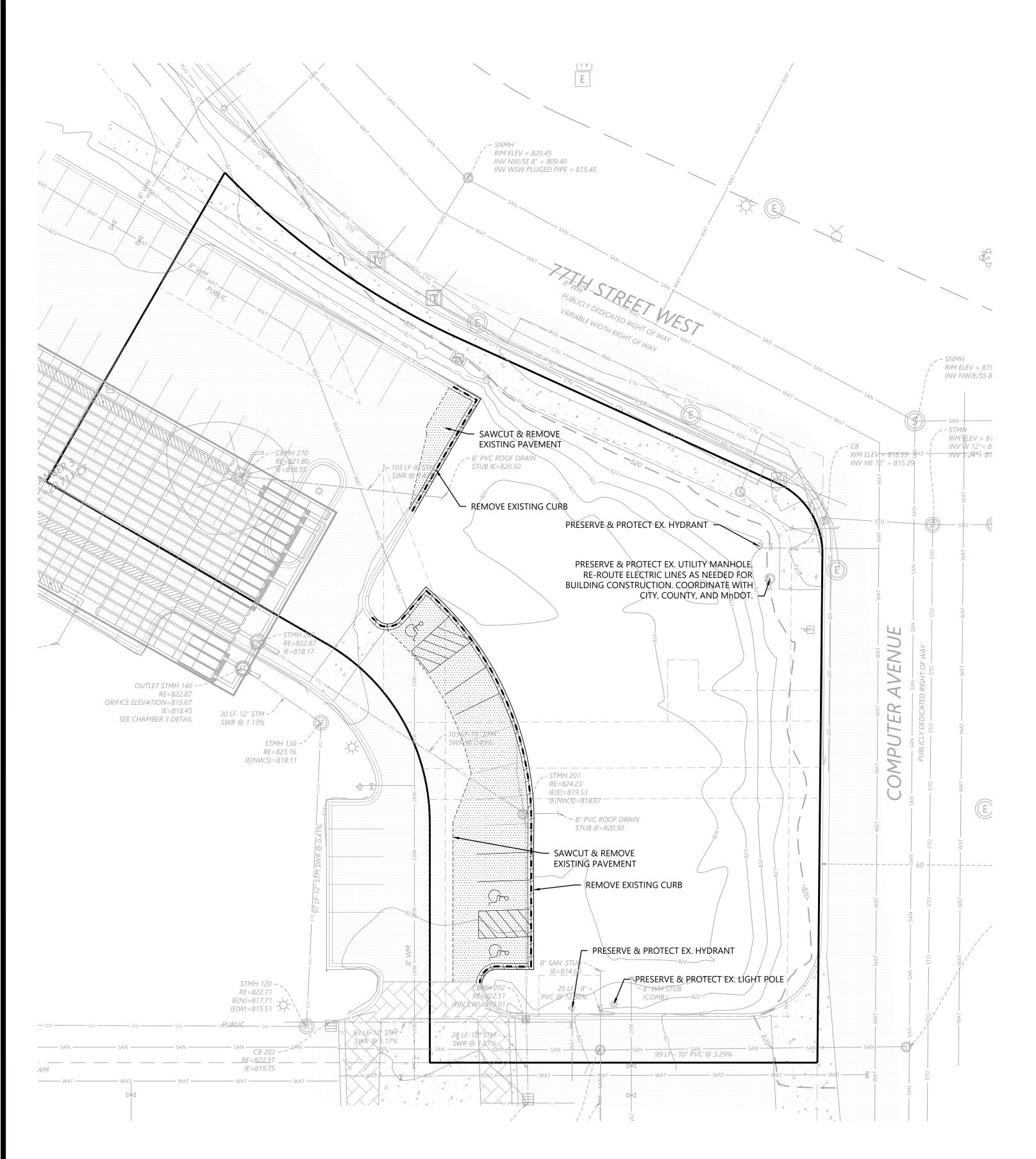
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		Westwood Professional Services, Inc.	EDINA, MN	DATE: 09/26/22 LICENSE NO	1010 MAINSIREET HOPKINS, MN 55343	· ·	VERTICAL SCALE:
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Call 48 Hours before digging:

811 or call811.com

DATE: **09/26/22**

PROJECT NUMBER: 0037303.00



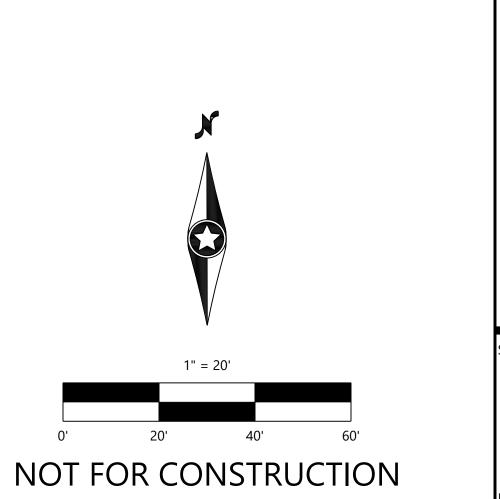
REMOVAL LEGEND

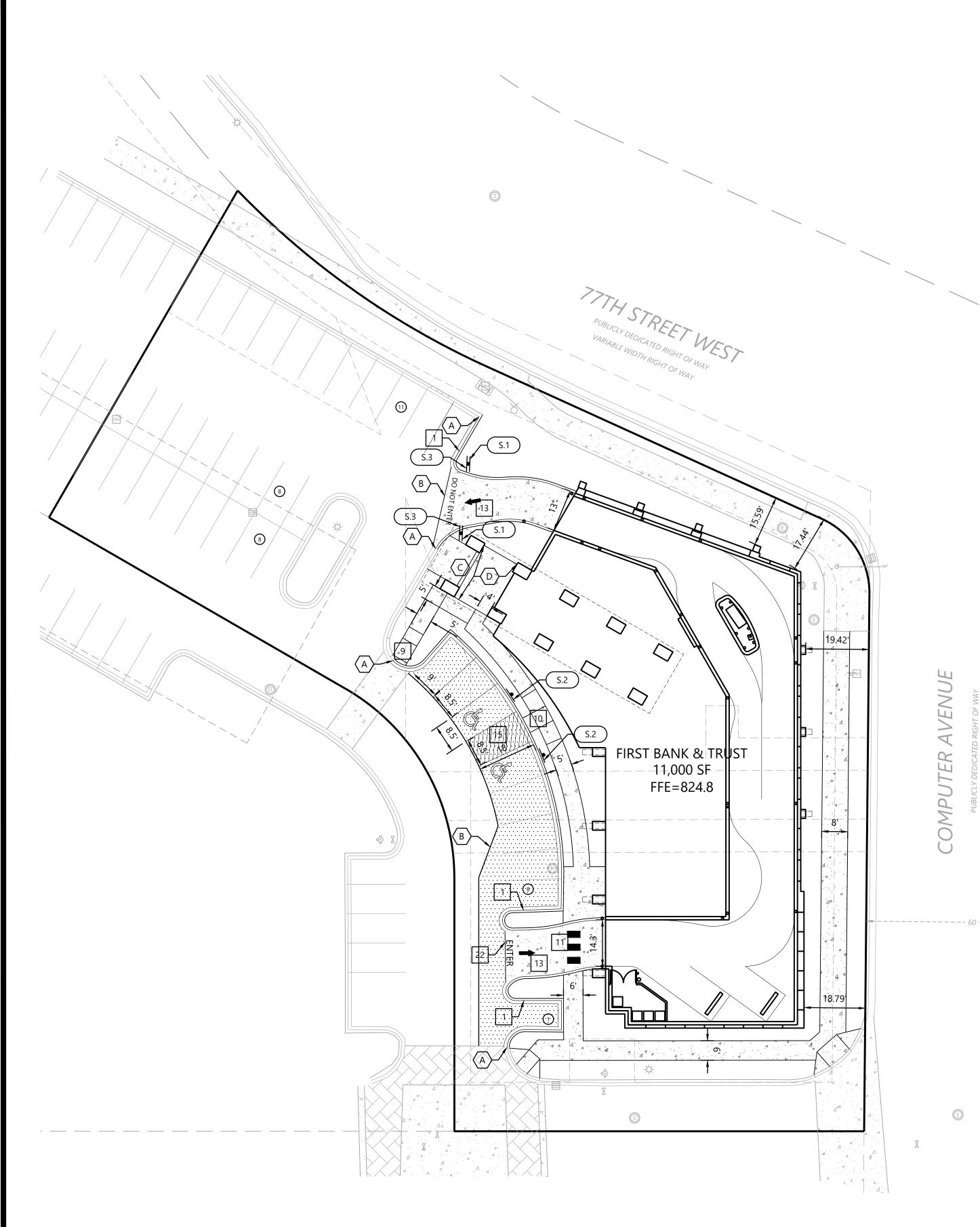
EXISTING	PROPOSED	
		PROPERTY LINE
		SAW CUT PAVEMENT
EXISTING	REMOVALS	
	_____	CURB & GUTTER
SAN(S)		SANITARY SEWER
WAT		WATER MAIN
		HYDRANT
STO		STORM SEWER
GAS		GAS
PUG		UNDERGROUND ELECTRIC
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=		RETAINING WALL
X	_ _ _ _ _ _ _	FENCE
		CONCRETE
		BITUMINOUS
		BUILDING
		TREE
÷¢-	*	LIGHT POLE
- <u>o</u> -	X	TRAFFIC SIGN
	00	CONSTRUCTION BARRICADE
● SB-19		SOIL BORING LOCATION
\mathcal{M}		TREE LINE

- 1. LOCATIONS AND ELEVATIONS OF EXISTING TOPOGRAPHY AND UTILITIES AS SHOWN ON THIS PLAN ARE APPROXIMATE. CONTRACTOR SHALL FIELD VERIFY SITE CONDITIONS AND UTILITY LOCATIONS PRIOR TO EXCAVATION/CONSTRUCTION. THE ENGINEER SHALL BE NOTIFIED IMMEDIATELY IF ANY DISCREPANCIES ARE FOUND.
- 2. CONTRACTOR SHALL COORDINATE LIMITS OF REMOVALS WITH PROPOSED IMPROVEMENTS AND FIELD VERIFY CONDITION OF EXISTING APPURTENANCES TO REMAIN. CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING OR REPLACING MISCELLANEOUS ITEMS (SUCH AS FENCES, SIGNS, IRRIGATION HEADS, ETC.) THAT MAY BE DAMAGED BY CONSTRUCTION.
- 3. CONTRACTOR SHALL PLACE ALL NECESSARY EROSION CONTROL MEASURES REQUIRED TO MAINTAIN SITE STABILITY PRIOR TO EXECUTING ANY SITE REMOVALS.
- 4. CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION WITH UTILITY PROVIDERS FOR REMOVAL AND/OR RELOCATION OF EXISTING UTILITIES AFFECTED BY SITE DEVELOPMENT. ALL PERMITS, APPLICATIONS AND FEES ARE THE RESPONSIBILITY OF THE CONTRACTOR.

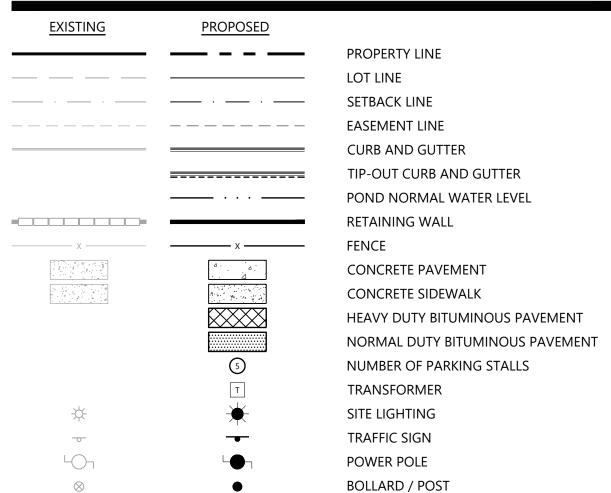
ore digging: 11.com und Alliance	DESIGNED: CHECKED: DRAWN: HORIZONTAL SCALE: 20' VERTICAL SCALE:
	INITIAL ISSUE: 09/26/22 REVISIONS:
	PREPARED FOR: HTG ARCHITECTS 1010 MAINSTREET HOPKINS, MN 55343
	I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA
	FIRST BANK & TRUST PENTAGON VILLAGE EDINA, MN
	Phone (952) 937-5150 12701 Whitewater Drive, Suite #300 Fax (952) 937-5150 12701 Whitewater Drive, Suite #300 Fax (952) 937-5150 westwoodps.com Toll Free (888) 937-5150 westwoodps.com Westwood Professional Services, Inc. Montemport Montemport
	REMOVALS PLAN RET NOMBEL: CONDITIONS & CONDITIONS & CONDI
SH	C2.0
	ATE: 09/26/22

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SITE LEGEND



GENERAL SITE NOTES

- 1. BACKGROUND INFORMATION FOR THIS PROJECT PROVIDED BY WESTWOOD PROFESSIONAL SERVICES, MINNETONKA, MN, 08/24/2022.
- 2. LOCATIONS AND ELEVATIONS OF EXISTING TOPOGRAPHY AND UTILITIES AS SHOWN ON THIS PLAN ARE APPROXIMATE. CONTRACTOR SHALL FIELD VERIFY SITE CONDITIONS AND UTILITY LOCATIONS PRIOR TO EXCAVATION/CONSTRUCTION. IF ANY DISCREPANCIES ARE FOUND, THE ENGINEER SHOULD BE NOTIFIED IMMEDIATELY.
- 3. REFER TO BOUNDARY SURVEY FOR LOT BEARINGS, DIMENSIONS AND AREAS.
- 4. ALL DIMENSIONS ARE TO FACE OF CURB OR EXTERIOR FACE OF BUILDING UNLESS OTHERWISE NOTED.
- 5. REFER TO ARCHITECTURAL PLANS FOR EXACT BUILDING DIMENSIONS AND LOCATIONS OF EXITS, RAMPS, AND TRUCK DOCKS.
- 6. ALL CURB RADII ARE SHALL BE 3.0 FEET (TO FACE OF CURB) UNLESS OTHERWISE NOTED.
- 7. ALL CURB AND GUTTER SHALL BE B612 UNLESS OTHERWISE NOTED.
- 8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING AND MAINTAINING TRAFFIC CONTROL DEVICES SUCH AS BARRICADES, WARNING SIGNS, DIRECTIONAL SIGNS, FLAGGERS AND LIGHTS TO CONTROL THE MOVEMENT OF TRAFFIC WHERE NECESSARY. PLACEMENT OF THESE DEVICES SHALL BE APPROVED BY THE CITY AND ENGINEER PRIOR TO PLACEMENT. TRAFFIC CONTROL DEVICES SHALL CONFORM TO APPROPRIATE MNDOT STANDARDS.
- 9. BITUMINOUS PAVEMENT AND CONCRETE SECTIONS TO BE IN ACCORDANCE WITH THE RECOMMENDATIONS OF THE GEOTECHNICAL ENGINEER.
- 10. CONTRACTOR SHALL MAINTAIN FULL ACCESS TO ADJACENT PROPERTIES DURING CONSTRUCTION AND TAKE ALL PRECAUTIONS NECESSARY TO AVOID PROPERTY DAMAGE TO ADJACENT PROPERTIES.
- 11. SITE LIGHTING SHOWN ON PLAN IS FOR REFERENCE ONLY. REFER TO LIGHTING PLAN PREPARED BY OTHERS FOR SITE LIGHTING DETAILS AND PHOTOMETRICS.

SITE DEVELOPMENT SUMMARY

- PARCEL DESCRIPTION:
- PROPERTY AREA:
- PERVIOUS SURFACE:IMPERVIOUS SURFACE(RATIO):
- BUILDING INTERIOR FLOOR AREA:
- TOTAL ENCLOSED BUILDING AREA:
- BUILDING SETBACK PER CODE:
- PARKING SETBACK:
- PARKING PROVIDED:

- SEE ALTA SURVEY
- 39,177 SF (0.899 AC)
- 6,138 SF (15.7%) 33,039 SF (84.3%)

15,000 SF

20,600 SF

15'= 77TH STREET W 15'= COMPUTER AVENUE

20'=FRONT AND ROW

36 STALLS

SITE DETAILS (SI-0XX)

1 B612 CURB AND GUTTER

22 SAW CUT CONTROL JOINT

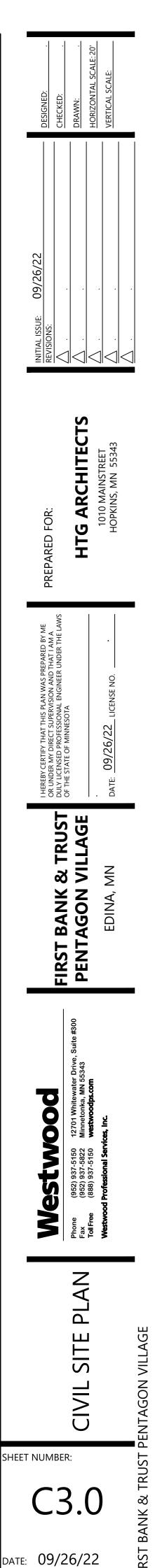
- 9 PRIVATE PEDESTRIAN CURB RAMP
- PRIVATE PARALLEL PEDESTRIAN CURB RAMP
 CROSS WALK STRIPING
- 13 TRAFFIC ARROW
- 15 HANDICAP ACCESSIBLE SIGNAGE AND STRIPING

SIGN LEGEND

REFE	RENCE	SIZE	MnDOT DESIGNATION
S.1	STOP SIGN	30" X 30"	R1-1
S.2	HANDICAP ACCESSIBLE	12" X 18"	R7-8M
S.3	DO NOT ENTER	30" X 30"	R5-1

\bigcirc SITE KEYNOTES

- A MATCH EX. CURB & GUTTER
- B MATCH EX. ASPHALT PAVEMENT W/ SAWCUT
- C STAIRS D MASONRY KNEE WALLS W/ PRECAST CAP (SEE ARCH.)



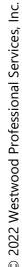
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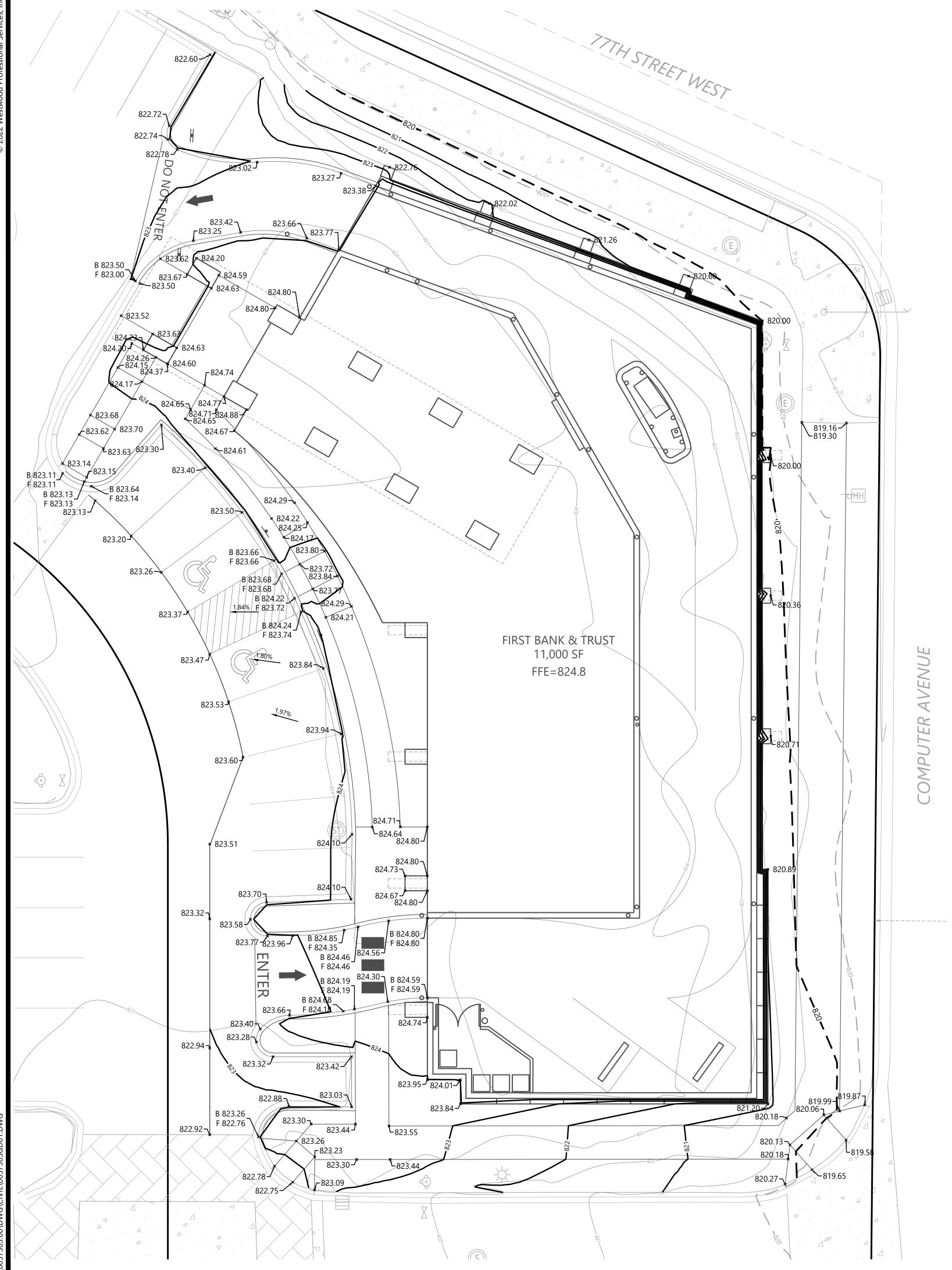
1" = 20'

NOT FOR CONSTRUCTION

40'

20'





GRADING LEGEND

EXISTING	PROPOSED
980	980 982
STO	
× 900.00	GL × 900.00 0.00% TW=X BW=X
● SB-19	E.O.F.→× ■ ● SB-1

GRADING NOTES

- IMMEDIATELY IF ANY DISCREPANCIES ARE FOUND.
- LOCATIONS AND NUMBER OF DOWNSPOUTS.
- PREPARED BY THE CITY ENGINEERS ASSOCIATION OF MINNESOTA.
- LANDSCAPE PLAN FOR PLANTING AND TURF ESTABLISHMENT.
- CONTROL DEVICES SHALL CONFORM TO APPROPRIATE MNDOT STANDARDS.
- SHEET.
- ARE SHOWN OR BETWEEN SUCH POINTS AND EXISTING GRADES.
- GRADE.
- 9. SEE SOILS REPORT FOR PAVEMENT THICKNESSES AND HOLD DOWNS.
- AGENCIES.
- LICENSED PROFESSIONAL ENGINEER.
- EXCAVATED AND RE-COMPACTED AS SPECIFIED HEREIN.
- COMPACTION METHOD AS OUTLINED IN MNDOT 2105.3F2.
- INSPECTIONS WITH THE GEOTECHNICAL ENGINEER.

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	PROPERTY LINE
-	INDEX CONTOUR
_	INTERVAL CONTOUR
	CURB AND GUTTER
	POND NORMAL WATER LEVEL
-8	STORM SEWER
	FLARED END SECTION (WITH RIPRAP)
	WATER MAIN
	SANITARY SEWER
	RETAINING WALL
	DRAIN TILE
	RIDGE LINE
	GRADING LIMITS
	SPOT ELEVATION
	FLOW DIRECTION
X.XX X.XX	TOP AND BOTTOM OF RETAINING WALL
	EMERGENCY OVERFLOW
	SOIL BORING LOCATION

1. LOCATIONS AND ELEVATIONS OF EXISTING TOPOGRAPHY AND UTILITIES AS SHOWN ON THIS PLAN ARE APPROXIMATE. CONTRACTOR SHALL FIELD VERIFY SITE CONDITIONS AND UTILITY LOCATIONS PRIOR TO EXCAVATION/CONSTRUCTION. THE ENGINEER SHALL BE NOTIFIED

2. CONTRACTORS SHALL REFER TO ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF VESTIBULE, SLOPED PAVEMENT, EXIT PORCHES, RAMPS, TRUCK DOCKS, PRECISE BUILDING DIMENSIONS, EXACT BUILDING UTILITY ENTRANCE LOCATIONS, AND EXACT

3. ALL EXCAVATION SHALL BE IN ACCORDANCE WITH THE CURRENT EDITION OF "STANDARD SPECIFICATIONS FOR TRENCH EXCAVATION AND BACKFILL/SURFACE RESTORATION" AS

4. ALL DISTURBED UNPAVED AREAS ARE TO RECEIVE SIX INCHES OF TOPSOIL AND SOD OR SEED. THESE AREAS SHALL BE WATERED UNTIL A HEALTHY STAND OF GRASS IS OBTAINED. SEE

5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING AND MAINTAINING TRAFFIC CONTROL DEVICES SUCH AS BARRICADES, WARNING SIGNS, DIRECTIONAL SIGNS, FLAGMEN AND LIGHTS TO CONTROL THE MOVEMENT OF TRAFFIC WHERE NECESSARY. PLACEMENT OF THESE DEVICES SHALL BE APPROVED BY THE ENGINEER PRIOR TO PLACEMENT. TRAFFIC

6. ALL SLOPES SHALL BE GRADED TO 3:1 OR FLATTER, UNLESS OTHERWISE INDICATED ON THIS

7. CONTRACTOR SHALL UNIFORMLY GRADE AREAS WITHIN LIMITS OF GRADING AND PROVIDE A SMOOTH FINISHED SURFACE WITH UNIFORM SLOPES BETWEEN POINTS WHERE ELEVATIONS

8. SPOT ELEVATIONS SHOWN INDICATE FINISHED PAVEMENT ELEVATIONS & GUTTER FLOW LINE UNLESS OTHERWISE NOTED. PROPOSED CONTOURS ARE TO FINISHED SURFACE

10. CONTRACTOR SHALL DISPOSE OF ANY EXCESS SOIL MATERIAL THAT EXISTS AFTER THE SITE GRADING AND UTILITY CONSTRUCTION IS COMPLETED. THE CONTRACTOR SHALL DISPOSE OF ALL EXCESS SOIL MATERIAL IN A MANNER ACCEPTABLE TO THE OWNER AND THE REGULATING

11. CONTRACTOR SHALL PROVIDE A STRUCTURAL RETAINING WALL DESIGN CERTIFIED BY A

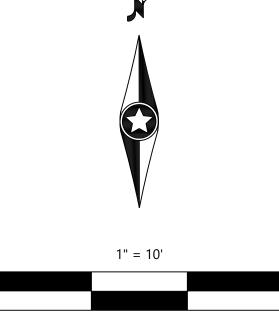
12. ALL CONSTRUCTION SHALL CONFORM TO LOCAL, STATE AND FEDERAL RULES INCLUDING THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT REQUIREMENTS.

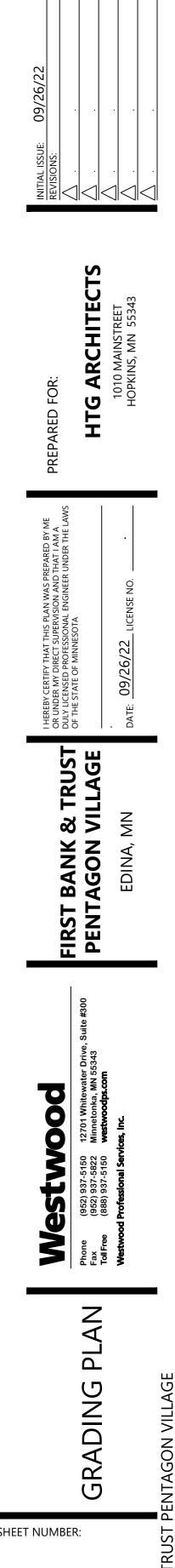
13. PRIOR TO PLACEMENT OF ANY STRUCTURE OR PAVEMENT, A PROOF ROLL, AT MINIMUM, WILL BE REQUIRED ON THE SUBGRADE. PROOF ROLLING SHALL BE ACCOMPLISHED BY MAKING MINIMUM OF 2 COMPLETE PASSES WITH FULLY-LOADED TANDEM-AXLE DUMP TRUCK, OR APPROVED EQUAL, IN EACH OF 2 PERPENDICULAR DIRECTIONS WHILE UNDER SUPERVISION AND DIRECTION OF THE INDEPENDENT TESTING LABORATORY. AREAS OF FAILURE SHALL BE

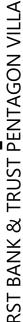
14. EMBANKMENT MATERIAL PLACED BENEATH BUILDINGS AND STREET OR PARKING AREAS SHALL BE COMPACTED IN ACCORDANCE WITH THE SPECIFIED DENSITY METHOD AS OUTLINED IN MNDOT 2105.3F1 AND THE REQUIREMENTS OF THE GEOTECHNICAL ENGINEER.

15. EMBANKMENT MATERIAL NOT PLACED IN THE BUILDING PAD, STREETS OR PARKING AREA, SHALL BE COMPACTED IN ACCORDANCE WITH REQUIREMENTS OF THE ORDINARY

16. ALL SOILS AND MATERIALS TESTING SHALL BE COMPLETED BY AN INDEPENDENT GEOTECHNICAL ENGINEER. EXCAVATION FOR THE PURPOSE OF REMOVING UNSTABLE OR UNSUITABLE SOILS SHALL BE COMPLETED AS REQUIRED BY THE GEOTECHNICAL ENGINEER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL REQUIRED SOILS TESTS AND







NOT FOR CONSTRUCTION

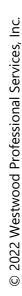
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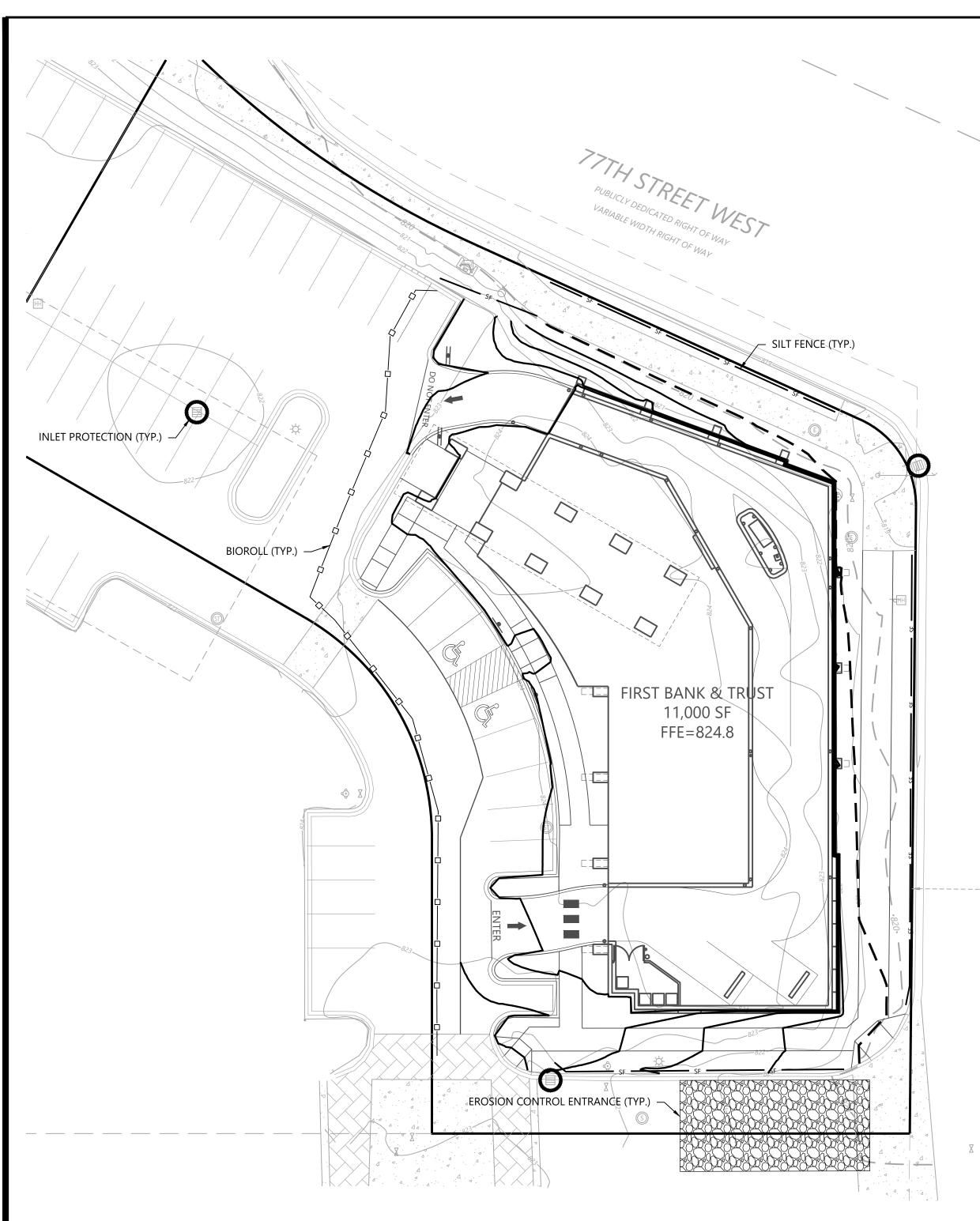
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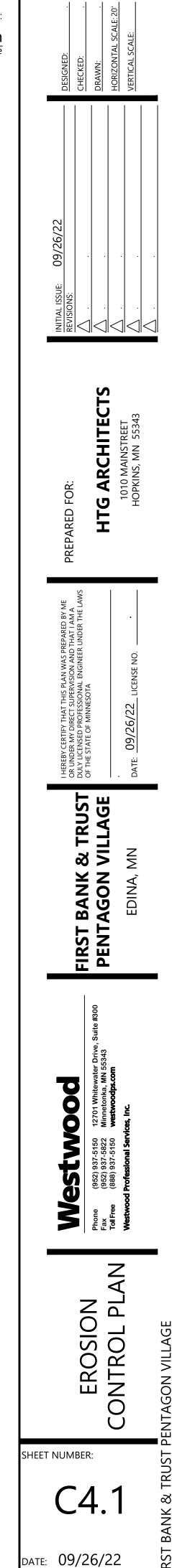
GENERAL EROSION CONTROL NOTES

- 1. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION EXISTING UTILITIES AS SHOWN ON THESE PLANS ARE BASED ON RECORDS OF THE VARIOU UTILITY COMPANIES AND LIMITED MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATIC SHALL NOT BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR SHALL VERIF EXISTING CONDITIONS PRIOR TO CONSTRUCTION AND NOTIFY THE OWNER OR ENGINEER DISCREPANCIES.
- 2. ALL SILT FENCE AND OTHER EROSION CONTROL FEATURES SHALL BE IN-PLACE PRIOR TO A EXCAVATION/CONSTRUCTION AND SHALL BE MAINTAINED UNTIL VIABLE TURF OR GROUN COVER HAS BEEN ESTABLISHED. EXISTING SILT FENCE ON-SITE SHALL BE MAINTAINED AND REMOVED AND SHALL BE CONSIDERED INCIDENTAL TO THE GRADING CONTRACT. IT IS OF EXTREME IMPORTANCE TO BE AWARE OF CURRENT FIELD CONDITIONS WITH RESPECT TO EROSION CONTROL. TEMPORARY PONDING, DIKES, HAYBALES, ETC., REQUIRED BY THE CIT SHALL BE INCIDENTAL TO THE GRADING CONTRACT.
- EROSION AND SILTATION CONTROL (ESC): THE CONTRACTOR SHALL ASSUME COMPLETE RESPONSIBILITY FOR CONTROLLING ALL SILTATION AND EROSION OF THE PROJECT AREA. CONTRACTOR SHALL USE WHATEVER MEANS NECESSARY TO CONTROL THE EROSION AND SILTATION INCLUDING BUT NOT LIMITED TO: CATCH BASIN INSERTS, CONSTRUCTION ENTRANCES, EROSION CONTROL BLANKET, AND SILT FENCE. ESC SHALL COMMENCE WITH GRADING AND CONTINUE THROUGHOUT THE PROJECT UNTIL ACCEPTANCE OF THE WORK THE OWNER. THE CONTRACTOR'S RESPONSIBILITY INCLUDES ALL IMPLEMENTATION AS REQUIRED TO PREVENT EROSION AND THE DEPOSITING OF SILT. THE OWNER MAY DIRECT CONTRACTOR'S METHODS AS DEEMED FIT TO PROTECT PROPERTY AND IMPROVEMENTS. DEPOSITION OF SILT OR MUD ON NEW OR EXISTING PAVEMENT OR IN EXISTING STORM SEWERS OR SWALES SHALL BE REMOVED AFTER EACH RAIN EVENT. AFFECTED AREAS SHAL CLEANED TO THE SATISFACTION OF THE OWNER, ALL AT THE EXPENSE OF THE CONTRACTO ALL TEMPORARY EROSION CONTROL SHALL BE REMOVED BY THE CONTRACTOR AFTER THE TURF IS ESTABLISHED.
- 4. ALL STREETS DISTURBED DURING WORKING HOURS MUST BE CLEANED AT THE END OF EAU WORKING DAY. A CONSTRUCTION ENTRANCE TO THE SITE MUST BE PROVIDED ACCORDIN DETAILS TO REDUCE TRACKING OF DIRT ONTO PUBLIC STREETS.
- 5. PROPOSED PONDS SHALL BE EXCAVATED FIRST AND USED AS TEMPORARY PONDING DURI CONSTRUCTION.
- 6. WHEN INSTALLING END-OF-LINE FLARED END SECTIONS, BRING THE SILT FENCE UP & OVE FLARED END SECTIONS & COVER DISTURBED AREAS WITH RIP RAP. THE UPSTREAM FLARED SECTIONS SHALL HAVE WOOD FIBER BLANKET INSTALLED ON THE DISTURBED SOILS.
- 7. ALL UNPAVED AREAS ALTERED DUE TO CONSTRUCTION ACTIVITIES MUST BE RESTORED WI SEED AND MULCH, SOD, EROSION CONTROL BLANKET OR BE HARD SURFACE WITHIN 2 WE OF COMPLETION OF CONSTRUCTION.
- 8. THE SITE MUST BE STABILIZED PER THE REQUIREMENTS OF THE MPCA, NPDES, MNDOT, AN CITY.
- TEMPORARY (GREATER THAN 1-YEAR) SEED SHALL BE MNDOT SEED MIX 22-111 AT Α. 30.5-POUNDS PER ACRE.
- TEMPORARY (LESS THAN 1-YEAR) SEED SHALL BE MNDOT SEED MIX 21-112 (FALL) OR В. 21-111 (SPRING/SUMMER) AT 100-POUNDS PER ACRE
- INFILTRATION/FILTRATION BASIN SHALL BE MNDOT SEED MIX 34-262 AT 14.5-POUND C. ACRE.
- D. POND SLOPES SHALL BE MNDOT SEED MIX 33-261 AT 35-POUNDS PER ACRE.
- GENERAL SEEDING SHALL BE MNDOT SEED MIX 25-151 AT 70-POUNDS PER ACRE. E.
- MULCH SHALL BE MNDOT TYPE 1 APPLIED AT 2-TONS PER ACRE. F.
- 9. FOR AREAS WITH SLOPE OF 3:1 OR GREATER, RESTORATION WITH SOD OR EROSION CONT BLANKET IS REQUIRED.
- 10. ALL TEMPORARY STOCKPILES MUST HAVE SILT FENCE INSTALLED AROUND THEM TO TRAP SEDIMENT.
- 11. ALL PERMANENT PONDS USED AS TEMPORARY SEDIMENT BASINS DURING CONSTRUCTION SHALL BE DREDGED AFTER THE SITE HAS BEEN STABILIZED TO RESTORE THE POND TO THE PROPOSED BOTTOM ELEVATION.
- 12. ALL CONSTRUCTION SHALL CONFORM TO LOCAL AND STATE RULES INCLUDING THE NATIO POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT REQUIREMENTS.
- 13. THE SITE MUST BE KEPT IN A WELL-DRAINED CONDITION AT ALL TIMES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TEMPORARY DITCHES, PIPING OR OTHER MEANS REQUIRED TO INSURE PROPER DRAINAGE DURING CONSTRUCTION. LOW POINTS IN ROADWAYS OR BUILDING PADS MUST BE PROVIDED WITH A POSITIVE OUTFLOW.
- 14. PUBLIC STREETS USED FOR HAULING SHALL BE KEPT FREE OF SOIL AND DEBRIS. STREET SWEEPING SHALL BE CONCURRENT WITH SITE WORK.

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EROSION CONTROL LEGEND

PROPERTY LINE INDEX CONTOUR INTERVAL CONTOUR CURB AND GUTTER POND NORMAL WATER LEVEL SILT FENCE HEAVY DUTY SILT FENCE REDUNDANT SILT CONTROL STORM SEWER FLARED END SECTION (WITH I WATER MAIN SANITARY SEWER RETAINING WALL DRAIN TILE GRADING LIMITS ROCK CONSTRUCTION ENTRA EROSION CONTROL BLANKET TURF REINFORCEMENT MAT EMERGENCY OVERFLOW SOIL BORING LOCATION
INTERVAL CONTOUR CURB AND GUTTER POND NORMAL WATER LEVEL SILT FENCE HEAVY DUTY SILT FENCE REDUNDANT SILT CONTROL STORM SEWER FLARED END SECTION (WITH I WATER MAIN SANITARY SEWER RETAINING WALL DRAIN TILE GRADING LIMITS ROCK CONSTRUCTION ENTRA EROSION CONTROL BLANKET TURF REINFORCEMENT MAT EMERGENCY OVERFLOW SOIL BORING LOCATION
CURB AND GUTTER POND NORMAL WATER LEVEL SILT FENCE HEAVY DUTY SILT FENCE REDUNDANT SILT CONTROL STORM SEWER FLARED END SECTION (WITH I WATER MAIN SANITARY SEWER RETAINING WALL DRAIN TILE GRADING LIMITS ROCK CONSTRUCTION ENTRA EROSION CONTROL BLANKET TURF REINFORCEMENT MAT EMERGENCY OVERFLOW SOIL BORING LOCATION
POND NORMAL WATER LEVEL SILT FENCE HEAVY DUTY SILT FENCE REDUNDANT SILT CONTROL STORM SEWER FLARED END SECTION (WITH I WATER MAIN SANITARY SEWER RETAINING WALL DRAIN TILE GRADING LIMITS ROCK CONSTRUCTION ENTRA EROSION CONTROL BLANKET TURF REINFORCEMENT MAT EMERGENCY OVERFLOW SOIL BORING LOCATION
SILT FENCE HEAVY DUTY SILT FENCE REDUNDANT SILT CONTROL STORM SEWER FLARED END SECTION (WITH I WATER MAIN SANITARY SEWER RETAINING WALL DRAIN TILE GRADING LIMITS ROCK CONSTRUCTION ENTRA EROSION CONTROL BLANKET TURF REINFORCEMENT MAT EMERGENCY OVERFLOW SOIL BORING LOCATION
HEAVY DUTY SILT FENCE REDUNDANT SILT CONTROL STORM SEWER FLARED END SECTION (WITH I WATER MAIN SANITARY SEWER RETAINING WALL DRAIN TILE GRADING LIMITS ROCK CONSTRUCTION ENTRA EROSION CONTROL BLANKET TURF REINFORCEMENT MAT EMERGENCY OVERFLOW SOIL BORING LOCATION
REDUNDANT SILT CONTROL STORM SEWER FLARED END SECTION (WITH I WATER MAIN SANITARY SEWER RETAINING WALL DRAIN TILE GRADING LIMITS ROCK CONSTRUCTION ENTRA EROSION CONTROL BLANKET TURF REINFORCEMENT MAT EMERGENCY OVERFLOW SOIL BORING LOCATION
FLARED END SECTION (WITH I WATER MAIN SANITARY SEWER RETAINING WALL DRAIN TILE GRADING LIMITS ROCK CONSTRUCTION ENTRA EROSION CONTROL BLANKET TURF REINFORCEMENT MAT EMERGENCY OVERFLOW SOIL BORING LOCATION
WATER MAIN SANITARY SEWER RETAINING WALL DRAIN TILE GRADING LIMITS ROCK CONSTRUCTION ENTRA EROSION CONTROL BLANKET TURF REINFORCEMENT MAT EMERGENCY OVERFLOW SOIL BORING LOCATION
SANITARY SEWER RETAINING WALL DRAIN TILE GRADING LIMITS ROCK CONSTRUCTION ENTRA EROSION CONTROL BLANKET TURF REINFORCEMENT MAT EMERGENCY OVERFLOW SOIL BORING LOCATION
RETAINING WALL DRAIN TILE GRADING LIMITS ROCK CONSTRUCTION ENTRA EROSION CONTROL BLANKET TURF REINFORCEMENT MAT EMERGENCY OVERFLOW SOIL BORING LOCATION
DRAIN TILE GRADING LIMITS ROCK CONSTRUCTION ENTRA EROSION CONTROL BLANKET TURF REINFORCEMENT MAT EMERGENCY OVERFLOW SOIL BORING LOCATION
GRADING LIMITS ROCK CONSTRUCTION ENTRA EROSION CONTROL BLANKET TURF REINFORCEMENT MAT EMERGENCY OVERFLOW SOIL BORING LOCATION
ROCK CONSTRUCTION ENTRA EROSION CONTROL BLANKET TURF REINFORCEMENT MAT EMERGENCY OVERFLOW SOIL BORING LOCATION
EROSION CONTROL BLANKET TURF REINFORCEMENT MAT EMERGENCY OVERFLOW SOIL BORING LOCATION
EMERGENCY OVERFLOW SOIL BORING LOCATION
SOIL BORING LOCATION
INLET PROTECTION



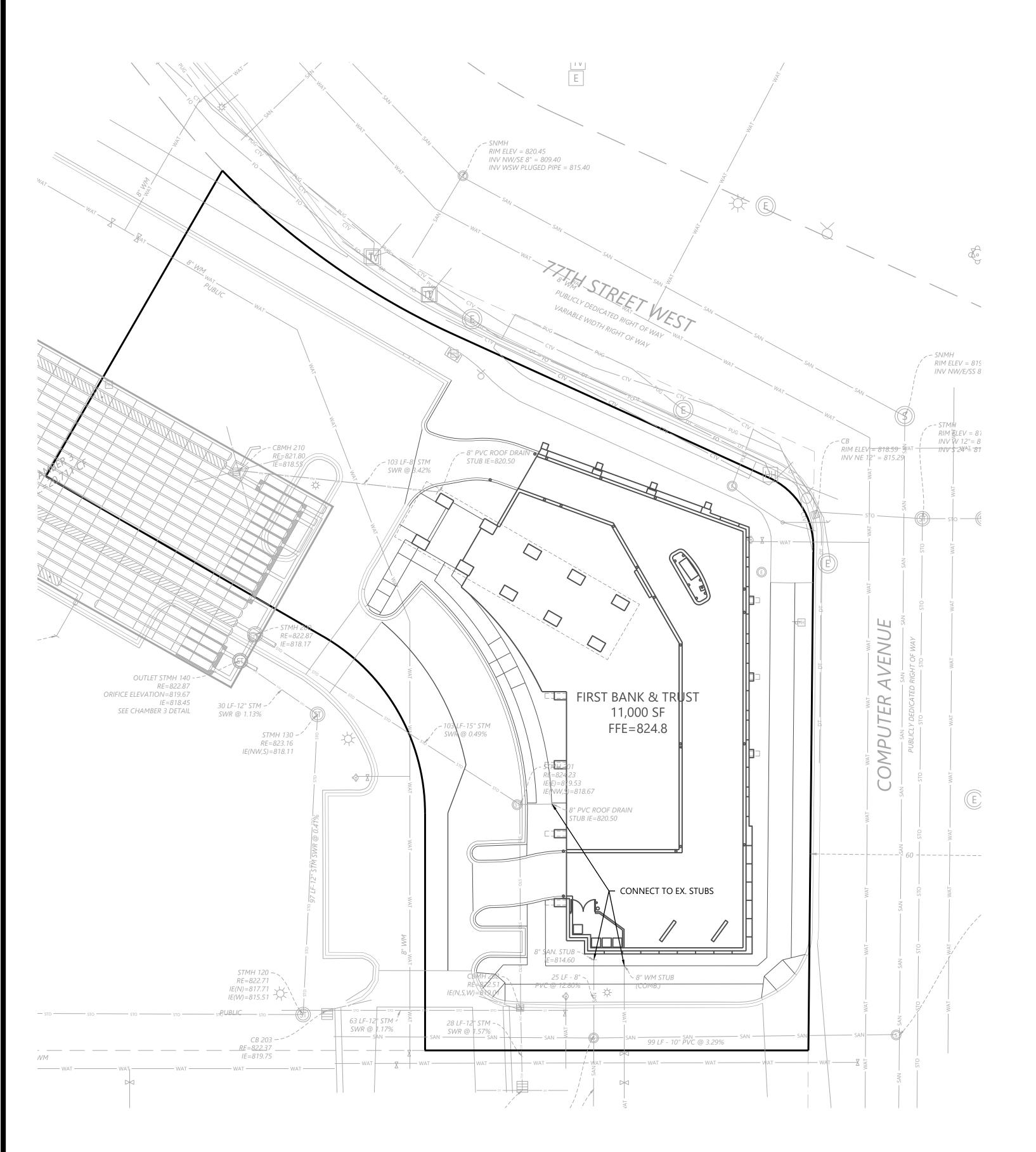
PROJECT NUMBER: 0037303.00

1" = 20'

NOT FOR CONSTRUCTION

40'

20'



GENERAL UTILITY NOTES

- 1. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS ARE BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND LIMITED MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION SHALL NOT BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR SHALL VERIFY EXISTING CONDITIONS PRIOR TO CONSTRUCTION AND NOTIFY THE OWNER OR ENGINEER OF DISCREPANCIES.
- 2. ALL SANITARY SEWER, STORM SEWER AND WATER MAIN MATERIAL AND INSTALLATIONS SHALL BE PER CITY REQUIREMENTS, MINNESOTA PLUMBING CODE, AND IN ACCORDANCE WITH THE CURRENT EDITION OF "STANDARD SPECIFICATIONS FOR WATER MAIN AND SERVICE LINE INSTALLATION AND SANITARY SEWER AND STORM SEWER INSTALLATION" AS PREPARED BY THE CITY ENGINEERS ASSOCIATION OF MINNESOTA.
- 3. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL OBTAIN THE NECESSARY FEDERAL, STATE AND LOCAL PERMITS FOR THE PROPOSED WORK OR VERIFY WITH THE OWNER OR ENGINEER THAT PERMITS HAVE BEEN OBTAINED. PERMIT FEES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR UNLESS OTHERWISE ARRANGED WITH THE OWNER.
- 4. CONTRACTOR SHALL REFER TO ARCHITECTURAL PLANS FOR EXACT LOCATION AND DIMENSIONS OF DOORWAYS, RAMPS, TRUCK DOCKS, PRECISE BUILDING DIMENSIONS AND EXACT BUILDING UTILITY CONNECTION LOCATIONS.
- 5. ALL PRIVATE UTILITIES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE SPECIFICATIONS OF THE APPROPRIATE UTILITY COMPANY. THE CONTRACTOR SHALL COORDINATE THE SERVICE LINE CONSTRUCTION WITH THE UTILITY COMPANIES.
- 6. CONTRACTOR SHALL OBTAIN ALL NECESSARY CITY PERMITS FOR UTILITY CONNECTIONS, AND UTILITIES SHALL BE INSPECTED AND APPROVED BY THE CITY. THE CITY SHALL BE NOTIFIED 48-HOURS PRIOR TO COMMENCING WITH THE UTILITY CONSTRUCTION OR ANY REQUIRED TESTING. CONTRACTOR SHALL NOT OPERATE, INTERFERE WITH, CONNECT ANY PIPE OR HOSE TO, OR TAP ANY WATER MAIN BELONGING TO THE CITY UNLESS DULY AUTHORIZED TO DO SO BY THE CITY. ANY ADVERSE CONSEQUENCES OF SCHEDULED OR UNSCHEDULED DISRUPTIONS OF SERVICE TO THE PUBLIC ARE TO BE THE RESPONSIBILITY OF THE CONTRACTOR.
- 7. WATER MAIN LENGTHS AS SHOWN ARE APPROXIMATE HORIZONTAL LENGTHS. ALLOW FOR ADDITIONAL PIPE WHEN INSTALLING ON SLOPES OR WHEN DEFLECTIONS ARE REQUIRED. THE JOINT DEFLECTIONS SHALL NOT EXCEED THE MAXIMUM RECOMMENDED BY THE PIPE MANUFACTURER OR BY LOCAL GOVERNING SPECIFICATIONS. FITTINGS REQUIRED TO CONSTRUCT WATER MAIN SHALL BE INCLUDED IN WATER MAIN CONSTRUCTION.
- 8. PROVIDE WATER MAIN THRUST RESTRAINTS PER CITY STANDARD REQUIREMENTS.
- 9. A MINIMUM VERTICAL SEPARATION OF 18 INCHES IS REQUIRED AT ALL WATER LINE CROSSINGS WITH SANITARY SEWER OR STORM SEWER. THE WATER LINE SHALL NOT HAVE JOINTS OR CONNECTION WITHIN 10-FEET OF THE CROSSING. INSULATE CROSSINGS WITH STORM SEWER.
- 10. UTILITY SERVICES TYPICALLY TERMINATE 5' OUTSIDE BUILDING WALL UNLESS OTHERWISE SHOWN OR NOTED.
- 11. DUCTILE IRON WATER LINES SHALL BE CLASS 52, PER AWWA C115 OR C151. COPPER WATER LINES SHALL BE TYPE K PER ASTM B88. PVC WATER LINES SHALL BE PER AWWA C900 AND INSTALLED PER AWWA C605 IF ALLOWED BY CITY.
- ALL WATER LINES SHALL HAVE <<7.5' OR 8'>> MINIMUM COVER. INSULATE WATER MAIN IF LESS THAN 8' OF COVER. INSULATION SHALL BE DOW STYROFOAM HI BRAND 35 OR EQUIVALENT, WITH 4 INCHES OF THICKNESS.
- 13. SANITARY SEWER PIPE OUTSIDE THE BUILDING ENVELOPE SHALL BE POLYVINYL CHLORIDE (PVC) SDR 35 OR 26. SDR 26 IS REQUIRED FOR DEPTHS GREATER THAN 15 FEET. SANITARY SEWER PIPE WITHIN 5 FEET OF THE BUILDING AND UNDER FOOTINGS SHALL BE SCHEDULE 40 PER ASTM D2665. ALL PLASTIC SANITARY SEWER SHALL BE INSTALLED PER D2321. SOLVENT WELD JOINTS MUST INCLUDE USE OF A PRIMER WHICH IS OF A CONTRASTING COLOR TO THE PIPE AND CEMENT. ALL SANITARY SEWER SHALL BE TESTED ACCORDING TO MINNESOTA PLUMBING CODE, PART 712.0.
- 14. STORM SEWER PIPE:
- A. RCP AND HDPE PIPE MAY BE INSTALLED WITH APPROVAL OF LOCAL GOVERNING AGENCY.
- B. REINFORCED CONCRETE PIPE SHALL BE CLASS 5 FOR PIPE DIAMETERS 18" AND SMALLER, CLASS 3 FOR PIPE DIAMETERS 21" AND LARGER UNLESS OTHERWISE NOTED, PER ASTM C76, WITH GASKETS PER ASTM C443.
- C. HDPE STORM PIPE 4- TO 10-INCHES IN DIAMETER SHALL MEET REQUIREMENTS OF AASHTO M252. HDPE STORM PIPE 12- TO 60-INCHES IN DIAMETER SHALL MEET REQUIREMENTS OF ASTM F2306. FITTINGS SHALL BE PER ASTM D3212 AND INSTALLED PER ASTM D2321.
- D. PVC STORM SEWER PIPE AND FITTINGS SHALL BE SCHEDULE 40 PIPE PER ASTM D2665 AND INSTALLED PER ASTM D2321.
- E. CORRUGATED METAL PIPE (CMP) FOR SIZES 18- TO 120-INCH AND MUST MEET ASTM A760 OR ASTM A796 AND BE INSTALLED PER ASTM A798. CMP MAY NOT BE INSTALLED WITHIN 10-FEET OF A WATERMAIN, WATER SERVICE, OR A BUILDING.
- F. ALL STORM SEWER JOINTS AND STRUCTURE CONNECTIONS SHALL BE GASTIGHT OR WATERTIGHT AS REQUIRED BY MINNESOTA PLUMBING CODE, PART 707.3. STORM SEWER LOCATED WITHIN 10-FEET OF A BUILDING AND/OR WATER LINE SHALL BE TESTED PER MINNESOTA PLUMBING CODE, PART 712.
- 15. ALL NONCONDUCTIVE PIPE SHALL BE INSTALLED WITH A LOCATE (TRACER) WIRE PER MINNESOTA RULES, PART 7560.0150.
- 16. POST INDICATOR VALVES SHALL BE CLOW F-5750 (OR EQUIVALENT) MEETING AWWA STANDARD C509 AND CITY STANDARDS. VALVE TO BE MECHANICAL JOINT RESILIENT WEDGE GATE VALVE. POST TO BE ADJUSTABLE FOR 8 FEET WATER MAIN DEPTH. THE ELECTRICAL ALARM SWITCH SHALL BE PART NO. PCVS2 (OR EQUIVALENT).
- 17. AFTER CONSTRUCTION IS COMPLETED, THE CONTRACTOR SHALL PROVIDE THE OWNER WITH AN AS-BUILT RECORD OF UTILITY CONSTRUCTION. THE AS-BUILT SHALL INCLUDE LOCATION AND LENGTH DEVIATIONS OR CHANGES TO THE PLAN. CONTRACTOR TO VERIFY WITH OWNER OR ENGINEER WHETHER A PLAN WITH POST-CONSTRUCTION ELEVATIONS IS REQUIRED.
- 18. ALL MANHOLE CASTINGS IN PAVED AREAS SHALL BE SUMPED 0.05 FEET. RIM ELEVATIONS ON PLAN REFLECT THE SUMPED ELEVATIONS.
- 19. ALL CATCH BASIN CASTINGS IN CURB SHALL BE SUMPED 0.15 FEET AND MANHOLE CASTINGS IN PAVED AREAS SHALL BE SUMPED 0.05 FEET. RIM ELEVATIONS ON PLAN REFLECT THE SUMPED ELEVATIONS.

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UTILITY LEGEND

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PROPERTY LINE
EASEMENT LINE
CURB AND GUTTER
SANITARY SEWER
SANITARY SEWER FORCE MAIN
STORM SEWER
WATER MAIN
HYDRANT
GAS
UNDERGROUND ELECTRIC
OVERHEAD ELECTRIC
UNDERGROUND TELEPHONE
OVERHEAD TELEPHONE
TELEPHONE FIBER OPTIC
CABLE TELEVISION
DRAIN TILE
GATE VALVE
FLARED END SECTION (WITH RIPRAP)
LIGHT POLE

SHEET							
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		Westwood	EIDCT BANK 8, TDLICT	OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS	PREPARED FOR:	REVISIONS:	CHECKED:
ER:				OF THE STATE OF MINNESOTA		· · · · · · · · · · · · · · · · · · ·	
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0		Toll Free (888) 937-5150 westwoodps.com			1010 MAINISTBEET	\sum	HORIZONTAL SCALE: 20'
		Westwood Professional Services, Inc.	EDINA, MN	DATE: 09/26/22_LICENSE NO.	HOPKINS, MN 55343		VERTICAL SCALE:
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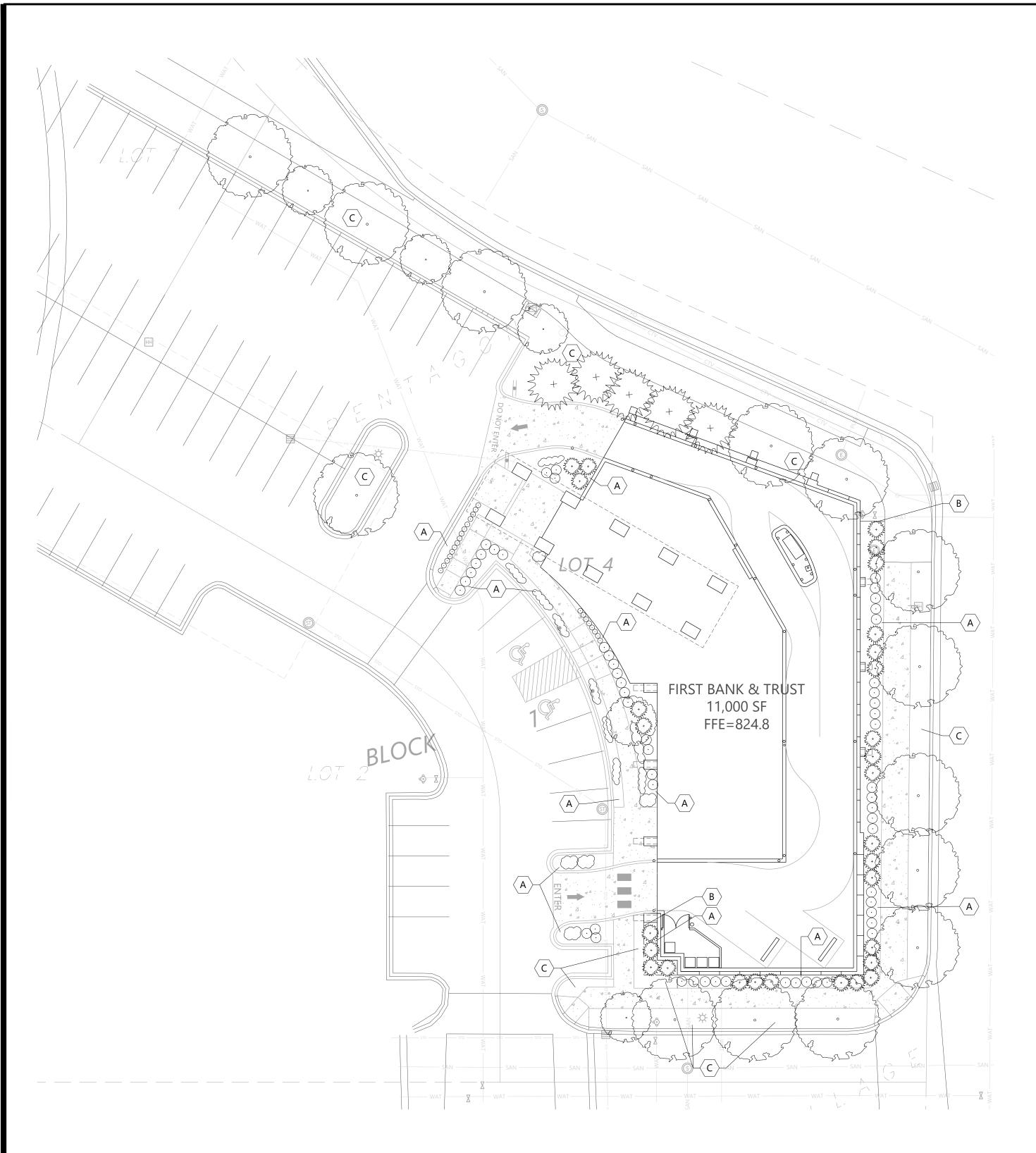
FIRST BANK & TRUST PENTAGON VILLAGE

DATE: 09/26/22 PROJECT NUMBER: 0037303.00

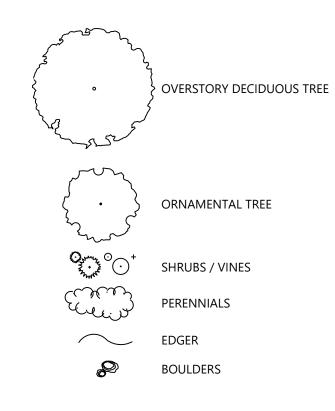
1" = 20'

NOT FOR CONSTRUCTION

20'



LANDSCAPE LEGEND



LANDSCAPE KEYNOTES A

- A SHREDDED HARDWOOD MULCH (TYP.)
- B EDGER (TYP.) C SOD (TYP.)
- D ROCK MULCH

LANDSCAPE SUMMARY

OVERSTORY TREE REQUIREMENT

TREES REQUIRED ON SITE IS DETERMINED BY THE SITE PERIMETER OF THE LOT DIVIDED BY 40. IN ADDITION TO THE REQUIRED NUMBER OF OVERSTORY TREES, A FULL COMPLIMENT OF UNDERSTORY TREES AND

SHRUBS SHALL BE PROVIDED TO COMPLETE A QUALITY LANDSCAPE TREATMENT.

ORNAMENTAL COMPLIMENTARY ACCENT PRIMARY	DECIDUOUS 2" CAL. OR LESS 2.5" CAL. 3.5" CAL. 4.5" CAL.	CONIFEROUS 5' HT. OR LESS 6' HT. 8' HT 10' HT.	% OF TOTAL 5% 25% 25% 20%	MIN. REQ. 2 6 6 5
FULL TOTAL	5.5" CAL.	12' HT	20%	5 24

1 OVERSTORY TREE PER 40 LF OF SITE PERIMETER (902/40= 22.55 TREES)

TREES REQUIRED BASED ON SITE PERIMETER= 23

TREES REQUIRED AFTER CALCULATING MINIMUM SIZING= 24

TREES PROVIDED= 24

PROPOSED TREE PLANTINGS

DECIDUOUS TREES= 19 SIZES: 2 @ 1.5", 3 @ 2.5", 4 @ 3.5", 5 @ 4.5", 5 @ 5.5" CONIFEROUS TREES= 5 SIZES: 0 @ 5', 3 @ 6', 2 @ 8', 0 @ 10', 0 @ 12'

CODE	QTY	COMMON	BOTANIC
OVERSTORY TREE	14		
SGM		SIENNA GLEN MAPLE	ACER X FF
WSB		WHITESPIRE BIRCH	BETULA PO
IMH		IMPERIAL HONEYLOCUST	GLEDITSIA
СНВ		COMMON HACKBERRY	CELTIS OC
AGG		AUTUMN GOLD GINKO	GINKO BIL
ORNAMENTAL TRE	E 5		
PFC		PRAIRIEFIRE CRAB	MALUS 'PF
JTL		JAPANESE TREE LILAC	SYRINGA F
CONIFEROUS TREE	5		
BHS		BLACK HILLS SPRUCE	PICEA GLA
SCP		SCOTCH PINE	PINUS SYL
CONIFEROUS SHRU	B 29		
BNS		BIRD'S NEST SPRUCE	PICEA ABIE
HZA		HETZ MIDGET ARBORVITAE	THUJA OC
MJJ		MINT JULEP JUNIPER	JUNIPERU
MUP		MUGO PINE	PINUS MU
TAY		TAUNTON YEW	TAXUS ME
ВРЈ		BLUE PRINCE JUNIPER	JUNIPERU
DECIDUOUS SHRUB	55		
NFS		NEON FLASH SPIREA	SPIRAEA JA
ALC		ALPINE CURRANT	RIBES ALPI
DBH		DWARF BUSH HONEYSUCKLE	DIERVILLA
MWG		MINUET WEIGELA	WEIGELA F
DHW		DARK HORSE WEIGELA	WEIGELA F
ANH		ANNABELLE HYDRANGEA	HYDRANG
AJN		AMBER JUBILEE NINEBARK	PHYSOCAR
DGN		DART'S GOLD NINEBARK	PHYSOCAR
RGD		RED GNOME DOGWOOD	CORNUS A
BLC		BLACK CHOKEBERRY	ARONIA M
DKL		DWARF KOREAN LILAC	SYRINGA N
PERENNIALS	125		
ICD		ICE CARNIVAL DAYLILY	HEMEROCA
PMD		PARDON ME DAYLILY	HEMEROCA
SDO		STELLA DE ORO DAYLILY	HEMEROC
AFS		AUTUMN FIRE SEDUM	SEDUM X '/
MNS		MAY NIGHT SALVIA	SALVIA X I
BLB		BLAZE LITTLE BLUESTEM GRASS	SCHIZACH
KFG		KARL FOERSTER FEATHER REED GRASS	CALAMAG

PLANT SCHEDULE IICAL NAME SIZE ROOT SPACING REMARKS FREEMANII 'SIENNA' 2.5" CAL. AS SHOWN SINGLE B&B POPULIFOLIA 'WHITESPIRE' 2.5" CAL. AS SHOWN SINGLE B&B IA TRIACANTHOS VAR. INERMIS 'IMPCOLE' 2.5" CAL. B&B AS SHOWN SINGLE OCCIDENTALIS 2.5" CAL. B&B AS SHOWN SINGLE ILOBA 'AUTUMN GOLD' 2.5" CAL. B&B AS SHOWN SINGLE 'PRAIRIEFIRE' 1.5" CAL. B&B AS SHOWN SINGLE 1.5" CAL. B&B AS SHOWN SINGLE RETICULATA LAUCA DENSATA 6' HT. B&B AS SHOWN FULL YLVESTRIS 6' HT. B&B AS SHOWN FULL BIES 'NIDIFORMIS' 5'-0" O.C. #5 CONT. X'-X" O.C. CCIDENTALIS 'HETZ MIDGET 3'-0" O.C. #5 CONT. X'-X" O.C. US CHINENSIS 'MONLEP' 4'-0" O.C. #5 CONT. X'-X" O.C. IUGO PUMILIO 4'-0" O.C. #5 CONT. X'-X" O.C. AEDII 'TAUNTON' 4'-0" O.C. #5 CONT. X'-X" O.C. US HORIZONTALIS 'BLUE PRINCE' 4'-0" O.C. #5 CONT. X'-X" O.C. JAPONICA 'NEON FLASH' CONT. 3'-0" O.C. #5 PINUM CONT. 3'-0" O.C. #5 A LONICERA 3'-0" O.C. CONT. #5 FLORIDA 'MINUET' CONT. 3'-0" O.C. #5 A FLORIDA 'DARK HORSE' CONT. 3'-0" O.C. #5 IGEA ARBORESCENS 'ANNABELLE' #5 CONT. 4'-0" O.C. ARPUS OPULIFOLUS 'JEFAM' CONT. 4'-0" O.C. #5 ARPUS OPULIFOLIUS 'DART'S GOLD' CONT. 4'-0" O.C. #5 S ALBA SIBIRICA CONT. 4'-0" O.C. #5 MELANOCARPA CONT. 4'-0" O.C. #5 MEYERI 'PALIBIN' CONT. 5'-0" O.C. #5 CALLIS 'ICE CARNIVAL' #1 CONT. #1 18" O.C CALLIS 'PARDON ME' #1 CONT. 12" O.C. #1 CALLIS 'STELLA DE ORO' 12" O.C. #1 CONT. #1 'AUTUMN FIRE' #1 CONT. 18" O.C. #1 K NEMOROSA 'MAY NIGHT' #1 #1 CONT. 18" O.C. CHYRIUM SCOPARIUM 'BLAZE' #1 CONT. 18" O.C. #1 AGROSTIS X ACUTIFLORA 'KARL FOERSTER' #1 #1 CONT. 24" O.C. --MIN. = MINIMUM O.C. = ON CENTER SP. = SPREAD QTY .= QUANTITY CONT. = CONTAINER NOTE: QUANTITIES ON PLAN SUPERSEDE LIST QUANTITIES IN THE EVENT OF A DISCREPANCY.

Call 48 Hours before digging: 811 or call811.com Common Ground Alliance

MINIMUM SIZE OF REQUIRED OVERSTORY TREES FOR BUILDING HEIGHT GREATER THAN 24' AS FOLLOWS:

SHEET							
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M				OR UNDER MY DIRECT SUPERVISION AND THAT I AM A	PREPARED FOR:	REVISIONS:	
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.(Phone (952) 937-5150 12701 Whitewater Drive, Suite #300 Fax (952) 937-5822 Minnetonka, MN 55343	VILLAGE		HTG ARCHITECTS		DRAWN:
C	PLAN	Toll Free (888) 937-5150 westwoodps.com				· · ·	HORIZONTAL SCALE: 20'
		Westwood Professional Services, Inc.	EDINA, MN	DATE: 09/26/22 LICENSE NO.	HOPKINS, MN 55343		VERTICAL SCALE: 4' or 2'
- BANK & TRUST F	BANK & TRUST PENTAGON VILLAGE						

1" = 20'

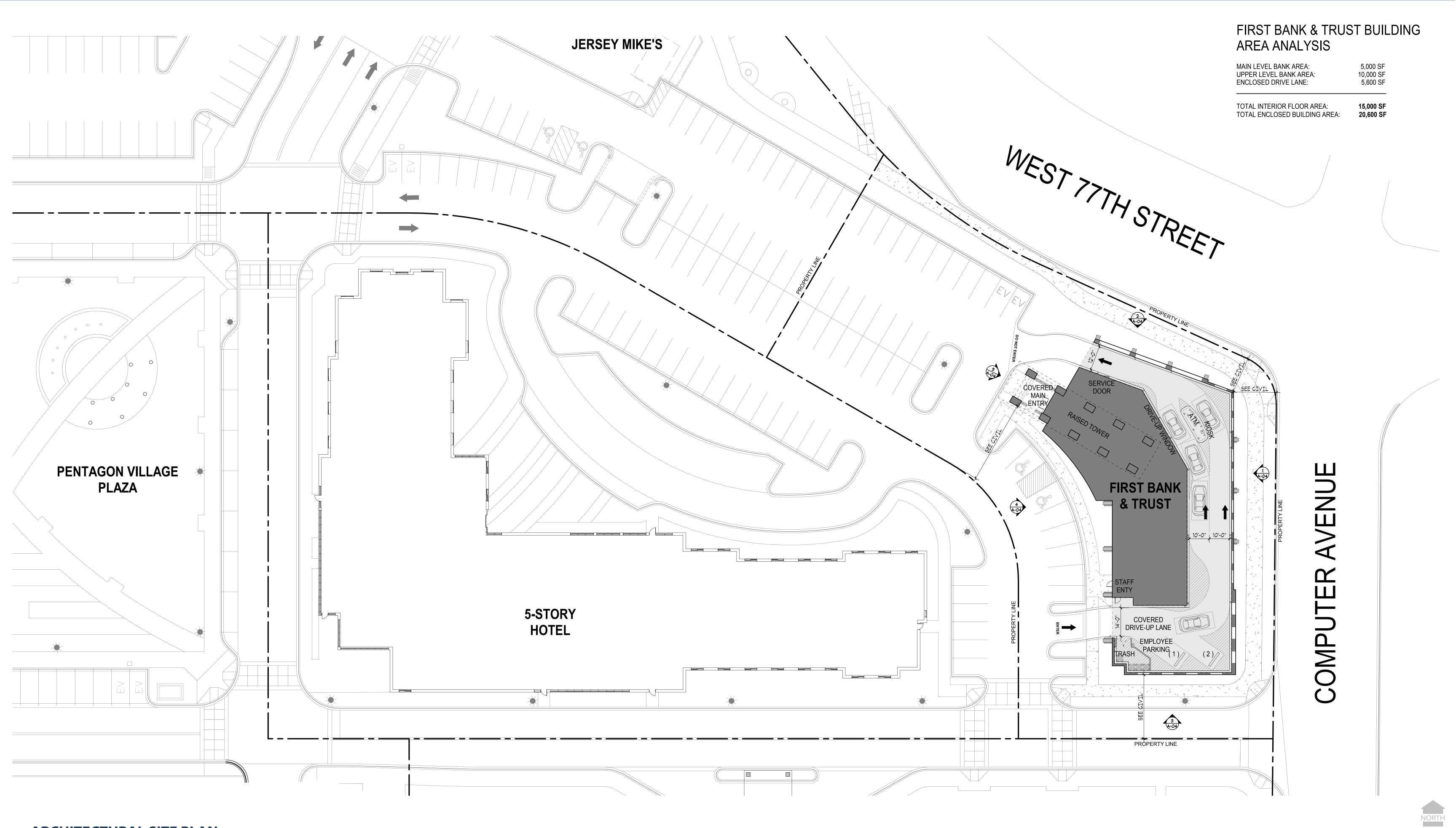
20' 40'

NOT FOR CONSTRUCTION

60'

DATE: 09/26/22

PROJECT NUMBER: 0037303.00



ARCHITECTURAL SITE PLAN



FIRST BANK & TRUST - PENTAGON VILLAGE

EDINA MINNESOTA 09/26/2022





FIRST BANK & TRUST - PENTAGON VILLAGE

COMPARISON SITE PLAN

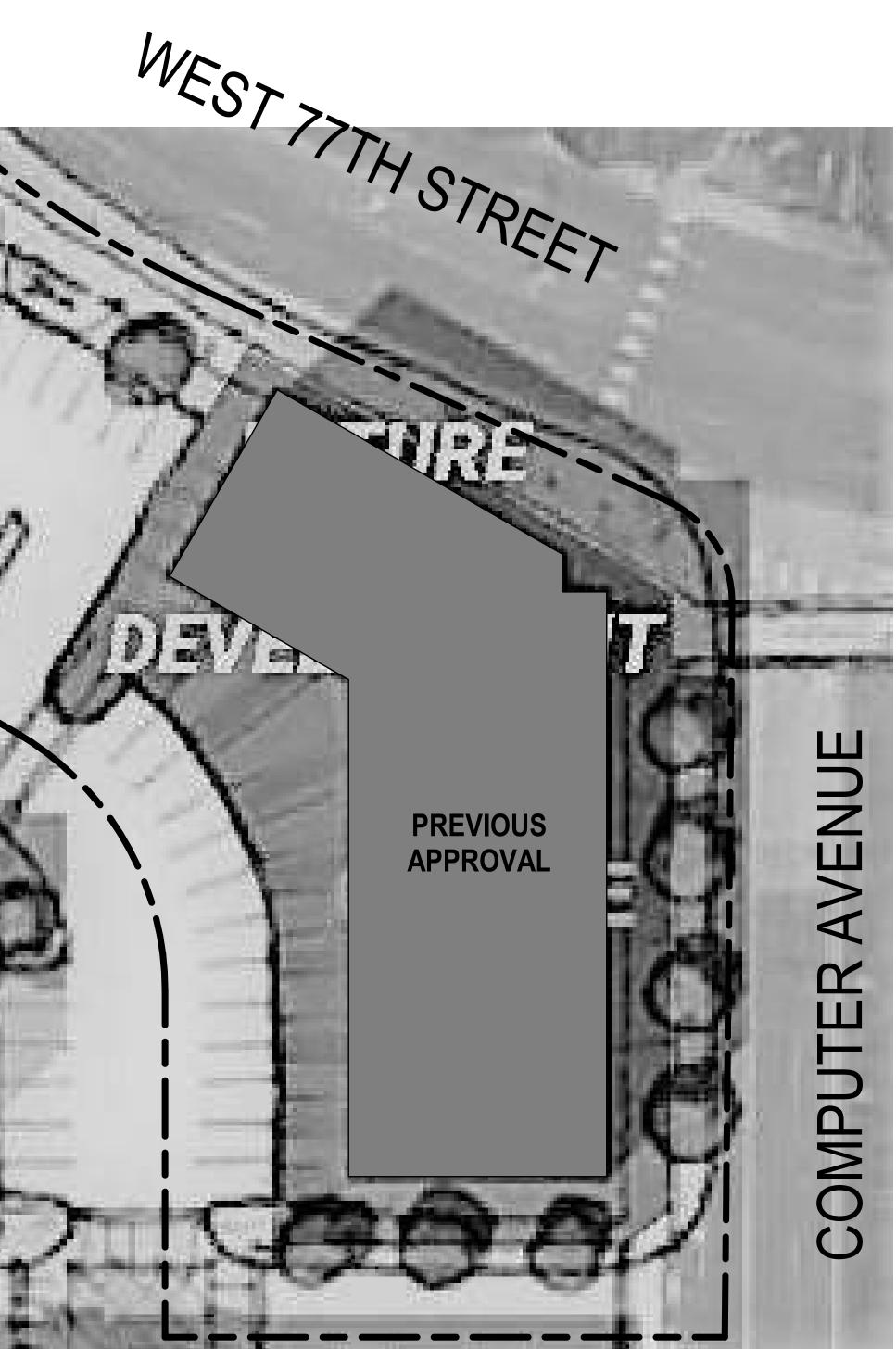
EDINA

MINNESOTA 09/26/2022









PREVIOUSLY APPROVED SITE PLAN (SHOWN FOR COMPARISON)

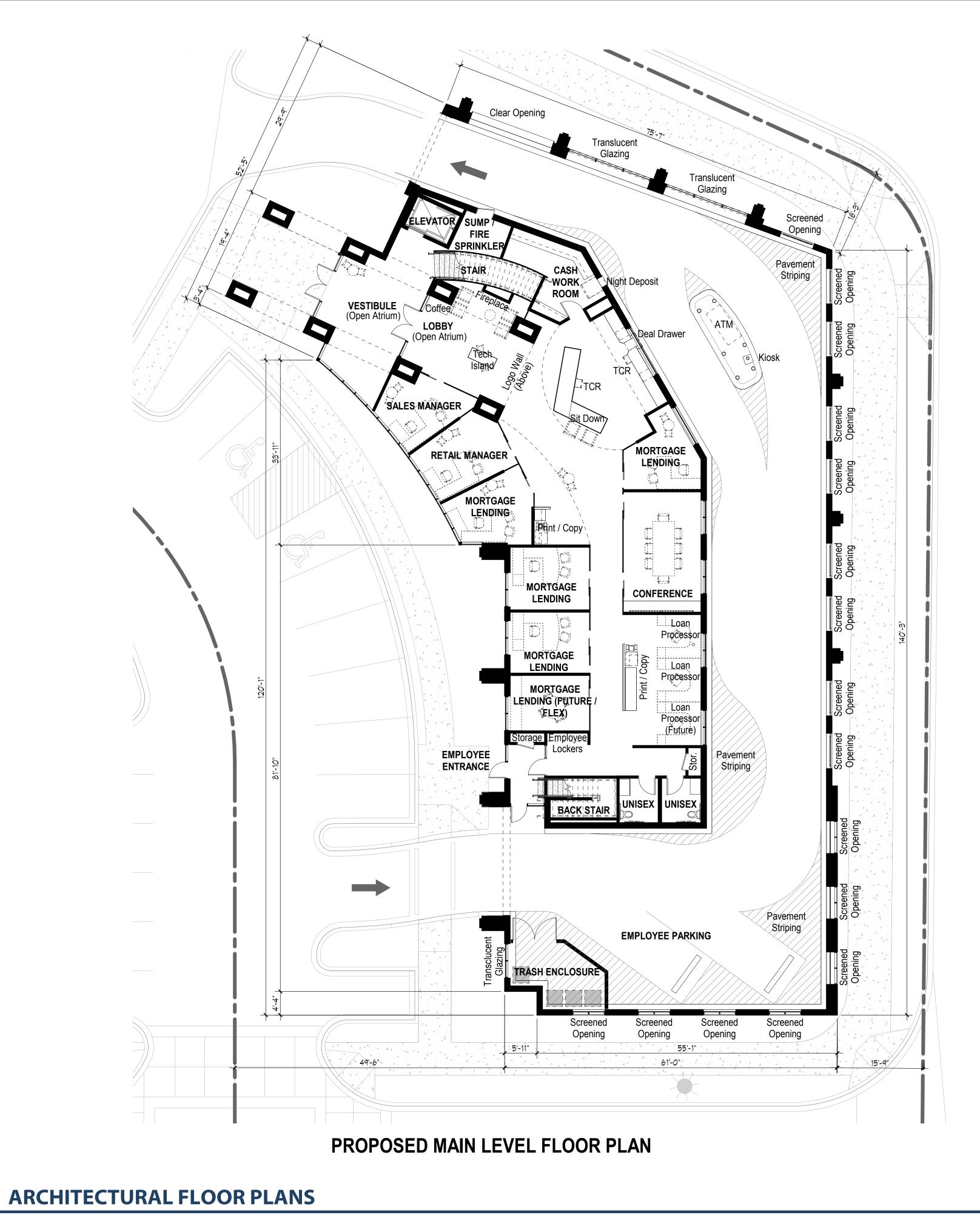


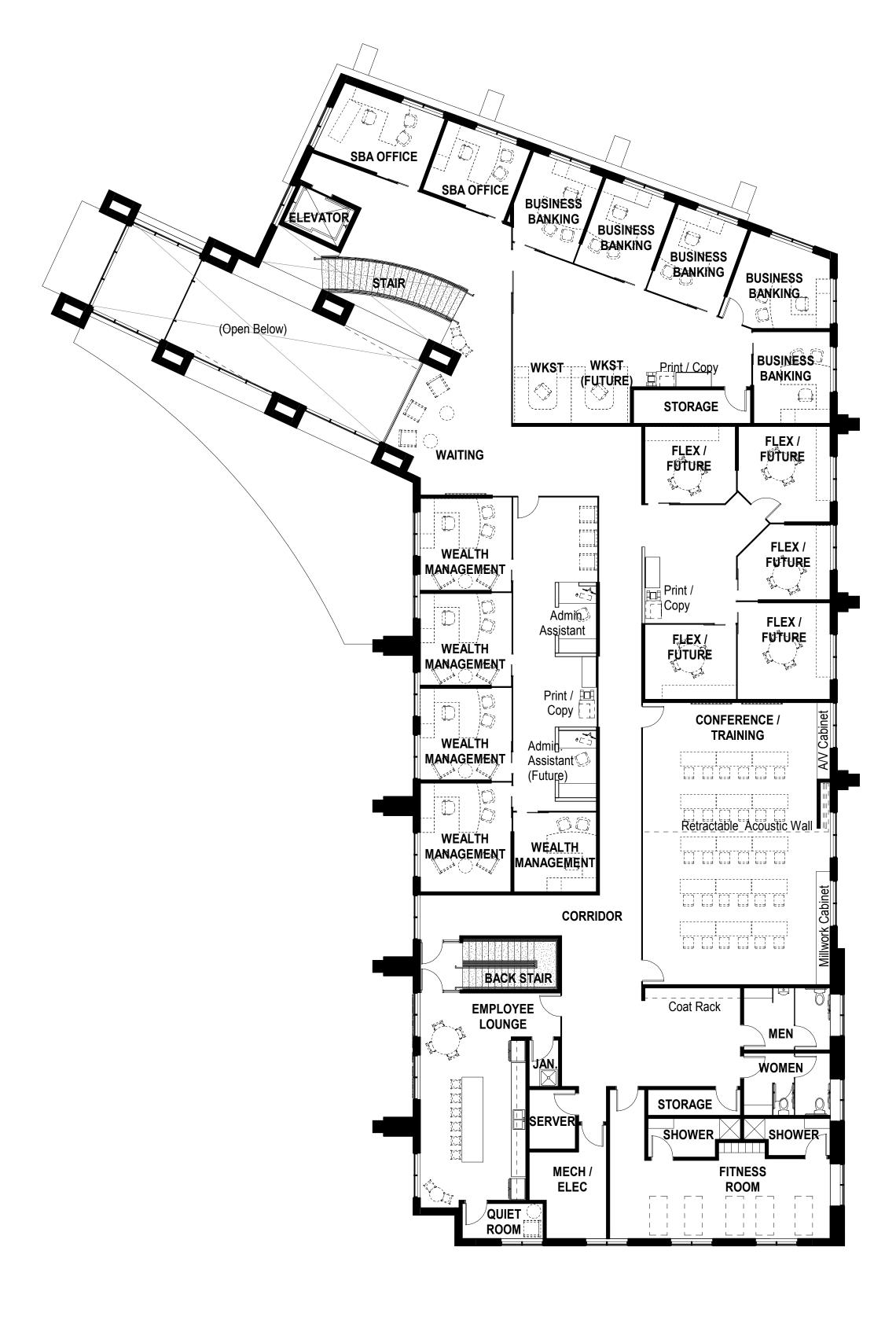




FIRST BANK & TRUST - PENTAGON VILLAGE

EDINA MINNESOTA 09/26/2022







NORTH

PROPOSED UPPER LEVEL FLOOR PLAN



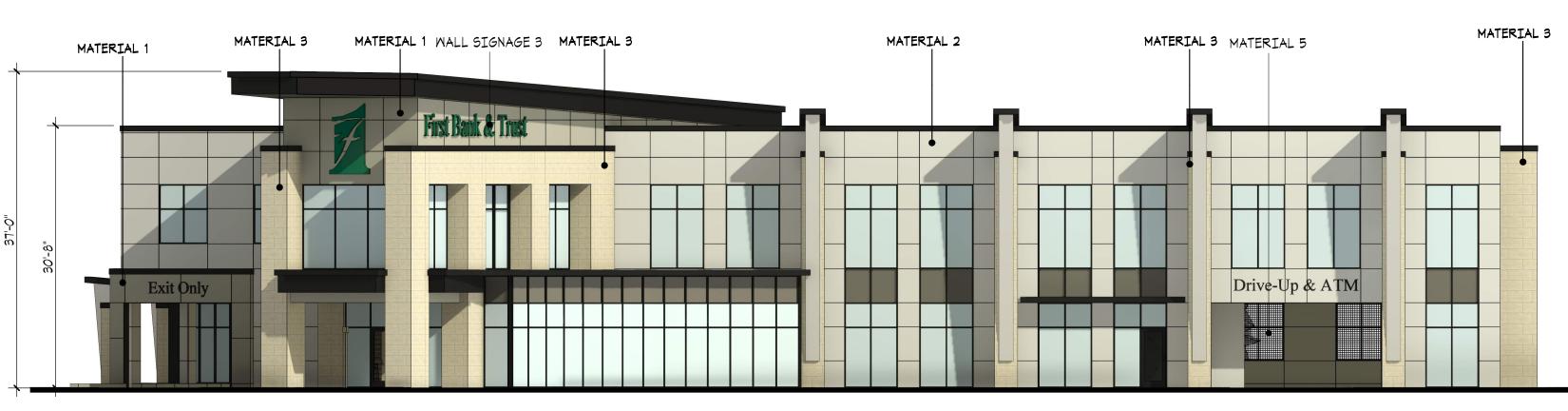
FIRST BANK & TRUST - PENTAGON VILLAGE

EXTERIOR ELEVATIONS

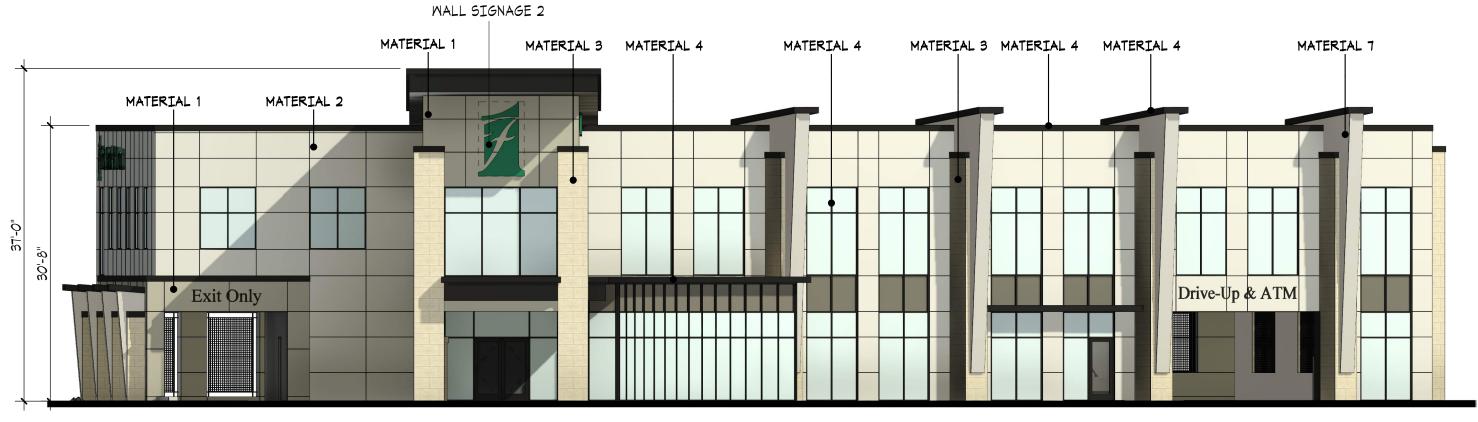
EDINA

MINNESOTA 09/26/2022

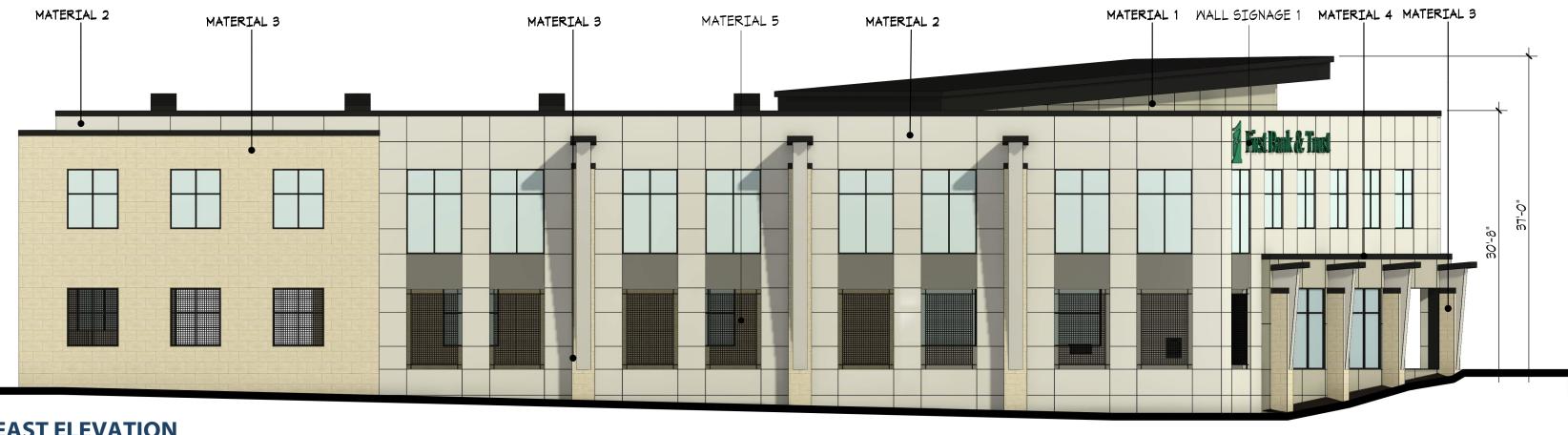




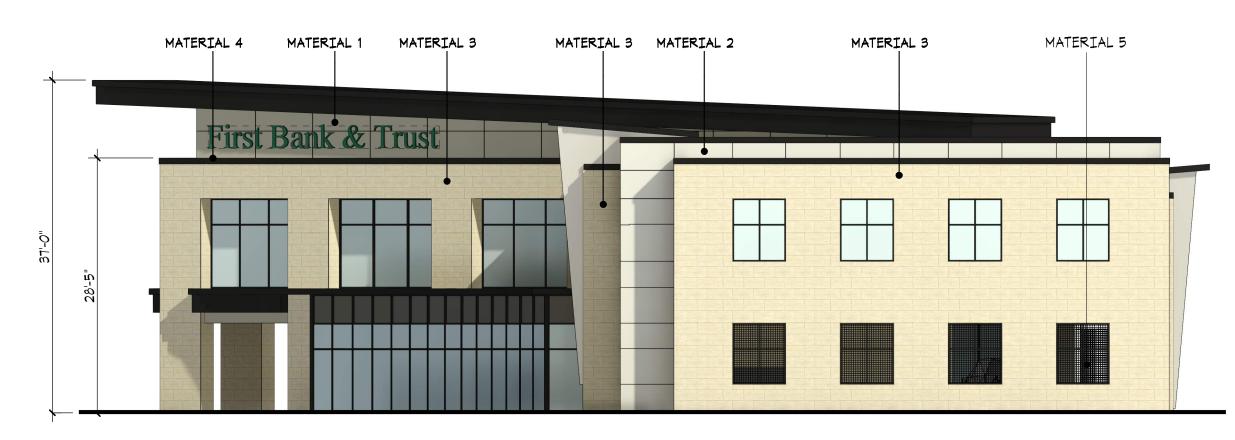
NORTHWEST ELEVATION 3/32" = 1'-0"







SOUTH ELEVATION 3/32" = 1'-0"







LIGHT GRAY METAL PANEL



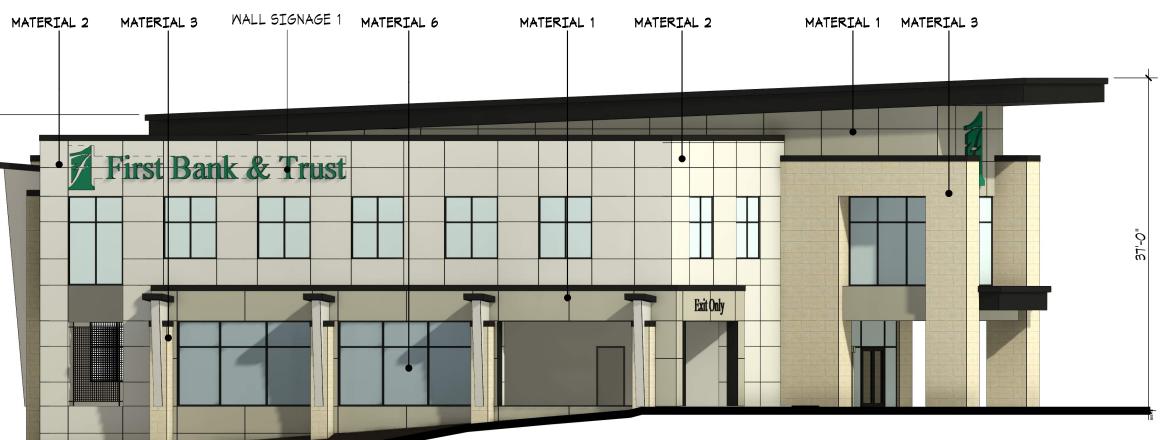
BEIGE STONE PANEL

MATERIAL 1





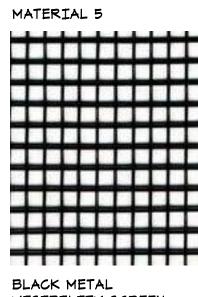
NORTH ELEVATION 3/32" = 1'-0"



MATERIAL 4



BLACK ALUMINIUM (STOREFRONT AND COPING)



VISIBILITY SCREEN



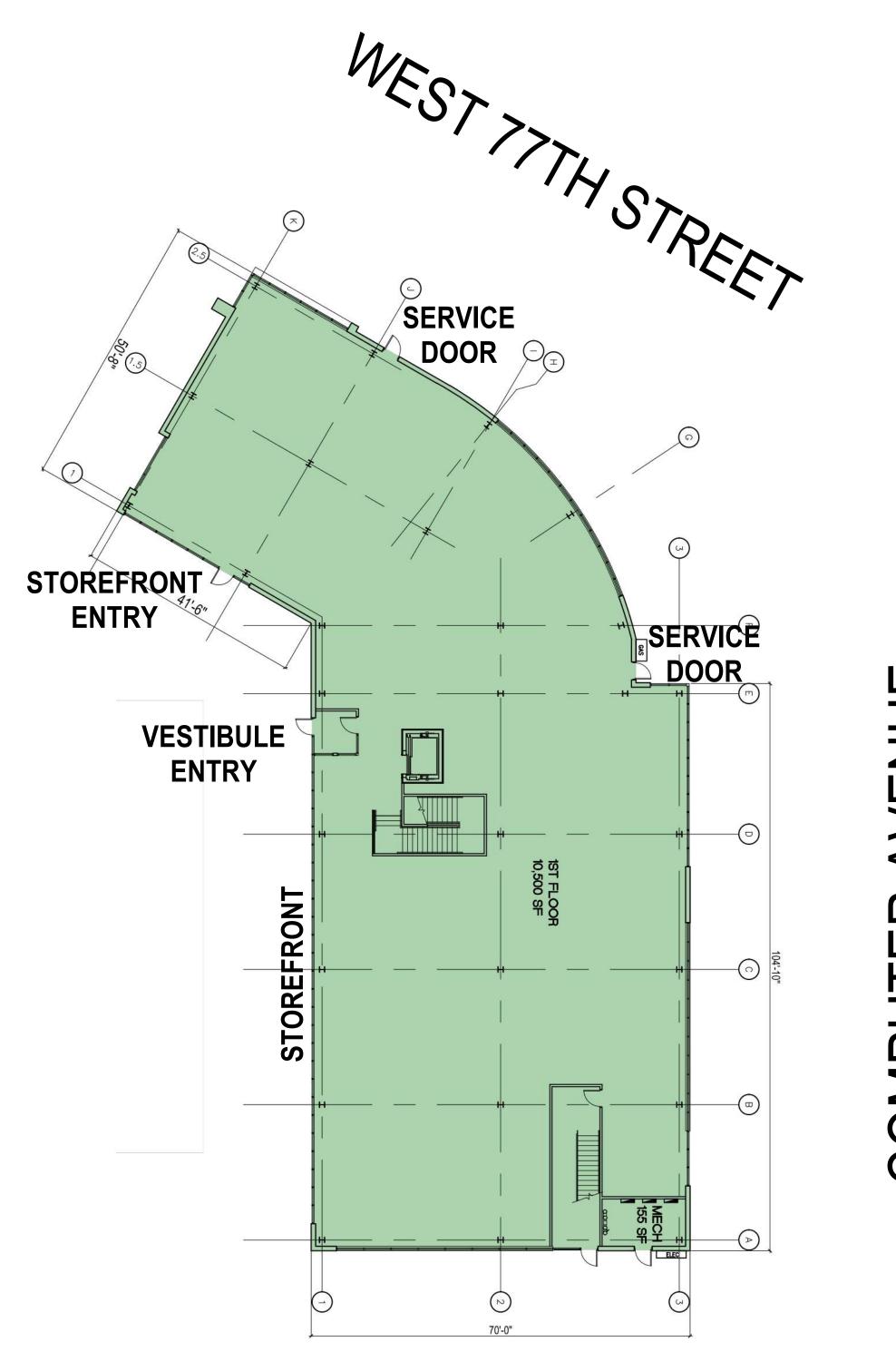
TRANSLUCENT STOREFRONT GLAZING





ILLUMINATED DECORATIVE FIN W/SOFT LIGHTING





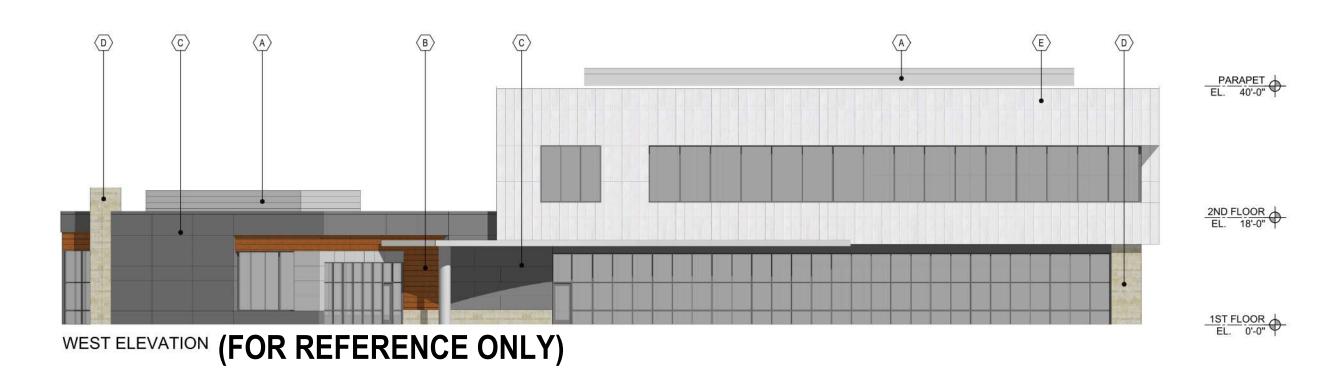
PREVIOUSLY APPROVED FLOOR PLAN (FOR REFERENCE ONLY)

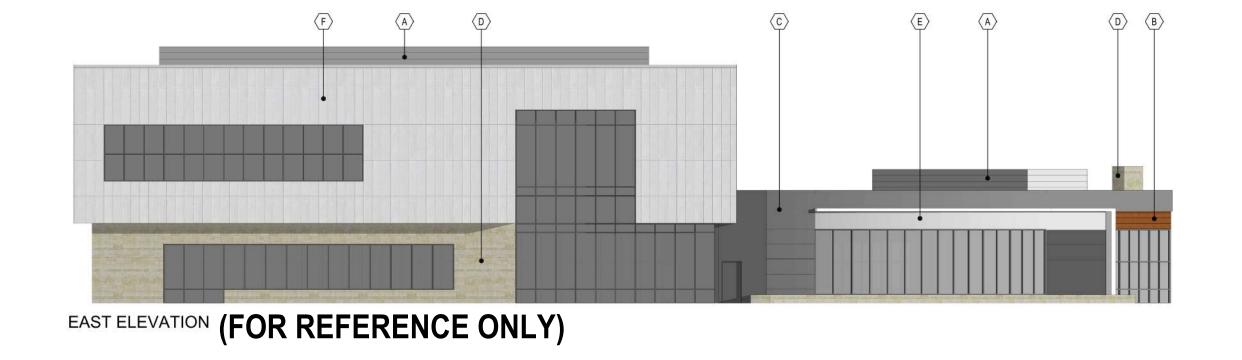
PREVIOUSLY APPROVED ELEVATIONS (APPROVED DECEMBER 2018 LOT 4 DRAWINGS INCLUDED FOR REFERENCE ONLY)

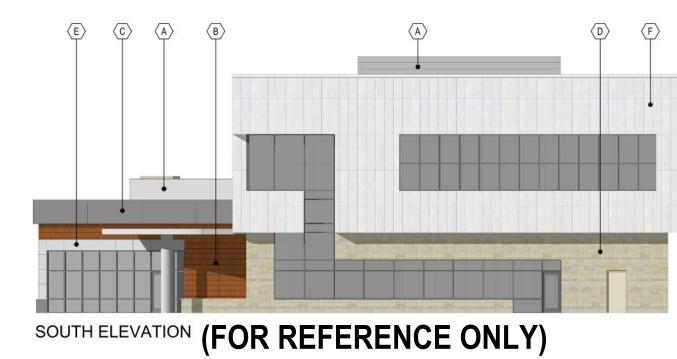
FIRST BANK & TRUST - PENTAGON VILLAGE

EDINA MINNESOTA 09/26/22

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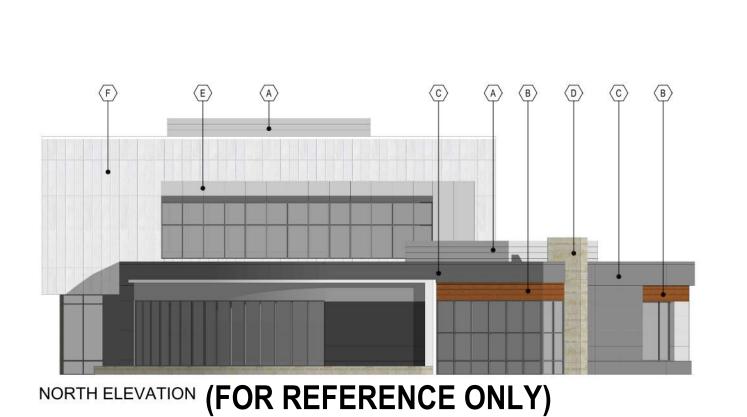






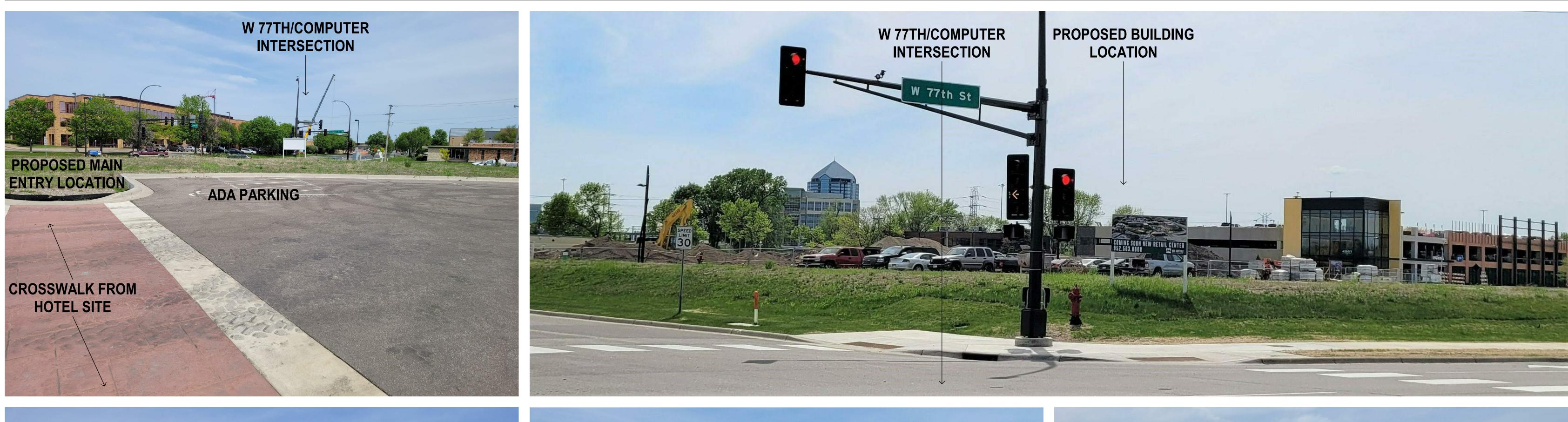
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A	B	C		E		
CORRUGATE METAL PANE MECH SCREE	L METAL PANEL	METAL PANEL	MASONRY	METAL PANEL	METAL PANEL	

EXISTING SITE CONDITIONS





PROJECT LOCATION PHOTOGRAPHS



FIRST BANK & TRUST - PENTAGON VILLAGE

EDINA MINNESOTA 09/26/2022



ADJACENT SITES





PROJECT LOCATION PHOTOGRAPHS



FIRST BANK & TRUST - PENTAGON VILLAGE

EDINA MINNESOTA 09/26/2022









EXTERIOR RENDERING AT MAIN ENTRY



FIRST BANK & TRUST - PENTAGON VILLAGE

EDINA MINNESOTA 09/25/22





EXTERIOR RENDERING FROM INTERSECTION OF WEST 77TH STREET & COMPUTER AVENUE



FIRST BANK & TRUST - PENTAGON VILLAGE

EDINA MINNESOTA 09/25/22







EXTERIOR RENDERING FROM COMPUTER AVENUE



FIRST BANK & TRUST - PENTAGON VILLAGE

EDINA MINNESOTA 10/07/22







FIRST BANK & TRUST - PENTAGON VILLAGE

EDINA MINNESOTA 09/25/22







EXTERIOR RENDERING AT NIGHT



FIRST BANK & TRUST - PENTAGON VILLAGE

EDINA MINNESOTA 09/26/22



DATE: 09/26/2022



SITE PLAN APPLICATION WRITTEN STATEMENT

FIRST BANK & TRUST – PENTAGON VILLAGE (NEW CONSTRUCTION) 4901 West 77th Street, Edina, MN

First Bank & Trust is a 142-year-old, privately held community bank founded by entrepreneur Horace Fishback in Brookings, South Dakota. FB&T is a full-service bank with \$4.2B in total assets. The bank serves 17 communities in South Dakota and Minnesota with 22 banking locations. FB&T entered the Edina market in December 2017 by way of acquiring Peoples Bank of Commerce. PBC has a location at 3316 West 66th Street, Edina which it occupied since December of 2005 following a relocation from Bloomington. Today, FB&T continues to offer banking services from the West 66th Street location. As FB&T continues to grow and expand both services and staff in Edina and throughout the Twin Cities, it has created a need to increase the physical capacity of the branch.

Anticipating this growth, FB&T engaged a commercial real estate broker to assist in identifying potential sites within Edina for consideration. After reviewing approximately 20 sites for compatibility with the bank's goals, this Pentagon Village location emerged as a prime opportunity to bring FB&T's growth to the area while deepening its connection with the Edina community and offering a wealth of high-level career opportunities. FB&T management anticipates staff increasing from 10 FTEs to approximately 20 within 12 months of commencing banking services at the proposed Edina branch. The new facility will incorporate space for FB&T's Chief Mortgage Officer, SBA Director, Twin Cities Wealth Management team, and private banking, each of these roles new to the Edina branch. Hoping to merge this vision with the goals of Pentagon Village and the City of Edina, FB&T began developing an all-new design incorporating the character of their existing branches in a custom-tailored approach for Pentagon Village.

The layout prioritizes a welcoming client lobby surrounded by clear glass façades and full height windows showcasing lobby and office activity. Over 30 individual workspaces fill the 20,600 square foot structure plus a 30-seat staff training facility to support existing and future Twin Cities branches, a fitness room, and other employee amenities. A compact drive-up lane supporting the bank's retail growth is an essential client-experience feature. In the wake of COVID-19, the necessity of this element has become even more significant.

After considering numerous layouts for the site including traditional surface drive-up lanes and various configurations of shielding the lanes from view, the bank landed on a concept that A) minimized disruption of the existing parking layout, B) utilized a building footprint similar to what has already been approved for the site, and C) included a completely integrated drive-up lane enclosed within the building structure and architecturally screened from view. The bank felt this was the best approach to uphold the city and community goals for this location as well as to establish an engaging architectural presence at this focal corner of the development.

A final factor influencing the project is the exceptionally high-water table. Per the watershed district, the lowest allowable floor level is 824.8', slightly above exiting parking surfaces, but nearly 6 feet above the sidewalk that begins at the northeast corner and encircles the development. The only viable point of entry was determined to be the northwest corner, accessible from the parking area. A raised tower emphasizes this location while an array of architecturally similar 'fin' features flanks the sides, visually connecting the main entry with the surrounding perimeter and guiding guests to the entrance, whichever direction they may be arriving from.

First Bank & Trust is excited to reestablish their presence in the community with the design of this local market flagship office location. A positive recommendation from Edina's Planning Commission and an approval from the City Council will be a major step forward in bringing a vision of growth to this Edina location. First Bank & Trust looks forward to partnering with Pentagon Village and the City of Edina to provide a design that improves upon an already thriving concept for this development.

DATE: 09/26/2022



ORDINANCE AMENDMENT WRITTEN STATEMENT

FIRST BANK & TRUST – PENTAGON VILLAGE (NEW CONSTRUCTION)

4901 West 77th Street, Edina, MN

First Bank & Trust hereby requests consideration for an ordinance amendment intended to address a design challenge being encountered in the Pentagon Village Planned Unit Development (PUD) as it relates to office building identification. The current language of the code for this PUD simply defers to the sign codes intended for the Mixed Development District (MDD). The concern is that the typical needs of these districts may differ, and that the ordinance could be improved to recognize these differences.

As is the case with the Pentagon Village development, a PUD tends to be arranged such that buildings wrap the development perimeter while parking and other interconnecting infrastructure is centralized. As currently written, the zoning ordinance allows for a maximum of two wall signs for any given building within the development with the ordinance dictating that one wall sign is allowed per street frontage and no building, current or planned, has more than two street frontages. However, it is essential that buildings are identifiable from within the development as well and we believe this would warrant an additional signage allowance in these cases. This may be a phenomenon that is more common in PUDs given their clustered arrangement while freestanding sites more typical of MDDs may be more likely to have street frontages on more sides and less likely to require separate signage for street frontages versus building fronts and entries oriented toward a development interior.

The current text of the code supports excellent visibility and identification from outside the development assuming one sign is used to face each street frontage but does not adequately support the ability of current and future uses to be identified from within the development. This challenge is exacerbated by irregular lot perimeters such as Lot 4, upon which First Bank & Trust intends to build. Lot 4 follows a dog-leg shape requiring a 6-sided building structure to fill the lot to its required density. Identification and brand presence become problematic in this case when no more than two wall signs are allowed.

This dilemma is not unique to First Bank & Trust as it should be noted that at least two other developed lots within Pentagon Village (The Reserve and Jersey Mike's) currently exceed the signage per street frontage allowance as specified in the current ordinance. Both buildings currently have 3 total wall signs with 2 street frontage-facing signs and 1 development interior-facing sign above their respective main entries. First Bank & Trust recommends amending the ordinance to allow the same visibility to future buildings in the same development.

(A summary of current and proposed zoning ordinance language as it relates to this request is outlined on the following page)

Original Text:

Sec. 36-510. - Planned Unit Development District-17 (PUD-17) Pentagon Park South.

••••

(h) Signs shall be regulated per the Mixed Development District.

Supplementary Text for Reference:

Sec. 36-1717. - Mixed Development District.

•••

(5) Building identification signs: office.

a. *MDD-6.* One wall sign per building per frontage and one freestanding sign per building per frontage. The maximum area of the first wall sign or freestanding sign shall not exceed 50 square feet and the maximum area of each additional sign shall not exceed 36 square feet. Maximum height: eight feet.

b. *MDD-3, MDD-4 and MDD-5.* One building identification sign per frontage. The first sign shall not exceed 50 square feet and each additional sign shall not exceed 36 square feet. Maximum height: eight feet.

Proposed Language to Amend the Zoning Ordinance:

Sec. 36-510. - Planned Unit Development District-17 (PUD-17) Pentagon Park South.

••••

(h) Signs shall be regulated per the Mixed Development District *plus one additional building identification sign facing the development interior. The additional sign is not to exceed 50 square feet. Maximum height: eight feet.*

Final Development Plan Site Improvements

Pentagon Village Edina, MN

Prepared for:

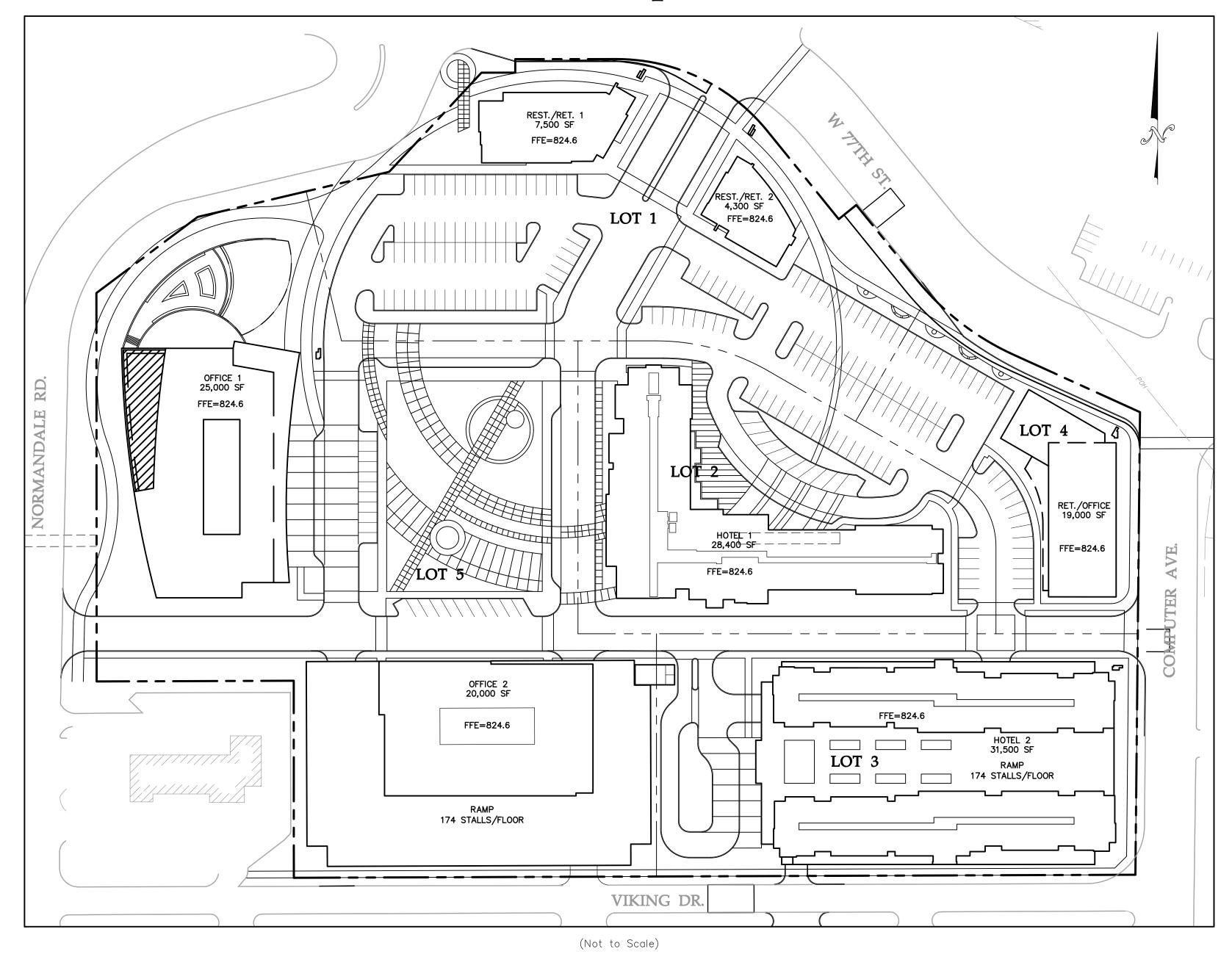
Solomon Real Estate Group 8560 Kelzer Pond Drive Victoria, MN 55386 Contact: Jay Scott Phone: (651) 336-6060 Email: JayScott@solomonre.com

Prepared by: Westwood

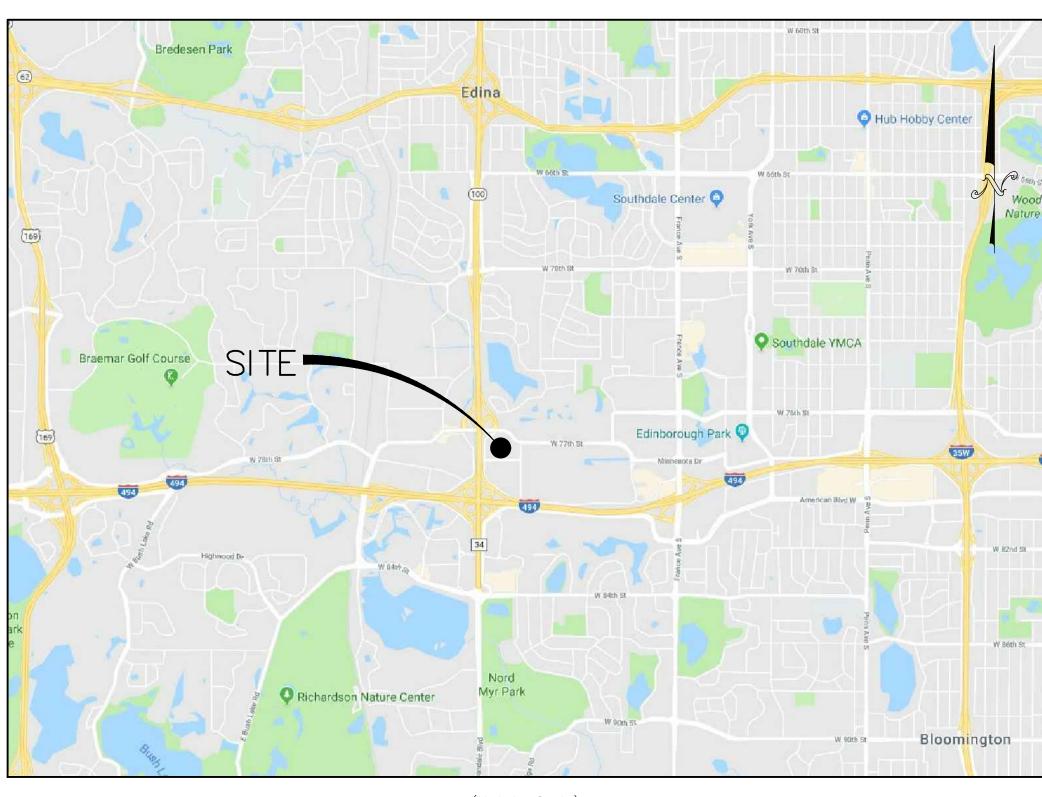
(952) 937-5150 12701 Whitewater Drive, Suite #300 (952) 937-5822 Minnetonka, MN 55343 (888) 937-5150 westwoodps.com Vestwood Professional Services, Inc.

Project number: 0013450.00 Contact: Daniel M. Parks

Overall Development Plan



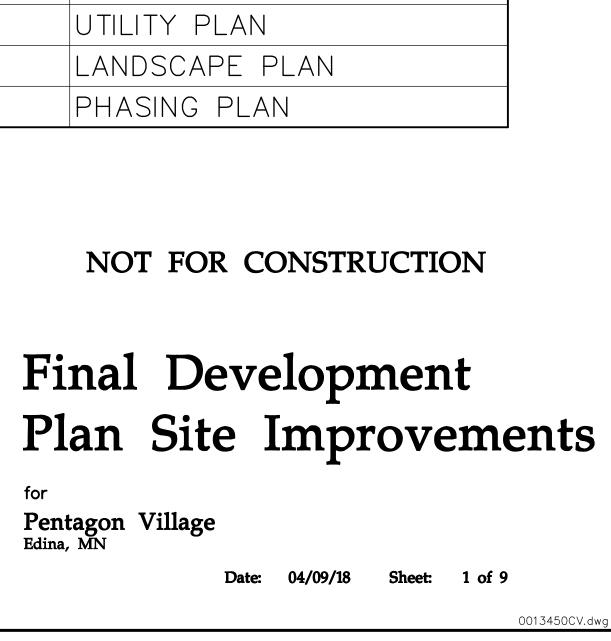
Vicinity Map



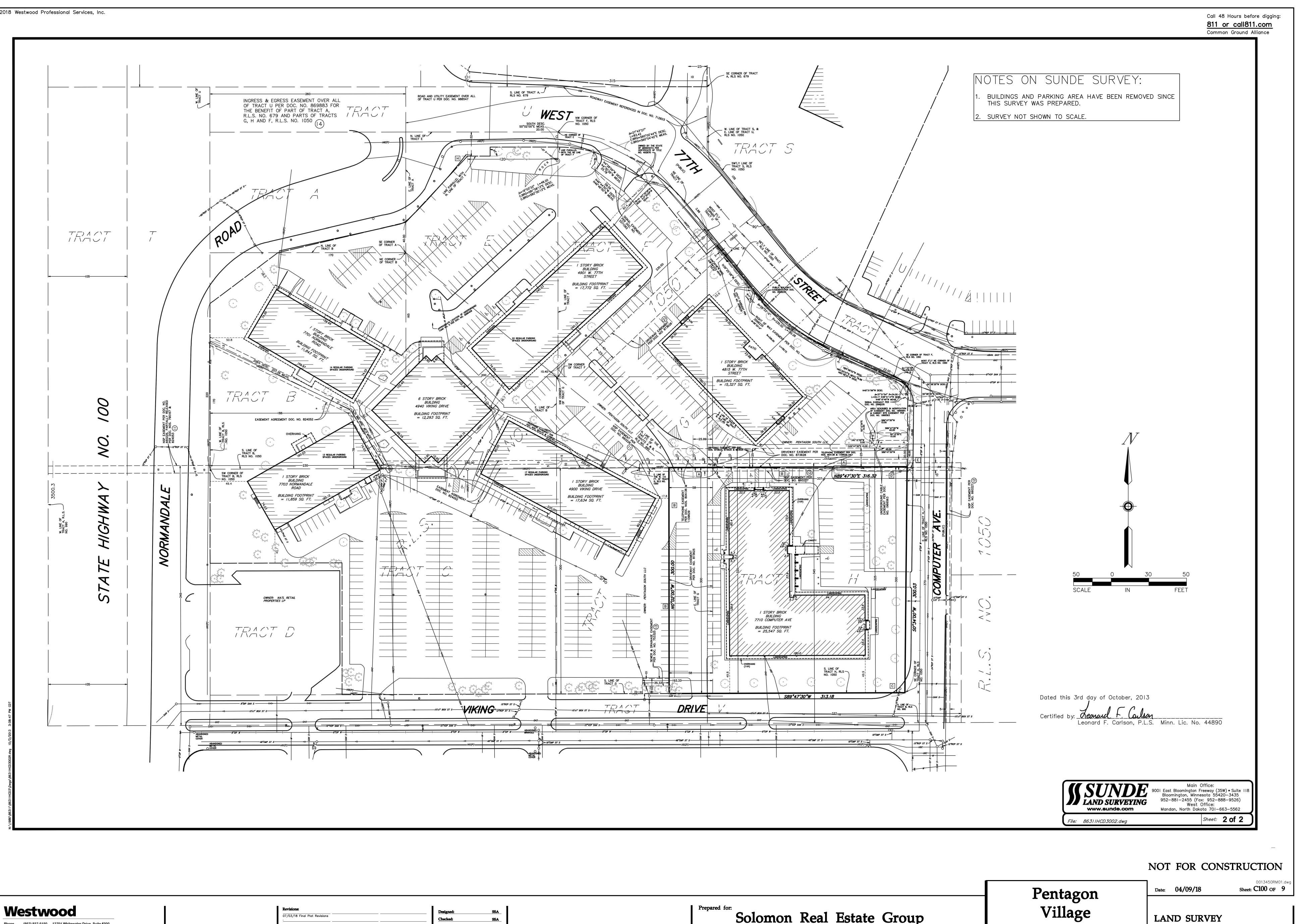
(Not to Scale)

Sheet List Table		
Sheet Number	Sheet Title	
C000	COVER SHEET	
C100	LAND SURVEY	
C200	FINAL PLAT	
C300	CIVIL SITE PLAN	
C400	GRADING & DRAINAGE PLAN	
C401	EROSION CONTROL PLAN	
C500	UTILITY PLAN	
C600	LANDSCAPE PLAN	
C700	PHASING PLAN	

for Pentagon Village Edina, MN







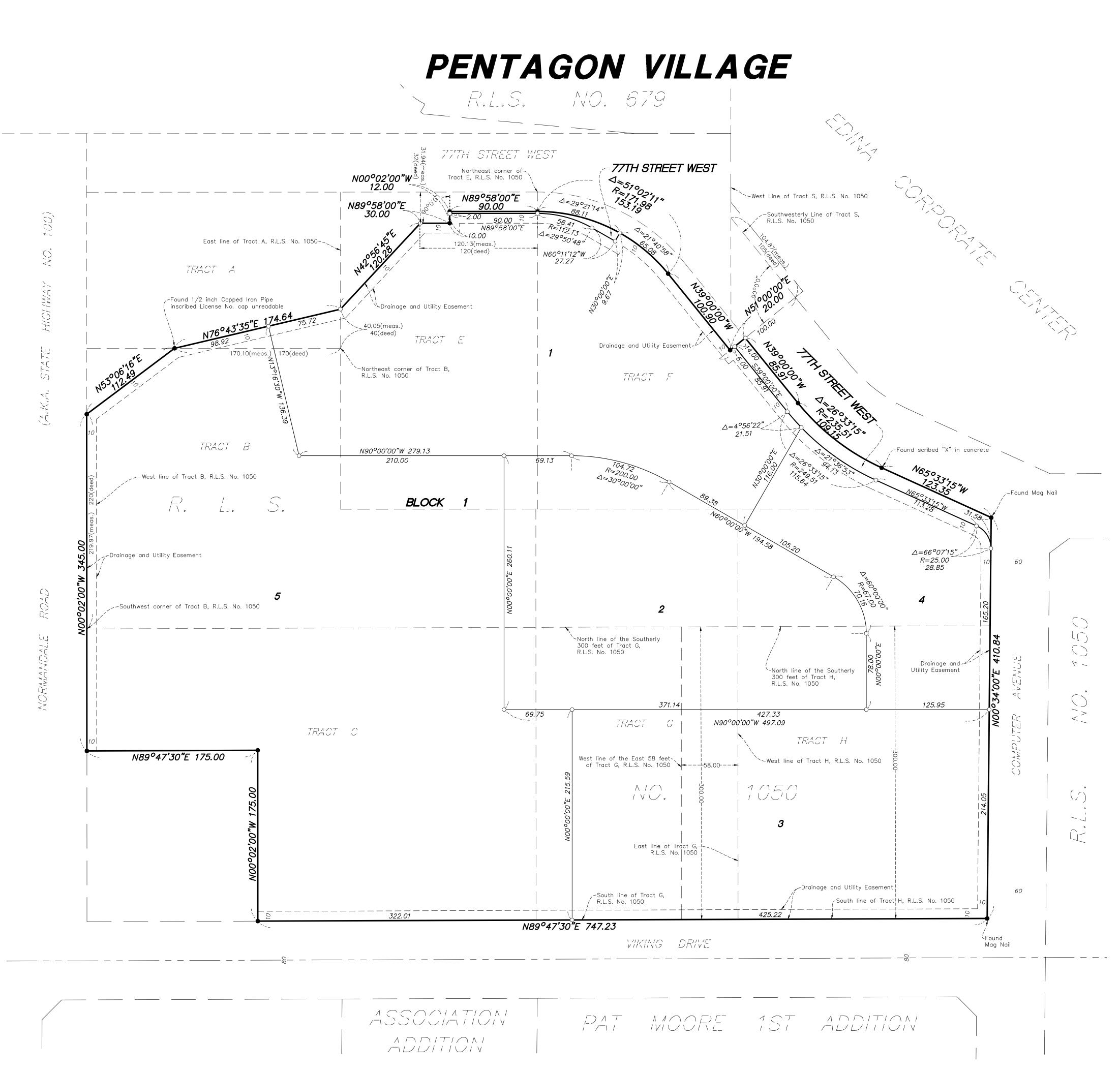
Phone (952) 937-5150 12701 Whitewater Drive, Suite #300 Fax (952) 937-5822 Minnetonka, MN 55343 Toll Free (888) 937-5150 westwoodps.com Westwood Professional Services, Inc

Designed:	SSA
Checked:	SSA
Drawn:	TDM
Record Drawing by	/date:



Edina, MN





Westwood

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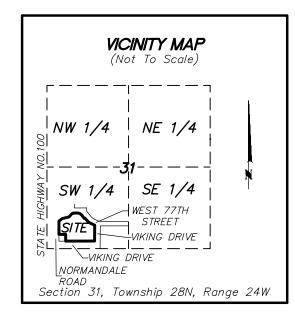
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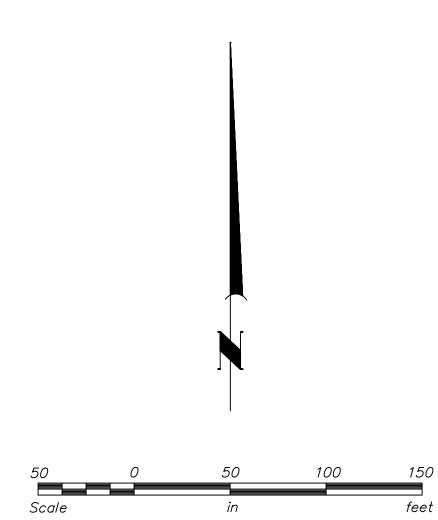
Revisions: 07/03/18 Final Plat Revisions

Vanale Junho Daniel M. Parks

Date: 07/03/18 18919 _ License No. _

-
 Designed: .
 Checked: .
 Drawn: .
 Record Drawing by/date:





The orientation of this bearing system is based on the South line of Tract H, R.L.S. No. 1050, which is assumed to bear N 89⁰47'30" E.

- Denotes set 1/2 inch x 14 inch iron rebar marked with plastic cap inscribed LS 23021, which has been set or will be set in accordance with MS 505.021, Subd. 10
- Denotes Found 1/2 inch Capped Iron Pipe marked with Plastic Cap inscribed with Lic. No. 17256, unless otherwise noted.

Date: 04/09/18

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Edina, MN

Pentagon Village

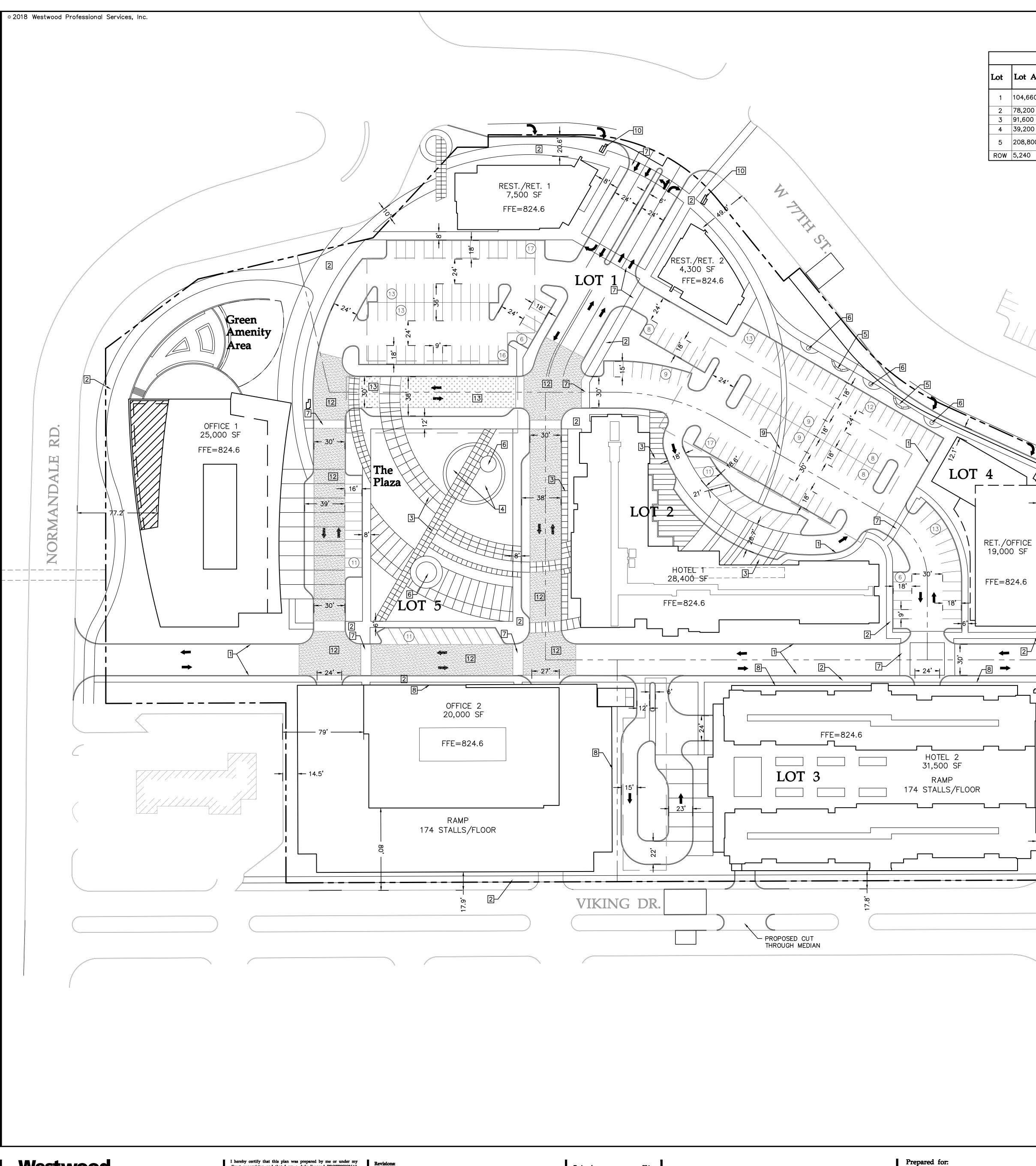
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NOT FOR CONSTRUCTION

0013450FPP02.dwg

Sheet: C200 OF 9

FINAL PLAT



Westwood Phone (952) 937-5150 12701 Whitewater Drive, Suite #300 (952) 937-5822 Minnetonka, MN 55343 Fax

Toll Free (888) 937-5150 westwoodps.com

Westwood Professional Services, Inc.

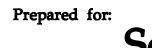
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07/03/18 Final Plat Revisions

Daniel M. Parks Date: 07/03/18 18919 License No. _

Danale Juchs

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 Designed:	SSA
 Checked:	SSA
 Drawn:	TDM
 Record Drawing by/date	



Site Development Summary							
Lot	Lot Area [SF]	Building Type	Building Area [SF]	Floors	Gross Building Area [SF]	Units	Parking Stalls
1	104,660	REST./RET. 1 REST./RET. 2	7,500 4,300	1	7,500 4,300		65 48
2	78,200	HOTEL 1	28,400	4	113,600	193	28
3	91,600	HOTEL/RAMP 2	31,500	3 RAMP, 4 HOTEL	157,500	153	522
4	39,200	RET./OFFICE	11,100	2	19,000		41
5	208,800	OFFICE 1 OFFICE/RAMP 2	25,000 20,000	5 4 RAMP, 5 OFFICE	125,000 100,000	 	22 696
ROW	5,240						

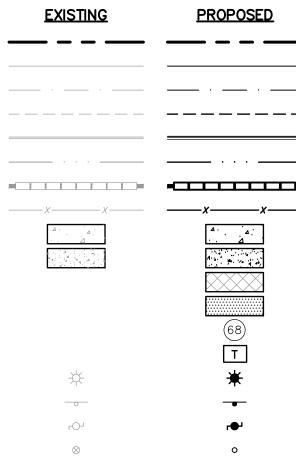
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-10

General Site Notes

- SITE CONDITIONS AND UTILITY LOCATIONS PRIOR TO ENGINEER SHOULD BE NOTIFIED IMMEDIATELY.
- UNLESS OTHERWISE NOTED.
- LOCATIONS OF EXITS, RAMPS, AND TRUCK DOCKS. NOTED.
- 7. ALL CURB AND GUTTER SHALL BE B612 UNLESS OTHERWISE NOTED.

Site Legend

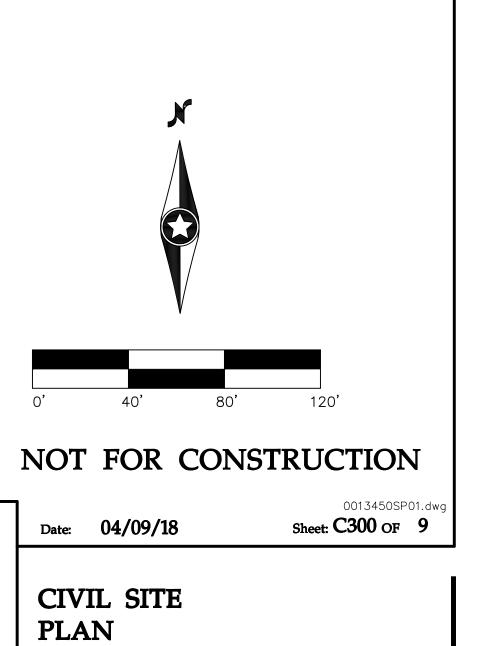


I Site Details

1	B612 CURB & GUTTER (TYP.)
2	PEDESTRIAN CONCRETE SIDEWALK
3	DECORATIVE CONCRETE
4	WATER FEATURE
5	BENCH
6	ARTWORK DISPLAY

- 7 DECORATIVE CROSSWALK 8 GREEN SCREEN FOR VINES 9 CONCRETE BAND 10 MONUMENT SIGN

- 11 ROOFTOP PLANTERS 12 PERVIOUS PAVERS 13 PAVERS



Solomon Real Estate Group 8560 Kelzer Pond Drive Victoria, MN 55386





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1. BACKGROUND INFORMATION FOR THIS PROJECT PROVIDED BY SUNDE SURVEY AND WESTWOOD FOR BOUNDARY SURVEY.

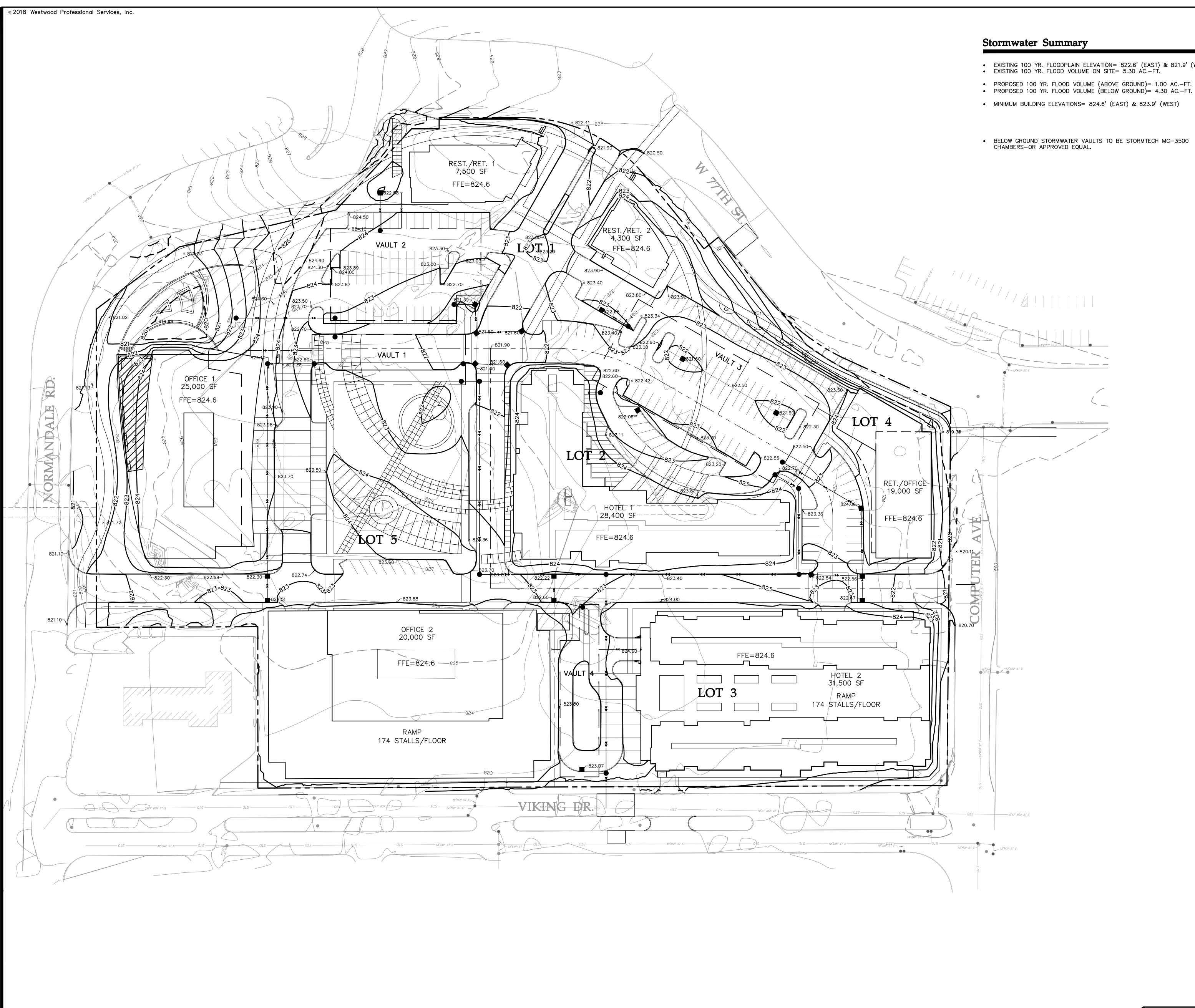
2. LOCATIONS AND ELEVATIONS OF EXISTING TOPOGRAPHY AND UTILITIES AS SHOWN ON THIS PLAN ARE APPROXIMATE. CONTRACTOR SHALL FIELD VERIFY EXCAVATION/CONSTRUCTION. IF ANY DISCREPANCIES ARE FOUND, THE

3. REFER TO BOUNDARY SURVEY FOR LOT BEARINGS, DIMENSIONS AND AREAS. 4. ALL DIMENSIONS ARE TO FACE OF CURB OR EXTERIOR FACE OF BUILDING

5. REFER TO ARCHITECTURAL PLANS FOR EXACT BUILDING DIMENSIONS AND

6. ALL CURB RADII SHALL BE 3.0 FEET (TO FACE OF CURB) UNLESS OTHERWISE

_	PROPERTY LINE
	LOT LINE
	SETBACK LINE
	EASEMENT LINE
	CURB AND GUTTER
	POND NORMAL WATER LEVEL
	RETAINING WALL
	FENCE
	CONCRETE PAVEMENT
	CONCRETE SIDEWALK
	HEAVY DUTY BITUMINOUS PAVEMENT
	STANDARD DUTY BITUMINOUS PAVEMENT
	NUMBER OF PARKING STALLS
	TRANSFORMER
	SITE LIGHTING
	TRAFFIC SIGN
	POWER POLE
	BOLLARD / POST





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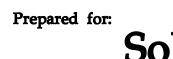
07/03/18 Final Plat Revisions

Date: 07/03/18 18919 License No. -

Vanue kuhs

Daniel M. Parks

 	Designed:	SSA
 	Checked:	SSA
 	Drawn:	TDM
 	Record Drawing by/date:	



- EXISTING 100 YR. FLOODPLAIN ELEVATION= 822.6' (EAST) & 821.9' (WEST) • EXISTING 100 YR. FLOOD VOLUME ON SITE= 5.30 AC.-FT.
- PROPOSED 100 YR. FLOOD VOLUME (ABOVE GROUND)= 1.00 AC.-FT.
- MINIMUM BUILDING ELEVATIONS= 824.6' (EAST) & 823.9' (WEST)
- BELOW GROUND STORMWATER VAULTS TO BE STORMTECH MC-3500

Grading Notes

- FOUND.
- MINNESOTA.
- ESTABLISHMENT.
- TO APPROPRIATE MNDOT STANDARDS.
- INDICATED ON THIS SHEET.
- GRADES.
- SURFACE GRADE.
- ACCEPTABLE TO THE OWNER AND THE REGULATING AGENCIES.
- CERTIFIED BY A LICENSED PROFESSIONAL ENGINEER.
- PERMIT REQUIREMENTS.
- GEOTECHNICAL ENGINEER.
- GEOTECHNICAL ENGINEER.

Grading Legend

EXISTING	PROPOSED
980	980 982
SF STO D WA T	SF
SAN	
× ^{91.00}	×91.00 1.50%
€ SB-19	E.O.F. 85.00 BB-19
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Pentagon Village

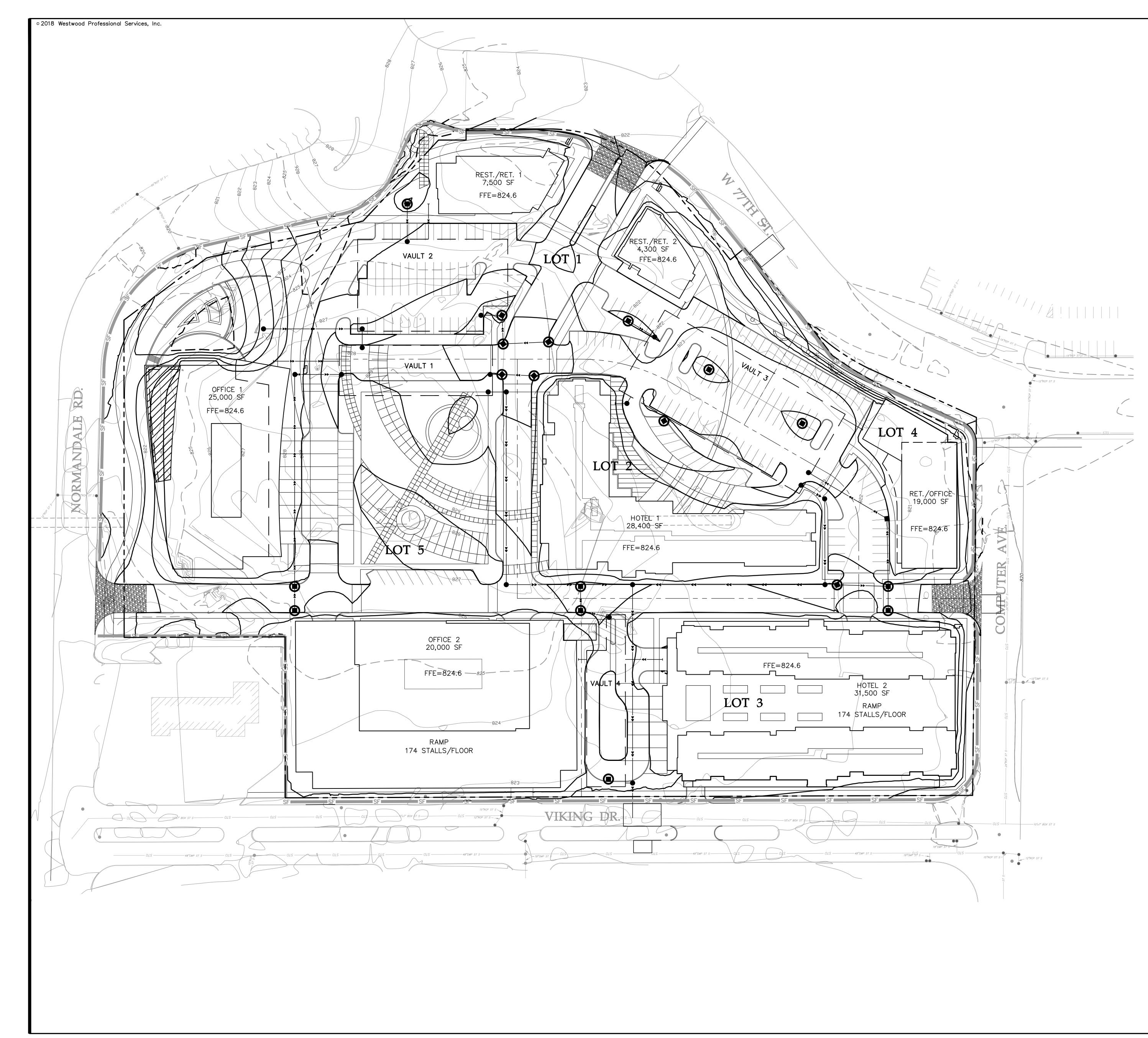
Edina, MN



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811 or call811.com Common Ground Alliance 1. LOCATIONS AND ELEVATIONS OF EXISTING TOPOGRAPHY AND UTILITIES AS SHOWN ON THIS PLAN ARE APPROXIMATE. CONTRACTOR SHALL FIELD VERIFY SITE CONDITIONS AND UTILITY LOCATIONS PRIOR TO EXCAVATION/CONSTRUCTION. THE ENGINEER SHALL BE NOTIFIED IMMEDIATELY IF ANY DISCREPANCIES ARE 2. CONTRACTORS SHALL REFER TO ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF VESTIBULE, SLOPED PAVEMENT, EXIT PORCHES, RAMPS, TRUCK DOCKS, PRECISE BUILDING DIMENSIONS, EXACT BUILDING UTILITY ENTRANCE LOCATIONS, AND EXACT LOCATIONS AND NUMBER OF DOWNSPOUTS. 3. ALL EXCAVATION SHALL BE IN ACCORDANCE WITH THE CURRENT EDITION OF "STANDARD SPECIFICATIONS FOR TRENCH EXCAVATION AND BACKFILL/SURFACE RESTORATION" AS PREPARED BY THE CITY ENGINEERS ASSOCIATION OF 4. ALL DISTURBED UNPAVED AREAS ARE TO RECEIVE FOUR INCHES OF TOPSOIL AND SOD OR SEED. THESE AREAS SHALL BE WATERED UNTIL A HEALTHY STAND OF GRASS IS OBTAINED. SEE LANDSCAPE PLAN FOR PLANTING AND TURF 5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING AND MAINTAINING TRAFFIC CONTROL DEVICES SUCH AS BARRICADES, WARNING SIGNS, DIRECTIONAL SIGNS, FLAGMEN AND LIGHTS TO CONTROL THE MOVEMENT OF TRAFFIC WHERE NECESSARY. PLACEMENT OF THESE DEVICES SHALL BE APPROVED BY THE ENGINEER PRIOR TO PLACEMENT. TRAFFIC CONTROL DEVICES SHALL CONFORM 6. ALL SLOPES SHALL BE GRADED TO 3:1 OR FLATTER, UNLESS OTHERWISE 7. CONTRACTOR SHALL UNIFORMLY GRADE AREAS WITHIN LIMITS OF GRADING AND PROVIDE A SMOOTH FINISHED SURFACE WITH UNIFORM SLOPES BETWEEN POINTS WHERE ELEVATIONS ARE SHOWN OR BETWEEN SUCH POINTS AND EXISTING 8. SPOT ELEVATIONS SHOWN INDICATE FINISHED PAVEMENT ELEVATIONS & GUTTER FLOW LINE UNLESS OTHERWISE NOTED. PROPOSED CONTOURS ARE TO FINISHED 9. SEE SOILS REPORT FOR PAVEMENT THICKNESSES AND HOLD DOWNS. 10. CONTRACTOR SHALL DISPOSE OF ANY EXCESS SOIL MATERIAL THAT EXISTS AFTER THE SITE GRADING AND UTILITY CONSTRUCTION IS COMPLETED. THE CONTRACTOR SHALL DISPOSE OF ALL EXCESS SOIL MATERIAL IN A MANNER 11. CONTRACTOR SHALL PROVIDED A STRUCTURAL RETAINING WALL DESIGN 12. ALL CONSTRUCTION SHALL CONFORM TO LOCAL, STATE AND FEDERAL RULES INCLUDING THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) 13. PRIOR TO PLACEMENT OF ANY STRUCTURE OR PAVEMENT, A PROOF ROLL, AT MINIMUM, WILL BE REQUIRED ON THE SUBGRADE. PROOF ROLLING SHALL BE ACCOMPLISHED BY MAKING MINIMUM OF 2 COMPLETE PASSES WITH FULLY-LOADED TANDEM-AXLE DUMP TRUCK, OR APPROVED EQUAL, IN EACH OF 2 PERPENDICULAR DIRECTIONS WHILE UNDER SUPERVISION AND DIRECTION OF THE INDEPENDENT TESTING LABORATORY. AREAS OF FAILURE SHALL BE EXCAVATED AND RECOMPACTED AS SPECIFIED HEREIN. 14. EMBANKMENT MATERIAL PLACED BENEATH BUILDINGS AND STREET OR PARKING AREAS SHALL BE COMPACTED IN ACCORDANCE WITH THE SPECIFIED DENSITY METHOD AS OUTLINED IN MNDOT 2105.3F1 AND THE REQUIREMENTS OF THE 15. EMBANKMENT MATERIAL NOT PLACED IN THE BUILDING PAD, STREETS OR PARKING AREA, SHALL BE COMPACTED IN ACCORDANCE WITH REQUIREMENTS OF THE ORDINARY COMPACTION METHOD AS OUTLINED IN MNDOT 2105.3F2. 16. ALL SOILS AND MATERIALS TESTING SHALL BE COMPLETED BY AN INDEPENDENT GEOTECHNICAL ENGINEER. EXCAVATION FOR THE PURPOSE OF REMOVING UNSTABLE OR UNSUITABLE SOILS SHALL BE COMPLETED AS REQUIRED BY THE GEOTECHNICAL ENGINEER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL REQUIRED SOILS TESTS AND INSPECTIONS WITH THE - PROPERTY LINE INDEX CONTOUR _ INTERVAL CONTOUR CURB AND GUTTER ____ ---- POND NORMAL WATER LEVEL SILT FENCE BIOLOGS STORM SEWER FLARED END SECTION (WITH RIPRAP) _____

WATER MAIN SANITARY SEWER _____ RETAINING WALL DRAIN TILE _____ RIDGE LINE _____ GRADING LIMITS ROCK CONSTRUCTION ENTRANCE EROSION CONTROL BLANKET TURF REINFORCEMENT MAT SPOT ELEVATION FLOW DIRECTION TOP AND BOTTOM OF RETAINING WALL EMERGENCY OVERFLOW SOIL BORING LOCATION INLET PROTECTION TEMPORARY ROCK CHECK DAM 80' NOT FOR CONSTRUCTION 0013450GD01.dwg Sheet: C400 OF 9 Date: 04/09/18 GRADING & DRAINAGE PLAN





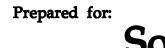
I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly licensed PROFESSIONAL ENGINEER under the laws of the State of Minnesota.

Revisions: 07/03/18 Final Plat Revisions

Daniel M. Parks Date: 07/03/18 18919 License No. _

Danale Junho

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 Designed:	SSA
 Checked:	SSA
 Drawn:	TDM
 Record Drawing by/date:	

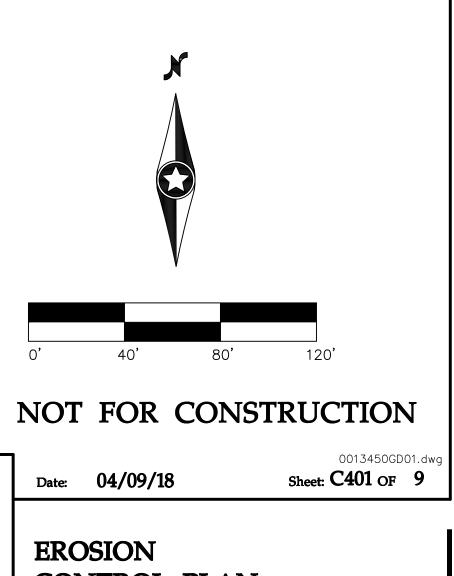


Erosion Control Notes

- EXCAVATION/CONSTRUCTION ACTIVITIES.
- 2. ROCK CONSTRUCTION ENTRANCE WILL BE INSTALLED AT ALL CONSTRUCTION ENTRANCES.
- 3. SILTATION AND EROSION CONTROL: THE CONTRACTOR SHALL ASSUME COMPLETE AFTER THE TURF IS ESTABLISHED.
- CONSTRUCTION.
- SEVEN (7) DAYS.
- 6. FOR AREAS WITH SLOPE OF 3:1 OR GREATER, RESTORATION WITH SOD OR WOOD FIBER BLANKET IS REQUIRED.
- 7. PUBLIC STREETS USED FOR HAULING SHALL BE KEPT FREE OF SOIL AND DEBRIS. STREET SWEEPING SHALL BE COMPLETED DAILY.

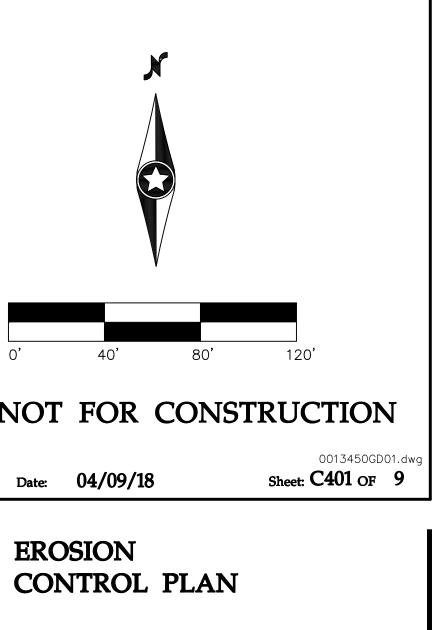
Grading Legend

<u>EXISTING</u>	PROPOSED
980	9 ₈₀
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SF	SF
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Pentagon Village Edina, MN



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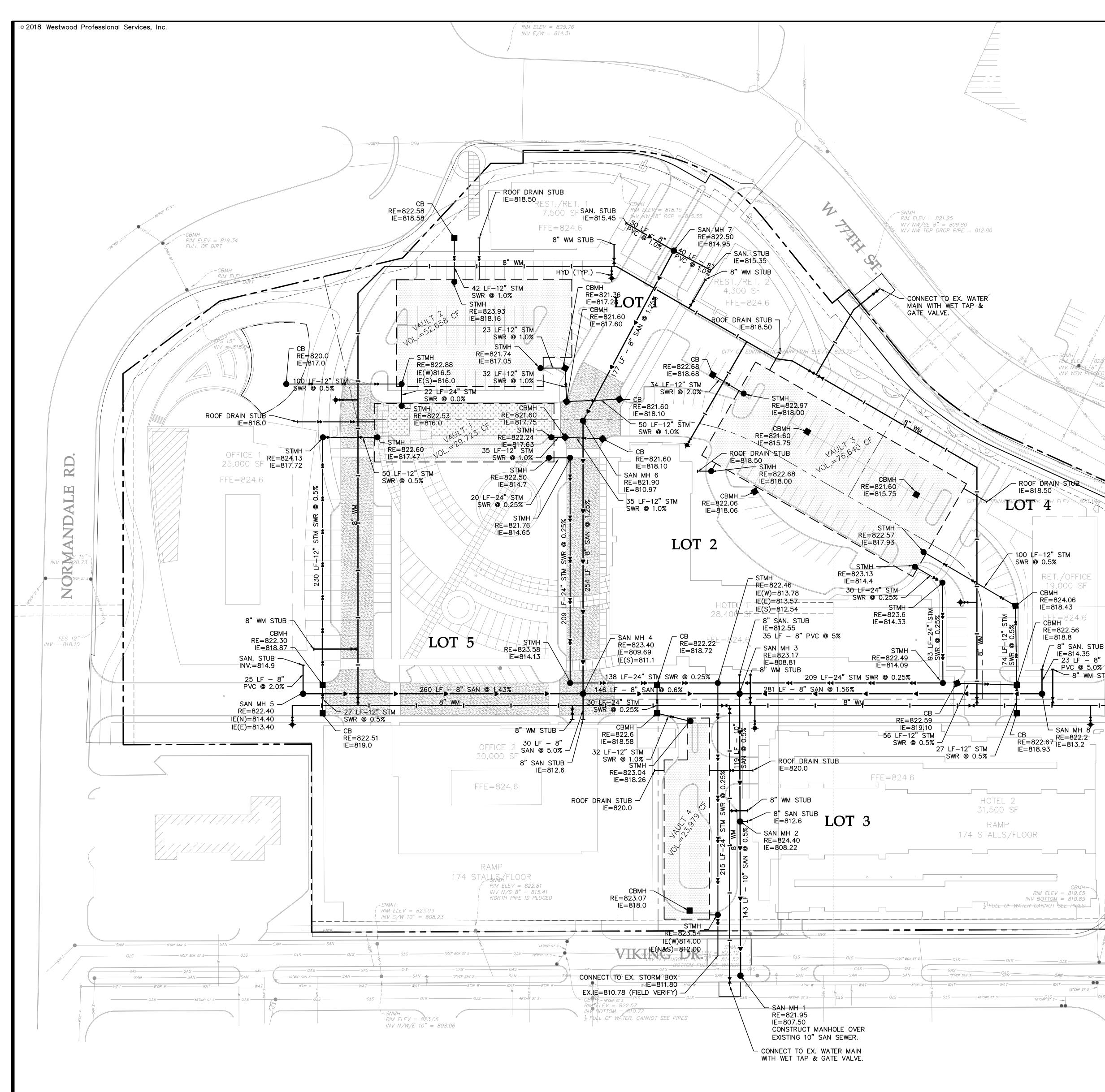
1. SILT FENCE WILL BE INSTALLED AROUND SITE IN ALL FILL AREAS AND LOCATIONS WHERE STORM WATER RUNOFF MAY LEAVE THE SITE, PRIOR TO ANY

RESPONSIBILITY FOR CONTROLLING ALL SILTATION AND EROSION OF THE PROJECT AREA. THE CONTRACTOR SHALL USE WHATEVER MEANS NECESSARY TO CONTROL THE EROSION AND SILTATION INCLUDING BUT NOT LIMITED CATCH BASIN INSERTS, ROCK CONSTRUCTION ENTRANCES, EROSION CONTROL BLANKET, AND SILT FENCE. CONTROL SHALL COMMENCE WITH GRADING AND CONTINUE THROUGHOUT THE PROJECT UNTIL ACCEPTANCE OF THE WORK BY THE OWNER. THE CONTRACTOR'S RESPONSIBILITY INCLUDES ALL IMPLEMENTATION AS REQUIRED TO PREVENT EROSION AND THE DEPOSITING OF SILT. THE OWNER MAY DIRECT THE CONTRACTOR'S METHODS AS DEEMED FIT TO PROTECT PROPERTY AND IMPROVEMENTS. ANY DEPOSITION OF SILT OR MUD ON NEW OR EXISTING PAVEMENT OR IN EXISTING STORM SEWERS OR SWALES SHALL BE REMOVED AFTER EACH RAIN AFFECTED AREAS SHALL BE CLEANED TO THE SATISFACTION OF THE OWNER, ALL AT THE EXPENSE OF THE CONTRACTOR. ALL TEMPORARY EROSION CONTROL SHALL BE REMOVED BY THE CONTRACTOR

4. CONTRACTOR SHALL INSTALL TEMPORARY INLET PROTECTION (WIMCO OR EQUIVALENT) AROUND ALL CATCH BASIN GRATE INLETS, AFFECTED BY THIS

5. ALL DISTURBED AREAS SHALL HAVE TEMPORARY PROTECTION OR PERMANENT COVER OVER EXPOSED SOIL AREAS IF NOT BEING ACTIVELY GRADED WITHIN

_	PROPERTY LINE
-	INDEX CONTOUR
-	INTERVAL CONTOUR
_	CURB AND GUTTER
	POND NORMAL WATER LEVEL
	SILT FENCE
	BIOLOGS
-8	STORM SEWER
	FLARED END SECTION (WITH RIPRAP)
	WATER MAIN
	SANITARY SEWER
]=	RETAINING WALL
	DRAIN TILE
	RIDGE LINE
	GRADING LIMITS
	ROCK CONSTRUCTION ENTRANCE
	EROSION CONTROL BLANKET
	TURF REINFORCEMENT MAT
	SPOT ELEVATION
	FLOW DIRECTION
	TOP AND BOTTOM OF RETAINING WALL
	EMERGENCY OVERFLOW
	SOIL BORING LOCATION
	INLET PROTECTION
\mathbf{c}	TEMPORARY ROCK CHECK DAM



Westwood (952) 937-5150 12701 Whitewater Drive, Suite #300 (952) 937-5822 Minnetonka, MN 55343 Toll Free (888) 937-5150 westwoodps.com

Phone

stwood Professional Services, Ind

Fax

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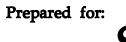
07/03/18 Final Plat Revisions

Date: 07/03/18 18919

Vanala Hucho

Daniel M. Parks

 Designed:	SSA
 Checked:	SSA
 Drawn:	TDM
 Record Drawing by/date:	



General Utility Notes

- DISCREPANCIES.
- PREPARED BY THE CITY ENGINEERS ASSOCIATION OF MINNESOTA.
- ARRANGED WITH THE OWNER.
- 5. ALL PRIVATE UTILITIES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE COMPANIES.
- 6. CONTRACTOR SHALL OBTAIN ALL NECESSARY CITY PERMITS FOR UTILITY RESPONSIBILITY OF THE CONTRACTOR.
- SHALL BE INCLUDED IN WATER MAIN CONSTRUCTION.
- 8. PROVIDE WATER MAIN THRUST RESTRAINTS PER CITY STANDARD REQUIREMENTS.
- MAIN CROSSINGS WITH SANITARY SEWER OR STORM SEWER.
- 10. UTILITY SERVICES TYPICALLY TERMINATE 5' OUTSIDE BUILDING WALL UNLESS OTHERWISE SHOWN OR NOTED.
- SANITARY SEWER & STORM SEWER. INSULATE WATER MAIN IF LESS THAN 7.5' OF COVER.
- INCHES OF THICKNESS.
- FOOTINGS, PIPE SHALL BE PVC SCHEDULE 40.
- END SECTIONS SHALL BE RCP WITH TRASH GUARDS & RIP-RAP.
- WITH POST-CONSTRUCTION ELEVATIONS IS REQUIRED.

Utility Legend

EXISTING	PROPOSED
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	- <u> </u>
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TUG	
——— ТОН ———	тон —
FOP	
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*	*
	<u>(X.X</u>



IE=814.35 <u>− 23 LF − 8</u>" <u>− 23 LF − 8</u>" 🛏 8" WM STUPB | CONNECT TO EX. WATER MAIN WITH WET TAP & GATE VALVE. └─ SAN MH , RE=822.2 15"CMP ST S $RIM \ ELEV = 819.65$ 18"CMP ST S - ' 15"RCP ST 1-SNMH RIN ELEV = 820.50 $\frac{1}{8} \frac{1}{10 \text{ W} \text{ SE/W}} 10^{"} = 806.10$

809 40

)/PIPF =/ 81.5.40

ABAN. GAS

1 E EV = 819.42

5 5 5 5

/ NGW/F/SS 8" = 808.3

Solomon Real Estate Group 8560 Kelzer Pond Drive Victoria, MN 55386

Village Edina, MN

Pentagon

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1. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS ARE BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND LIMITED MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION SHALL NOT BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR SHALL VERIFY EXISTING CONDITIONS PRIOR TO CONSTRUCTION AND NOTIFY THE OWNER OR ENGINEER OF

2. ALL SANITARY SEWER, STORM SEWER AND WATER MAIN INSTALLATIONS SHALL BE PER MINNESOTA PLUMBING CODE AND IN ACCORDANCE WITH THE CURRENT EDITION OF "STANDARD SPECIFICATIONS FOR WATER MAIN AND SERVICE LINE INSTALLATION AND SANITARY SEWER AND STORM SEWER INSTALLATION" AS

3. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL OBTAIN THE NECESSARY FEDERAL, STATE AND LOCAL PERMITS FOR THE PROPOSED WORK OR VERIFY WITH THE OWNER OR ENGINEER THAT PERMITS HAVE BEEN OBTAINED. PERMIT FEES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR UNLESS OTHERWISE

4. CONTRACTOR SHALL REFER TO ARCHITECTURAL PLANS FOR EXACT LOCATION AND DIMENSIONS OF DOORWAYS, RAMPS, TRUCK DOCKS, PRECISE BUILDING DIMENSIONS AND EXACT BUILDING UTILITY CONNECTION LOCATIONS.

SPECIFICATIONS OF THE APPROPRIATE UTILITY COMPANY. THE CONTRACTOR SHALL COORDINATE THE SERVICE LINE CONSTRUCTION WITH THE UTILITY

CONNECTIONS, AND UTILITIES SHALL BE INSPECTED AND APPROVED BY THE CITY. THE CITY SHALL BE NOTIFIED 48-HOURS PRIOR TO COMMENCING WITH THE UTILITY CONSTRUCTION OR ANY REQUIRED TESTING. CONTRACTOR SHALL NOT OPERATE, INTERFERE WITH, CONNECT ANY PIPE OR HOSE TO, OR TAP ANY WATER MAIN BELONGING TO THE CITY UNLESS DULY AUTHORIZED TO DO SO BY THE CITY. ANY ADVERSE CONSEQUENCES OF SCHEDULED OR UNSCHEDULED DISRUPTIONS OF SERVICE TO THE PUBLIC ARE TO BE THE

7. WATER MAIN LENGTHS AS SHOWN ARE APPROXIMATE HORIZONTAL LENGTHS. ALLOW FOR ADDITIONAL PIPE WHEN INSTALLING ON SLOPES OR WHEN DEFLECTIONS ARE REQUIRED. THE JOINT DEFLECTIONS SHALL NOT EXCEED THE MAXIMUM RECOMMENDED BY THE PIPE MANUFACTURER OR BY LOCAL GOVERNING SPECIFICATIONS. FITTINGS REQUIRED TO CONSTRUCT WATER MAIN

9. A MINIMUM VERTICAL SEPARATION OF 18 INCHES IS REQUIRED AT ALL WATER

11. ALL MATERIALS SHALL COMPLY WITH THE REQUIREMENTS OF THE CITY. 12. ALL WATER LINES SHALL BE DUCTILE IRON WRAPPED IN POLYETHYLENE. CLASS 52 WITH 7.5' MINIMUM COVER. PROVIDE MINIMUM SEPARATION OF 18" FROM

13. INSULATION SHALL BE DOW STYROFOAM HI BRAND 35 OR EQUIVALENT, WITH 4

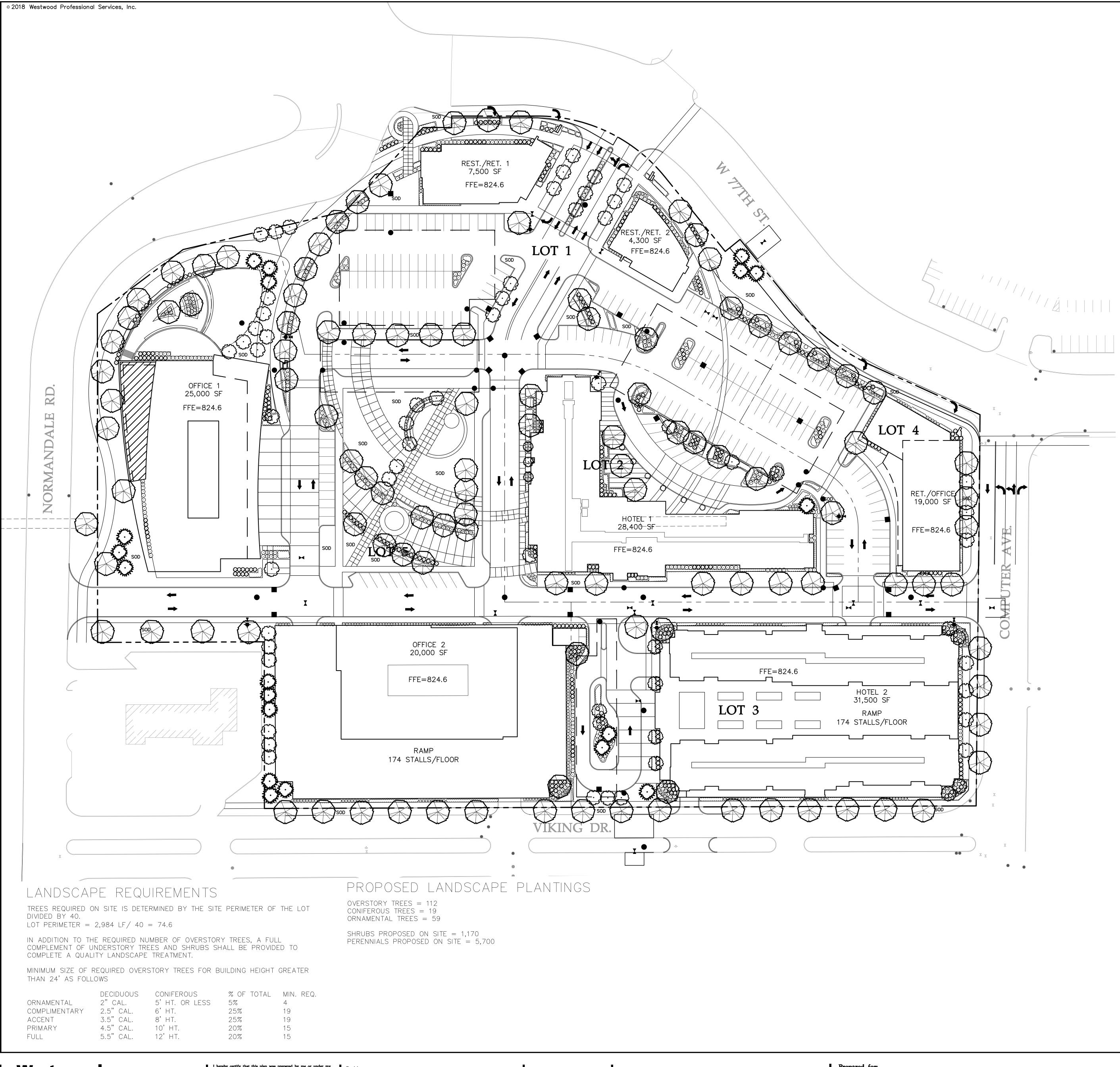
14. SANITARY SEWER PIPE OUTSIDE THE BUILDING ENVELOPE SHALL BE POLYVINYL CHLORIDE (PVC) SDR 35 OR 26. SDR 26 IS REQUIRED FOR DEPTHS GREATER THAN 15 FEET. SANITARY SEWER PIPE WITHIN 5 FEET OF BUILDING AND UNDER

15. STORM SEWER PIPE SHALL BE REINFORCED CONCRETE PIPE (CLASS 5 FOR PIPE DIAMETERS 18" AND SMALLER, CLASS 3 FOR PIPE DIAMETERS 21" AND LARGER UNLESS OTHERWISE NOTED) WITH R-4 GASKETS, OR HDPE STORM SEWER PIPE IF ALLOWED BY THE CITY. HDPE STORM PIPE SHALL MEET REQUIREMENTS OF ASTM F2648. PIPE SHALL BE WATER TIGHT ACCORDING TO ASTM D3212 REQUIREMENTS. SEE PLAN FOR LOCATIONS WHERE RCP IS REQUIRED. PVC STORM SEWER PIPE SHALL BE SCHEDULE 40 PIPE. FLARED

16. AFTER CONSTRUCTION IS COMPLETED, THE CONTRACTOR SHALL PROVIDE THE OWNER WITH AN AS-BUILT RECORD OF UTILITY CONSTRUCTION. THE AS-BUILT SHALL INCLUDE LOCATION AND LENGTH DEVIATIONS OR CHANGES TO THE PLAN. CONTRACTOR TO VERIFY WITH OWNER OR ENGINEER WHETHER A PLAN

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UTILITY PLAN



Westwood Phone (952) 937-5150 12701 Whitewater Drive, Suite #300 Fax (952) 937-5822 Minnetonka, MN 55343 Toll Free (888) 937-5150 westwoodps.com

Westwood Professional Services, Inc.

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Revisions:

07/03/18 Final Plat Revisions

Date: 07/03/18 44018 License No. ____

 	Designed:	JRW
 	Checked:	JRW_
 	Drawn:	TDM
 	Record Drawing by/date:	

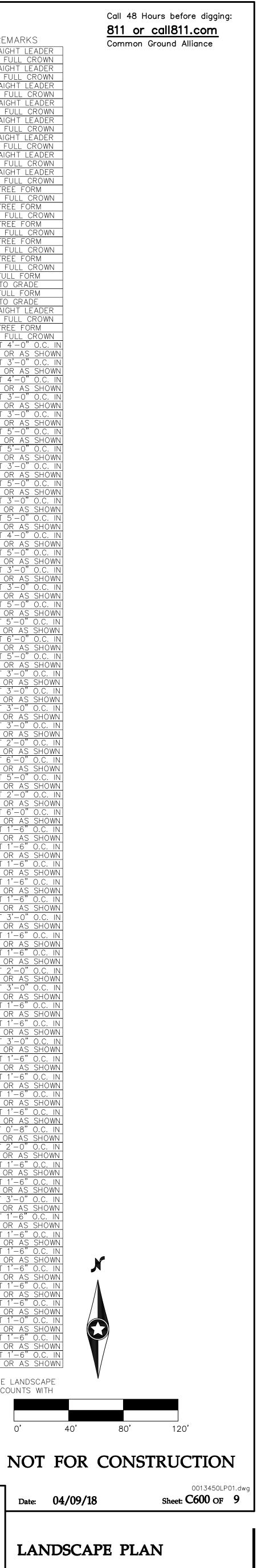
ABP	AUTUMN BLAZE MAPLE Acer x freemanni 'Jeffersred'	4.5" CAL. 2.5"	B&B	STRAIGHT L
AB BO	AUTUMN BLAZE MAPLE Acer x freemanni 'Jeffersred' BUR OAK	2.5" CAL. 2.5" CAL. 5.5"	B&B B&B	STRAIGHT L AND FULL STRAIGHT L
FSF	Quercus macrocarpa SIENNA GLEN MAPLE Acer x freemanii 'Sienna' (PP11,322)	CAL. 5.5" CAL. 4.5"	B&B	AND FULL STRAIGHT L AND FULL
FSP FS	SIENNA GLEN MAPLE Acer x freemanii 'Sienna' (PP11,322) SIENNA GLEN MAPLE	<u>CAL.</u> 2.5"	B&B B&B	STRAIGHT L AND FULL STRAIGHT L
IH	Acer x freemanii 'Sienna' (PP11,322) IMPERIAL HONEYLOCUST Gleditsia triacanthos var. inermis 'Impcole'	CAL. 2.5" CAL. 3.5"	B&B	AND FULL STRAIGHT L AND FULL
RO RBT	RED OAK Quercus rubra RIVER BIRCH	<u>CAL.</u> 2.0"	B&B B&B	STRAIGHT L AND FULL TREE FO
	Betula nigra RIVER BIRCH CLUMP Betula nigra	CAL. 8' HT.	B&B	AND FULL TREE FO AND FULL
S SS ABC	SPRING SNOW CRABAPPLE Malus 'Spring Snow' AUTUMN BRILLIANCE CLUMP SERVICEBERRY	1.5" CAL. 8'	B&B B&B	TREE FO AND FULL TREE FO
MP	Amelanchier X grandiflora 'Autumn Brilliance' PRAIRIFIRE CRABAPPLE Malus 'Prairifire'	HT. 1.5" CAL.	B&B	AND FULL TREE FO AND FULL
BH NSA	BLACKHILLS SPRUCE Picea glauca densata NORWAY SPRUCE	6' <u>HT.</u> 8'	B&B B&B	FULL FO TO GRA FULL FO
GSL	Picea abies GREENSPIRE LINDEN Tilia cordata ' Greenspire'	HT. 3.5" CAL.	B&B	TO GRA STRAIGHT L AND FULL
JT	JAPANESE TREE LILAC Syringa recticulata ANTHONY WATERER SPIREA	1.5" CAL. #5	B&B	TREE FO AND FULL PLANT 4'-0'
DB	Spiraea x bumalda 'Anthony Waterer' DWARF BUSH HONEYSUCKLE Diervilla Ionicera	#5		MASS OR AS Plant 3'-0' Mass or as
RG AH	RED GNOME DOGWOOD Cornus alba sibirica 'Red Gnome' ANNABELLE HYDRANGEA	#5 #5	CONT.	PLANT 4'-0' MASS OR AS PLANT 3'-0'
SF	Hydrangea arborescens 'Annabelle' SEM FALSESPIREA	#5 #5	CONT.	MASS OR AS Plant 3'-0'
GL	Sorbaria sorbifolia 'Sem' (PP16,336) GRO-LOW FRAGRANT SUMAC Rhus aromatica 'Gro-Low'	#5	CONT.	MASS OR AS PLANT 5'-0' MASS OR AS PLANT 5'-0'
SG PM	MINT JULEP JUNIPER / SEA GREEN Juniperus chinensis 'Monlep' PINE, MUGO SLOWMOUND	#5 #5	CONT.	MASS OR AS Plant 3'-0'
HJ	Pinus mugo 'Slowmound' HUGES JUNIPER Juniperus horizontalis 'Huges'	#5	CONT.	MASS OR AS PLANT 5'-0' MASS OR AS PLANT 3'-0'
JB BC	KOBOLD BARBERRY Berberis thunbergii 'Kobold' BLACK CHOKEBERRY	#5 #5	CONT. CONT.	MASS OR AS Plant 5'-0'
TY	Aronia melanocarpa elata TAUNTON YEW Taxus x media 'Tauntonii'	#5		MASS OR AS Plant 4'-0' Mass or as
RT CW	RED TWIGGED DOGWOOD Cornus sericea 'Baileyi' CAREFREE WONDER ROSE (PINK)	#5 #5	CONT.	PLANT 5'-0' MASS OR AS PLANT 3'-0'
ES	Rosa 'Cartefree Wonder' ENDLESS SUMMER HYDRANGEA Hydrangea macrophylla 'Endless Summer'	#5	CONT.	MASS OR AS Plant 3'-0'
WC BB	WILTON CARPET JUNIPER Juniperus horizontalis 'Wiltonii' DWARF BURNING BUSH	#5 #5	CONT.	MASS OR AS PLANT 5'-0' MASS OR AS PLANT 5'-0"
MK	Euonymus alatus 'Compactus' MISS KIM LILAC Syringa patula 'Miss Kim'	#5	CONT.	MASS OR AS Plant 6'-0'
BJ	BUFFALO JUNIPER Juniperus sabina 'Buffalo' CRIMSON PYGMY BARBERRY	#5 #5	CONT.	MASS OR AS Plant 5'-0' Mass or as Plant 3'-0"
MB	Berberis thunbergii atropurpurea 'Crimson Pygi MARIE BUGNET ROSE (WHITE)	#3 my' #5	CONT.	MASS OR AS Plant 3'-0"
КО	Rosa 'Marie Bugnet' KNOCK OUT ROSE (RED) Rosa 'Radrazz' (PP11,836)	#5	CONT.	MASS OR AS Plant 3'-0" Mass or As Plant 3'-0"
GS JW	GOLDFLAME SPIREA Spiraea x bumalda 'Goldflame' JAPANESE WHITE SPIREA	#5 #5	CONT.	MASS OR AS Plant 2'-0"
CC	Spiraea albiflora CALGARY CARPET JUNIPER Juniperus sabina 'Monna'	#5		MASS OR AS PLANT 6'-0" MASS OR AS
SJ WB	SCANDIA JUNIPER Juniperus sabina 'Scandia' WINTERGREEN BOXWOOD	#5 #5		PLANT 5'-0' MASS OR AS PLANT 2'-0'
TG	Buxus microphylla 'Koreana' TECHNY ARBORVITAE Thuja occidentalis 'Techny'	#5		MASS OR AS PLANT 6'-0' MASS OR AS
SD BD	STELLA SUPREME DAYLILY Hemerocallis 'Stella Supreme' BAJA DAYLILLY	<u>#1</u> #1	CONT. CONT.	PLANT 1'-6" MASS OR AS PLANT 1'-6"
GC	Hemerocallis 'Baja' GOLDSTRUM CONEFLOWER Rudbeckia 'Goldsturm'	#1	CONT.	MASS OR AS Plant 1'-6" Mass or as
PC KF	PURPLE CONEFLOWER Echinacea purpurea KARL FOERSTER GRASS	#1 #1	CONT.	PLANT 1'-6" MASS OR AS PLANT 1'-6"
MF	Calamagrostis x acutiflora 'Karl Foerster' MISCANTHUS FLAME Miscanthus sinensis 'Purpurascens'	#1		MASS OR AS PLANT 3'-0' MASS OR AS
FA	FANAL ASTILBE Astilbe x arendsii 'Fanal'	#1		PLANT 1'-6" MASS OR AS PLANT 1'-6"
BE PB	BLACK-EYED SUSAN Rudbeckia hirta PRAIRIE BLAZING STAR	#1	CONT.	MASS OR AS PLANT 2'-0'
LB	Liatris pychnostachya LITTLE BLUESTEM Schizachyrium scoparius	#1		MASS OR AS PLANT 3'-O' MASS OR AS
KH WG	KING OF HEARTS DICENTRA Dicentra 'King of Hearts' WINTER GLOW BERGENIA	<u>#1</u> #1		PLANT 1'-6" MASS OR AS PLANT 1'-6"
JP	Bergenia cordifolia 'Winter Glow' JOE PYE WEED Eupatorium maculatum	#1	CONT.	MASS OR AS Plant 3'-0' Mass or as
MN - JH	MAY NIGHT SALVIA Salvia nemorosa 'Mainacht' JUNE HOSTA	#1 #1		PLANT 1'-6" MASS OR AS PLANT 1'-6"
AJ	Hosta 'June' AUTUMN JOY STONECROP	#1	CONT.	MASS OR AS PLANT 1'-6"
WS	Sedum "Autumn Joy" WHITE SWAN ECHINACEA Echinacea purpurea 'White Swan'	#1		MASS OR AS PLANT 1'-6" MASS OR AS
PD	BLUE FESCUE Festuca glauca 'Elijah Blue' PRAIRIE DROPSEED	#1 #1		PLANT 0'-8" MASS OR AS PLANT 2'-0"
KL	Sporobolus heterolepis KOBOLD LIATRIS Liatris spicata 'Kobold'	#1		MASS OR AS PLANT 1'-6" MASS OR AS
RS GSH	RUBY STAR CONEFLOWER Echinacea purpurea 'Rubinstern' GOLD STANDARD HOSTA	#1 #1		PLANT 1'-6" MASS OR AS PLANT 3'-0"
SO	Hosta 'Gold Standard' STELLA D' ORO DAYLILY Hemerocallis 'Stella D' Oro'	#1		MASS OR AS PLANT 1'-6" MASS OR AS
GA CF	GOLDEN ALEXANDER Zizia aurea CARDINAL FLOWER	#1 #1	CONT.	PLANT 1'-6" MASS OR AS PLANT 1'-6"
PS	Lobelia cardinalis PRAIRIE SMOKE Geum triflorium	#1#1		MASS OR AS PLANT 1'-6" MASS OR AS
BF	BUTTERFLY FLOWER Asclepias tuberosa	#1 #1	CONT.	MASS OR AS PLANT 1'-6" MASS OR AS PLANT 1'-6"
AF FP	AUTUMN FIRE SEDUM Sedum x 'Autumn Fire' FLAME PINK PHLOX	#1 #1		MASS OR AS PLANT 1'-0"
GM	Phlox paniculata Pink 'Bartwelve' (PP11,804) GRAPE MAGIC DAYLILY Hemerocallis 'Grape Magic'	#1	CONT.	MASS OR AS PLANT 1'-6" MASS OR AS PLANT 1'-6"
MR	FROSTED VIOLET HEUCHERA Heuchera 'Frosted Violet'	#1	CONT.	PLANT 1'-6' Mass or as

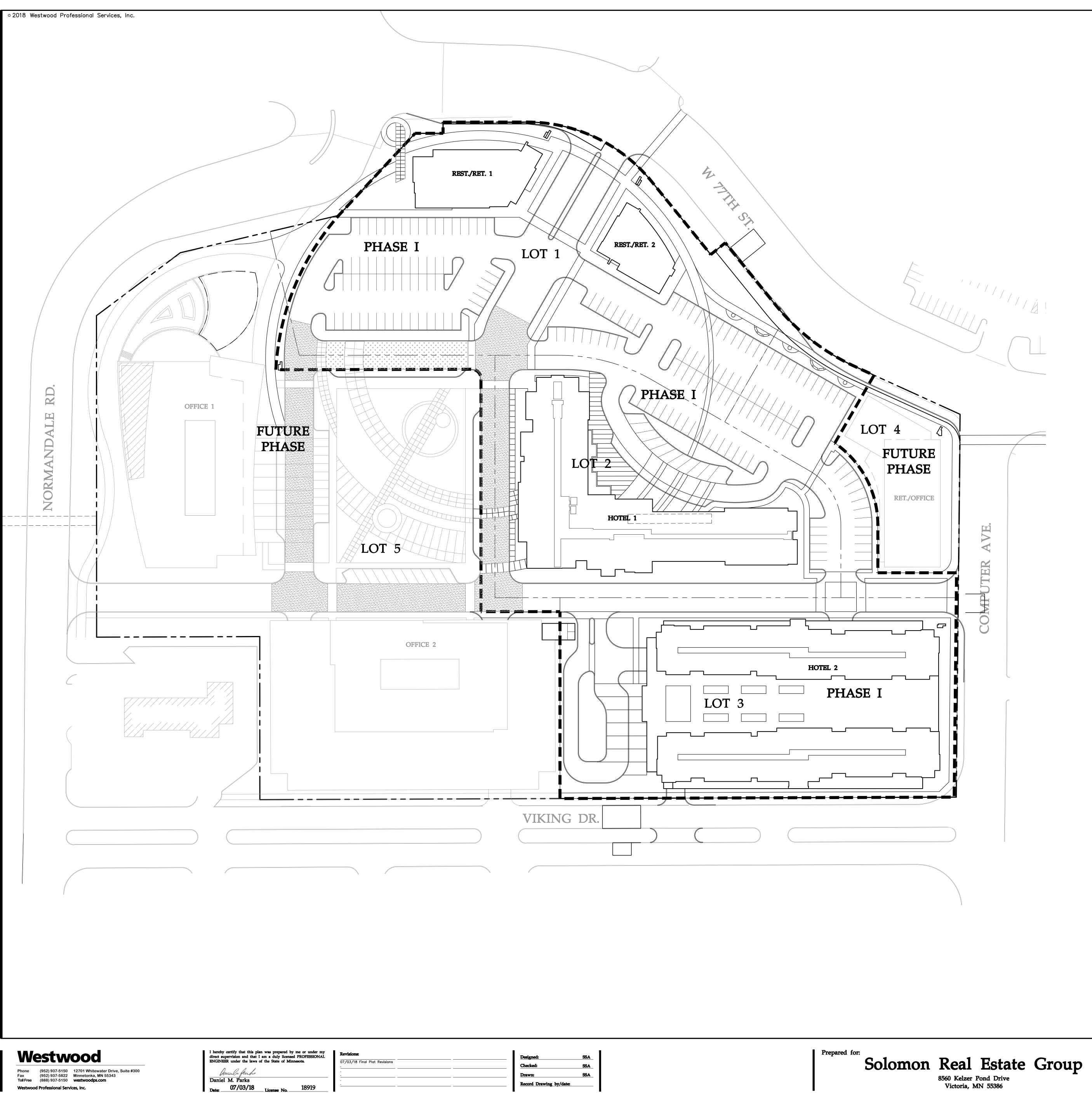
NOTE: THE ABOVE LANDSCAPE TOTALS ARE GIVEN AS A CONVENIENCE TO THE LANDSCAPE CONTRACTOR. THE LANDSCAPE CONTRACTOR IS RESPONSIBLE FOR VERIFYING COUNTS WITH LANDSCAPE PLAN.

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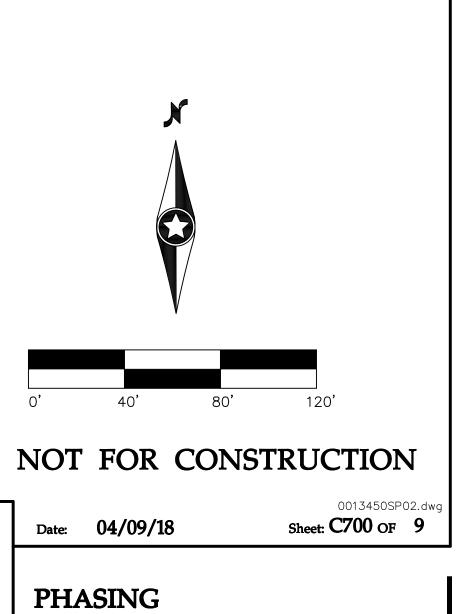
Solomon Real Estate Group 8560 Kelzer Pond Drive Victoria, MN 55386

Pentagon Village Edina, MN





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Pentagon Village Edina, MN





PLAN



Infrastructure Engineering Planning Construction

701 Xenia Avenue South Suite #300 Minneapolis, MN 55416 Tel: 763 541-4800 Fax: 763 541-1700

Memorandum

To:	Mr. Cary Teague, Community Development Director City of Edina
FROM:	Charles Rickart, P.E., PTOE, Project Manager WSB & Associates, Inc.
DATE:	May 2, 2018
RE:	Solomon Real Estate – South Pentagon Park Development Traffic and Parking Study City of Edina, MN WSB Project No. 11953-00

Background

The purpose of this memorandum is to document the transportation impacts from the proposed redevelopment of the Pentagon Park south area (Solomon Real Estate Group) in relationship to the approved development and mitigation from the Gateway Area, Alternative Urban Areawide Review (AUAR). The site is located on the south side of W. 77th Street between TH 100 and Computer Avenue and is shown on the attached *Figure 1*.

A Traffic analysis was completed in conjunction with the AUAR in 2007 which included the Pentagon Park area. The AUAR was updated in 2013 and it was concluded that because no Gateway area development had occurred in the area, and that the area traffic levels have not changed significantly from those assumed in the AUAR for the baseline conditions the future year analysis and recommended mitigation in the 2007 AUAR were still valid. AUAR updates are required every five years from the original date of the approved AUAR. Currently the second AUAR update is being prepared. Data and results from the current draft (February 2, 2018) updated AUAR Traffic Study will be used as part of this analysis. This document is attached in the *Appendix*.

The development of the South Pentagon Park development is planned to be completed in two phases. The first phase of the development is planned to include 346 hotel rooms in two buildings and 11,800 square feet of retail / restaurant uses. The second phase will include 225,000 square feet of office in two buildings and 19,000 square feet of retail uses. Access to the site will be from public streets and driveways off W. 77th Street, Computer Avenue, Viking Drive and Normandale Road. The full development of the South Pentagon Park development is the subject of this Study and is shown on the attached *Figure 2*.

Solomon South Pentagon Park Development – Traffic and Parking Study City of Edina May 2, 2018 Page 2 of 16

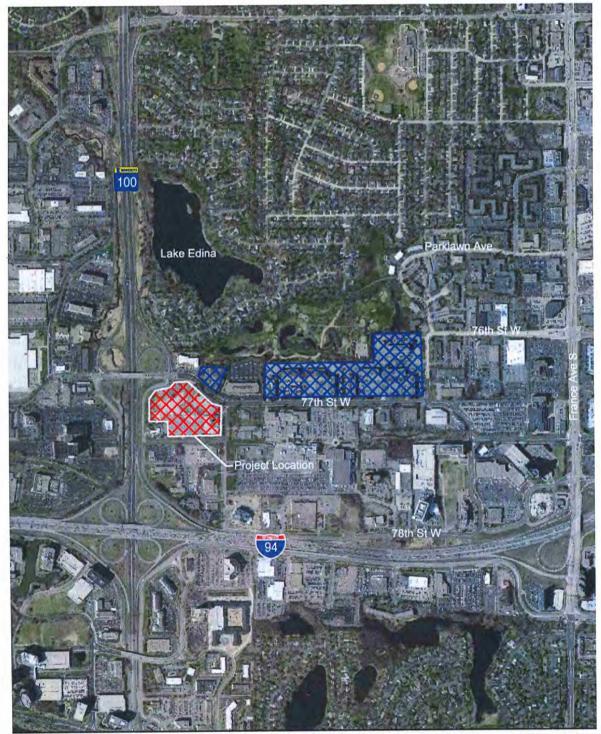




Figure 1 - Location Map Solomon - Pentagon South Traffic and Parking Study

Legend Pentagon North





Figure 2 - Site Plan Solomon - Pentagon South Traffic and Parking Study



Solomon South Pentagon Park Development – Traffic and Parking Study City of Edina May 2, 2018 Page 4 of 16

The traffic impacts of the proposed site redevelopment were evaluated for: the existing conditions; projected 2020 the anticipated completion of the South Pentagon Park development only, and; projected 2025 and 2040 from the AUAR. The following locations were included as part of the analysis:

- France Avenue at 76th Street
- France Avenue at Minnesota Drive
- 77th Street at Minnesota Drive
- 77th Street at Parklawn Avenue
- 77th Street at Computer Avenue
- 77th Street at Burgundy Place Driveway
- 77th Street at SB TH 100 Ramp
- 77th Street at NB TH 100 Ramp
- Computer Avenue at Site Access
- Computer Avenue at Viking Drive
- Viking Drive at Normandale Road
- Normandale Road at Site Access

The following sections of this report document the analysis and anticipated impacts of the proposed first phase of the South Pentagon Park redevelopment.

Existing Traffic Conditions

Updated AM and PM peak hour turning movement counts were conducted the weeks of December 4th and December 11th, 2017. These counts were used as the existing baseline conditions for the area. The draft updated AUAR Traffic Study in the *Appendix* shows the existing intersections and driveways in the Study Area, with the existing AM and PM peak hour traffic volumes.

Background (Non-Development) Traffic Growth

Traffic growth in the vicinity of a proposed site will occur between existing conditions and any given future year due to other development within the region. This background growth must be accounted for and included in future year traffic forecasts. Reviewing the historical traffic counts in the area, traffic has stayed somewhat constant or dropped in the past few years. In order to account for other development background growth in traffic the Hennepin County State Aid traffic growth projection factor of 1.1 over a 20-year period was used to project traffic to the 2020 analysis year. The Gateway AUAR also identified adjacent development projects in Edina and Bloomington that have yet to be completed. These developments were assumed to be completed and included in the 2025 and 2040 future full build scenarios included in the draft updated AUAR Traffic Study in the *Appendix*.

Trip Generation

The estimated trip generation from the South Pentagon Park development is shown below in *Table 1.* The trip generation rates used to estimate the proposed site traffic is based on extensive surveys of the trip-generation rates for other similar land uses as documented in the Institute of Transportation Engineers *Trip Generation Manual*, 10th Edition. The table show the total daily, AM peak hour and PM peak hour trip generation for the proposed site.

	ITE			Time of	Trip		Trip Generation				
Land Use	Code	Size		Day	Rate	Total		In		Out	
	Cour			,		Total	%	Trips	%	Trips	
				Daily	9.74	2,192	50	1,096	50	1,096	
Office	710	225,000	gsf	AM Peak	1.16	261	86	224	14	37	
				PM Peak	1.15	259	16	41	84	218	
				Daily	37.75	718	50	359	50	359	
Retail	820	19,000	gsf	AM Peak	0.94	18	57	10	43	8	
				PM Peak	3.81	72	52	37	48	35	
				Daily	112.18	1,324	50	662	50	662	
Retail / Restaurant	932	11,800	gsf	AM Peak	9.94	117	55	64	45	53	
Restaurant			_	PM Peak	9.77	115	62	71	38	44	
				Daily	8.36	2,892	50	1,446	50	1,446	
Hotel	310	346	rms	AM Peak	0.47	163	59	96	41	67	
				PM Peak	0.6	208	51	106	49	102	
				Daily	7,126			3,563	3,563		
	Tota	l		AM Peak	5:	559		394		165	
				PM Peak	654		255		399		
				Daily	-7	96	-398		-398		
	Internal 7	Frips		AM Peak	-8	39	-48		-41		
•		PM Peak	-8	37	-56		-31				
· · ·				Daily	6,3	330	4	2,665	2	,665	
Net	t Increase	in Trips		AM Peak	4	70		346		124	
		Ĩ		PM Peak	5	67		199		368	

 Table 1 - Estimated Trip Generation – South Pentagon Park

Source: Institute of Transportation Engineers Trip Generation Manual, 10th Edition

Based on current planes the remainder of the Pentagon Park development area is anticipated to include on the north side of W. 77th Street; 1,250 market rate apartment units, 225 senior adult housing units and 125 affordable housing units on the Welsh Title site.

In addition, the Gateway Area AUAR assumed additional development adjacent to the Pentagon Park area, including; 519,300 sf office and, 1,296,000 office and warehouse uses.

Solomon South Pentagon Park Development – Traffic and Parking Study City of Edina May 2, 2018 Page 6 of 16

The estimated trip generation from the remainder of the proposed Pentagon Park development and Gateway AUAR area is shown below in *Table 2*.

	ITE	T		Time of	Trip		Tri	p Genera	tion	
Land Use	Code	Size		Day	Rate	Total		In		Out
	couc			Day	Tunte	Total	%	Trips	%	Trips
				Daily	5.44	6,800	50	3,400	50	3,400
Market Rate	221	1,250	units	AM Peak	0.36	450	26	117	74	333
Apartment				PM Peak	0.44	550	61	335	39	215
Senior Adult				Daily	3.7	832	50	416	50	416
Housing -	252	225	25 units	AM Peak	0.2	45	35	16	65	29
Attached				PM Peak	0.26	59	55	32	45	27
Welsh Title		-		Daily	5.44	680	50	340	50	340
Site –	221	125	125 units	AM Peak	0.36	45	26	12	74	33
Affordable Housing	221			PM Peak	0.44	55	61	34	39	21
Office 71		0 519,300 gs	0 gsf	Daily	9.74	5,058	50	2,529	50	2,529
	710			AM Peak	1.16	602	86	518	14	84
				PM Peak	1.15	597	16	96	84	501
	1			Daily	1.71	1,516	50	758	50	758
Office and Warehousing	150	1,296,000	gsf	AM Peak	0.17	220	77	169	23	51
warenousing				PM Peak	0.19	246	27	66	73	180
				Daily	14	,886		7,443	7	,443
	Tota	1		AM Peak	1,362		843		519	
				PM Peak	1,507		561		946	
	1.0.1			Daily	-1,	208	-604		-604	
Internal Trips			AM Peak	-2	298	-171		-127		
	1	2.62		PM Peak	-2	269		-98		-171
				Daily	13	,678		6,839	6	5,839
Nei	t Increase	in Trips		AM Peak	1,	064		672		392
				PM Peak	1,	238		463		775

Table 2 - Estimated Trip Generation – Additional Gateway Area

Proposed North Pentagon Park

Remaining Gateway Area

Source: Institute of Transportation Engineers Trip Generation Manual, 10th Edition

The AUAR identified four potential land use scenarios that were evaluated. Trips for each of the scenarios were generated and are shown in the updated AUAR Traffic Study in the *Appendix*. All the proposed land use scenarios assume replacing all the existing office space.

Solomon South Pentagon Park Development – Traffic and Parking Study City of Edina May 2, 2018 Page 7 of 16

Table 3 shows a comparison between the current full build proposal of the area (Pentagon Park proposals both north and south of W. 77^{th} Street with the estimated remainder of the Gateway Area) and the AUAR Scenarios. Based on the full build of the area the current Pentagon Park proposed development would generate fewer trips than those included in the AUAR except the daily traffic for the AUAR Scenario 1 condition.

Table 3 – Traffic	Generation	Comparison	
		an a	

Scenario	ADT	AM Peak	PM Peak
Current Pentagon Park Proposals w/ Remainder of Gateway Area	20,008	1,534	1,805
AUAR Scenario 1	17,771	2,068	2,078
AUAR Scenario 2	27,825	2,778	2,931
AUAR Scenario 3	34,475	4,057	4,050
AUAR Scenario 4	22,789	2,123	2,270

South Pentagon Park Traffic Operations Analysis

Existing and/or forecasted traffic operations were evaluated for the impacted intersections and driveway adjacent to the proposed development. The analysis was conducted for the following scenarios.

- 1. Existing 2017 (in draft AUAR Traffic Study update)
- 2. Projected 2020 Build South Pentagon Park Development
- 3. Projected 2025 Build Full Gateway Area (in draft AUAR Traffic Study update)
- 4. Projected 2040 Build Full Gateway Area (in draft AUAR Traffic Study update)

This section describes the methodology used to assess the operations and provides a summary of traffic operations for each analysis year.

Analysis Methodology

The traffic operations analysis is derived from established methodologies documented in the *Highway Capacity Manual 2000* (HCM). The HCM provides a series of analysis techniques that are used to evaluate traffic operations.

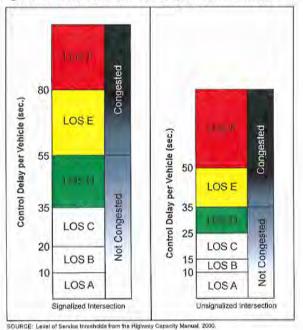
Intersections are given a Level of Service (LOS) grade from "A" to "F" to describe the average amount of control delay per vehicle as defined in the HCM. The LOS is primarily a function of peak traffic hour turning movement volumes, intersection lane configuration, and the traffic controls at the intersection. LOS A is the best traffic operating condition, and drivers experience minimal delay at an intersection operating at that level. LOS E represents the condition where the intersection is at capacity, and some drivers may have to wait through more than one green phase to make it through an intersection controlled by traffic signals.

Solomon South Pentagon Park Development – Traffic and Parking Study City of Edina May 2, 2018 Page 8 of 16

LOS F represents a condition where there is more traffic than can be handled by the intersection, and many vehicle operators may have to wait through more than one green phase to make it through the intersection. At a stop sign-controlled intersection, LOS F would be characterized by exceptionally long vehicle queues on each approach at an all-way stop, or long queues and/or great difficulty in finding an acceptable gap for drivers on the minor legs at a through-street intersection.

The LOS ranges for both signalized and un-signalized intersections are shown in *Figure 3*. The threshold LOS values for un-signalized intersections are slightly less than for signalized intersections. This variance was instituted because drivers' expectations at intersections differ with the type of traffic control. A given LOS can be altered by increasing (or decreasing) the number of lanes, changing traffic control arrangements, adjusting the timing at signalized intersections, or other lesser geometric improvements. LOS also changes as traffic volumes increase or decrease.





LOS, as described above, can also be determined for the individual legs (sometimes referred to as "approaches") or lanes (turn lanes in particular) of an intersection. It should be noted that a LOS E or F might be acceptable or justified in those cases where a leg(s) or lane(s) has a very low traffic volume as compared to the volume on the other legs. For example, improving LOS on such low-volume legs by converting a two-way stop condition to an all-way stop, or adjusting timing at a signalized intersection, could result in a significant penalty for the many drivers on the major road while benefiting the few on the minor road. Also, geometric improvements on minor legs, such as additional lanes or longer turn lanes, could have limited positive effects and might be prohibitive in terms of benefit to cost. Solomon South Pentagon Park Development – Traffic and Parking Study City of Edina May 2, 2018 Page 9 of 16

Although LOS A represents the best possible level of traffic flow, the cost to construct roadways and intersection to such a high standard often exceeds the benefit to the user. Funding availability might also lead to acceptance of intersection or roadway designs with a lower LOS. LOS D/E is generally accepted as the lowest acceptable level in urban areas such as Edina. LOS C is often considered to be the desirable minimum level for rural areas. LOS E/F may be acceptable in highly congested urban areas for limited durations or distances, or for low-volume legs of some intersections.

The LOS analysis was performed using Synchro/SimTraffic:

- Synchro, a software package that implements Highway Capacity Manual (HCM) methodologies, was used to build each signalized intersection and provide an input database for turning-movement volumes, lane geometrics, and signal design and timing characteristics. In addition, Synchro was used to optimize signal timing parameters for future conditions. Output from Synchro is transferred to SimTraffic, the traffic simulation model.
- SimTraffic is a micro-simulation computer modeling software that simulates each individual vehicle's characteristics and driver behavior in response to traffic volumes, intersection configuration, and signal operations. The model simulates drivers' behaviors and responses to surrounding traffic flow as well as different vehicle types and speeds. It outputs estimated vehicle delay and queue lengths at each intersection being analyzed.

Existing Level of Service Summary

The existing intersection operations were evaluated for the AM and PM peak hour assuming the current lane geometry, traffic control and traffic volumes. The results of this analysis are found in the draft updated Traffic Study included in the *Appendix*. Based on the analysis, the only intersection that is operating at an overall deficient level of service (LOS E or F), is France Avenue at Minnesota Drive which is operating at an overall LOS E during the PM peak hour. All other intersections are operating at an overall LOS D or better. However, there are several movements that are operating at LOS F including:

- France Ave at W. 76^{th} St Northbound Left AM Peak = LOS F
- W. 77^{th} St at SB TH 100 Ramp Eastbound Through PM Peak = LOS F
- W. 77th St at NB TH 100 Ramp Eastbound Left PM Peak = LOS F
- France Ave at Minnesota Dr Westbound Left PM Peak = LOS F

Forecasted 2020 Traffic Operations

A capacity and LOS analysis was prepared for the study area intersections for 2020 which is assumed to the year after the proposed South Pentagon Park development would be completed. The analysis assumes full build of the South Pentagon Park development with the existing lane configurations and traffic control.

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The results of the analysis are shown below in *Table 4*. It shows that all intersection will continue to operate at overall LOS D or better in 2020 during the AM peak hour. However, during the PM peak hour in 2020 with the increase in traffic from the proposed South Pentagon Park development some intersections and movements will be operating at LOS E/F. Specifically, the intersections of 77th at the TH 100 ramps and France Avenue at Minnesota Street will have overall levels of service at E.

Intersection	2020					
	AM Peak Hour			PM Peak Hour		
Intersection	LOS	Delay	Movement (LOS)	LOS	Delay	Movement (LOS)
SB TH 100 at W 77th St	C	34.1		Е	61.2	EBT (F)
NB TH 100 at W 77th St	С	32.6	EBR (E)	E	64.8	EBL (F)
Commercial Access at W 77th St	С	27.5		D	40.2	NBL (E) EBR (E)
Computer Ave at W 77th St	С	31.3	EBR (E)	D	41.4	NBL (E) EBR (E)
Parklawn Ave at W 77th St	С	24.1		D	39.7	
W 77th St at Minnesota Dr	С	21.2		D	38.1	SBL (E)
France Ave at W 76th St	D	36.4	NBL (F)	D	42.6	
France Ave at Minnesota Dr	С	32.3		Е	62.3	WBL (F)
Computer Ave at Site Access	А	8.2		В	11.8	
Computer Ave at Viking Drive	В	12.4		В	14.9	
Viking Drive at Normandale Rd	А	7.6		A	9.1	
Normandale Rd at Site Access	A	6.3		A	8.4	1

Table 4 - Forecasted 2020 South Pentagon Park Development

With the addition of the South Pentagon Park development several movements will be operating at LOS E or F as outline in *Table 4*. In order to mitigate the unsatisfactory movements, specifically at the main access intersections for the South Pentagon Park development, the following improvements should be considered:

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- Addition of a northbound left turn lane, southbound striped left turn lane and eastbound right turn lane at 77th Street and Commercial Access driveway
- Addition of a northbound dual left turn lane and eastbound right turn lane at 77th Street and Computer Avenue
- Signal timing improvements/optimization on W. 77th Street from TH 100 to Parklawn Avenue.

Forecasted 2025 and 2040 Traffic Operations

The 2025 and 2040 intersection operations were evaluated for the AM and PM peak hour assuming the full build of the Gateway AUAR area development is completed including the Pentagon Park-development. The summary of the expected traffic operations for the key intersections for each development scenario is included in the draft updated Traffic Study in the *Appendix*.

The results of the analysis show that all scenarios will have some operational deficiencies in years 2025 and/or 2040. Deficiencies include operations on France Avenue, the TH 100 interchange, and accesses to the Gateway Study Area. The majority of these locations do not have right or left turn lanes or the existing turn lanes do not have adequate capacity to handle the forecast traffic volumes.

During both the AM peak hour PM peak hours in both 2025 and 2040 assuming with the increase in traffic for each land use scenario, several intersections and movements will be operating at LOS E or F. Specifically, the intersections of 77th Street at the TH 100 ramps, 77th Street at Computer Drive, 77th Street at Parklawn Ave, 77th at Minnesota Drive, France Avenue at 76th Street and France Avenue at Minnesota Street.

The AUAR identified mitigation improvements that would improve all intersections and movement to an acceptable LOS E or better depending on the development scenario included:

2025 Mitigation:

- 1. 2020 Improvement identified for the South Pentagon Park development.
- 2. Addition of a northbound dual right-turn lane at 77th Street and TH 100 SB Ramp.
- 3. Addition of a northbound through lane at France Avenue and Minnesota Street.

2040 Mitigation:

- 1. 2025 Improvements
- 2. Addition of a northbound through lane at France Avenue and Minnesota Drive.
- 3. Addition of a southbound through lane at France Avenue and 76th Street.
- 4. Addition of an eastbound and westbound third lane on 77th Street from TH 100 SB Ramp through Computer Drive.
- 5. Addition of an eastbound and westbound dual left turn lane at France Avenue and Minnesota Street.
- 6. Addition of a southbound left turn lane at 77th Street and Minnesota Street.
- 7. Addition of an eastbound right turn lane at 77th Street and Parklawn Avenue.

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AUAR Mitigation Requirements

The AUAR completed in 2007, updated in 2013 and is currently being updated, identified several required mitigation measures to be completed at various levels and stages of development. The AUAR identified development scenarios were used to prepare the Traffic and Transportation Mitigation measures included in the draft updated Traffic Study in the *Appendix*.

Based on the traffic generation for the current Pentagon Park development plans including the remainder of the Gateway Area, Scenario 1 or 4 provides similar traffic conditions (see *Table 3*). The following mitigation measures were identified in the draft updated AUAR Traffic Study in the *Appendix*, for Scenarios 1 and 4 to accommodate both 2025 and 2040 traffic projections.

Scenarios 1 and 4:

Intersection: Improvement: Need By:	France Avenue at West 76 th Street Extend one southbound thru lane on France Avenue to create a total of four thru lanes 2040 No-Build
Intersection: Improvement:	Northbound TH 100 at West 77 th Street Add 150-foot northbound right turn lane on Frontage Road
Need By:	Westbound dual right turn lanes on West 77 th Street 2040 Full Build
Intersection: Improvement: Need By:	Edina Industrial Boulevard / West 78 th Street Eastbound dual left turn lanes on West 78 th Street 2040 Full Build
Intersection: Improvement:	Edina Industrial Boulevard / Metro Boulevard Add southbound right turn lane on Metro Boulevard, restriping the existing two southbound lanes to accommodate an exclusive left turn lane, and a thru/left lane, providing dual left turn lanes. Add 300-foot eastbound left turn lane on Edina Industrial Boulevard
Need By:	2025 No-Build

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Parking Demand

The parking demand for the proposed South Pentagon Park development was analyzed based on the anticipated use for the site. Based on the current City Code the proposed development would require a total of 1,718 parking spaces. The current site plan includes 1,422 spaces. *Table 5* shows a breakdown of the parking required per City Code for each anticipated development phase.

Based on the results of the parking analysis the spaces provided with the first phase of the development will meet City Code. However, with the addition of the second phase of development or assuming the full development the site does not meet City Code. A 296-space parking variance would be required for the site development with the current plan.

Use	Size	Rate	Parking Required	Parking Provided
Hotel	346 Rooms / 50 Employees	1/room + 1/employee	396	550
Retail / Restaurant	Rest =7,000 sf/120 seats /10employees Retail = 4,800 sf	Rest = 1/3 seats + 1/employee Retail = 8/first 1000sf + 6/additional 1000sf	81	113
Total Parking Phase 1		477	663	
Office	225,000 sf	1/200sf	1,125	718
Retail	19,000 sf	Retail = 8/first 1000sf + 6/additional 1000sf	116	41
Total Parking P	hase 2		1,241	759
Total South Pen	tagon Park Parking		1,718	1422

Table 5 – Parking Required per City Code

Source: City of Edina

The parking demand was also analyzed based on industry standards. The parking generation rates used to estimate the parking demand was based on surveys of the parking generation for other similar land uses as documented in the Institute of Transportation Engineers *Parking Generation Manual*, 4th Edition. **Table 6** below shows the estimated parking generation rate and the anticipated peak parking demand on a typical weekday. This would represent the worst-case conditions for the parking of the site.

Comparing the provided parking stalls to the industry standard (ITE Rates) the first phase has an adequate number of stalls. Comparing the provided parking stalls to the industry standard the second phase would be short parked by 77 stalls. However, comparing the total number of stalls for the full build the site to the industry standard, an adequate number of stalls would be provided.

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Use	Size	Rate	Parking Required
Hotel	346 Rooms	1.2/room	415
Retail / Restaurant	Rest = 7,000 sf Retail = 4,800 sf	Rest = 13.3 / 1000 sf Retail = 3.16 / 1000 sf	108
Total Parking Phase 1			523
Office	225,000 sf	3.45 / 1000 sf	776
Retail	19,000 sf	Retail = 3.16 / 1000 sf	60
Total Parking Phase 2			836
Total South Pentagon Park Parking			1359

Table 6 – S	Site Parking	Demand	per ITE
10000 0			r + · ·

Source: Institute of Transportation Engineers Parking Generation Manual, 4th Edition

Conclusions / Recommendation

Based on the analysis documented in this memorandum, WSB has concluded the following:

- The proposed South Pentagon Park development includes: 346 hotel rooms in two buildings; 11,800 square feet of retail / restaurant uses; 225,000 square feet of office in two buildings, and; 19,000 square feet of retail uses. The site is anticipated to generate an additional 6,330 daily trips, 470 AM peak hour trips and 567 PM peak hour trips.
- The remainder of the Pentagon Park development on the north side and south side of W. 77th Street and the remainder of the Gateway Area AUAR development area site is anticipated to generate 13,678 daily trips, 1,069 AM peak hour trips and 1,238 PM peak hour trips.
- The full build of the area with the current proposed Pentagon Park development will generate fewer trips than any of the land use scenarios included in the AUAR except the daily traffic for the Scenario 1 condition.
- A draft updated Traffic Study has been prepared for the Gateway Area AUAR. This document was used as the baseline for the existing conditions and projected 2025 and 2040 conditions. The document is included in the *Appendix* of this memorandum.
- The existing operations analysis shows that the only intersection which is operating at an overall deficient level of service (LOS E or F), is France Avenue at Minnesota Drive which is operating at an overall LOS E during the PM peak hour. All other intersections are operating at an overall LOS D or better.

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- The forecasted 2020 traffic operations with the South Pentagon Park development shows that all intersection will continue to operate at overall LOS D or better during the AM peak hour. However, during the PM peak hour some intersections and movements will be operating at LOS E/F. Specifically, the intersections of 77th at the TH 100 ramps and France Avenue at Minnesota Street will have overall levels of service at E.
- With the addition of the South Pentagon Park development by 2020 several movements will be operating at LOS E or F. In order to mitigate the unsatisfactory movements, turn lane and traffic signal timing improvements at the W. 77th Street/Commercial Access driveway and W. 77th Street/Computer Avenue intersection would be needed.
- The Gateway Area AUAR completed in 2007, updated in 2013 and is currently being updated, which included the Pentagon Park development area identified several required mitigation measures to be completed at various levels and stages of development for each land use scenario.
- The full build traffic generation for proposed Gateway area including the current Pentagon Park development most closely represents the AUAR land use Scenario 1 and 4. The AUAR mitigation for these Scenarios was assumed for the 2025 and 2040 forecasted conditions.
- The parking shown on the current site plan does not meet the City's Code for the proposed uses. The current plan provides for 1,422 parking spaces with 1,718 required by City Code. Based on the ITE parking generation estimates the total parking needed for the proposed uses on the site would be 1,359. This represents the worst-case condition and therefore the proposed parking would be adequate for this site, requiring a parking variance.

Based on these conclusions the following is recommended with the development of the North Pentagon Park first phase:

- 1. Addition of a northbound left turn lane, southbound striped left turn lane and eastbound right turn lane at 77th Street and Commercial Access driveway.
- 2. Addition of a northbound dual left turn lane and eastbound right turn lane at 77th Street and Computer Avenue
- 3. Review the traffic signal timing and coordination of the signal systems on W. 77th Street from the TH 100 ramps to Parkland Avenue.
- 4. Secure a 296-space parking variance for the full site development.
- 5. As development continues on the remainder of the Pentagon Park site prepare traffic analysis to identify the required mitigation from the Gateway AUAR.

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APPENDIX



Gateway AUAR Update Traffic Study

For:



City of Edina 4801 W. 50th Street Edina, MN 55024

February 2, 2018

Prepared By:



WSB & Associates, Inc. 701 Xenia Avenue South Minneapolis, MN 55416

CERTIFICATION

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly registered professional engineer under the laws of the State of Minnesota.

iala

Charles T. Rickart, P.E., P.T.O.E.

Date: February 2, 2018

Reg. No. 26082

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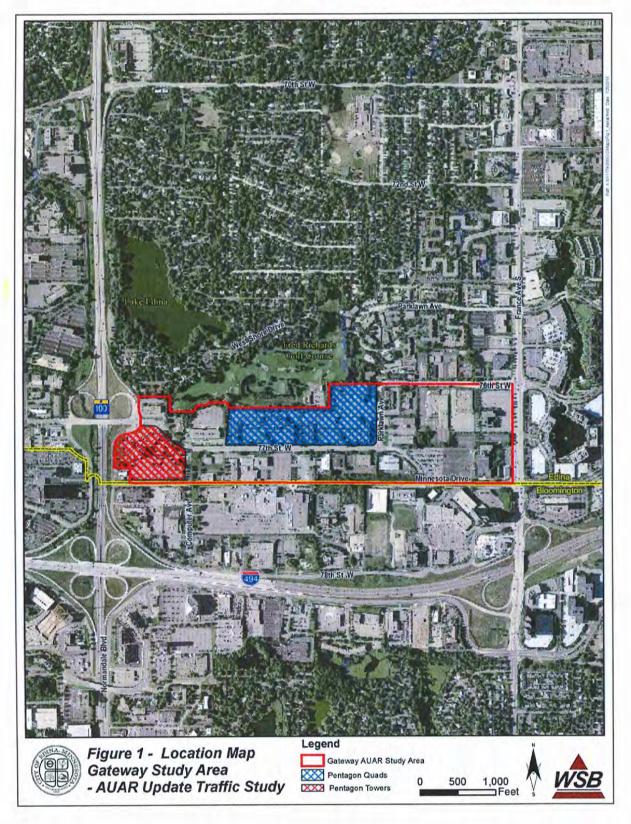
INTRODUCTION / BACKGROUND

The original Alternative Urban Areawide Review (AUAR) was completed and approved in September 2007 and analyzed the impacts of the four development scenarios for the years 2014 and 2030. The analysis for both years assumed a 1% per year growth in general background traffic, the approved development in the Cities of Bloomington and Edina at that time and the proposed Gateway Development traffic.

AUAR updates are required every five years from the original date of the approved AUAR. The first AUAR update was prepared and approved in June 2013. This included updating the traffic counts at selected intersection and roadway segments on 77th Avenue. The updated traffic counts were then compared to those assumed in the 2007 AUAR to determine if the analysis and recommended mitigation measures were still valid. Based on the facts that: no Gateway Development had occurred in the area; most of the additional development has been in Bloomington and Edina and their traffic generation was included in the new 2013 traffic counts; and, the area traffic levels had not changed significantly from those assumed in the original AUAR for the baseline conditions, it was concluded that the future year analysis and recommended mitigation was still valid and no additional analysis was completed.

The second AUAR update is currently being prepared. In order to update and document the impact the proposed redevelopment of the Gateway AUAR Area adjacent to W. 77th Street between TH 100 and Parklawn Avenue has on the area traffic operations; this Traffic Impact Study is being prepared. The project location is shown on the attached *Figure 1*.

The following sections of this report document the analysis and anticipated impacts of the proposed Gateway AUAR development area.



EXISTING TRAFFIC CONDITIONS

In order to evaluate the existing conditions, key roadway segments and intersections were selected that are expected to provide the primary access to the regional roadway system when the Study Area redevelops. This section documents the existing land use, geometry, traffic volumes, and functional class at these locations, and uses these traffic characteristics to estimate their existing traffic operations.

A. Key Roadways

The following five roadways were selected as the key roadway segments for the Study Area:

- Edina Industrial Boulevard/West 77th Street from Metro Avenue to Computer Avenue
- West 77th Street from Computer Avenue to Parklawn Avenue
- Minnesota Drive from West 77th Street to France Avenue
- Parklawn Avenue/West 76th Street from West 77th Street to France Avenue
- France Avenue from West 76th Street to Minnesota Drive

The transportation characteristics for the roadways are displayed in **Table 1**. The existing roadway segment is documented, along with the existing functional classification. Also displayed are average annual daily traffic (AADT) volumes were obtained from *Year 2016 MnDOT Traffic Flow Maps*.

Segment	Location	Functional Classification	Facility Type	Existing AADT
Edina Industrial Blvd / W. 77 th Street	Metro Blvd to Computer Dr	A Minor Arterial – Reliever	Four-Lane with Turn Lanes	12,000
W. 77 th Street	Computer Dr to Parklawn Ave	A Minor Arterial – Reliever	Four Lane with Center Turn Lane	11,500
Parklawn Avenue / W. 76 th Street	W. 77 th St to France Ave	A Minor Arterial – Reliever	Four-Lane	8,700
, Minnesota Drive	W. 77 th St to France Ave	Other Minor Arterial	Four Lane with Turn Lanes	7,500
France Avenue	W. 76 th St to Minnesota Dr	A Minor Arterial - Reliever	Seven Lane with Turn Lane	30,000

Table 1: Characteristics of Key Roadways

Source: 2016 MnDOT Traffic Flow Maps and 2008 Edina Comprehensive Plan

B. Key Intersection

The following eight intersections, displayed on *Figure 2* were selected because they provide primary access to the regional roadway system from the Study Area:

- Southbound TH 100 at West 77th Street
- Northbound TH 100 at West 77th Street
- Commercial Access at West 77th Street
- Computer Avenue at West 77th Street
- Parklawn Avenue at West 77th Street
- West 77th Street at Minnesota Drive/Johnson Avenue
- France Avenue at West 76th Street
- France Avenue at Minnesota Drive

The existing lane configurations at each of the study area intersection are as follows:

- W. 77th Street at TH 100 Southbound Ramp/Frontage Road Traffic Signal Control SB TH 100 Ramp approaching 77th St – one free right, one through, two left NB Frontage Road approaching 77th St – one right, one through, one left EB 77th St approaching TH 100 NB Ramp – one right/through, one through, one left WB 77th St approaching TH 100 SB Ramp – one free right, two through, one left
- W. 77th Street at TH 100 Northbound Ramp/Frontage Road Traffic Signal Control SB TH 100 Ramp approaching 77th St – one free right, one through, two left NB Frontage Road approaching 77th St – one right/through, two left EB 77th St approaching TH 100 NB Ramp – one right/through, one through, one left WB 77th St approaching TH 100 SB Ramp – one right, two through, one left
- W. 77th Street at Burgundy Place Traffic Signal Control SB Driveway approaching 77th St – one right/through, one left NB Driveway approaching 77th St – one right/through, one left EB 77th St approaching Driveway – one right/through, one through, one left WB 77th St approaching Driveway – one right/through, one through, one left
- W. 77th Street at Computer Avenue Traffic Signal Control
 SB Drivcway approaching 77th St one right, one through/left
 NB Computer Ave approaching 77th St one right, one through/left
 EB 77th St approaching Computer Ave one right/through, one through, one left
 WB 77th St approaching Computer Ave one right/through, one through, one left
- W. 77th Street at Parklawn Avenue Traffic Signal Control
 SB Parklawn Ave approaching 77th St one right, one right/through, one left
 NB Driveway approaching 77th St one right/through/left
 EB 77th St approaching Parklawn Ave one right/through, one through, one left
 WB 77th St approaching Parklawn Ave one right/through, one through, one left

W. 77th Street at Minnesota Drive – Traffic Signal Control

SB 77th St approaching Minnesota Dr – one right/through, one through/left NB 77th St approaching Minnesota Dr – one free right, one through, one left EB Driveway approaching 77th St – one right/through, one through/left WB Minnesota Dr approaching 77th St – one free right, one through, one left

France Avenue at 76th Street – Traffic Signal Control

SB France Ave approaching 76th St – one free right, three through, one left NB France Ave approaching 76th St – one free right, four through, one left EB 76th St approaching France Ave – one free right, two through, two left WB 76th St approaching France Ave – one free right, two through, two left

France Avenue at Minnesota Drive - Traffic Signal Control

SB France Ave approaching Minnesota Dr – one free right, four through, one left NB France Ave approaching Minnesota Dr – one free right, three through, one left EB Minnesota Dr approaching France Ave – one free right, two through, one left WB Minnesota Dr approaching France Ave – one free right, two through, one left

The majority of traffic exiting and entering the study area will use at least one of these intersections. The a.m. and p.m. peak hour turn movements, lane geometry, and traffic control are displayed on *Figure 3A* and *Figure 3B* in the *Appendix*.

All analyzed intersections are controlled by traffic signals. For purposed of analysis, traffic signal timing was obtained from MnDOT, Hennepin County, and the City of Edina.

C. Existing Traffic Volumes

Updated AM and PM peak hour turning movement counts were conducted the weeks of December 4th and December 11th, 2017. These counts were used as the existing baseline conditions for the area. *Figure 3A* and *Figure 3B* in the *Appendix* shows the existing intersections and driveways in the Study Area that were analyzed as part of this traffic study, with the existing AM and PM peak hour traffic volumes.



D. Existing Land Use

The existing land use consists mostly of office and office/warehouse uses. Currently about 1,736,000 gsf of building space is available within the Study Area, of which approximately 190,000 gsf of office space is unoccupied. Sense the original AUAR was completed the Pentagon Tower building were torn down. Building areas were measured from aerial photographs. The amount of unoccupied space was estimated by the City of Edina.

The existing land use including the estimated trip generation is calculated and shown in *Table 2*. As shown, the Study Area is currently generating approximately 13,000 vehicle trips per day (vpd). If the office space was fully occupied, the Gateway Study Area has the potential to generate 14,900 vpd.

ALC: NOT THE OWNER OF THE OWNER		The second second			1	Tri	p Genera	tion	
Development	Size	Occupancy	Time of Day	Trip Rate	Total		In	(Dut
a ner (c)			Day	Mate	Total	%	Trips	%	Trips
			Daily	9.74	1,729	50	865	50	864
Pentagon Quads	355,054 gsf	50.0%	AM Peak	1.16	206	86	177	14	29
4			PM Peak	1.15	204	16	33	84	171
Sector of Course	Berger Character		Daily	9.74	1,211	50	606	50	605
Pentagon East	136,611 gsf	91.0%	AM Peak	1.16	144	86	124	14	20
and the second			PM Peak	1.15	143	16	23	84	120
	Sector and		Daily	9.74	9,669	50	4,835	50	4,834
Other Office	992,700 gsf	100.0%	AM Peak	1.16	1,152	86	991	14	161
			PM Peak	1.15	1,142	16	183	84	959
S. S. Standard	1.0.0		Daily	1.74	360	50	180	50	180
Other Office / Warehousing	207,000 gsf	100.0%	AM Peak	0.17	35	77	27	23	8
in an enrousing			PM Peak	0.19	39	27	11	73	28
1.000			Daily	1.51	68	50	34	50	34
Other Mini Storage	45,000 gsf	100.0%	AM Peak	0.1	5	60	3	40	2
Storage		P. Connect II	PM Peak	0.17	8	47	4	53	4
					037	6,520		6,517	
	Total			1,5	42	1,322		220	
				1,536		254		1,282	

Table 2: Existing Area Trip Generation

Source: ITE Trip Generation Manual (10th Edition) and WSB & Associates, Inc.

TRAFFIC PROJECTIONS

In order to analyze the lane configuration and traffic control needs projected traffic volumes were determined for the area. Traffic forecasts were prepared for the year 2025 which is the year the proposed site is anticipated to be fully developed and; for the 2040 conditions which represents the City's Comprehensive Plan development time frame. The following sections outline the traffic generation, as well as the traffic distribution and projected traffic volumes.

A. Background (Non-Development) Traffic Growth

Traffic growth in the vicinity of a proposed site will occur between existing conditions and any given future year due to other development within the region. This background growth must be accounted for and included in future year traffic forecasts. Reviewing the historical traffic counts in the area, traffic has stayed somewhat constant or dropped in the past few years.

The Gateway AUAR identified adjacent development projects in Edina and Bloomington that have yet to be completed. These developments for the projects in Edina and Bloomington are shown in **Table 3**. In order to account for these and other development background growth in traffic the Hennepin County State Aid traffic growth projection factor of 1.1 over a 20-year period was used to project traffic to the 2025 and 2040 analysis years.



City	Development	Summary of Impacts				
	Duke-Weeks Realty Limited Partnership (Norman Pointe)	Phase 1 and 2 completed. Phase 3 to add an additional 312,000 sq. ft. of office in the future				
	Ryan Companies US, Inc. (Marketpoint)	Phase 1 and 2 completed. Phase 3 to add an additional 250,000 sq. ft. of office in the future				
	8100 Office Tower	255,000 ft ² of office - Future				
Bloomington	Hotel	100 Rooms – Future				
	OATI Office/Data Center	100,000 ft ² of office - Future				
	Hotel	257 Rooms - Future				
	Norman Pointe III Office Tower	312,000 ft² Office - Future				
	Marketpoint III Office Tower	250,000 ft ² Office – Future				
	6500 France Avenue (Aurora on France)	180 units of senior housing & 7 care suites				
	Southdale Medical Building	60,000 s.f. medical office addition and new parking ramp				
	66 West (3330 66 th Street - Affordable Housing project	39 units				
	The Millennium (3250 66 th Street)	230 units of apartments				
	Envi Edina (3200 Southdale Circle)	190 units of apartments				
	Homewood Suites Hotel at Southdale	150 rooms				
	The Onyx (6725 York)	240 units of apartments				
Edina	Southdale One Apartments at Southdale	232 units of apartments				
	Byerly's Redevelopment (71 France)	234 units of housing 47,000 s.f. new Byerly's 21,000 s.f. new retail 9,000 s.f. Think Bank				
	Continental Gardens (7001 York)	100 senior housing				
	Restoration Hardware at Southdale	58,000 s.f. furniture store and restaurant				
	Lifetime Fitness	Replacement of 247,000 s.f. JCPenney 120,000 s.f. Lifetime Fitness 65,000 s.f. Retail				
	Shake Shack at Southdale	4,000 s.f. restaurant				

Table 3: Summary of Adjacent Redevelopment Proposals



B. AUAR Area Scenario Trip Generation

The purpose of this section is to identify the traffic impacts associated with the future redevelopment within the AUAR Study Area. Four potential land use scenarios were evaluated. Trips for each of the scenarios were generated and distributed on the regional system and analyzed for years 2025 and 2040.

In order to estimate the traffic generated by the Study Area, land use assumptions were applied to trip generation rates from the *ITE Trip Generation Manual* (10th Edition) as illustrated in *Table 4* to *Table 7*. All of the proposed scenarios replace existing office space. Trips generated from the existing buildings were shown previously in *Table 2*. These trips were removed from the network before applying the new land uses. It should be noted only the portion of space that is currently occupied was taken into consideration.

Scenario 1 consists of office and office/warehouse land uses. See *Figure 4*. This scenario is taken from the Edina Comprehensive Plan. The plan will generate approximately 17,800 vpd. The net increase equates to 4,700 vpd with about 550 trips during each of the a.m. and p.m. peak hours.

Scenario 2 adds residential, retail, and a hotel, increasing the production to about 30,900 vpd. However, the proposed retail will be developed to serve the residential. See *Figure 5*. To account for trips traveling from the residential to the retail, internal trips were taken into account. Using estimates from the manual, about 3,100 trips were considered internal and removed from the net. The net increase in vehicle trips is 14,800 vpd with 1,300 during each of the a.m. and p.m. peak hours.

Scenario 3 produces the largest number of trips, at about 34,500 trips per day due to the large amount of office space. The net increase is 21,400 vpd with about 2,500 trips during each of the a.m. and p.m. peak hours. See *Figure 6*.

Scenario 4 includes office, office/warehousing, and residential uses creating 23,000 trips per day. For reasons described above under Scenario 2, about 190 internal trips were removed. The net increase in vehicle trips is 9,700 vpd with 600 and 700 trips during the a.m. and p.m. peak hour, respectively. See *Figure 7*.

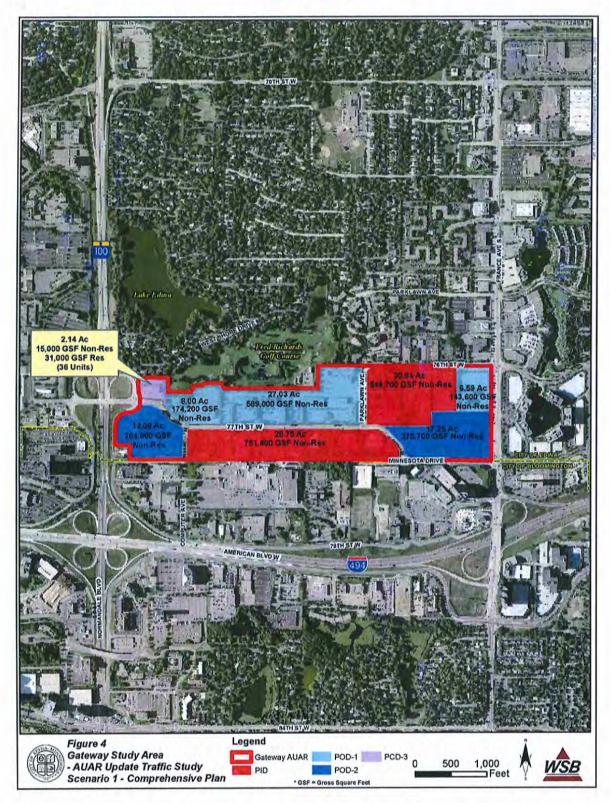


Table 4: Estimated Trip Generation - Scenario 1

	ITE		1 martine and			Trip	o General	tion	
Land Use	Land	Size	Time of	Trip		In		(Jut
	Use Code		Day	Rate	Total	%	Trips	%	Trips
			Daily	9.74	15,516	50	7,758	50	7,758
Office	710	1,593,000 gsf	AM Peak	1.16	1,848	86	1,589	14	259
		Constantia series	PM Peak	1.15	1,832	16	293	84	1,539
Office and Warehousing			Daily	1.74	2,255	50	1,128	50	1,127
	150	1,296,000 gsf	AM Peak	0.17	220	77	169	23	51
warenousing			PM Peak	0.19	246	27	66	73	180
			Daily	17,	771	8,886		8,885	
	Total		AM Peak	2,0	068	1,758		310	
			PM Peak	2,0	078	3	359	1,719	
			Daily	4,5	734	2,	366	2,	.368
Net I	Net Increase in Trips			5	26	4	436		90
				542		105		437	

Source: ITE Trip Generation Manual (10th Edition) and WSB & Associates, Inc.

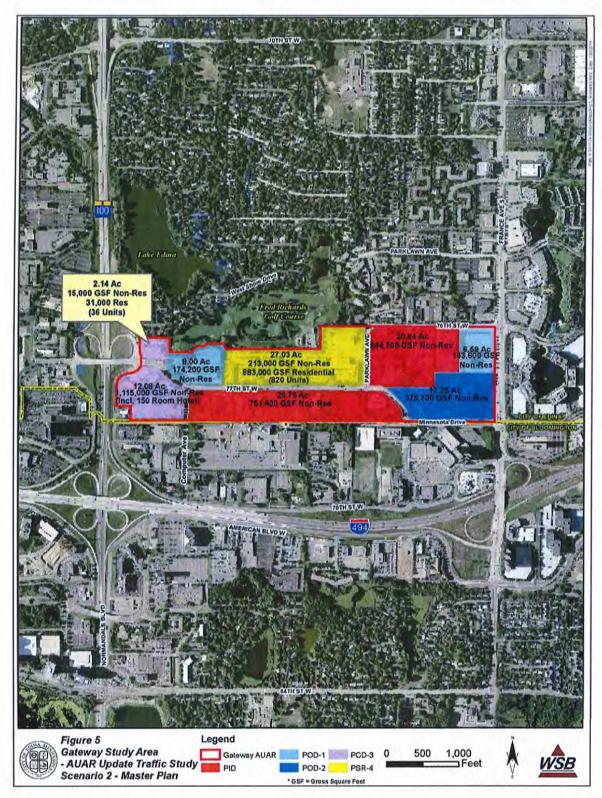


Table 5: Estimated Trip Generation - Scenario 2

	ITE	1	In the second			Trip	Genera	tion	
Land Use	Land Use	Size	Time of Day	Trip Rate			In	(Out
	Code	and the second	Duy		Total	%	Trips	%	Trips
			Daily	9.74	18,584	50	9,292	50	9,292
Office	710	1,908,000 gsf	AM Peak	1.16	2,213	86	1,903	14	310
			PM Peak	1.15	2,194	16	351	84	1,843
and a contraction			Daily	1.74	2,255	50	1,128	50	1,127
Office and Warehousing	150	1,296,000 gsf	AM Peak	0.17	220	77	169	23	51
Warenousing			PM Peak	0.19	246	27	66	73	180
		1.000	Daily	63.47	5,078	50	2,539	50	2,539
Retail	814	80,000 gsf	AM Peak	3.18	254	57	145	43	109
			PM Peak	6.84	547	52	284	48	263
			Daily	8.36	1,254	50	627	50	627
Hotel	310	150 rooms	AM Peak	0.47	71	59	42	41	29
			PM Peak	0.6	90	51	46	49	44
Abidet to			Daily	7.32	1,501	50	751	50	750
Condominium / Townhome	230	205 units	AM Peak	0.46	94	23	22	77	72
,			PM Peak	0.56	115	63	72	37	43
Senior Adult			Daily	3.7	2,276	50	1,138	50	1,138
Housing -	252	615 units	AM Peak	0.2	123	35	43	65	80
Attached			PM Peak	0.26	160	55	88	45	72
			Daily	30	,948	15	5,475	15	5,473
	Tota	1	AM Peak	2,	975	2	,324	- I	651
			PM Peak	3,	352		907	2	,445
			Daily	-3	123	-1	,561	-1	L,562
1	nternal	Trips	AM Peak	-	197		112	100	-85
			PM Peak	-4	421	1	219		202
			Daily	14	,788	7,394		7	,394
Net	Net Increase in Trips			1,	236	890			346
			PM Peak	1,395		434		961	

Source: ITE Trip Generation Manual (10th Edition) and WSB & Associates, Inc.



Table 6: Estimated Trip Generation - Scenario 3

	ITE			-	1	Tri	p Genera	tion	
Land Use	Land	Size	Time of	Trip	12		In		Out
Jana 202	Use Code	0000	Day	Rate	Total	%	Trips	%	Trips
			Daily	9.74	32,220	50	16,110	50	16,110
Office	710	3,308,000 gsf	AM Peak	1.16	3,837	86	3,300	14	537
		the state of the	PM Peak	1.15	3,804	16	609	84	3,195
Office and Warehousing			Daily	1.74	2,255	50	1,128	50	1,127
	150	1,296,000 gsf	AM Peak	0.17	220	77	169	23	51
warenousing			PM Peak	0.19	246	27	66	73	180
			Daily	34	,475	17,238		17,237	
	Total		AM Peak	4,	057	3	,469	588	
			PM Peak	4,	050		675	3,375	
	Net Increase in Trips			21,	,438	10	0,718	10,720	
Net I				2,.	515	2	,147		368
				2,514		421		2,093	

Source: ITE Trip Generation Manual (10th Edition) and WSB & Associates, Inc.



Table 7: Estimated Trip Generation - Scenario 4

	ITE	1		Tank Inc.	and a		Trip	Genera	tion	
Land Use	Land	Size	6	Time of	Trip			In	(Dut
	Use Code	1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1		Day	Rate	Total	%	Trips	%	Trips
				Daily	9.74	11,104	50	5,552	50	5,552
Office	710	1,140,000	gsf	AM Peak	1.16	1,322	86	1,137	14	185
				PM Peak	1.15	1,311	17	223	83	1,088
Constant of the				Daily	1.74	2,255	50	1,128	50	1,127
Office and Warehousing	150	1,296,000	gsf	AM Peak	0.17	220	77	169	23	51
warenousing				PM Peak	0.19	246	27	66	73	180
the difference of				Daily	7.32	8,235	50	4,118	50	4,117
Condominium / Townhome	230	1,125	units	AM Peak	0.46	518	23	119	77	399
			_	PM Peak	0.56	630	63	397	37	233
Senior Adult		375		Daily	3.7	1,388	50	694	50	694
Housing -	252		units	AM Peak	0.2	75	35	26	65	49
Attached				PM Peak	0.26	98	55	54	45	44
				Daily	22	,982	11,492		11,490	
	Total			AM Peak	2,	135	1	,451	(584
				PM Peak	2,:	285		740	1	,545
	-			Daily	-1	.93		-97)	-96
Ir	ternal T	rips		AM Peak	-	12		-2		-10
				PM Peak	-	15		-9		-6
				Daily	9,	752	4	,875	4	,877
Net I	Net Increase in Trips			AM Peak	5	81	127			454
				PM Peak	734			477		257

Source: ITE Trip Generation Manual (10th Edition) and WSB & Associates, Inc.

C. Traffic Distribution

Background and site-generated trips were distributed to the adjacent roadway system based on several factors including the existing Annual Average Daily Traffic (AADT), the travel sheds for the major routes that serve the area and data provided in the Gateway AUAR. In general, the Trip Distribution was assumed as shown in **Table 8**:

Discution	A	M	РМ			
Direction	In	Out	In	Out		
North	27%	22%	21%	26%		
South	24%	13%	18%	25%		
East	21%	35%	22%	20%		
West	28%	30%	39%	29%		

Table 8: Development Traffic Distribution

The generated trips for the proposed Gateway AUAR development area were assumed to arrive or exit using the accesses on 77th Street. The development will access the site via Computer Drive, driveways directly onto 77th Street and Parklawn Avenue. Background non-site and site-generated trips were distributed to the adjacent roadway system based on several factors including:

- Previous traffic and transportation studies in the area.
- Anticipated origins and destinations for specific land use (i.e. location of commercial uses in relationship to residential).
- Existing travel patterns.
- City's current Transportation Plan model.

D. Future Year Traffic Forecasts

Future year traffic forecast turning movements were estimated by applying the approach direction distribution percentages to the site-generated traffic. The traffic forecasts were prepared by adding the projected annual background traffic growth and the projected non-development background traffic growth to the existing 2017 traffic counts to determine the "No-Build" traffic conditions. The anticipated Gateway Area development area traffic was then added to the no-build to determine the "Build" traffic conditions. *Figures 8-15* in the *Appendix* shows the projected 2025 and 2040 Build AM and PM peak hour traffic volumes.

TRAFFIC IMPACT ANALYSIS

Existing and/or forecasted traffic operations were evaluated for the impacted intersections and driveway in the Gateway AUAR development area for each land use scenario. The analysis was conducted for the following conditions.

- 1. Existing 2017 Conditions
- 2. Projected 2025 Build
- 3. Projected 2040 Build

This section describes the methodology used to assess the operations and provides a summary of traffic operations for each scenario.

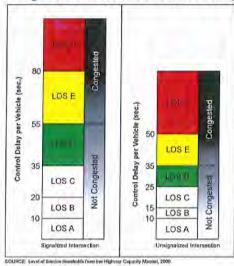
A. Methodology

The traffic operations analysis is derived from established methodologies documented in the Highway Capacity Manual 2000 (HCM). The HCM provides a series of analysis techniques that are used to evaluate traffic operations.

Intersections are given a Level of Service (LOS) grade from "A" to "F" to describe the average amount of control delay per vehicle as defined in the HCM. The LOS is primarily a function of peak traffic hour turning movement volumes, intersection lane configuration, and the traffic controls at the intersection. LOS A is the best traffic operating condition, and drivers experience minimal delay at an intersection operating at that level. LOS E represents the condition where the intersection is at capacity, and some drivers may have to wait through more than one green phase to make it through an intersection controlled by traffic signals. LOS F represents a condition where there is more traffic than can be handled by the intersection, and many vehicle operators may have to wait through more than one green phase to make it through the intersection. At a stop sign-controlled intersection, LOS F would be characterized by exceptionally long vehicle queues on each approach at an all-way stop, or long queues and/or great difficulty in finding an acceptable gap for drivers on the minor legs at a through-street intersection.

The LOS ranges for both signalized and un-signalized intersections are shown in *Figure 16.* The threshold LOS values for un-signalized intersections are slightly less than for signalized intersections. This variance was instituted because drivers' expectations at intersections differ with the type of traffic control. A given LOS can be altered by increasing (or decreasing) the number of lanes, changing traffic control arrangements, adjusting the timing at signalized intersections, or other lesser geometric improvements. LOS also changes as traffic volumes increase or decrease.

Figure 16 - Level of Service Ranges



LOS, as described above, can also be determined for the individual legs (sometimes referred to as "approaches") or lanes (turn lanes in particular) of an intersection. It should be noted that a LOS E or F might be acceptable or justified in those cases where a leg(s) or lane(s) has a very low traffic volume as compared to the volume on the other legs. For example, improving LOS on such low-volume legs by converting a two-way stop condition to an all-way stop, or adjusting timing at a signalized intersection, could result in a significant penalty for the many drivers on the major road while benefiting the few on the minor road. Also, geometric improvements on minor legs, such as additional lanes or longer turn lanes, could have limited positive effects and might be prohibitive in terms of benefit to cost.

Although LOS A represents the best possible level of traffic flow, the cost to construct roadways and intersection to such a high standard often exceeds the benefit to the user. Funding availability might also lead to acceptance of intersection or roadway designs with a lower LOS. LOS D is generally accepted as the lowest acceptable level in urban areas. LOS C is often considered to be the desirable minimum level for rural areas. LOS D or E may be acceptable for limited durations or distances, or for very low-volume legs of some intersections.

The LOS analysis was performed using Synchro/SimTraffic:

 Synchro, a software package that implements Highway Capacity Manual (HCM) methodologies, was used to build each signalized intersection and provide an input database for turning-movement volumes, lane geometrics, and signal design and timing characteristics. In addition, Synchro was used to optimize signal timing parameters for future conditions. Output from Synchro is transferred to SimTraffic, the traffic simulation model.

 SimTraffic is a micro-simulation computer modeling software that simulates each individual vehicle's characteristics and driver behavior in response to traffic volumes, intersection configuration, and signal operations. The model simulates drivers' behaviors and responses to surrounding traffic flow as well as different vehicle types and speeds. It outputs estimated vehicle delay and queue lengths at each intersection being analyzed.

B. Existing Level of Service Summary

The existing intersection operations were evaluated for the AM and PM peak hour based on the current lane geometry, traffic control and traffic volumes. The results of this analysis are illustrated in *Table 9*. Based on the analysis, the only intersection that is operating at an overall deficient level of service (LOS E or F), is France Avenue at Minnesota Drive which is operating at an overall LOS E during the PM peak hour. All other intersections are operating at an overall LOS D or better. However, there are several movements that are operating at LOS F including:

- France Ave at W. 76th St Northbound Left AM Peak = LOS F
- W. 77th St at SB TH 100 Ramp Eastbound Through PM Peak = LOS F
- W. 77th St at NB TH 100 Ramp Eastbound Left PM Peak = LOS F
- France Ave at Minnesota Dr Westbound Left PM Peak = LOS F

and the second sec			Existin	g 2017			
to have been been a		AM Peal	K Hour	PM Peak Hour			
Intersection	LOS	Delay	Movement (LOS)	LOS	Delay	Movement (LOS)	
SB TH 100 / W 77th St	C	27.1		D	51.3	EBT (F)	
NB TH 100 / W 77th St	C	24.5		D	52.6	EBL (F)	
Commercial Access / W 77th St	A	8.2		С	24.5		
Computer Ave / W 77th	В	18.4		С	25.6		
Parklawn Ave / W 77th St	C	20.1		С	31.3		
W 77th St / Minnesota Dr	В	16.3		С	28.4		
France Ave / W 76th St	С	32.1	NBL (F)	D	35.6		
France Ave / Minnesota Dr	С	24.3		Е	55.2	WBL (F)	

Table 9: Existing (2017) Intersection Level of Service

Source: WSB & Associates, Inc.

Note: Based upon criteria shown in Figure 16

C. Forecasted Traffic Operations

A capacity and LOS analysis was also completed for the study area intersections for each land use scenario for the years 2025, which is the anticipated year the proposed Gateway Area development would be completed, and; for the 2040 conditions which represents the City's Comprehensive Plan development time frame.

A summary of the expected traffic operations on the key intersections is displayed in *Table 10* to *13*. Based on the analysis all scenarios will have some operational deficiencies in years 2025 and/or 2040. Deficiencies include operations on France Avenue, the TH 100 interchange, and accesses to the Gateway Study Area. The majority of these locations do not have right or left turn lanes or the existing turn lanes do not have adequate capacity to handle the forecast traffic volumes.

During both the AM peak hour PM peak hours in both 2025 and 2040 assuming with the increase in traffic for each land use scenario, several intersections and movements will be operating at LOS E or F. Specifically, the intersections of 77th Street at the TH 100 ramps, 77th Street at Computer Drive, 77th Street at Parklawn Ave, 77th at Minnesota Drive, France Avenue at 76th Street and France Avenue at Minnesota Street.

100000000000000000000000000000000000000	In and	and a second				2025 AM	Peak H	our					
	Scenario 1			-	Scenario 2			Scenario 3			Scenario 4		
Intersection	LOS	Delay	Mvmt (LOS)	LOS	Delay	Mvmt (LOS)	LOS	Delay	Mvmt (LOS)	LOS	Delay	Mvmt (LOS)	
SB TH 100 / W 77th St	D	50.0		F	+100		F	+100		D	36.2	EBT (F) WBL (F)	
NB TH 100 / W 77th St	D	54.5	EBT (F) EBR (F)	E	77.4		Е	78.9		D	35.4	EBT (F)	
Commercial Access / W 77th St	В	11.8		В	14.5		в	12.3		В	11.1		
Computer Ave / W 77th	В	18.4		С	23.7		В	17.2		C	20.7		
Parklawn Ave / W 77th St	C	23.4	WBT (E) SBL (E)	С	30.0	WBT (E) SBL (E)	F	+100		С	22.2		
W 77th St / Minnesota Dr	В	13.9		В	13.9		В	12.2		В	16.1		
France Ave / W 76th St	D	35.6	NBL (F)	D	38.7	NBL (F)	Е	56.3	NBL (F)	С	33.1	NBL (F)	
France Ave / Minnesota Dr	с	33.3	SBL (E) EBL (E) WBL (E) NBL (F)	D	35.0	SBL (E) EBL (E) WBL (E) NBL (F)	D	40.6	EBL (E) SBL (E) WBL (E) NBL (F)	D	35.0	SBL (E) EBL (E) WBL (E) NBL (F)	

Table 10: 2025 AM Peak Hour Intersection Level of Service

Source: WSB & Associates, Inc.

Note: Based upon criteria shown in Figure 16

						2025 PM	Peak H	our					
for a stand stand stand	Scenario 1				Scenario 2			Scenario 3			Scenario 4		
Intersection	LOS	Delay	Mvmt (LOS)	LOS	Delay	Mvmt (LOS)	LOS	Delay	Mvmt (LOS)	LOS	Delay	Mvmt (LOS)	
SB TH 100 / W 77th St	Е	62.3	EB (F)	Е	59.8	EBT (F)	F	+100		Е	72.0	EBT (F)	
NB TH 100 / W 77th St	Е	68.7	EBL (F) EBT (F) NBL (F)	F	+100		F	+100		Е	66.1	EB (F)	
Commercial Access / W 77th St	D	39.5	EBL (F) NBT (F)	С	31.4	EBL (E) NBT (E) WBT (E)	D	47.4	NBL (F) SBL (F) EBL (F)	С	31.5	NBL (F) EBL (F)	
Computer Ave / W 77th	F	+100		F	+100		F	+100		E	62.5	WBT (F) NBL (F)	
Parklawn Ave / W 77th St	F	+100		F	+100		F	+100		С	27.5		
W 77th St / Minnesota Dr	E	61.4	SBL (F)	Е	66.1	SBL (F)	Е	71.7	SBL (F)	С	2 <mark>4.</mark> 0		
France Ave / W 76th St	F	+100		F	+100		F	+100		F	+100		
France Ave / Minnesota Dr	F	+100		F	+100		F	+100		F	+100		

Table 11: 2025 PM Peak Hour Intersection Level of Service

Source: WSB & Associates, Inc.

Note: Based upon criteria shown in Figure 16

Table 12: 2040 AM Peak Hour Intersection Level of Service

	2040 AM Peak Hour												
Testamontion	Ĩ	Scenar	io 1		Scenar	io 2	1000	Scenar	io 3	-	Scenar	io 4	
Intersection	LOS	Delay	Mvmt (LOS)	LOS	Delay	Mvmt (LOS)	LOS	Delay	Mvmt (LOS)	LOS	Delay	Mvmt (LOS)	
SB TH 100 / W 77th St	D	51.2	the second of	F	+100		F	+100		C	38.0	EBT (F) WBL (F)	
NB TH 100 / W 77th St	E	55.7	EBT (F) EBR (F)	F	82.2		F	84.0		D	41.2	EBT (F)	
Commercial Access / W 77th St	в	12.9		в	16.1		в	12.8		В	11.6		
Computer Ave / W 77th	В	19.6		С	24.1		В	18.0		С	22.6		
Parklawn Ave / W 77th St	С	24.4	WBT (E) SBL (E)	С	30.5	WBT (E) SBL (E)	F	+100		С	23.8	SBL (F) WBT (F)	
W 77th St / Minnesota Dr	В	15.7		В	14.8		В	12.8		В	17.3		
France Ave / W 76th St	D	36.4	NBL (F)	D	40.7	NBL (F)	E	68.3	NBL (F)	с	35.6	SBL (E) EBL (E) WBL (E) NBL (F)	
France Ave / Minnesota Dr	С	38.5	SBL (E) EBL (E) WBL (E) NBL (F)	D	46.8	SBL (E) SBT (E) NBL (F) WBL (F) SBL (F)	D	41.2	SBL (E) SBT (E) NBL (F) WBL (F) SBL (F)	с	37.0	SBL (E) EBL (E) WBL (E) NBL (F)	

Source: WSB & Associates, Inc.

Note: Based upon criteria shown in Figure 16

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Intersection	2040 PM Peak Hour											
	Scenario 1			Scenario 2			Scenario 3			Scenario 4		
	LOS	Dela y	Mvmt (LOS)	LOS	Dela y	Mvmt (LOS)	LOS	Dela y	Mvmt (LOS)	LOS	Dela y	Mvmt (LOS)
SB TH 100 / W 77th St	F	83.8		E	62.1	EBT (F) EBR (F)	F	+100		Е	75.1	EBT (F) EBR (F)
NB TH 100 / W 77th St	Е	77.6	NB (F) EB (F)	F	+100		F	+100		Е	68.3	EB (F)
Commercial Access / W 77th St	D	41.2	NB (F) SB (F)	С	32.9	EBL (E) NBL (E) WBT (E)	D	48.2	NBL (F) SBL (F) EBL (F)	С	33.4	NB (F) EBL (F)
Computer Ave / W 77th	F	+100		F	+100		F	+100		E	63.8	WBT (F) WBR (F) NBL (F)
Parklawn Ave / W 77th St	F	+100	-	F	+100		F	+100		С	28.2	SBT (F)
W 77th St / Minnesota Dr	F	+100		Е	66.5	SB (F)	Е	73.4	SBT (F) SBL (F)	C	26.1	
France Ave / W 76th St	F	+100		F	+100		F	+100	-	F	+100	
France Ave / Minnesota Dr	F	+100		F	+100	0	F	+100		F	+100	-

Table 13: 2040 PM Peak Hour Intersection Level of Service

Source: WSB & Associates, Inc.

Note: Based upon criteria shown in Figure 16

D. Mitigated Traffic Operations

Mitigation improvements that would improve all intersections and movement to an acceptable LOS E or better includes:

2025 Mitigation:

- 1. Addition of a northbound dual right-turn lane at 77th Street and TH 100 SB Ramp.
- 2. Addition of a westbound right-turn lane at 77th Street and TH 100 NB Ramp.
- 3. Addition of a northbound left turn lane, eastbound right turn lane and signal timing improvements at 77th Street and Commercial Access driveway.
- 4. Improved signal timing at 77th Street and Computer Avenue.
- 5. Addition of a northbound dual left turn lane, southbound left turn lane and eastbound right turn lane at 77th Street and Computer Drive.
- 6. Addition of a northbound through lane at France Avenue and Minnesota Street.

2040 Mitigation:

- 1. 2025 Improvements
- 2. Addition of a northbound through lane at France Avenue and Minnesota Drive.
- 3. Addition of a southbound through lane at France Avenue and 76th Street.
- 4. Addition of an eastbound and westbound third lane on 77th Street from TH 100 SB Ramp through Computer Drive.
- 5. Addition of an eastbound and westbound dual left turn lane at France Avenue and Minnesota Street.
- 6. Addition of a southbound left turn lane at 77th Street and Minnesota Street.
- 7. Addition of an eastbound right turn lane at 77th Street and Parklawn Avenue.

Based on the proposed mitigation improvements a capacity and level of service analysis was completed using the projected 2040 traffic volumes for each scenario. The results are shown in *Tables 14* and *15*. The results show that all intersections would be operating at overall LOS D or better for all scenarios in both the AM and PM peak hours.

	2040 AM Peak Hour									
Intersection	Scenario 1		Scenario 2		Scenario 3		Scenario 4			
and a stand	LOS	Delay	LOS	Delay	LOS	Delay	LOS	Delay		
SB TH 100 / W 77th St	D	44.9	D	54.0	D	53.3	D	46.8		
NB TH 100 / W 77th St	С	25.9	C	28.2	D	35.3	С	25.4		
Commercial Access / W 77th St	A	8.0	В	15.6	В	12.4	A	8,1		
Computer Ave / W 77th	А	6.0	В	11.5	A	7.2	А	6.6		
Parklawn Ave / W 77th St	С	29.0	С	22.2	В	16.9	C	26.8		
W 77th St / Minnesota Dr	В	19.1	В	16.1	В	16.4	С	20.6		
France Ave / W 76th St	С	26.7	C	27.2	С	34.8	С	26.2		
France Ave / Minnesota Dr	С	23.2	С	25.2	D	37.4	C	22.4		

Table 14: 2040 AM Peak Hour Mitigation Intersection Level of Service

	2040 PM Peak Hour									
Intersection	Scenario 1		Scenario 2		Scenario 3		Scenario 4			
	LOS	Delay	LOS	Delay	LOS	Delay	LOS	Delay		
SB TH 100 / W 77th St	D	47.7	D	45.9	с	33.2	D	53.2		
NB TH 100 / W 77th St	C	33.4	D	40.7	C	30.1	C	26.3		
Commercial Access / W 77th St	в	10.8	в	19.6	С	27.8	В	14.4		
Computer Ave / W 77th	С	22.4	С	24.7	D	46.5	C	26.2		
Parklawn Ave / W 77th St	D	37.0	С	32.4	D	47.0	D	40.7		
W 77th St / Minnesota Dr	С	30.3	С	23.9	Е	57.4	D	38.9		
France Ave / W 76th St	D	35.8	C	32.7	D	45.0	С	27.1		
France Ave / Minnesota Dr	D	50.5	D	40.8	C	28.8	D	50.8		

Table 15: 2040 PM Peak Hour Mitigation Intersection Level of Service

TRANSIT

Currently there are two forms of transit service within the City of Edina.

Paratransit: Paratransit services are currently provided by Edina Dial-A-Raid Transportation. Door-to-door services provided using a wheelchair lift equipped van on a first-come, first-service basis. Hours of operation are Monday through Friday, 9:00 a.m. to 3:00 p.m., 24-hour advance notice for scheduling is required. Anyone living in Edina is eligible.

Scheduled Transit: The key transit facility in Edina is a Southdale transit center. This is part of the Southdale shopping mall. It includes a covered shelter area with routes and schedule information. The Southdale transit center is one of the busier transit centers in the Twin Cities, with eight transit lines which stop and link at this location. There are also 100 parking spaces at a metro transit Park'n Ride lot at this location.

The existing scheduled services to Edina residents is depicted in Table 16.

Route	Service Route/Area	Service Description
6	Edina (includes Southdale Transit Center), Uptown, downtown Minneapolis, University of Minnesota	High frequency local service, all day/evening, all week; 5-15 minute headways
46	Edina (includes 50 th /France), south Minneapolis, St Paul	Local service all day/evening, all week: 30-60 minute headways
114	Edina (includes Southdale Transit Center), south Minneapolis, Uptown University of Minnesota	Commuter/student service during a.m. and p.m rush hours, weekdays
146	Edina (Vernon Ave.), southwest Minneapolis, downtown Minneapolis	Commuter express (I-35W) service during a.m. and p.m. rush hours, weekdays
152	Edina (includes Southdale Transit Center), Lake Street, University of Minnesota	Commuter/student express (I-35W) service during a.m. and p.m. rush hours, weekdays
515	Edina (Includes Southdale Transit Center), Richfield, South Minneapolis, Bloomington (includes Mall of America), Veterans Medical Center (alternate route)	Local service, all day/evening, all week; 10-30 minute headways
538	Edina (includes Southdale Transit Center), Bloomington (includes Mall of America)	Local service, all day/evening, all week; 30-60 minute headways
539	Edina (includes Southdale Transit Center), Bloomington (includes Normandale Community College, Mall of America)	Local service, all day/evening, all week; 30-60 minute headways
540	Edina, Richfield (includes Best Buy Headquarters), Bloomington (includes Mall of America)	Local service, all day/evening, all week; 15-30 minute headways during a.m./p.m. rush hours, otherwise 30-60 minute headways
568	Downtown Minneapolis, south Minneapolis, Edina, Minnetonka (Opportunity Partners)	Weekdays only, one a.m. run from Minneapolis to Opportunity Partners; one p.m. run from Opportunity Partners to Minneapolis
578	Edina (includes Southdale Transit Center), downtown Minneapolis	Commuter express service (TH 62 and I-35W) during a.m. and p.m. rush hours
587	Edina, downtown Minneapolis	Commuter express service (TH 100 and I-394) during a.m. and p.m. rush hours, weekdays
631 Southwest)	Chanhassen, Eden Prairie, Edina (Southdale Transit Center)	Weekday service, morning through evening; approximately 10 runs per day each direction

Table 16 - Existing Scheduled Transit Service in Edina

Note: all routes are Metro Transit with the exception of 631, which is Southwest Metro Transit.

NON-MOTORIZED TRANSPORTATION

Sidewalks and other pedestrian facilities are important components of Edina's transportation infrastructure. Sidewalks and paths provide safe movement for individuals of all ages, decrease dependence on motor vehicles, and encourage active lifestyles. An important key to an effective municipal sidewalk system is in providing networking continuity such that there is broad geographic coverage for a range of users without notable gaps. The Gateway Study Area is a critical link in Edina's non-motorized transportation system.

The City of Edina's 2008 Comprehensive Plan includes a variety of Travel Demand Management (TDM) and non-motorized vehicle transportation (transit, pedestrian/bike facilities) policies and guidelines for development of these facilities or expanded facilities. However, as development continues to grow, specifically in the Gateway Study Area, consideration of site-specific improvements as developments are proposed needs to be included. These would include upgrading the existing bus shelters to become ADA compliant and improvements of sidewalk and/or path connections.

AUAR MITIGATION REQIREMENTS

Many of the mitigation measures outlined in the 2007 AUAR still remain valid. The updated mitigation measures are outlined below and either remain in effect from the 2007 AUAR or have been updated based on new analysis as noted. Each mitigation includes if the improvement is needed with the no-build or build (with Gateway Area development) conditions and what year (2025 or 2040) the improvement is required.

Based upon the analysis, deficiencies exist for all scenarios near the TH 100/West 77th Street Interchange, on 77th Street and on France Avenue. Intersection signal timing was first modified to provide optimal operations in each scenario. Mitigation strategies were developed for each scenario are listed below.

Scenarios 1 and 4:

The following mitigation strategies are needed for Scenario 1 and 4 to accommodate both 2025 and 2040 traffic projections:

France Avenue at West 76 th Street
Extend one southbound thru lane on France Avenue to create a total of four thru lanes
2040 No-Build
Northbound TH 100 at West 77 th Street
Add 150 foot northbound right turn lane on Frontage Road Westbound dual right turn lanes on West 77 th Street
2040 Full Build
Edina Industrial Boulevard / West 78th Street
Eastbound dual left turn lanes on West 78th Street
2040 Full Build
Edina Industrial Boulevard / Metro Boulevard
Add southbound right turn lane on Metro Boulevard, restriping the existing two southbound lanes to accommodate an exclusive left turn lane, and a thru/left lane, providing dual left turn lanes. Add 300-foot eastbound left turn lane on Edina Industrial Boulevard
2025 No-Build

Scenario 2:

Scenario 2 will require all the improvements listed above in addition to the following:

Intersection:	Minnesota Drive at France Avenue
Improvement:	Dual westbound left turn lanes on Minnesota Drive
5101 2 (1.01) (1.01)	Eastbound dual left turn lanes on Minnesota Drive
Need By:	2040 Full Build



Intersection:	Northbound TH 100 at West 77th Street
Improvement:	Add 150 foot eastbound right turn lane on West 77 th Street
Need By:	2025 Build
Intersection:	Computer Avenue at West 77 th Street
Improvement:	Northbound dual left turn lanes on Computer Avenue
Need By:	2040 Full Build
Intersection:	Minnesota Drive / Johnson Avenue at West 77 th Street Avenue
Improvement:	Southbound dual left turn lanes on West 77 th Street
Need By:	2040 Full Build

Scenario 3:

Scenario 3 will require **all the above improvements listed under Scenarios 1, 2, and 4** in addition to the following:

Intersection: Improvement:	Minnesota Drive at France Avenue Eastbound dual right turn lanes on Minnesota Drive
Need By:	2040 Full Build
Intersection:	France Avenue at West 78 th Street
Improvement:	Westbound dual right turn lanes on West 78th Street
Need By:	2040 Full Build
Interchange:	TH 100 at West 77 th Street
Improvement:	Six-lane section from Metro Boulevard to Computer Avenue
	Dual left turn turns at both TH 100 Ramps (Hence an eight-lane
	bridge)
Need By:	2040 Full Build

Transit/Non-Motorized Transportation

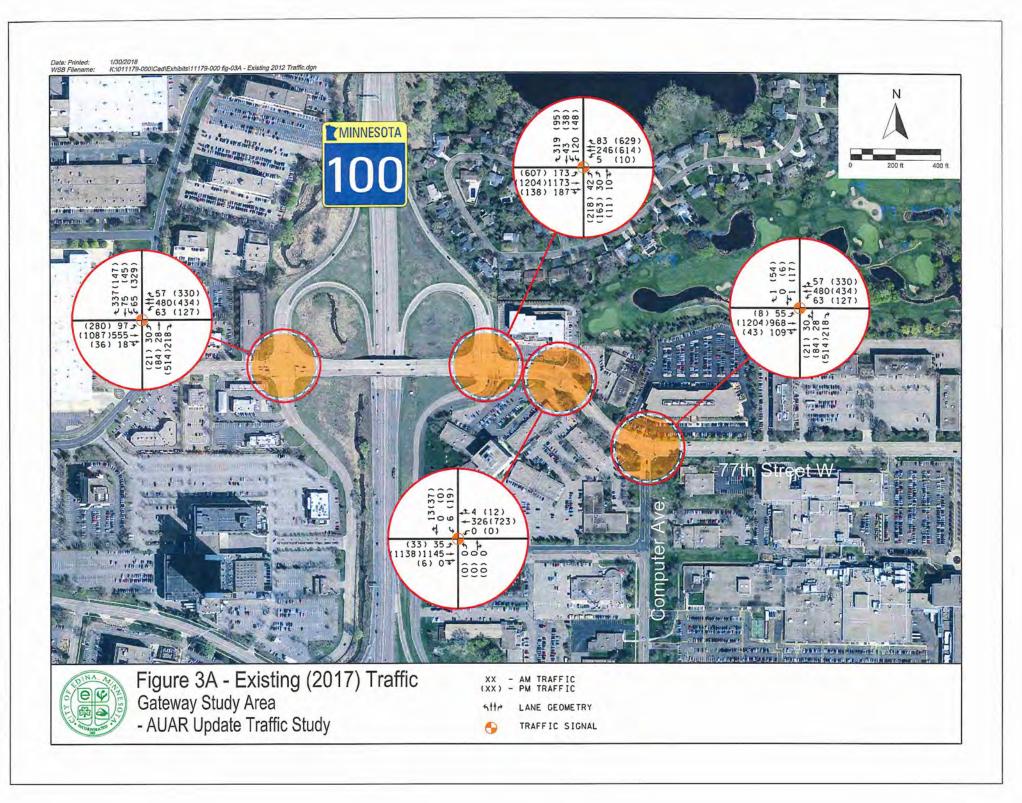
As redevelopment occurs in the Gateway Study Area, consideration of site-specific improvements needs to be included as developments are proposed. These would include upgrading the existing transit facilities, including bus shelters, to become ADA compliant and improving the sidewalk and/or path connections in and around each redevelopment.

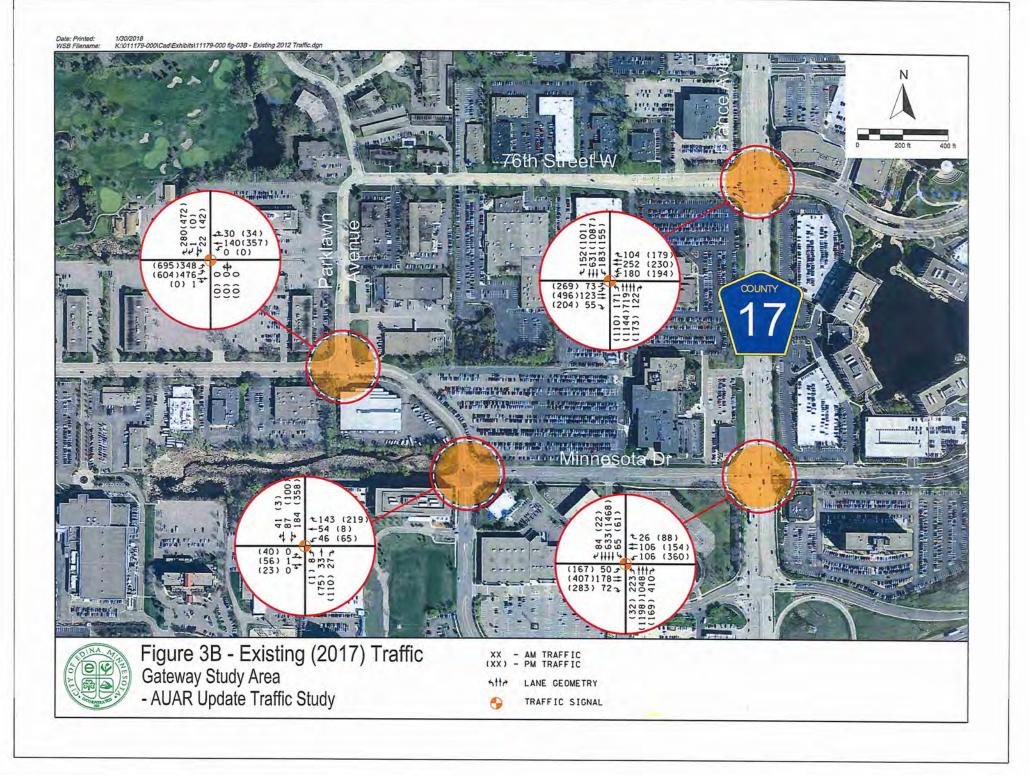
General

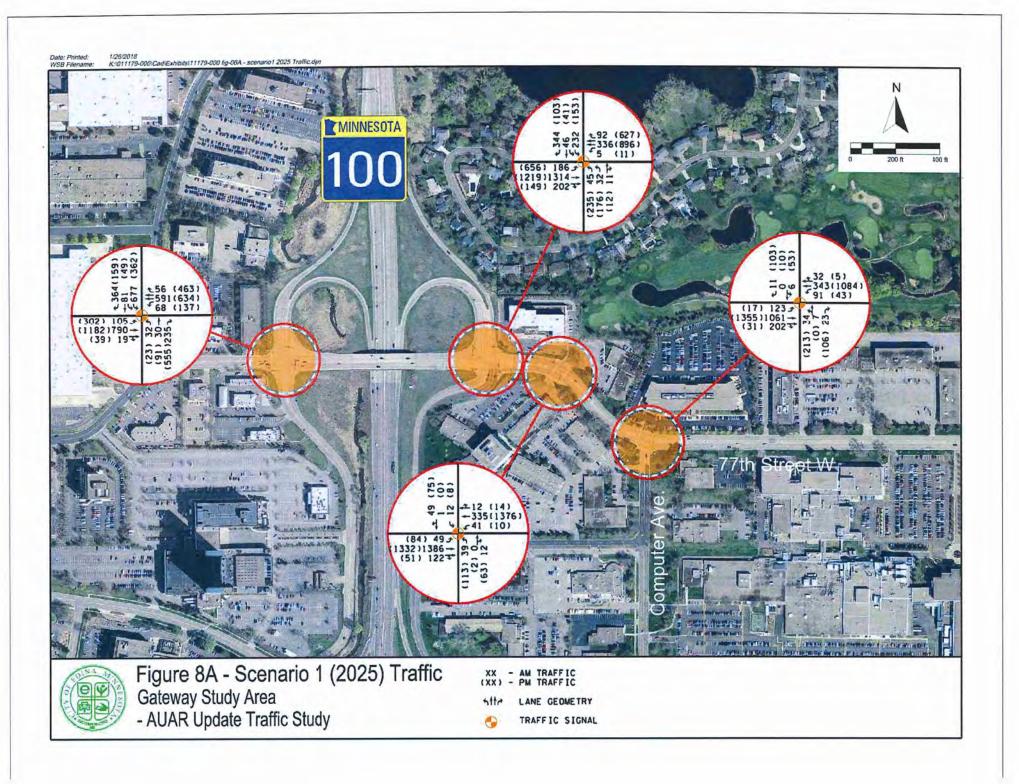
The mitigation measures discussion above are needed to address full build-out of the site and surrounding area. Specific mitigation measures required for proposed development plans will be established through traffic and transportation studies required for each development proposal. These proposals will need to document compliance with the Final AUAR and Mitigation Plan.

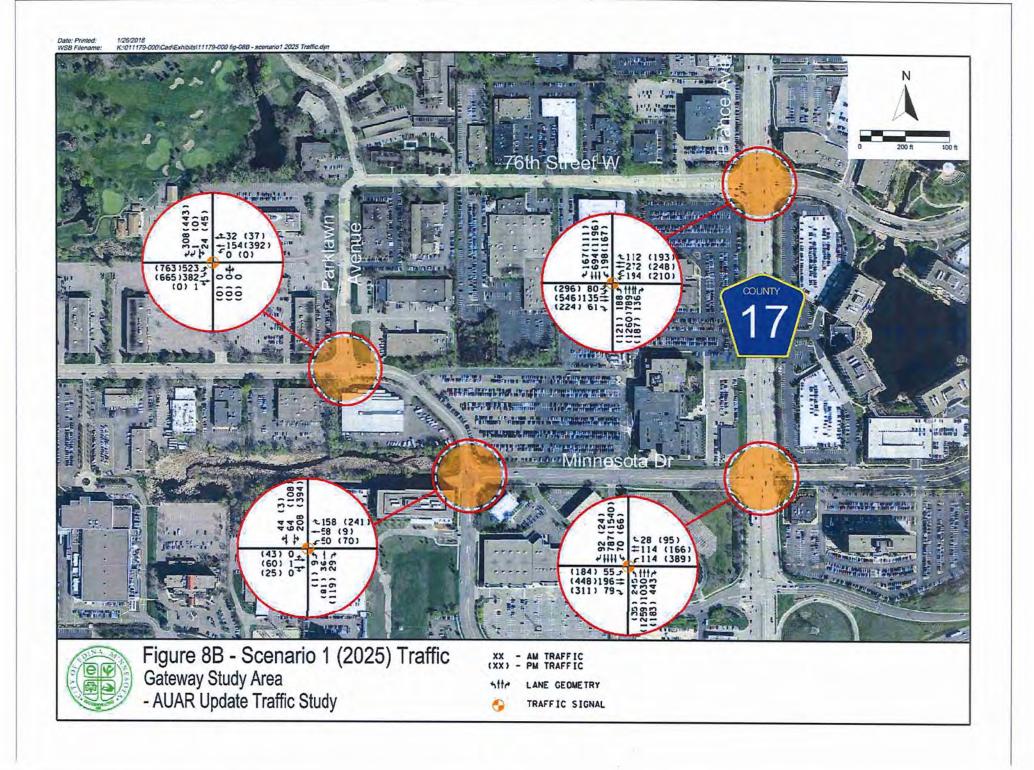


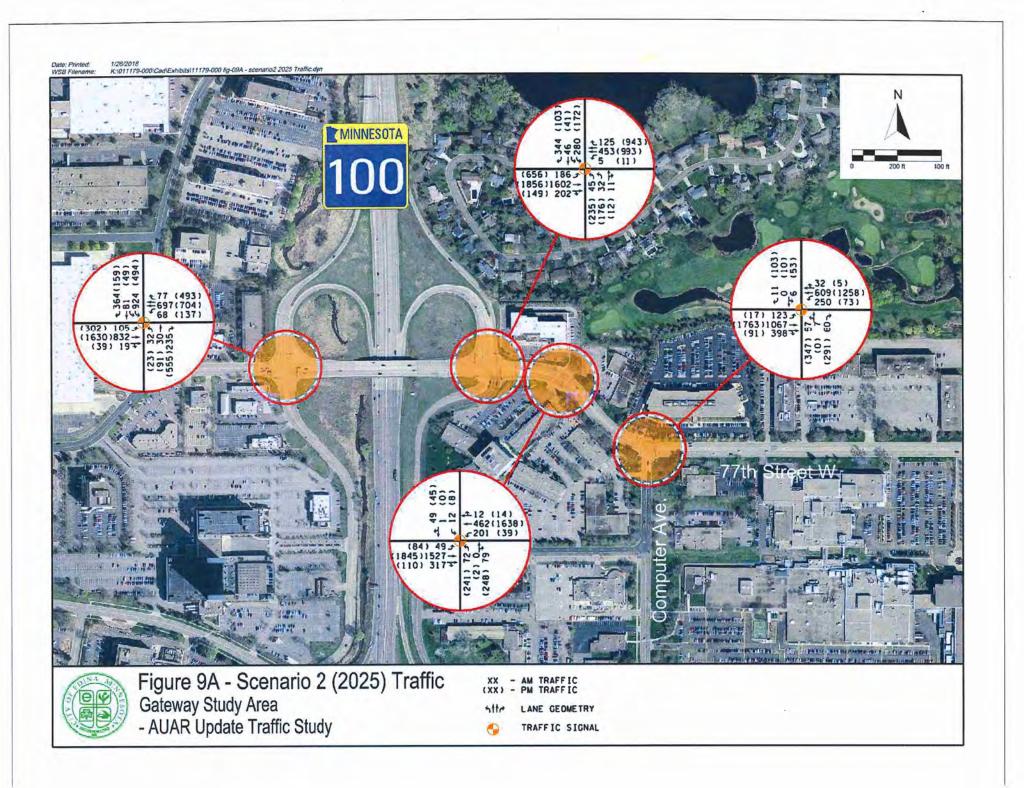
APPENDIX

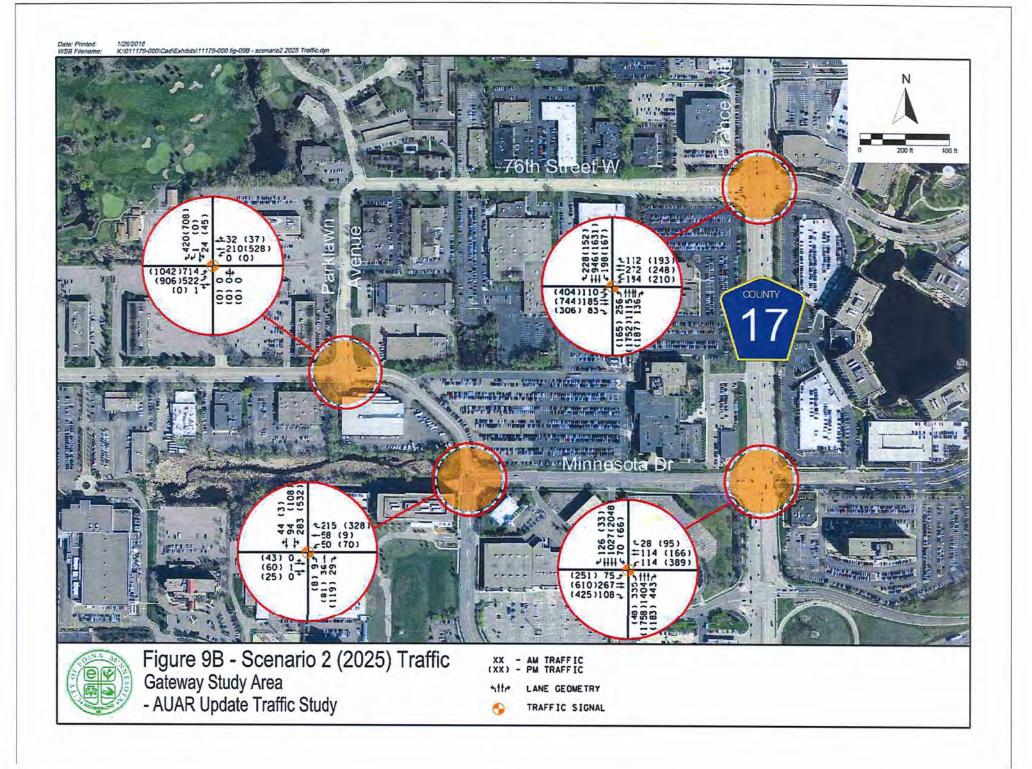


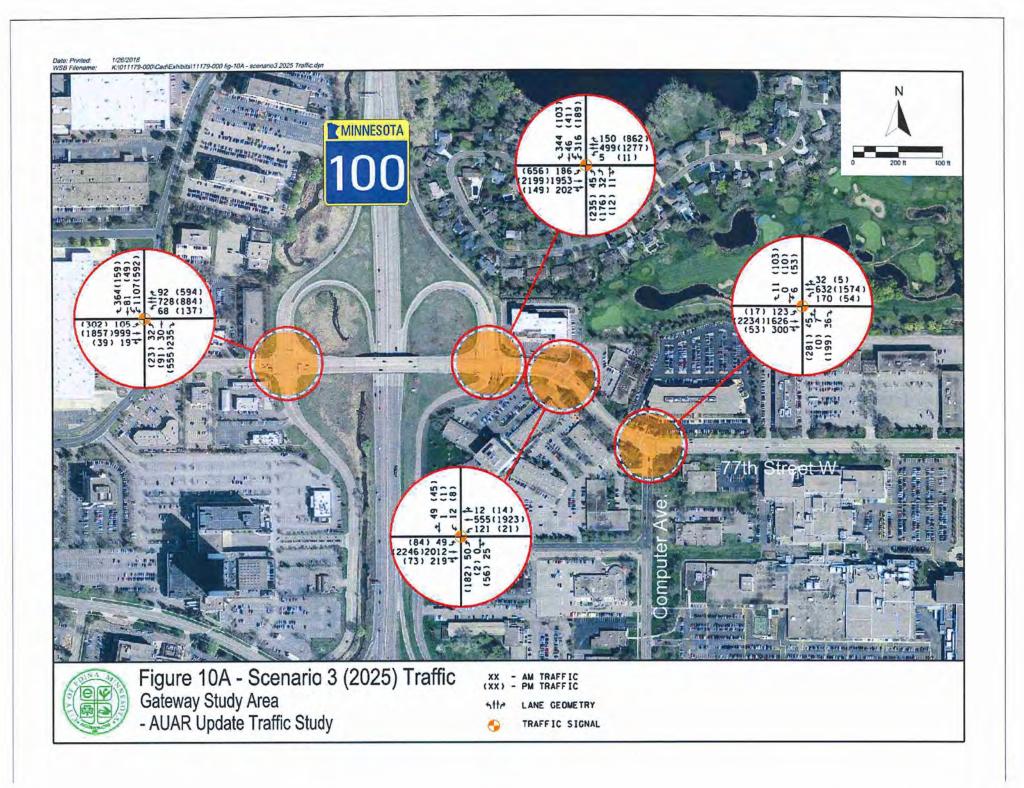


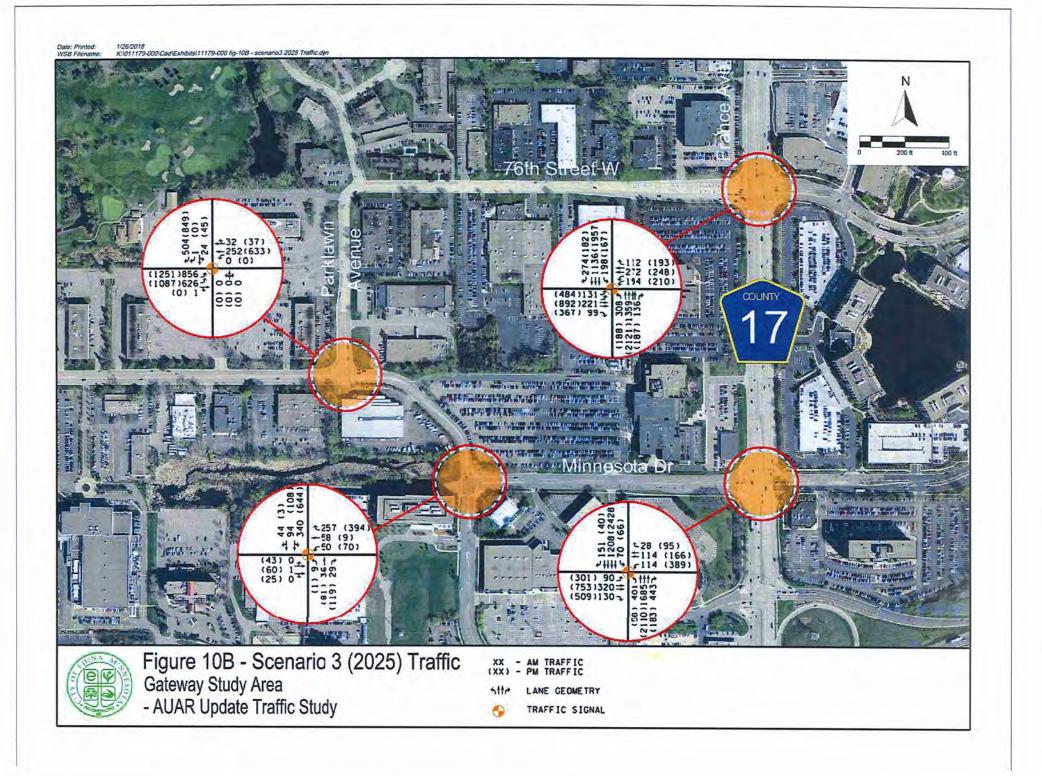


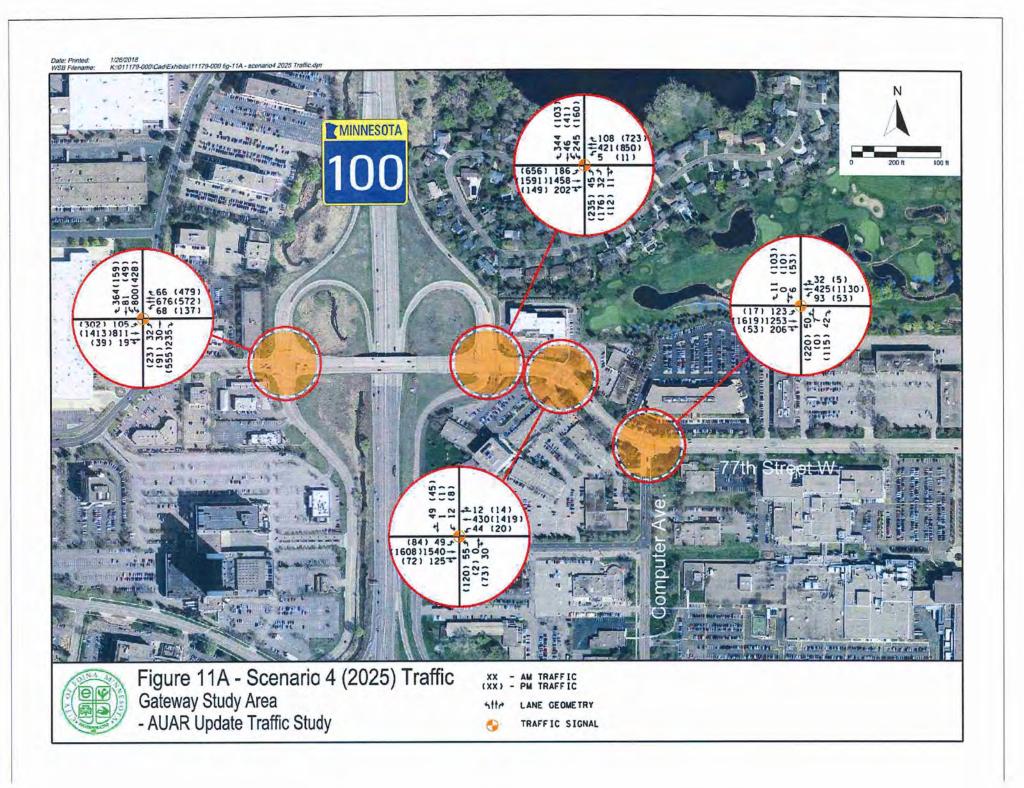


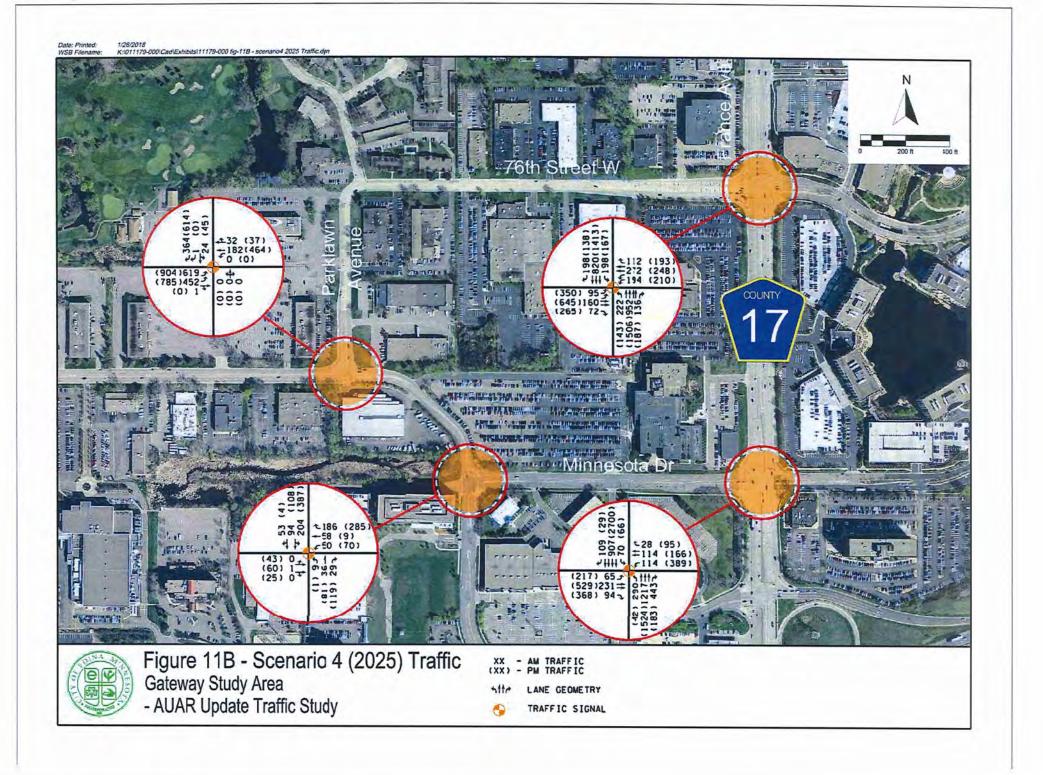


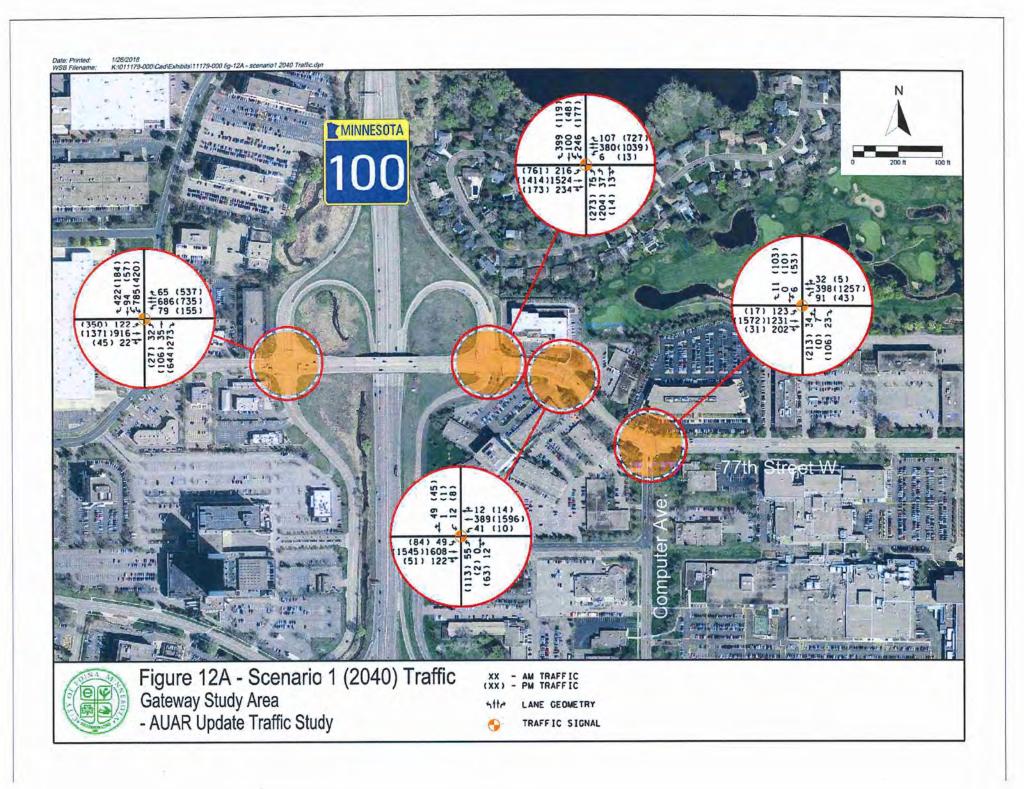


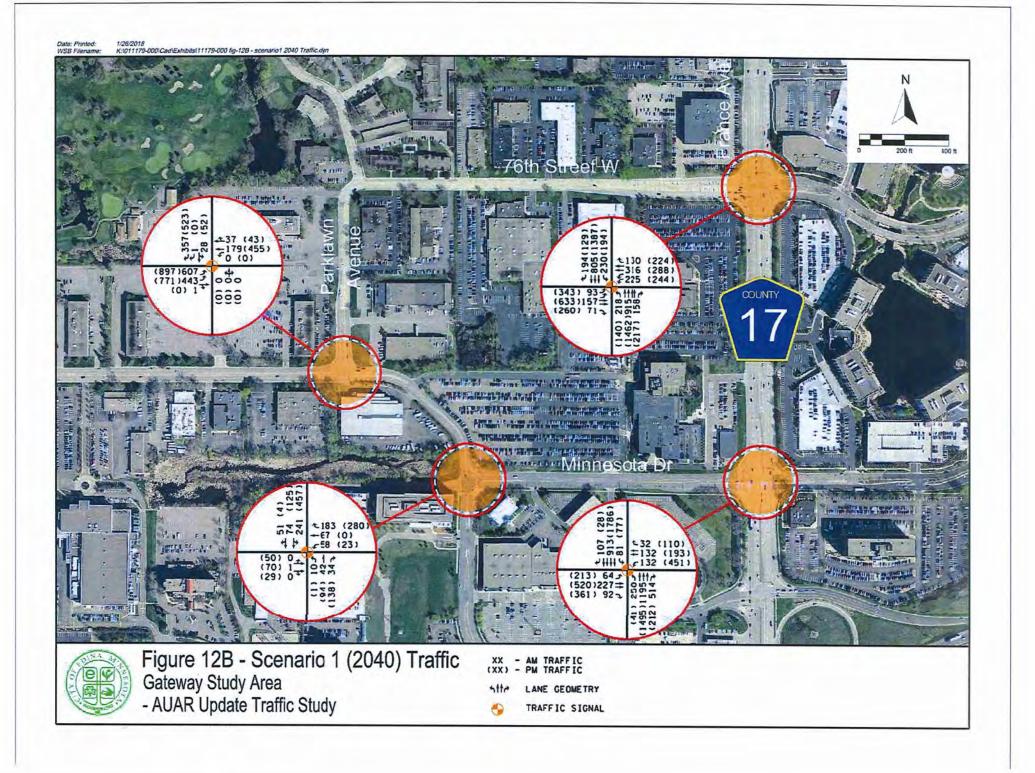


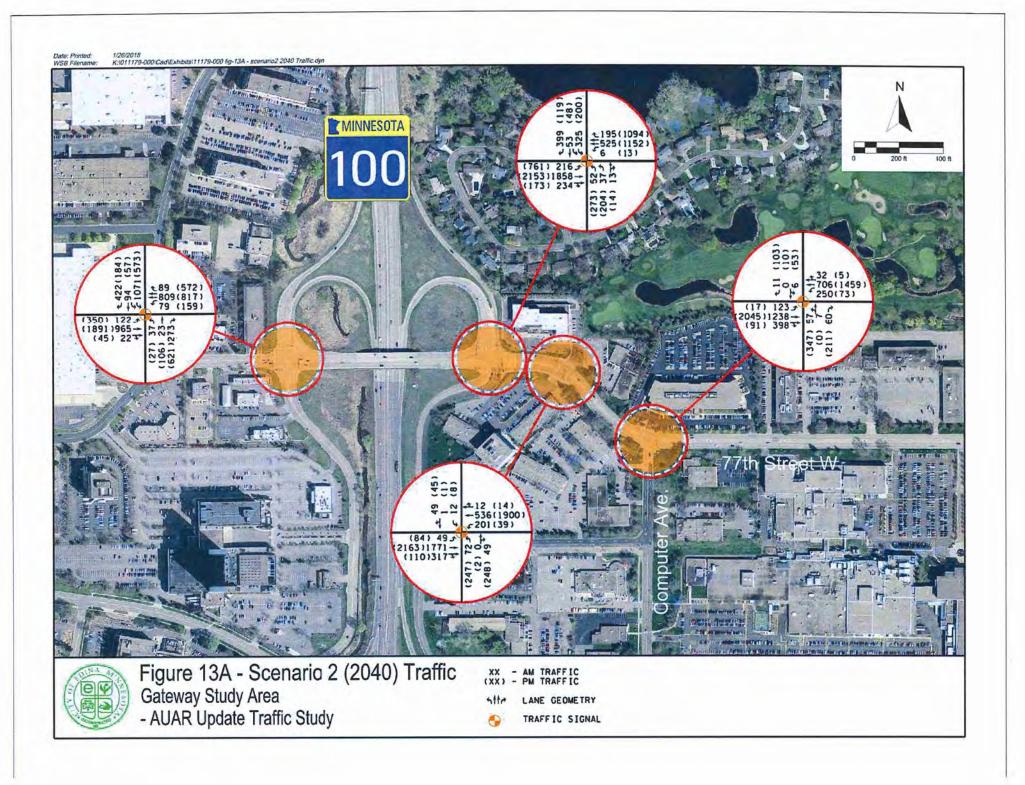


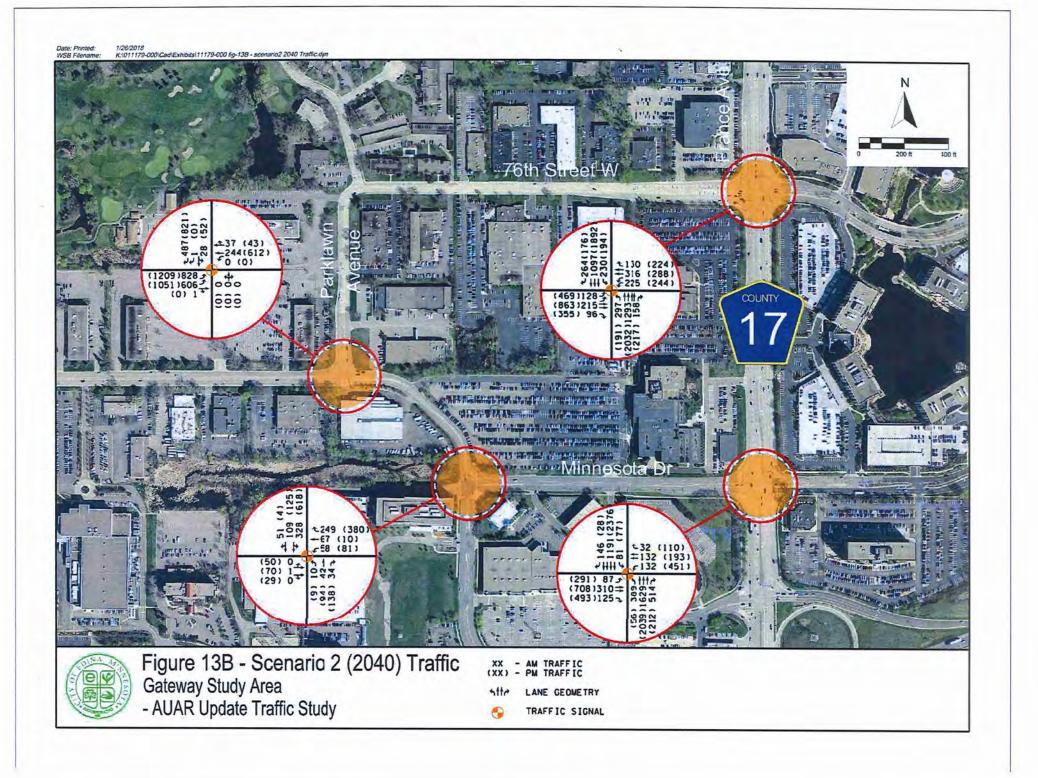


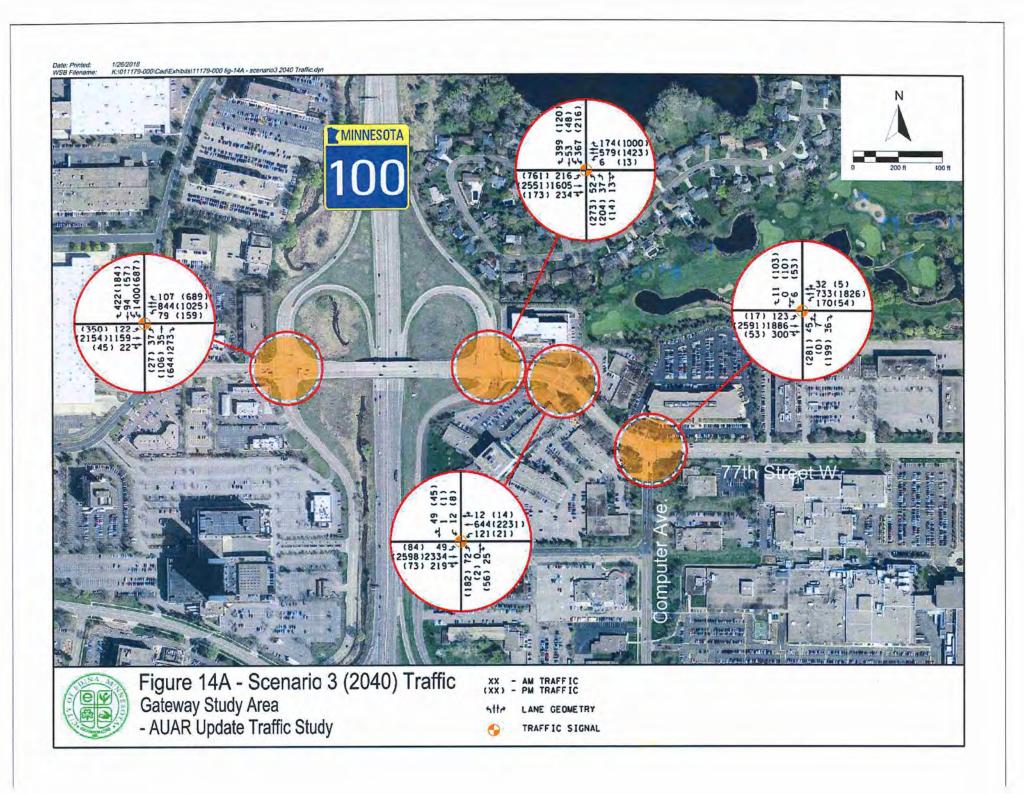


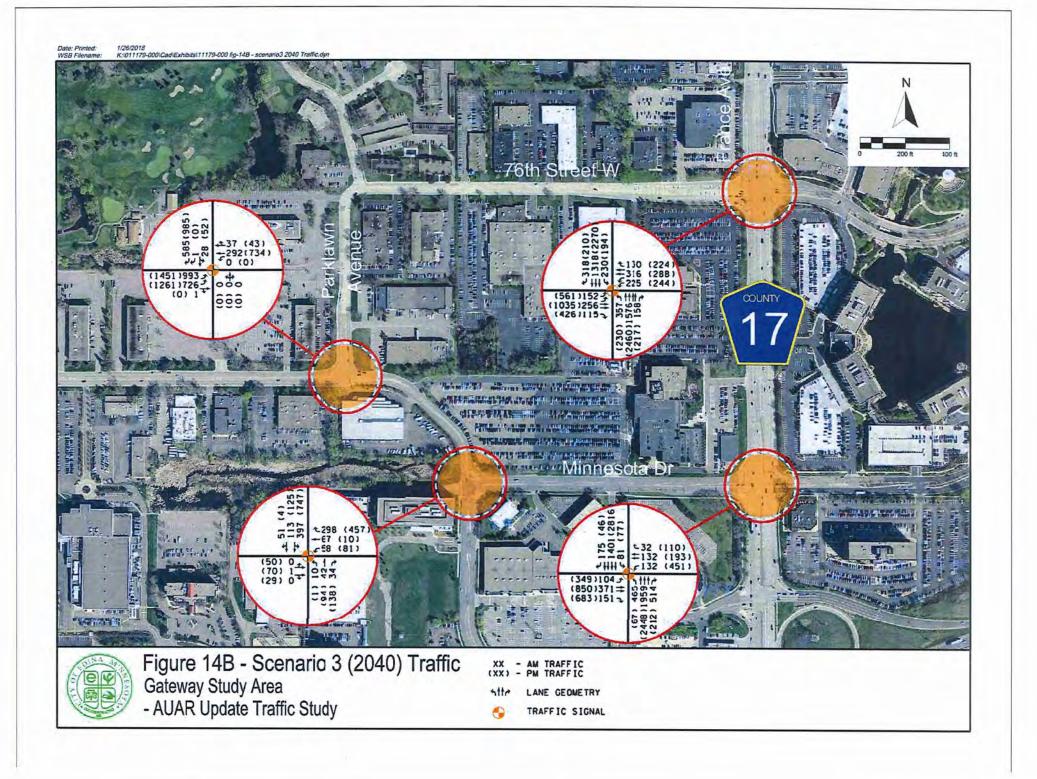


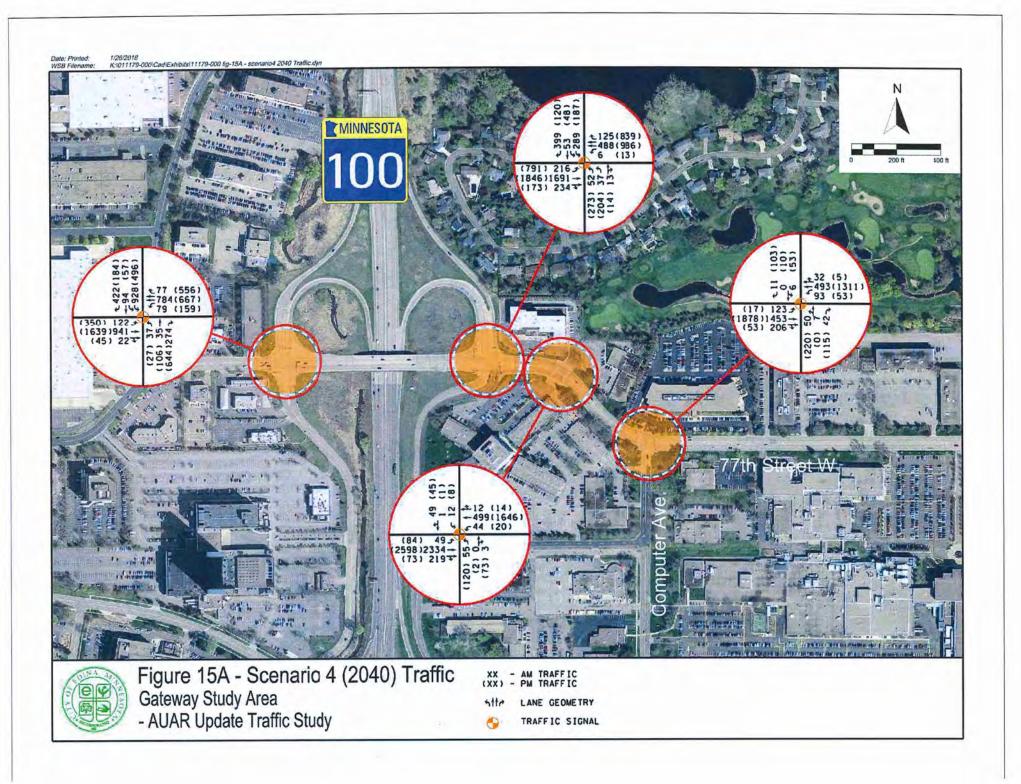


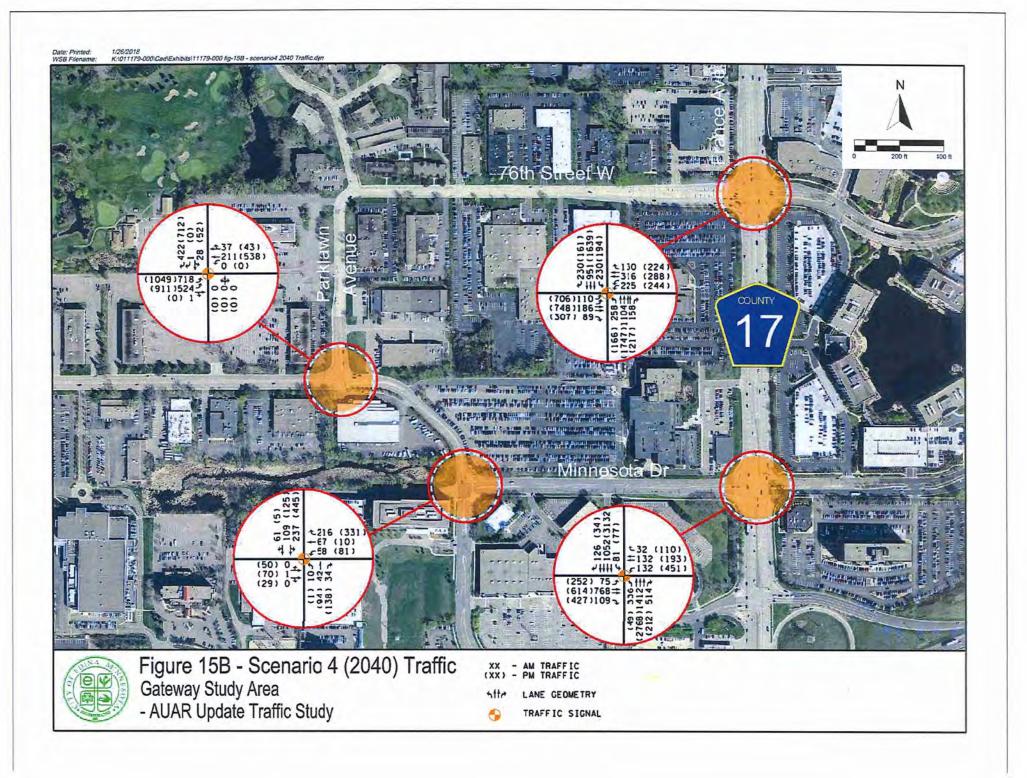












ORDINANCE NO. 2022-__ AN ORDINANCE AMENDING THE ZONING ORDINANCE TO REVISE THE PUD-17, PLANNED UNIT DEVELOPMENT-17 ZONING DISTRICT

The City Of Edina Ordains:

Section I. Chapter 36, Article VIII, Division 4 is hereby amended as follows:

Sec. 36-507 Planned Unit Development District-17 (PUD-17) Pentagon Park South

(a) Legal description:

See attached Exhibit A

- (b) Approved Plans. Incorporated herein by reference are the re-development plans, including the master development plan for the site received by the City on January 17, 2019, except as amended by City Council Resolution No. 2019-17 on file in the Office of the Planning Department.
- (c) Revised Plans. Incorporated herein by reference are the revised plans for 4911 77th Street West as approved by City Council Resolution No. 2021-61 on file in the office of the Planning Department.
- (d) Principal Uses:

All uses allowed in the MDD-6 Zoning District Multi-family Apartments/Condos.

(e) Accessory Uses:

All accessory uses allowed in the MDD-3-6 Zoning District.

(f) Conditional Uses:

All conditional uses allowed in the MDD-3-6 Zoning District.

(g) Development Standards.

	Required
Setbacks - Buildings 77 th Street Computer Avenue Normandale Boulevard Viking Drive	15 feet 15 feet 50 feet 10 Feet
Building Height	12 stories & 144 feet per the height overlay district
Parking lot and drive aisle setback	20 feet
Building Coverage	30%
Maximum Floor Area Ratio (FAR)	1.5%
Parking Stalls – Mixed Development District Phase I (Based on the uses)	602 stalls + 152 enclosed stalls for the residential use on Lot 3, Block 1

- (h) Signs shall be regulated per the Mixed Development District. Signs for the First Bank and Trust located on Lot 4, shall be allowed per their approved Site Plan, including the non-street front wall signage facing the interior of the development.
- (i) Ninety percent (90%) of the parking stalls within the development shall be available to all uses within the development.
- (j) Ten percent (10%) of the units must be provided for affordable housing for families or individuals earning 60% area median income (AMI) for the Twin Cities.

First Reading:

Second Reading:

Published:

Attest:

Sharon Allison, City Clerk

James B. Hovland, Mayor

Please publish in the Edina Sun Current on:

Send two affidavits of publication.

Bill to Edina City Clerk



PROPOSED UPDATED MASTER PLAN

PENTAGON VILLAGE APARTMENTSEdina, MinnesotaJuly 29, 2021 | PAGE 421-0068



MASTER PLAN UPDATE



VIEW FROM THE NW

PENTAGON VILLAGE APARTMENTS Edina, Minnesota July 29, 2021 | PAGE 2 21-0068



PENTAGON VILLAGE - RENDER





Respondent No: 1 Login: Anonymous

Email: n/a

 Responded At:
 Oct 06, 2022 06:13:47 am

 Last Seen:
 Oct 06, 2022 06:13:47 am

 IP Address:
 n/a

Q1.	First	and	Last	Name
Q. I.	1 11 51	unu	Luot	itunic

Avery Ticer

Q2. Address

Cushman & amp; Wakefield, Executive Director of Capital Markets Group

Q3. Comment

I am writing to express my support for the pending Site Plan Application for Lot #4 at Pentagon Park for First Bank & amp; Trust to expand its growing operation. As a resident of Edina for the past 20 years and father of three (3) young children, I am fully in support of this expansion and am confident it will be a tremendous value-add to Pentagon Park and the City of Edina. I drive by the proposed location multiple times per day and firmly believe that the proposed development will greatly enhance the overall area and fits with the vision at that location. The renderings fit nicely with the area, and it looks like First Bank & amp; Trust have gone above and beyond to make the final project aesthetically attractive at greater expense to them. Their growth and desire to expand their footprint at this location demonstrates the vibrancy in our community and successful firms who operate in the city. I hope the City of Edina appreciates the lengths First Bank & amp; Trust is undertaking to make the building align with the greater plan for Pentagon Park and how taxpayers in Edina want to see more projects like this get support and approved. We should all be in favor of this expansion particularly given the uncertainty in the current economic environment. I hope the City of Edina will be supportive of the economic growth that this project represents.

Respondent No: 2 Login: Anonymous Email: n/a	Responded At: Oct 19, 2022 09:40:53 am Last Seen: Oct 19, 2022 09:40:53 am IP Address: n/a	
Q1. First and Last Name	Erik Kvalseth	
Q2. Address	7465 W Shore Dr	
Q3. Comment		

I am opposed to the internally facing sign. They're already on a corner. Seems like two signs is plenty. They should be able to either point one of the two approved signs into the development or use the bank's windows. It's not like the internally facing sign needs to be seen from far away.



October 13, 2022

Statement of Support First Bank & Trust Relocation to Pentagon Village Lot #4 Edina Chamber of Commerce

To Whom It May Concern:

On behalf of the Edina Chamber of Commerce, and Explore Edina, we wish to express our full support of the First Bank & Trust Relocation to Pentagon Village Lot #4 by Hillcrest Development and HTG Architects. We understand the bank has a need to expand its space for growing operations and believe it will be a valuable addition to the overall Pentagon Village development.

Great care has been taken from a design perspective on integrating the bank architecturally with the existing buildings at Pentagon Village, including the apartment development on Lot #3 currently under construction, and the soon to be started dual flag hotel for Lot #2.

We support this project for the following reasons:

- Two stories in height is consistent with the previously approved plan for Lot #4 at Pentagon Village.
- It appears the architects have done a fine job of incorporating design elements quality building materials while balancing the needs for security and safety for a full-service banking operation.
- An internal drive through and parking concept has been incorporated, allowing the architecture to be the focus.

Thank you very much for your time and consideration. We appreciate your support of our growing business community in Edina.

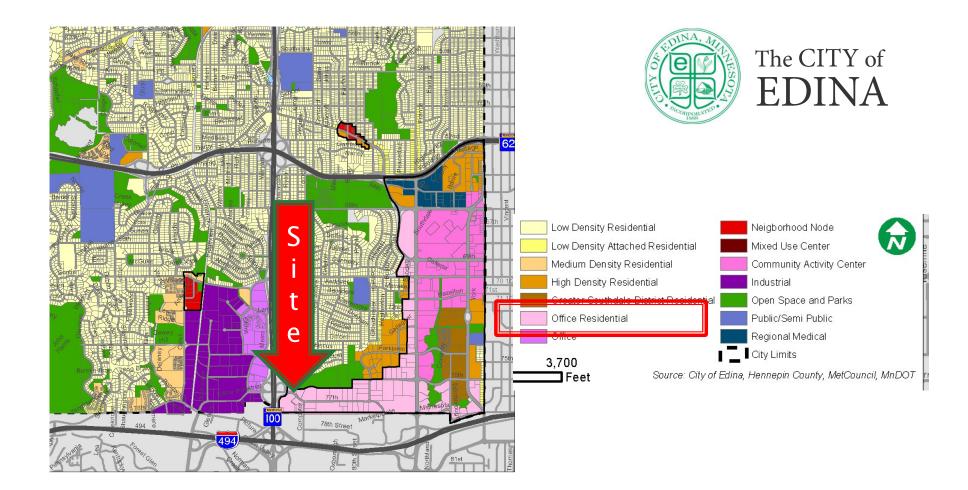
Sincerely,

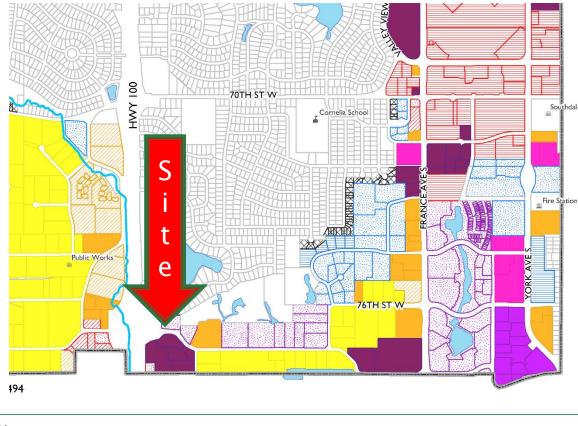
Shelly Loberg Vice President Edina Chamber of Commerce

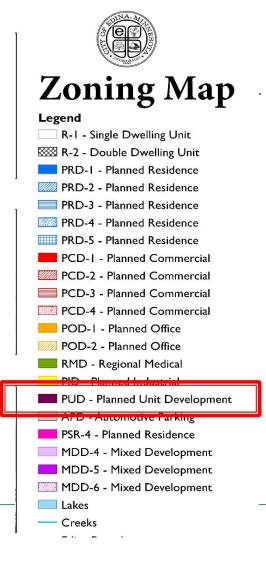


Zoning Ordinance Amendment & Site Plan Review – Pentagon Village, First Bank and Trust









EdinaMN.gov

3



VIEW FROM THE NW

PENTAGON VILLAGE APARTMENTS





DSED UPDATED MASTER PLAN



EXTERIOR RENDERING AT MAIN ENTRY



EXTERIOR RENDERING AT NIGHT

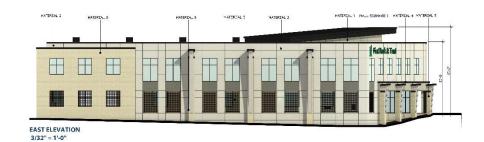






EXTERIOR RENDERING FROM DRIVE-UP INTERIOR

FIRST BANK & TRUST - PENTAGON VILLAGE





MATERIAL 5

NORTH ELEVATION 3/32" = 1'-0"



MATERIA MATERIAL 2 MATERICAL 5 MATERIAL # MATERIAL 6 MATERIAL 7 MATERIA, 5 +++++ BELGE STONE BLACK ALIMINIUM (STORE=RONT AND CC=ING) MEDIJM GRAY METAL PANEL LISH" GRAY METAL PANEL BLACK METAL VISIBILITY SCREEN TRANSLUCENT STOREFRONT GLAZING INTERNALLY ILLIMINATED DECORATIVE FIN W/SOFT LIG-TINS

NORTHWEST ELEVATION 3/32" = 1'-0"



EXTERIOR ELEVATIONS

EIDET DANIZ 9. TOLICT DENITACON VILLACE

This Request Requires:

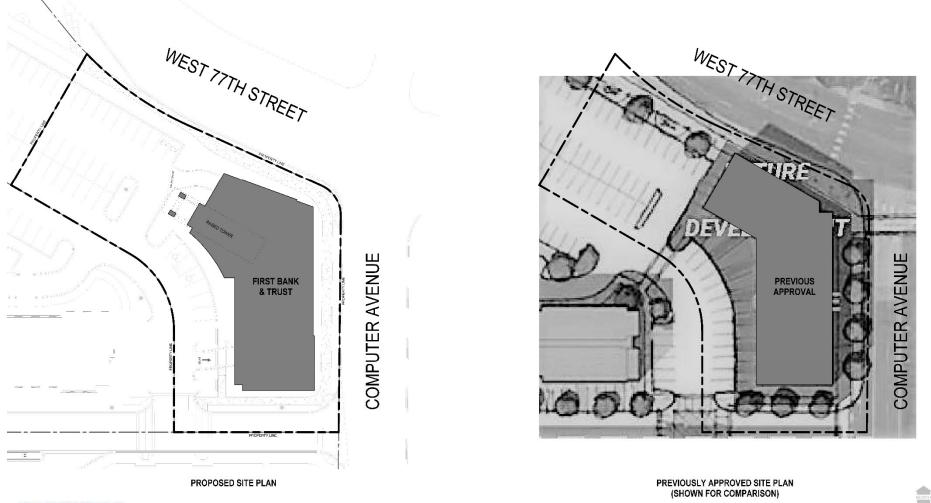


- 1. Site Plan review; and
- 2. Ordinance amendment to allow interior facing signage.

Review of the Site Plan

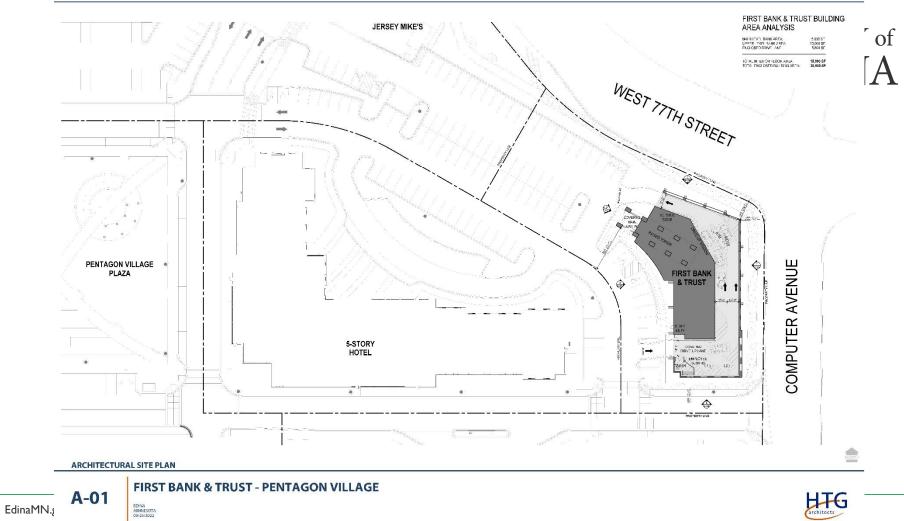


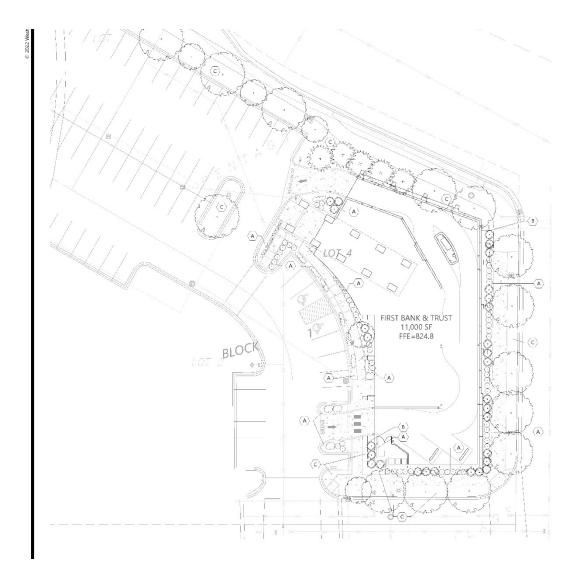
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COMPARISON SITE PLAN







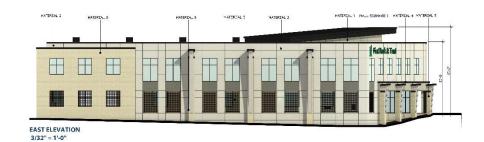


The CITY of EDINA

16



	PUD-17	Proposed
Setbacks - Buildings		
77 th Street	I5 feet	l6 feet
Computer Avenue	I5 feet	I5 feet
Normandale Boulevard	50 feet	NA
Viking Drive	10 Feet	NA
Building Height	12 stories & 144 feet per the height overlay district	2 stories
Parking lot and drive aisle setback	20 feet	20+ feet
Maximum Floor Area Ratio (FAR)	1.5%	1.5%



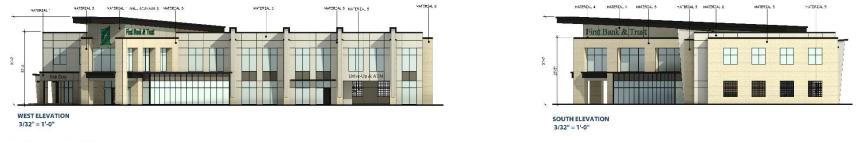


NORTH ELEVATION 3/32" = 1'-0"





3/32" = 1'-0"



EXTERIOR ELEVATIONS

EIDET DANIZ 9. TOLICT DENITACON VILLACE



Better Together Edina



The CITY of **EDINA**

EdinaMN.gov

Recommendation



The CITY of EDINA

EdinaMN.gov

EXISTING SITE CONDITIONS

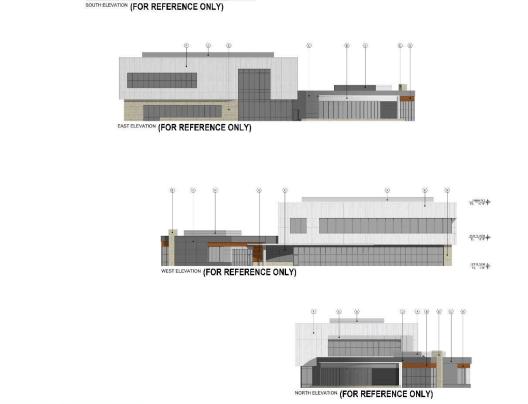


ADJACENT SITES

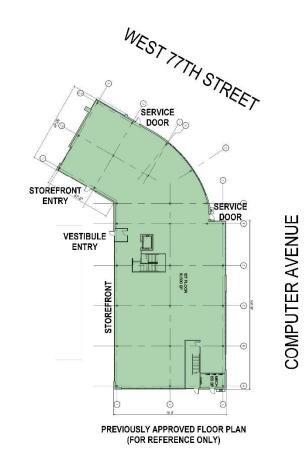


PROJECT LOCATION PHOTOGRAPHS

E CONTRACTOR OF CONTRACTOR



NETAL PAVEL NETAL PAVEL NEC+ SORIEN FALICINOUS NETAL PASEL



I.

PREVIOUSLY APPROVED ELEVATIONS (APPROVED DECEMBER 2018 LOT 4 DRAWINGS INCLUDED FOR REFERENCE ONLY)



The CITY of EDINA

EdinaMN.gov

architects











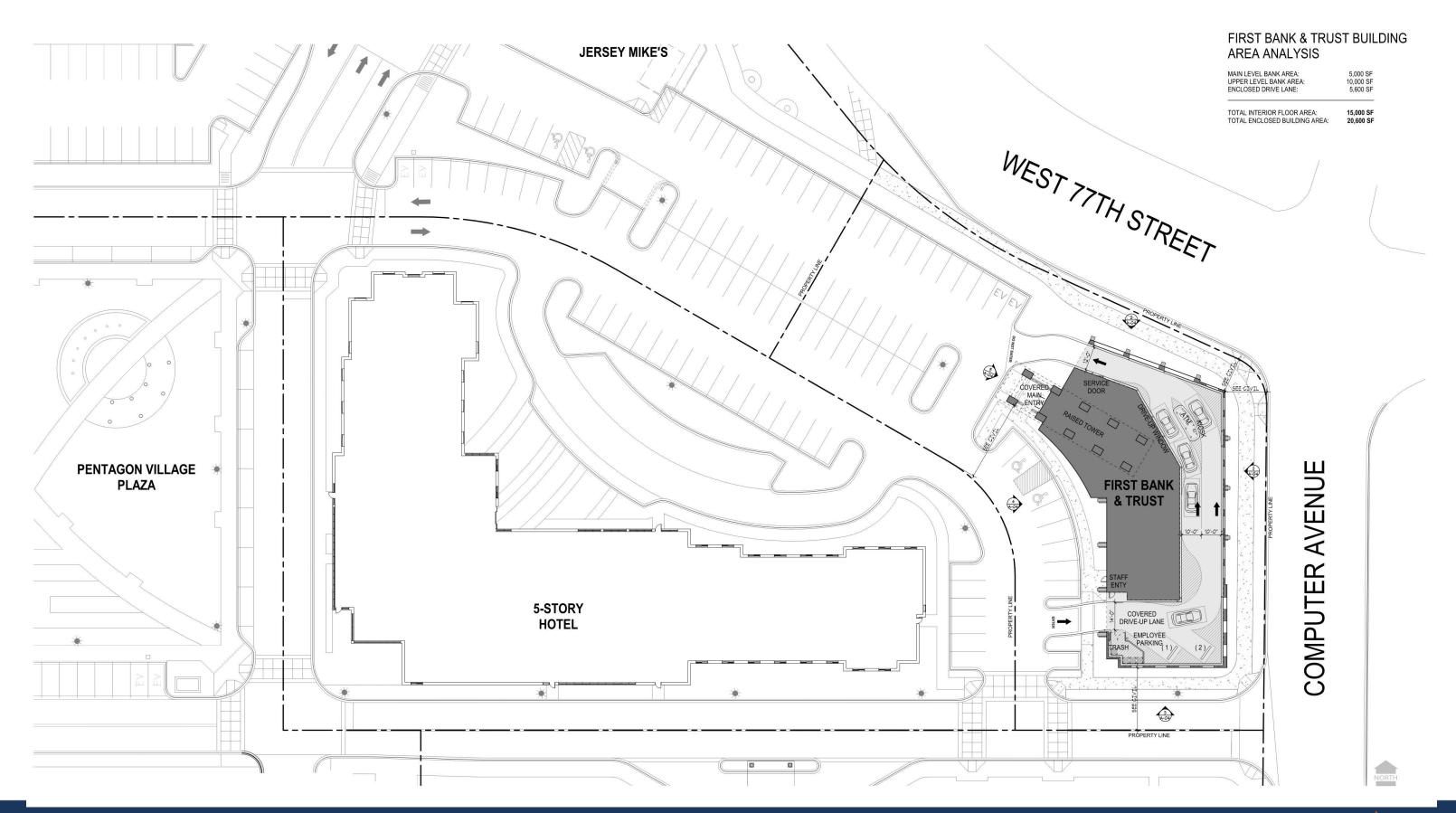
First Bank & Trust





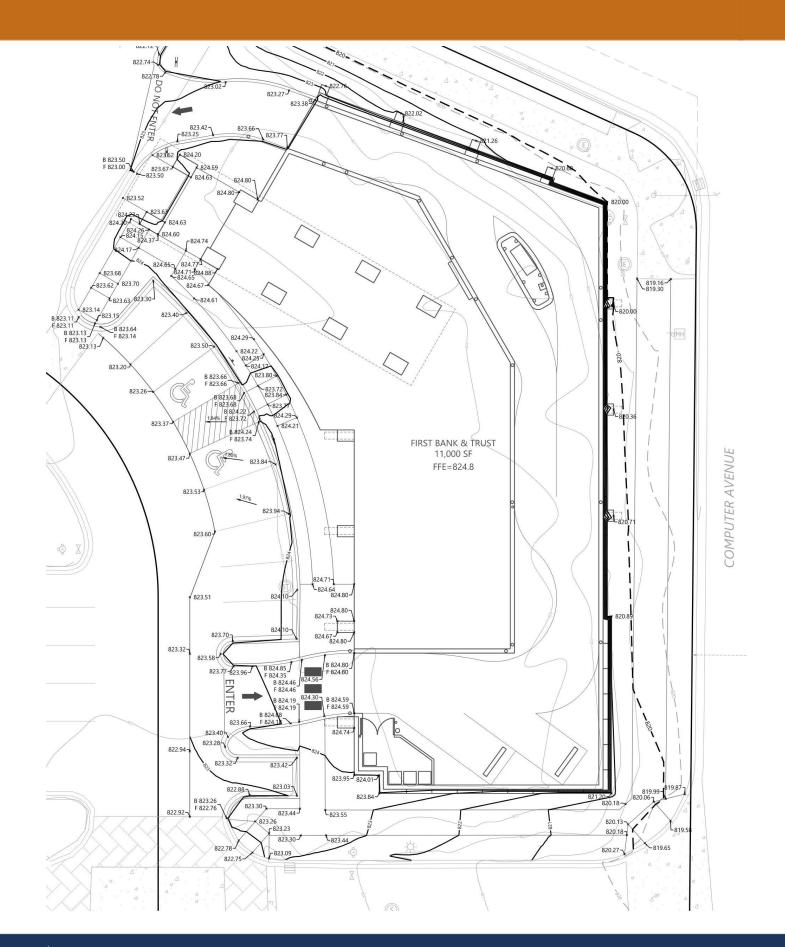


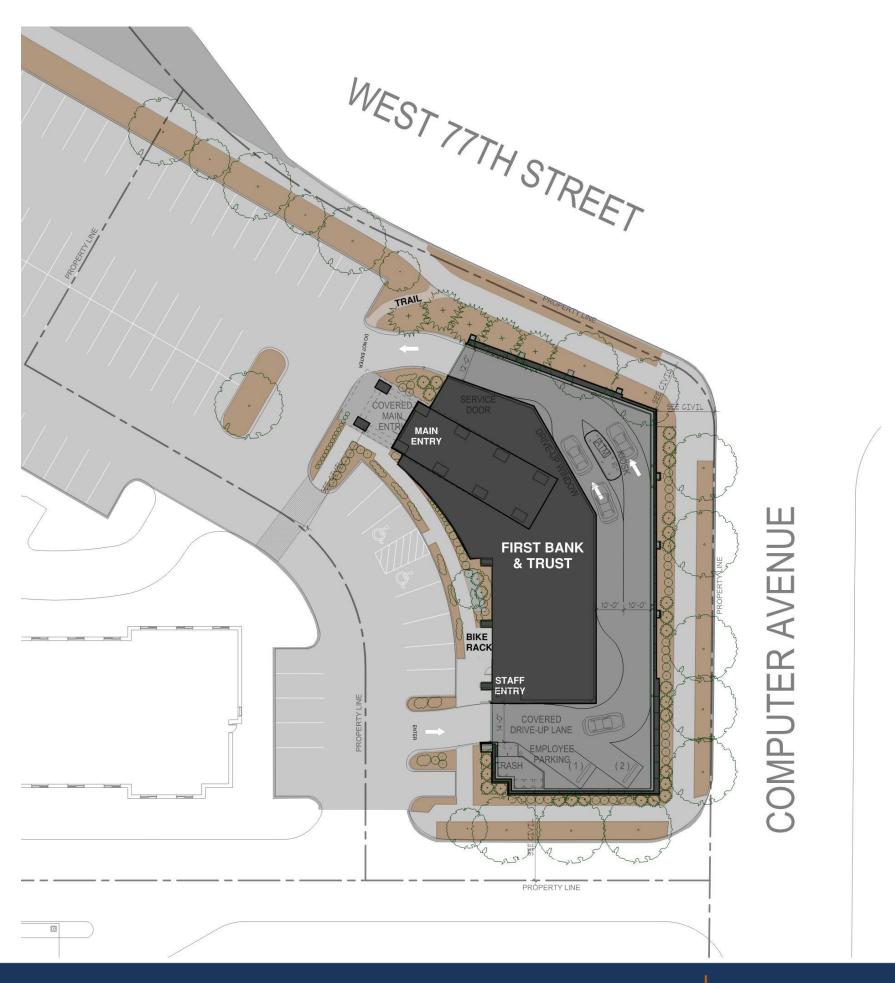






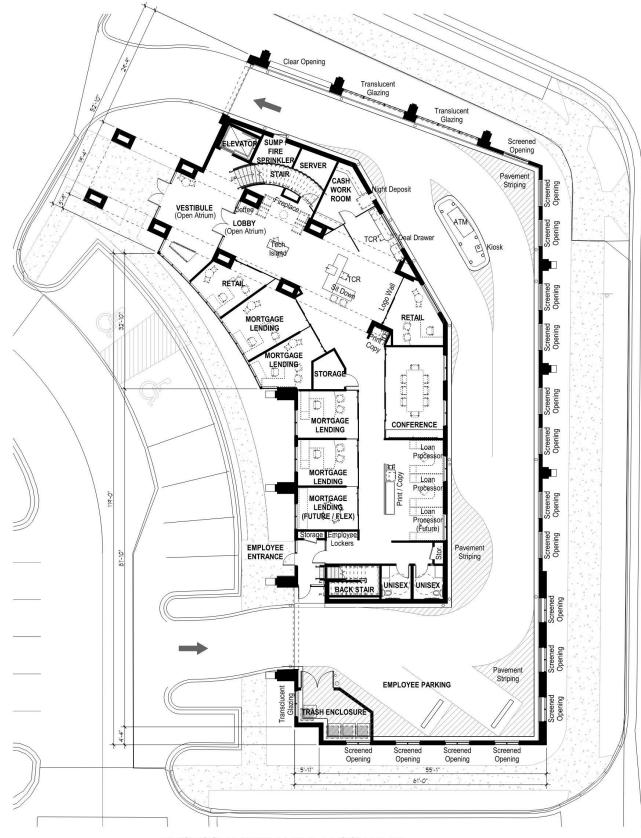






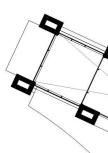
First Bank & Trust

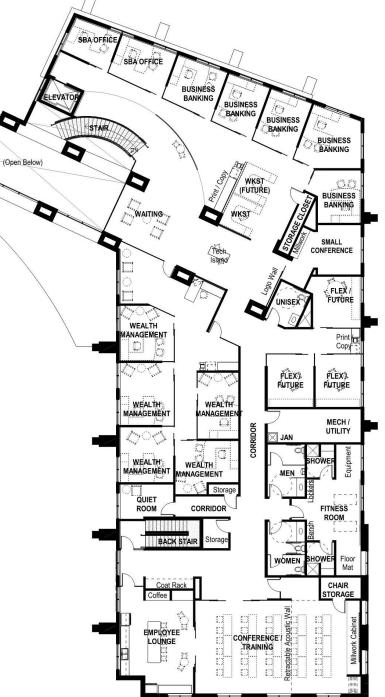




PROPOSED MAIN LEVEL FLOOR PLAN







PROPOSED UPPER LEVEL FLOOR PLAN





























CITY OF EDINA

4801 West 50th Street Edina, MN 55424 www.edinamn.gov

Date:	October 25, 2022	Agenda Item #: VI.B.
То:	Planning Commission	Item Type:
From:	Cary Teague, Community Development Director	Report and Recommendation
		Item Activity:
Subject:	Subdivision with Lot Area, Width and Depth Variances - 6416 McCauley Circle	Action

ACTION REQUESTED:

Recommend the City Council approve the Subdivision with Variances

INTRODUCTION:

Dipendra Mahaseth has requested a subdivision with multiple variances to tear down the existing home at 6416 McCauley Circle and build two new homes. One home would gain access off McCauley Circle and the other off McCauley Terrace.

Within this neighborhood, the minimum lot size is established by the median width, depth and area of all lots within 500 feet of the property. (See attached calculation done by Harry S. Johnson Surveyors) Therefore the minimum required lot area is 18,731 s.f., lot width 102 feet and lot depth 181 feet.

To accommodate the request the following is required:

- 1. A Subdivision; and
- 2. Lot Area, Width and Depth Variances for Lot 1; and Lot Depth Variance for Lot 2.

ATTACHMENTS:

Staff Report Proposed Subdivision Plans 500-Foot Median Calculation Tree Plan Applicant narrative and signed supporters of the project Site Location and Zoning Map Recent Subdivisions Lot Sizes on McCauley Terrace and McCauley Circle Better Together Public Hearing Comment Report Site Photos - McCauley Circle and Terrace Staff Presentation



Date: October 25, 2022

To: Planning Commission

From: Cary Teague, Community Development Director

Subject: Subdivision with Lot Area, Width and Depth Variances – 6416 McCauley Circle

Information / Background:

Dipendra Mahaseth has requested a subdivision with multiple variances to tear down the existing home at 6416 McCauley Circle and build two new homes. One home would gain access off McCauley Circle and the other off McCauley Terrace.

Within this neighborhood, the minimum lot size is established by the median width, depth and area of all lots within 500 feet of the property. (See attached calculation done by Harry S. Johnson Surveyors) Therefore the minimum required lot area is 18,731 s.f., lot width 102 feet, and lot depth 181 feet.

To accommodate the request the following is required:

- I. A Subdivision; and
- 2. Lot Area, Width and Depth Variances for Lot I; and Lot Depth and Street Frontage Variance for Lot 2.

Surrounding Land Uses

The lots to the north contain duplexes and are zoned R-2; the lots to the south contain single-family residential homes and are Zoned R-1. (See attached Zoning Map.)

Existing Site Features

The existing site contains a single-family home that would be removed. The site is wooded and contains slopes that fall toward Arrowhead Lake.

Planning

Guide Plan designation:	Single-dwelling residential
Zoning:	R-I, Single-dwelling district

Lot Dimensions

	Area	Lot Width	Depth
REQUIRED	18,731 s.f.	102 feet	181 feet
Lot I	16,431 s.f.*	80 feet*	120 feet*
Lot 2	19,047 s.f.	190 feet	100 feet*

*Variance Required

City Code requires a minimum of 30 feet of width at the street, the proposed Lot 2 has 28.58 feet of width.

City Considerations

<u>Sec. 32-130. – Considerations.</u> The planning commission, in reviewing proposed plats and subdivisions and in determining its recommendation to the council, and the council in determining whether to approve or disapprove of any plat or subdivision, may consider, among other matters, the following:

- Whether the physical characteristics of the property, including, without limitation, topography, vegetation, susceptibility to erosion or siltation, susceptibility to flooding, use as a natural recovery and ponding area for storm water, and potential disturbance of slopes with a grade of 18 percent or more, are such that the property is not suitable for the type of development or use proposed.
- (2) Whether development within the proposed plat or subdivision will cause the disturbance of more than 25 percent of the total area in such plat or subdivision containing slopes exceeding 18 percent.
- (3) Comply with section 10-82. (Tree Ordinance)Protected trees may be removed without mitigation, in the following areas:
 - a. Including, and within a ten-foot radius of, the building pad, deck or patio of a new or remodeled building.
 - b. Including, and within a five-foot radius of driveways and parking areas.
 - c. Including, and within ten-foot radius of installation of public infrastructure improvements including public roadways, stormwater retention areas and utilities.

Trees outside of these areas would have to be replaced.

Sec. 32-131. - Additional considerations

In addition to the foregoing matters, the commission, in connection with its recommendation to the council, and the council in determining whether to approve or disapprove a proposed plat or subdivision, shall specifically and especially consider the following matters:

- (1) Whether the proposed plat or subdivision complies with the policies, objectives and goals of the comprehensive plan.
- (2) Whether the proposed plat or subdivision complies with chapter 36.
- (3) Whether the design of the proposed plat or subdivision, or the design or type of improvements proposed to be placed thereon, may be detrimental to the health, safety or general welfare of the public.
- (4) Whether the proposed plat or subdivision conforms to, and complies with, the requirements of applicable state law.
- (5) Whether the proposed plat or subdivision complies with the policies, objectives, goals and requirements of this chapter, as varied by variances therefrom, if any.

The applicant has presented a plat that would not disturb the 18% slope areas by more than 25% on each lot. (See attached.) However, the request does require several variances.

Sec. 36-1257. - Drainage, retaining walls and site access.

(a) Drainage. No person shall obstruct or divert the natural flow of runoff so as to harm the public health, safety or general welfare. Surface water runoff shall be properly conveyed into storm sewers, watercourses, ponding areas or other public facilities. As part of the building permit, the applicant must submit a grading and erosion control plan along with a stormwater management plan that is signed by a licensed professional engineer. The stormwater management plan must detail how stormwater will be controlled to prevent damage to adjacent property and adverse impacts to the public stormwater drainage system. The plans must be approved by the city engineer and the permit holder must adhere to the approved plans.

As mentioned, the site drains toward Arrowhead Lake. The applicant is proposing to capture runoff in the rear yard of each site in a rain garden to slow it down before flowing to the east toward the lake. (See attached grading and drainage plan). The city engineer has reviewed the proposed plans and found them to be acceptable. There shall be no increase in peak rate or volume to neighboring private properties. The detailed grading plans would be reviewed by the city engineer at the time of building permit application for each lot. Each lot would be custom graded at the time of building permit. A construction management plan will be required for the construction of the new homes. (See the attached grading plan.)

Tree Removal

With the layout of the subdivision there would be 23 trees removed to accommodate the building pads, driveway and stormwater retention areas. The city recently amended its tree ordinance to require that these trees will need to be replace. The ordinance does not take effect until January 1, 2023. However, because there are variances being request, and to minimize impacts to the neighborhood, staff would recommend that if the proposal is approved one of the conditions of approval be that the new ordinance must be met, with trees being replaced on the site.

Park Dedication

As with all subdivision proposals, park dedication is required. Edina City Code requires a park dedication fee of \$5,000 for each additional lot created. Therefore, a park dedication fee of \$5,000 would be required for the one additional lot.

History of Subdivision Requests with Variances

The City of Edina has considered several subdivision requests with variances in this general area. (See attached area map.) The following is the history in the past 15 years:

History of Recent Subdivisions with Variances

- In 2006, the property at 5901 France Avenue received variances to build four (4) 66-foot wide lots consistent with the area. (Median = 9,269 s.f. & 73 feet wide.)
- In 2008, 6120 Brookview Avenue was proposed to be divided into two (2) 50-foot lots; however, the applicant withdrew the request before action was taken. (Median = 6,700 s.f. & 50 feet wide.)
- 3. In 2009, a 100-foot lot at 5920 Oaklawn was granted variances to divide into two (2) 50-foot lots. (Median = 6,699 s.f. & 50 feet wide.)
- 4. In 2011, the property at 5829 Brookview was granted variances to divide into two (2) 50foot lots. (Median = 6,769 s.f. & 50 feet wide.)
- 5. In 2012, the property at 6109 Oaklawn was denied the request to subdivide the property into two (2) 50-foot lots. (*Median = 6,701 s.f. & 50 feet wide.*)
- 6. In 2012, 6120 Brookview was again proposed for subdivision. That request was denied. (Median = 6,700 s.f. & 50 feet wide.)
- 7. In 2012, 5945 Concord was denied the request to subdivide the property into two (2)50foot lots. (Median = 10,028 s.f. & 77 feet wide.)

- In 2015, a 100-foot lot at 5825 Ashcroft Avenue was granted variances to divide into two (2) 50-foot lots. (Median = 6,790 s.f. & 50 feet wide.)
- 9 In 2015, 5945 Concord was approved for a request to subdivide the property into two (2) 50-foot lots. (Median = 10,028 s.f. & 77 feet wide.)
- In 2016, 5845 Kellogg Avenue was denied a request to subdivide the property into two (2) 50-foot lots. (Median = 6,715 s.f. & 50 feet wide.)
- 11. In 2017, 6124 Ewing Avenue was approved for a request to subdivide into two (2) lots with lot width and area variances. (Median = 11,500 s.f. & 80 feet wide.)
- 12. In 2017, 5404 Park Place was approved for a request to subdivide into two (2) lots with lot width variances from 75 feet to 65 feet for each lot; and lot area variances from 9,000 square feet to 8,705 and 8,840 square feet. (Median = 8,107 s.f. & width was 60 feet wide.)
- 13. In 2019, 5841 Oaklawn was approved for a request to subdivide into two (2) lots with lot width variances from 75 feet to 50 feet for one lot; and lot area variances from 9,000 square feet to 8,793 and 6,694 square feet.
- 14. In 2019, 4625 Lexington was approved for a request to subdivide into two (2) lots with lot width variances from 75 feet to 60 feet for both lots, and lot area variances from 9,306 square feet to 8,106 and 8,128 square feet. (Median area 9,306 square feet, median lot depth 135 feet, and the median lot width 68 feet)
- 15. In 2020, 5928 Ashcroft was denied a request to subdivide into two (2) lots with lot width variances from 75 feet to 50 feet for both lots, and lot area variances from 9,000 square feet to 6,813 and 6,790 square feet. (Median area 8,841 s.f., median lot depth 135 and median width was 66 feet.)
- In 2020, 5928 Abbott Avenue was approved for a two-lot subdivision with lot width variances from 75 feet to 50 feet for both lots, lot area variance from 9,000 square feet to 6,391 and 6,331 square feet, and lot depth variances from 135 feet to 127 feet for both lots. (Median lot area 6,745 square feet, median lot depth 129.9 feet, and the median lot width 50 feet.)

The City has approved and denied similar subdivisions. However, in the instances when subdivisions have been denied, the area immediately surrounding the site contained a wide variety of lot sizes. In instances when subdivisions have been approved, the requesting lots have been similar to lots in the immediate area. The proposed new lots are similar in size to other lots in this neighborhood. (See attached location maps and lot size maps of lots on McCauley Circle and Terrace.)

Primary Issues

• Is the proposed subdivision reasonable for the site?

Yes. Staff believes that the proposed subdivision is reasonable for the following reasons:

- 1. Lots within this area have a wide variety of shapes and sizes. (See attached neighborhood Map & Maps showing lot sizes on McCauley Terrace and McCauley Circle.)
- 2. The proposed lots are generally similar in size and shape to other lots within this area, including width at the street.
- 3. The existing lot is the largest in the area.
- 4. The lots on McCauley Terrace are all irregular in shape. There appears to be a vacant lot on McCauley Terrace, as the existing lot gains vehicular access off McCauley Circle. (See attached pictures.)
- 5. Upon compliance with all city and watershed district requirements for grading and drainage, the proposed subdivision would not have a negative impact on adjacent property.

• Are the findings for a variance met?

Yes. Staff believes that the findings for a Variance are met for this subdivision.

Per state law and the Zoning Ordinance, a variance should not be granted unless it is found that the enforcement of the ordinance would cause practical difficulties in complying with the zoning ordinance and that the use is reasonable. As demonstrated below, staff believes the proposal meets the variance standards, when applying the three conditions:

a) Will the proposal relieve practical difficulties that prevent a reasonable use from complying with the ordinance requirements?

Yes. Reasonable use does not mean that the applicant must show the land cannot be put to any reasonable use without the variance. Rather, the applicant must show that there are practical difficulties in complying with the code and that the proposed use is reasonable. "Practical difficulties" may include functional and aesthetic concerns.

The practical difficulty is caused by the unique shape of the existing lot with access available from two streets (McCauley Terrace and McCauley Circle.) Additional difficulties include the steep slopes toward Arrowhead Lake. The proposal to split the lot is reasonable in the context of the neighborhood which includes a wide variety of lot shapes and sizes, and the appearance of a vacant lot off McCauley Terrace where the second home would be located. The first home would gain access off McCauley Circle, as the current home does. The second home would gain access off McCauley Terrace. Two homes have access off the

street that are more narrow than proposed Lot 2. 6449 McCauley Terrace is 21.47 feet and 6445 McCauley Terrace is 25.5 feet.

b) There are circumstances that are unique to the property, not common to every similarly zoned property, and that are not self-created?

The circumstances that are unique to this property, the size and shape of the lot with access to two different streets is unique to the property and was not created by the applicant.

c) Will the variance alter the essential character of the neighborhood?

No. The proposed improvements requested by the variance would not alter the essential character of the neighborhood. Two new single-family homes would be constructed on lots similar in size to the lots in the neighborhood. The current lot is larger than most lots in this neighborhood. The impact to the lot on McCauley Circle will be the same as a tear down and rebuild.

Options for Consideration

Denial

Recommend the City Council deny the proposed Preliminary Plat with Variances. Denial is based on the following findings:

- 1. The proposed plat and proposed building pads do not meet the standards outlined in Sec. 32-130. – Considerations as follows:
 - (1) Whether the physical characteristics of the property, including, without limitation, topography, vegetation, susceptibility to erosion or siltation, susceptibility to flooding, use as a natural recovery and ponding area for storm water, and potential disturbance of slopes with a grade of 18 percent or more, are such that the property is not suitable for the type of development or use proposed.
 - (2) Whether development within the proposed plat or subdivision will cause the disturbance of more than 25 percent of the total area in such plat or subdivision containing slopes exceeding 18 percent.
 - (3) Comply with section 10-82.
- 2. Development of the site as proposed would be harmful to adjacent properties.
- 3. Proposed lots do not meet the City's lot size requirements for lot width, area and depth.

4. The building pads and drainage areas would have a negative impact to existing trees and steep slopes.

Approval

Recommend the City Council approve the proposed Preliminary Plat with the Lot Area, Width and Depth Variances. Approval is based on the following findings:

- 1. Lots within this area have a wide variety of shapes and sizes.
- 2. The proposed lots are generally similar in size and shape to other lots within this area, including width at the street.
- 3. The existing lot is the largest in the area.
- 4. The lots on McCauley Terrace are all irregular in shape. There appears to be a vacant lot on McCauley Terrace, as the existing lot gains vehicular access off McCauley Circle.
- 5. Upon compliance with all city and watershed district requirements for grading and drainage, the proposed subdivision would not have a negative impact on adjacent property.
- 6. The practical difficulty is caused by the size and shape of the existing lot with access available from two streets (McCauley Terrace and McCauley Circle.) Additional difficulties include the steep slopes toward Arrowhead Lake.
- 7. The proposal to split the lot is reasonable in the context of the neighborhood which includes a wide variety of lot shapes and sizes, and the appearance of a vacant lot off McCauley Terrace where the second home would be located.

Approval is subject to the following conditions:

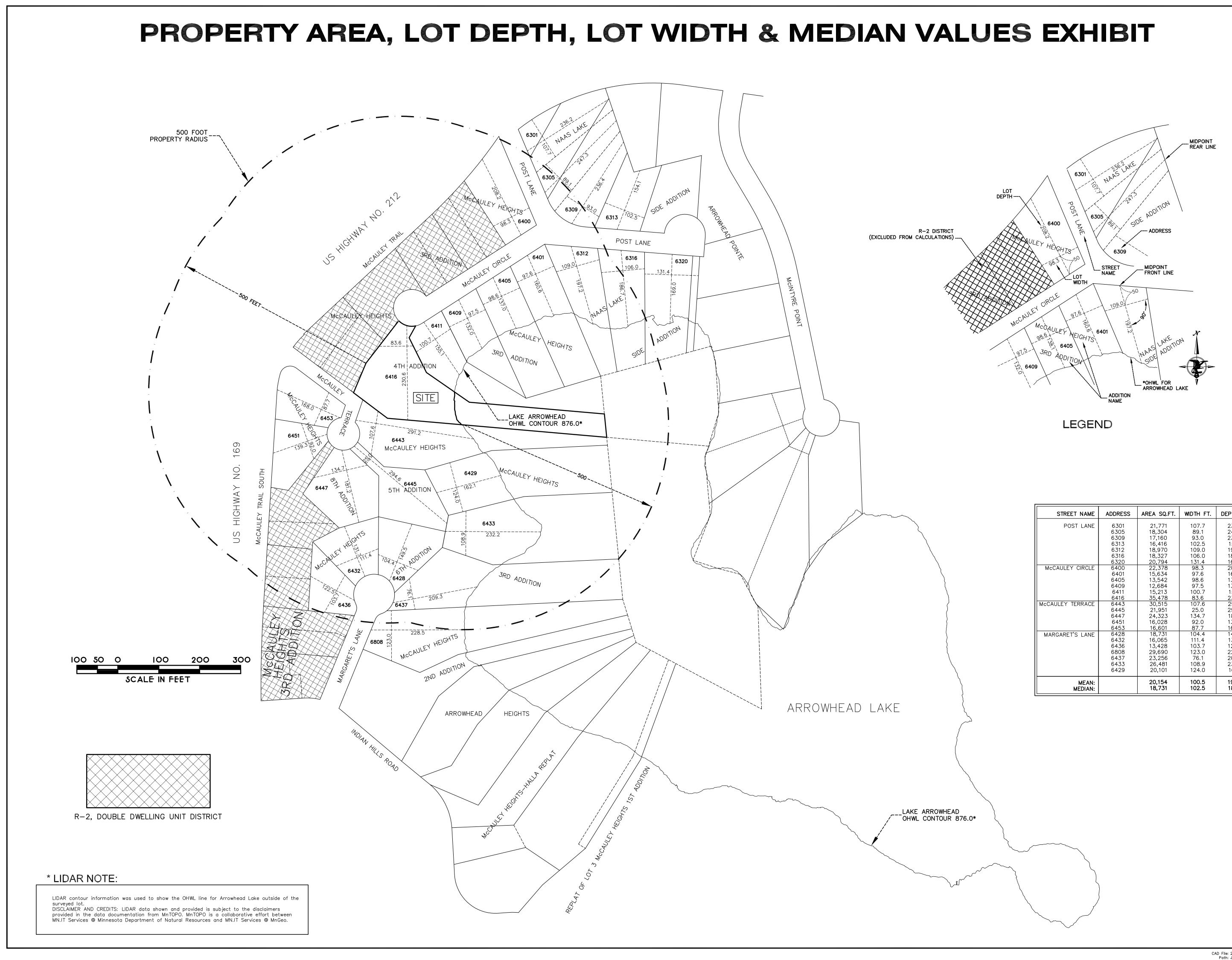
- 1. The City must approve the final plat within one year of preliminary approval or receive a written application for a time extension or the preliminary approval will be void. Final plat shall include a complete grading and drainage plan subject to review and approval of the city engineer. The grading and drainage plans must be revised to meet the conditions required herein.
- 2. Prior to issuance of a building permit, the following items must be submitted:
 - a. Park dedication fee of \$5,000 must be paid.
 - b. A construction management plan for the construction of each home.

- c. Submit evidence of a Nine Mile Creek Watershed District approval. The City may require revisions to the preliminary plat to meet the district's requirements.
- d. Curb-cut permits must be obtained from the Edina engineering department. Driveway plans must be consistent with the proposed grading plan to preserve as many trees as possible.
- e. Individual homes must comply with the overall grading plan for the site. Each individual building permit will be reviewed for compliance with the overall grading plan and development agreement subject to review and approval of the city engineer.
- f. Utility hook-ups are subject to review of the city engineer.
- g. There shall be no increase in peak rate or volume to neighboring private property.
- h. Any disturbance to the roadway caused by the construction of the new homes must be repaired by replacing the asphalt pavement from curb-to-curb and from saw-cut to saw-cut.
- 3. Compliance with the newly adopted tree ordinance, and replacement trees should be planted on site.

Staff Recommendation

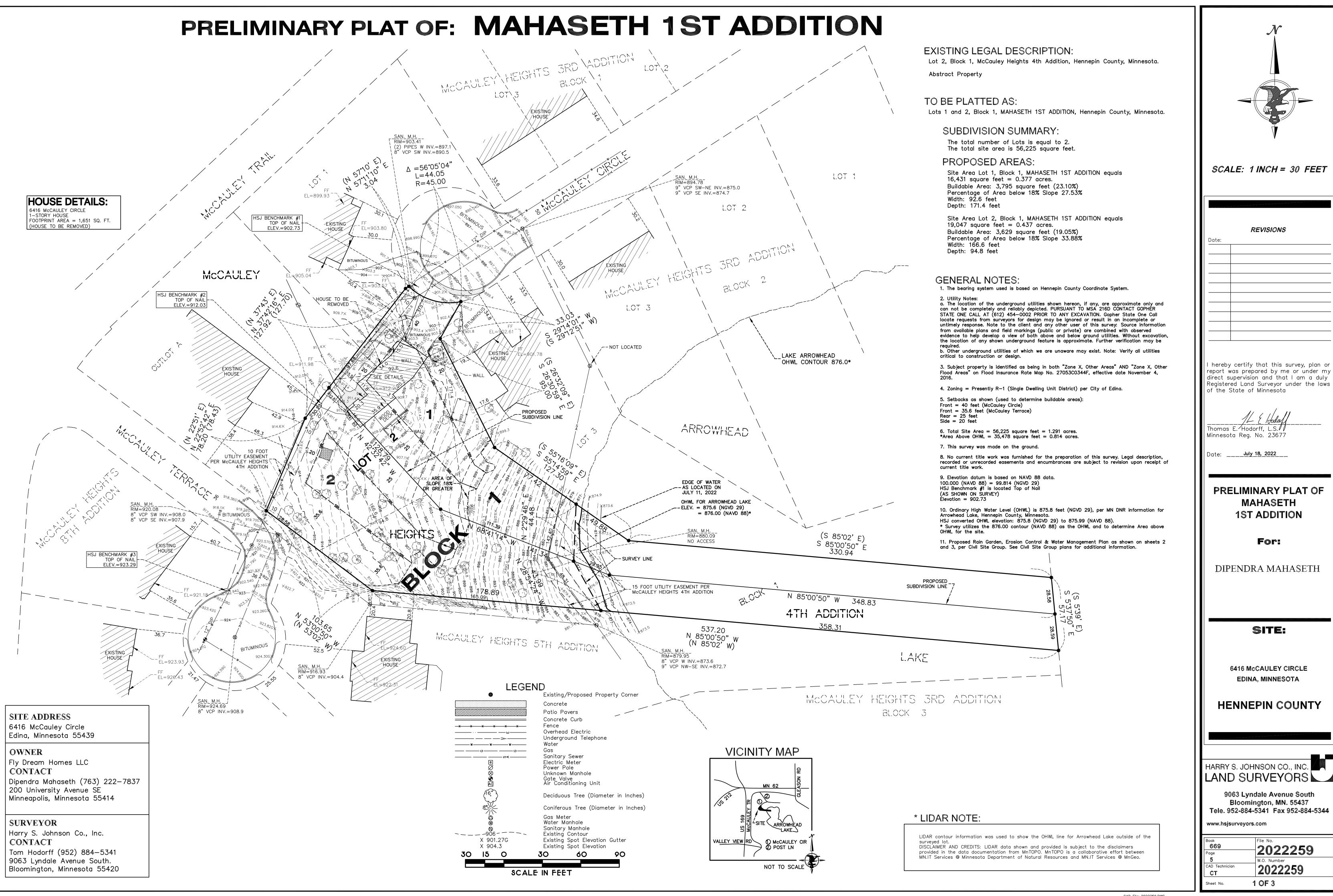
Staff recommends approval of the proposed Subdivision and Variances, subject to the findings and conditions listed above.

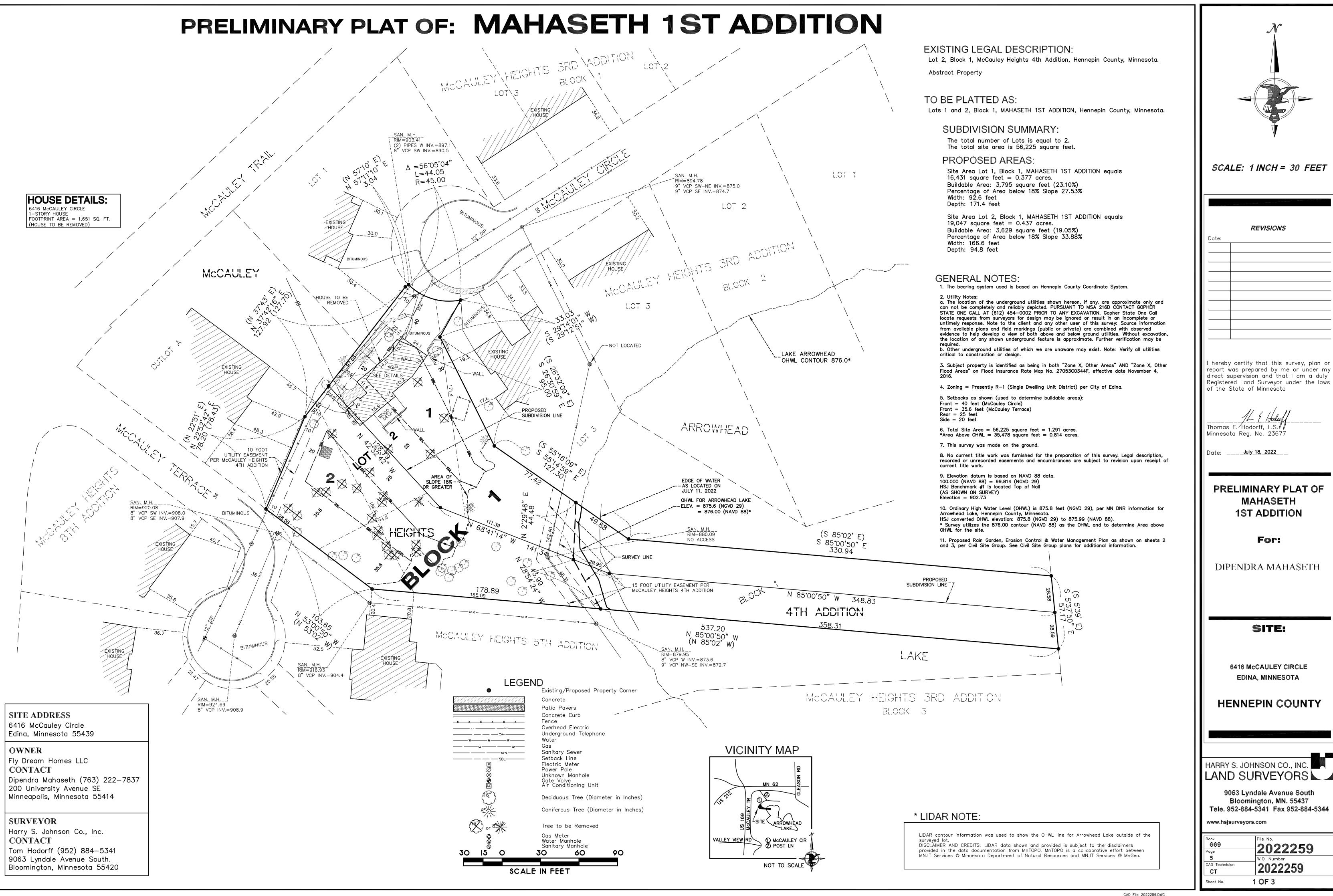
Deadline for a City Decision: December 20, 2022

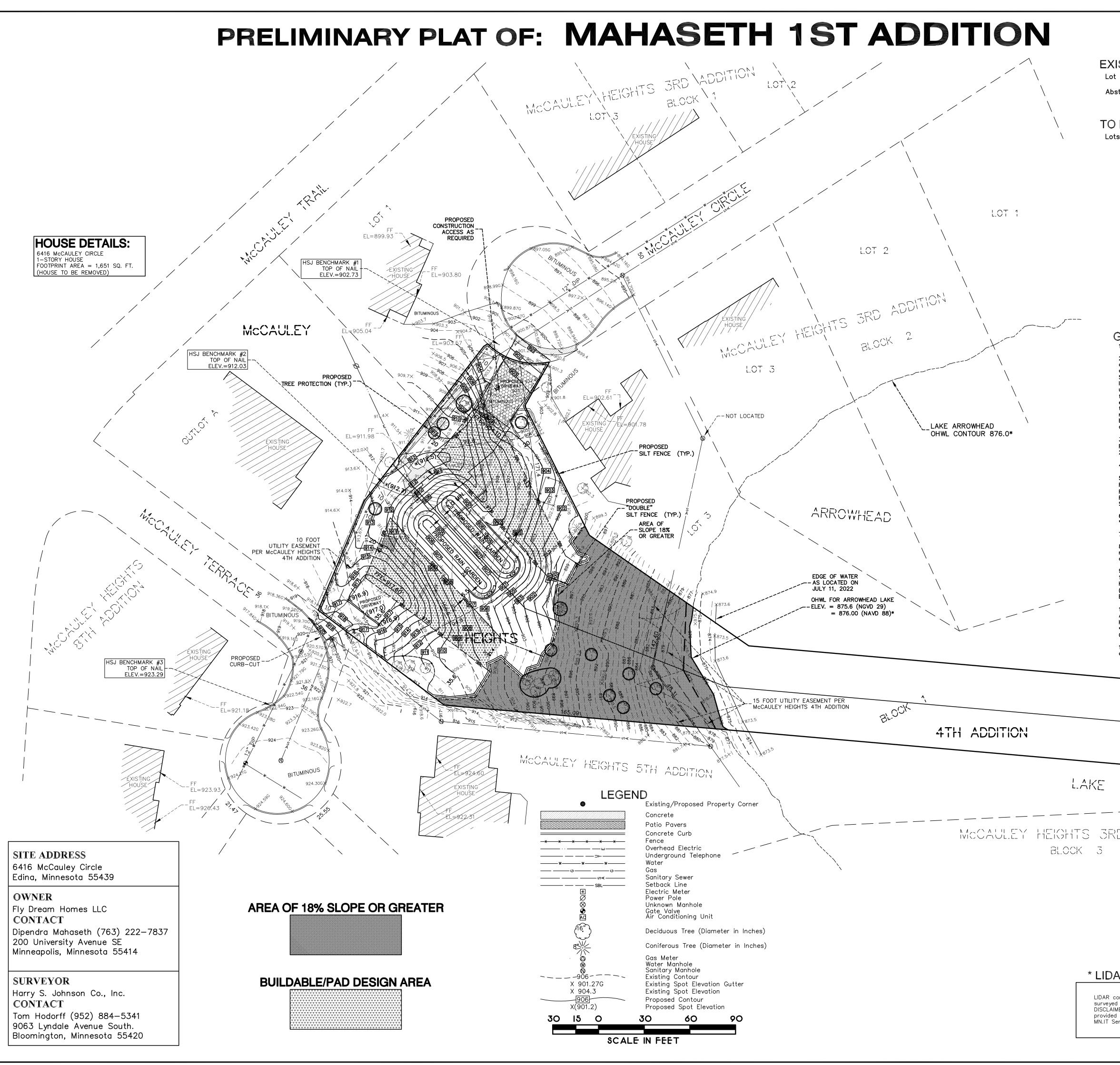


WIDTH FT. DEPTH FT. 236.2 247.3 236.4 154.1 197.2 186.7 169.0 208.2 160.6 137.0 132.0 155.1 230.6 291.2 291.2 294.6 181.2 139.3 168.0 149.5 131.7 122.5 228.5 209.3 232.2 162.1 190.4 181.2

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REVISIONS		
Date:		
I hereby certify that this survey, plan or report was prepared by me or under my direct supervision and that I am a duly Registered Land Surveyor under the laws of the State of Minnesota		
Thomas E. Hodorff, L.S. Minnesota Reg. No. 23677		
Date: July 28, 2022		
500 FOOT EXHIBIT FOR MAHASETH 1ST ADDITION		
For:		
DIPENDRA MAHASETH		
6416 McCAULEY CIRCLE		
EDINA, MINNESOTA		
HENNEPIN COUNTY		
HENNEPIN COUNTY		
HENNEPIN COUNTY		
HARRY S. JOHNSON CO., INC.		
HARRY S. JOHNSON CO., INC.		
HARRY S. JOHNSON CO., INC. LAND SURVEYORS 9063 Lyndale Avenue South Bloomington, MN. 55437 Tele. 952-884-5341 Fax 952-884-5344		



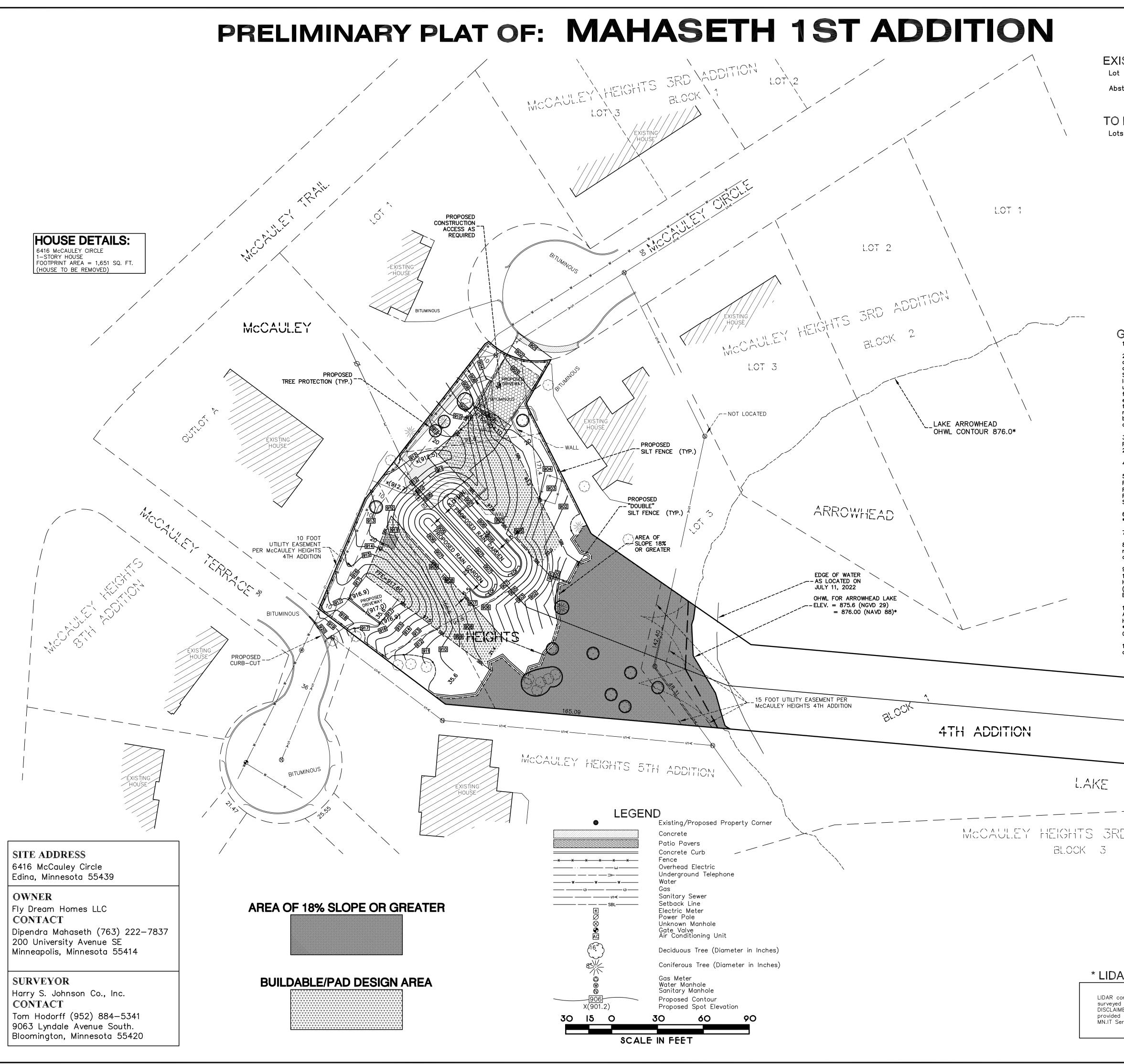




ISTING LEGAL DESCRIPTION: 2, Block 1, McCauley Heights 4th Addition, Hennepin County, Minnesota. stract Property	
BE PLATTED AS: is 1 and 2, Block 1, MAHASETH 1ST ADDITION, Hennepin County, Minnesota.	
SUBDIVISION SUMMARY: The total number of Lots is equal to 2. The total site area is 56,225 square feet.	Ţ.
PROPOSED AREAS: Site Area Lot 1, Block 1, MAHASETH 1ST ADDITION equals 16,431 square feet = 0.377 acres. Buildable Area: 3,795 square feet (23.10%) Percentage of Area below 18% Slope 27.53% Width: 92.6 feet Depth: 171.4 feet	SCALE: 1 INCH = 30 FEET
Site Area Lot 2, Block 1, MAHASETH 1ST ADDITION equals 19,047 square feet = 0.437 acres. Buildable Area: 3,629 square feet (19.05%) Percentage of Area below 18% Slope 33.88% Width: 166.6 feet Depth: 94.8 feet	REVISIONS Date: 9-21-22 revised rain gardens and contours
GENERAL NOTES: 1. The bearing system used is based on Hennepin County Coordinate System.	
2. Utility Notes: a. The location of the underground utilities shown hereon, if any, are approximate only and can not be completely and reliably depicted. PURSUANT TO MSA 216D CONTACT GOPHER STATE ONE CALL AT (612) 454-0002 PRIOR TO ANY EXCAVATION. Gopher State One Call locate requests from surveyors for design may be ignored or result in an incomplete or untimely response. Note to the client and any other user of this survey: Source information from available plans and field markings (public or private) are combined with observed evidence to help develop a view of both above and below ground utilities. Without excavation, the location of any shown underground feature is approximate. Further verification may be required.	
 b. Other underground utilities of which we are unaware may exist. Note: Verify all utilities critical to construction or design. 3. Subject property is identified as being in both "Zone X, Other Areas" AND "Zone X, Other Flood Areas" on Flood Insurance Rate Map No. 27053C0344F, effective date November 4, 2016. 4. Zoning = Presently R-1 (Single Dwelling Unit District) per City of Edina. 	I hereby certify that this survey, plan or report was prepared by me or under my direct supervision and that I am a duly Registered Land Surveyor under the laws of the State of Minnesota
5. Setbacks as shown (used to determine buildable areas): Front = 40 feet (McCauley Circle) Front = 35.6 feet (McCauley Terrace) Rear = 25 feet Side = 20 feet	11. 4 Hadall
6. Total Site Area = 56,225 square feet = 1.291 acres. *Area Above OHWL = 35,478 square feet = 0.814 acres.	Thomas E. Hodorff, L.S./ Minnesota Reg. No. 23677
 This survey was made on the ground. No current title work was furnished for the preparation of this survey. Legal description, recorded or unrecorded easements and encumbrances are subject to revision upon receipt of current title work. 	Date: July 18, 2022
9. Elevation datum is based on NAVD 88 data. 100.000 (NAVD 88) = 99.814 (NGVD 29) HSJ Benchmark #1 is located Top of Nail (AS SHOWN ON SURVEY) Elevation = 902.73	PRELIMINARY PLAT OF MAHASETH
 Ordinary High Water Level (OHWL) is 875.8 feet (NGVD 29), per MN DNR information for Arrowhead Lake, Hennepin County, Minnesota. HSJ converted OHWL elevation: 875.8 (NGVD 29) to 875.99 (NAVD 88). * Survey utilizes the 876.00 contour (NAVD 88) as the OHWL and to determine Area above 	1ST ADDITION
OHWL for the site. 11. Proposed Rain Garden, Erosion Control & Water Management Plan as shown on sheets 2 and 3, per Civil Site Group. See Civil Site Group plans for additional information.	For:
	DIPENDRA MAHASETH
	SITE:
	6416 McCAULEY CIRCLE EDINA, MINNESOTA
DADDITION	HENNEPIN COUNTY
	HARRY S. JOHNSON CO., INC. LAND SURVEYORS 9063 Lyndale Avenue South
AR NOTE:	Bloomington, MN. 55437 Tele. 952-884-5341 Fax 952-884-5344 www.hsjsurveyors.com
contour information was used to show the OHWL line for Arrowhead Lake outside of the d lot. MER AND CREDITS: LIDAR data shown and provided is subject to the disclaimers d in the data documentation from MnTOPO. MnTOPO is a collaborative effort between ervices @ Minnesota Department of Natural Resources and MN.IT Services @ MnGeo.	Book File No. 669 2022259 Page 2022259 5 W.O. Number CAD Technician 2022259 CT 2022259

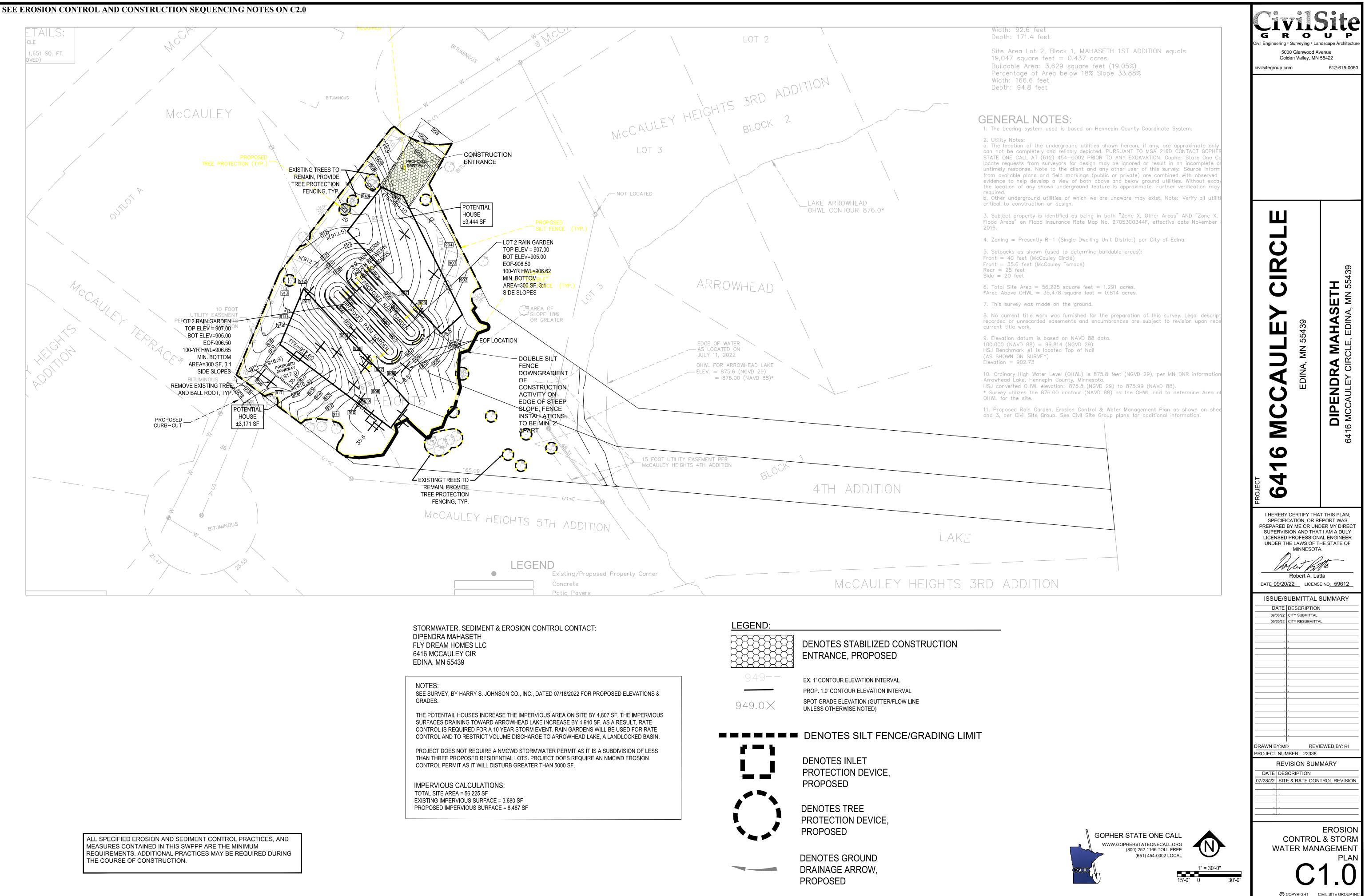
2 OF 3

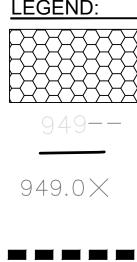
Sheet No.



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DADDITION	HENNEPIN COUNTY	
AR NOTE:	HARRY S. JOHNSON CO., INC. LAND SURVEYORS 9063 Lyndale Avenue South Bloomington, MN. 55437 Tele. 952-884-5341 Fax 952-884-534	4
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	CI ACAZACO Sheet No. 3 OF 3	

3 OF 3





STORMWATER, SEDIMENT & EROSION CONTROL CONTACT: DIPENDRA MAHASETH FLY DREAM HOMES LLC 6416 MCCAULEY CIR EDINA, MN 55439

SEDIMENT AND EROSION CONTROL NOTES

CONSTRUCTION SEQUENCING

INSTALLATION OF SILT FENCE OR BIO-ROLL AROUND SITE DEMOLITION OF EXISTING STRUCTURES IF ANY.

- CLEAR AND GRUB.
- CONSTRUCT NEW STRUCTURE

WHEN ALL CONSTRUCTION ACTIVITY IS COMPLETE AND THE SITE IS STABILIZED BY EITHER SEED OR SOD/LANDSCAPING, REMOVE SILT FENCE AND RESEED ANY AREAS DISTURBED BY THE REMOVAL.

EROSION PREVENTION

THE CONTRACTOR IS RESPONSIBLE FOR PLANING FOR AND IMPLEMENTING APPROPRIATE CONSTRUCTION PHASING, VEGETATIVE BUFFER STRIPS, HORIZONTAL SLOPE GRADING, AND OTHER CONSTRUCTION PRACTICES THAT MINIMIZE EROSION.

ALL EXPOSED SOIL AREAS MUST BE STABILIZED AS SOON AS POSSIBLE TO LIMIT SOIL EROSION BUT IN NO CASE LATER THAN 14 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

THE NORMAL WETTED PERIMETER OF ANY TEMPORARY OR PERMANENT DRAINAGE DITCH OR SWALE THAT DRAINS WATER FROM ANY PORTION OF THE CONSTRUCTION SITE, OR DIVERTS WATER AROUND THE SITE, MUST BE STABILIZED WITHIN 20 LINEAL FEET FROM THE PROPERTY EDGE, OR FROM THE POINT OF DISCHARGE INTO ANY SURFACE WATER. STABILIZATION OF THE LAST 20 LINEAL FEET MUST BE COMPLETED WITHIN 24 HOURS AFTER CONNECTING TO A SURFACE WATER.

STABILIZATION OF THE REMAINING PORTIONS OF ANY TEMPORARY OR PERMANENT DITCHES OR SWALES MUST BE COMPLETE WITHIN 14 DAYS AFTER CONNECTING TO A SURFACE WATER AND CONSTRUCTION IN THAT PORTION OF THE DITCH HAS TEMPORARILY OR PERMANENTLY CEASED.

TEMPORARY OR PERMANENT DITCHES OR SWALES THAT ARE BEING USED AS A SEDIMENT CONTAINMENT SYSTEM (WITH PROPERLY DESIGNED ROCK DITCH CHECKS, BIO ROLLS, SILT DIKES ETC.) DO NOT NEED TO BE STABILIZED. THESE AREAS MUST BE STABILIZED WITHIN 24 HOURS AFTER NO LONGER BEING USED AS A SEDIMENT CONTAINMENT SYSTEM.

PIPE OUTLETS MUST BE PROVIDED WITH TEMPORARY OR PERMANENT ENERGY DISSIPATION WITHIN 24 HOURS AFTER CONNECTION TO A SURFACE WATER.

SEDIMENT CONTROL

SEDIMENT CONTROL PRACTICES MUST MINIMIZE SEDIMENT FROM ENTERING SURFACE WATERS. INCLUDING CURB AND GUTTER SYSTEMS AND STORM SEWER INLETS.

SEDIMENT CONTROL PRACTICES MUST BE ESTABLISHED ON ALL DOWN GRADIENT PERIMETERS BEFORE ANY UPGRADIENT LAND DISTURBING ACTIVITIES BEGIN. THESE PRACTICES SHALL REMAIN IN PLACE UNTIL FINAL STABILIZATION HAS BEEN ESTABLISHED.

ALL STORM DRAIN INLETS MUST BE PROTECTED BY APPROPRIATE BMPS DURING CONSTRUCTION UNTIL ALL SOURCES WITH POTENTIAL FOR DISCHARGING TO THE INLET HAVE BEEN STABILIZED.

TEMPORARY SOIL STOCKPILES MUST HAVE SILT FENCE OR OTHER EFFECTIVE SEDIMENT CONTROLS, AND CANNOT BE PLACED IN SURFACE WATERS, INCLUDING STORMWATER CONVEYANCES SUCH AS CURB AND GUTTER SYSTEMS, OR CONDUITS AND DITCHES UNLESS THERE IS A BYPASS IN PLACE FOR THE STORMWATER.

VEHICLE TRACKING OF SEDIMENT FROM THE CONSTRUCTION SITE MUST BE MINIMIZED BY A ROCK CONSTRUCTION ENTRANCE. STREET SWEEPING MUST BE USED IF THE ROCK ENTRANCE IS NOT ADEQUATE TO PREVENT SEDIMENT FROM BEING TRACKED ONTO THE STREET.

TEMPORARY DE-WATERING - DEWATERING OR BASIN DRAINING (E.G., PUMPED DISCHARGES, TRENCH/DITCH CUTS FOR DRAINAGE) RELATED TO THE CONSTRUCTION ACTIVITY THAT MAY HAVE TURBID OR SEDIMENT LADEN DISCHARGE WATER MUST BE DISCHARGED TO A TEMPORARY OR PERMANENT SEDIMENTATION BASIN ON THE PROJECT SITE WHENEVER POSSIBLE. IF THE WATER CANNOT BE DISCHARGED TO A SEDIMENTATION BASIN PRIOR TO ENTERING THE SURFACE WATER. I MUST BE TREATED WITH THE APPROPRIATE BMP'S SUCH THAT THE DISCHARGE DOES NOT ADVERSELY AFFECT THE RECEIVING WATER, DOWNSTREAM LANDOWNERS OR WETLANDS. THE CONTRACTOR MUST ENSURE THAT DISCHARGE POINTS ARE ADEQUATELY PROTECTED FROM EROSION AND SCOUR. THE DISCHARGE MUST BE DISPERSED OVER NATURAL ROCK RIPRAP, SAND BAGS, PLASTIC SHEATHING OR OTHER ACCEPTED ENERGY DISSIPATION MEASURES. ADEQUATE SEDIMENTATION CONTROL MEASURES ARE REQUIRED FOR DISCHARGE WATER THAT CONTAINS SUSPENDED SOLIDS.

FILTER BACKWASH WATERS MUST BE HAULED AWAY FOR DISPOSAL, RETURNED TO THE BEGINNING OF THE TREATMENT PROCESS, OR INCORPORATE INTO THE SITE IN A MANNER THAT DOES NOT CAUSE EROSION. DISCHARGE OF THE BACKWASH WATER TO SANITARY SEWER IS ALLOWED WITH PERMISSION OF THE SANITARY SEWER AUTHORITY

SOIL COMPACTION PRECAUTIONS

THE PERMITEE MUST MINIMIZE SOIL COMPACTION AND, UNLESS INFEASIBLE, PRESERVE TOPSOIL. MINIMIZING SOIL COMPACTION IS NOT REQUIRED WHERE THE FUNCTION OF THE SPECIFIC AREA OF THE SITE DICTATES THAT IT BE COMPACTED. METHODS FOR MINIMIZING COMPACTION INCLUDE THE USE OF TRACKED EQUIPMENT, AND STAYING OFF OF AREAS TO BE LEFT UN-COMPACTED. METHODS TO PRESERVE TOPSOIL INCLUDE STRIPPING AND STOCKPILING TOPSOIL PRIOR TO GRADING OR EXCAVATION OPERATIONS.

INSPECTIONS AND MAINTENANCE

THE CONTRACTOR IS RESPONSIBLE AT ALL TIMES FOR THE MAINTENANCE AND PROPER OPERATION OF EROSION AND SEDIMENT CONTROL FACILITIES. THE CONTRACTOR SHALL AT A MINIMUM, INSPECT, MAINTAIN AND REPAIR ALL DISTURBED SURFACES AND ALL EROSION AND SEDIMENT CONTROL FACILITIES AND SOIL STABILIZATION MEASURES.

BASED ON INSPECTION RESULTS THE CONTRACTOR MAY AND SHALL MODIFY THE EROSION AND SEDIMENT CONTROL PLAN IN ORDER TO PREVENT POLLUTANTS FROM LEAVING THE SITE VIA STORM WATER RUNOFF.

POLLUTION PREVENTION

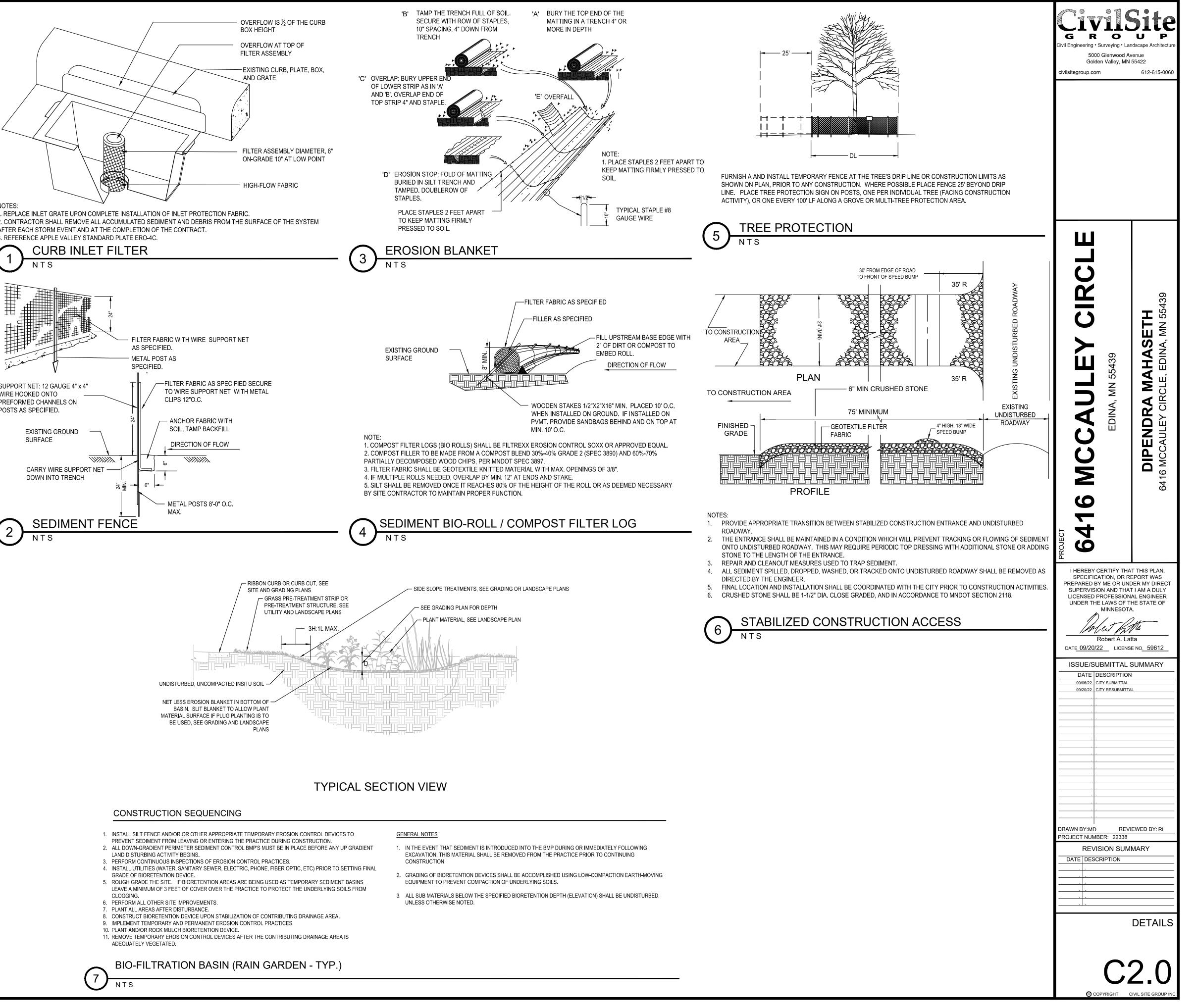
CONCRETE WASHOUT - SHALL OCCUR OFF-SITE

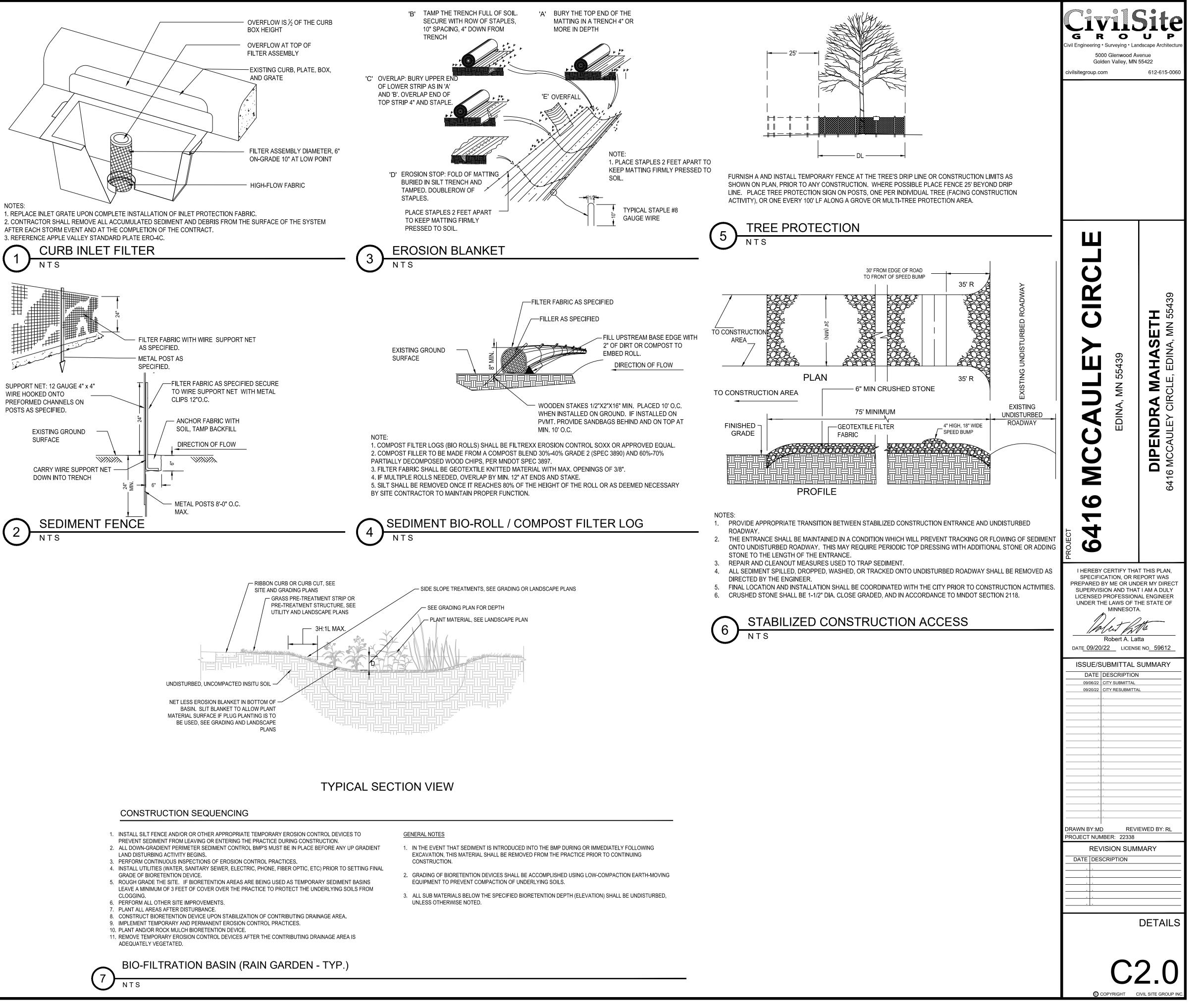
HAZARDOUS WASTE STORAGE AND HANDLING - OIL, GASOLINE, PAINT AND ANY HAZARDOUS SUBSTANCES MUST BE PROPERLY STORED, INCLUDING SECONDARY CONTAINMENT, TO PREVENT SPILLS, LEAKS OR OTHER DISCHARGE. RESTRICTED ACCESS TO STORAGE AREAS MUST BE PROVIDED TO PREVENT VANDALISM. STORAGE AND DISPOSAL OF HAZARDOUS WASTE MUST BE IN COMPLIANCE WITH MPCA REGULATIONS.

STORMWATER

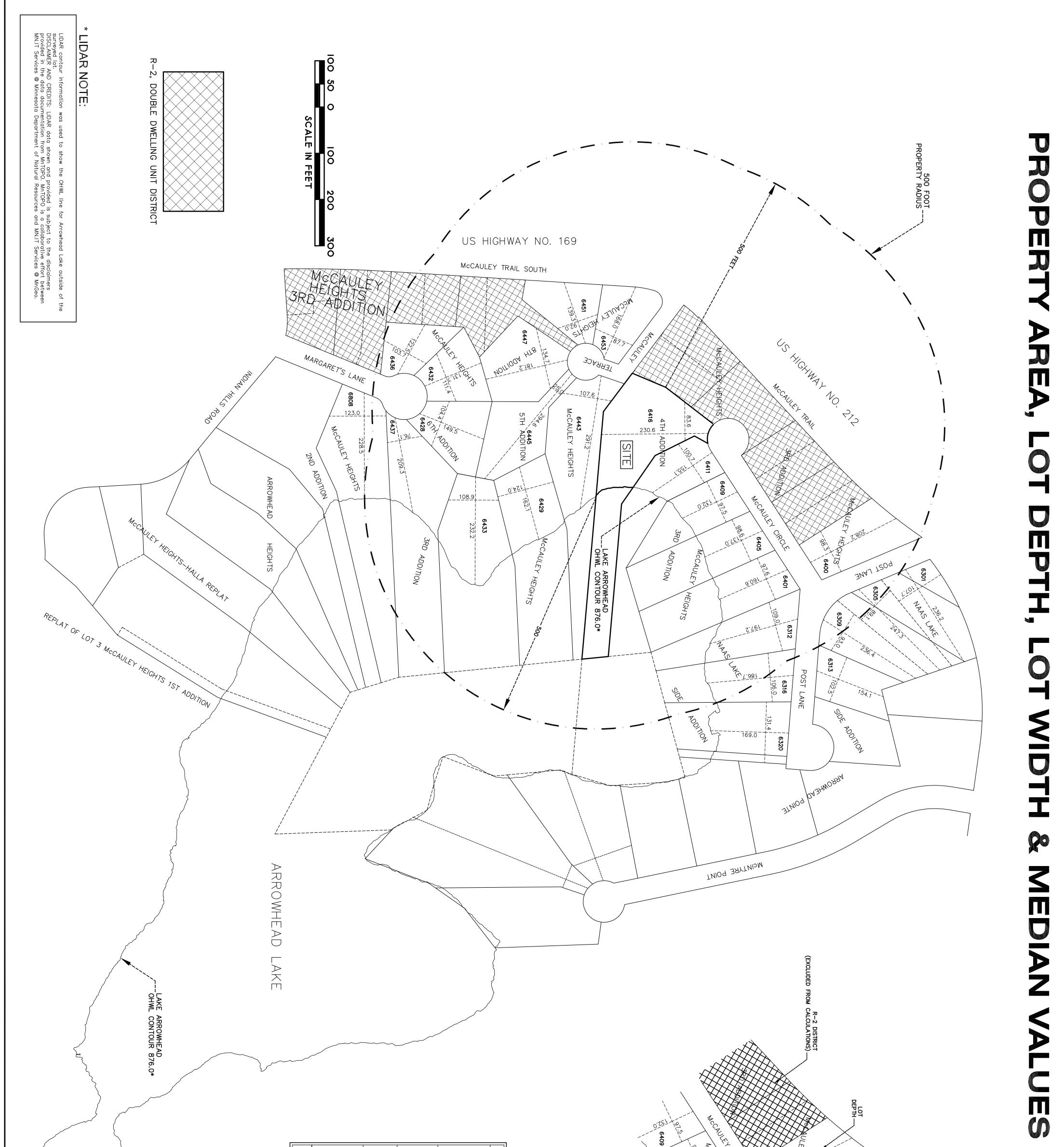
FINAL GRADING OF THE LOT SHALL PROMOTE SHEET DRAINING AND AVOID CONCENTRATION OF STORM WATER FLOWS.

FINAL GRADING SHALL MAINTAIN THE EXISTING STORM WATER DRAINAGE PATTERNS TO THE EXTENT POSSIBLE AND PRACTICAL AS TO NOT CAUSE ANY DAMAGE TO ADJACENT PROPERTIES.





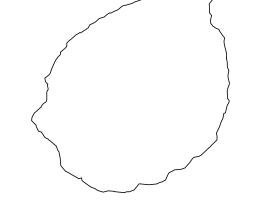




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OF 1



190.4 181.2	100.5 102.5	20,154 18,731		MEAN: MEDIAN:
16	124.0	20,101	6429	
23	108.9	26,481	6433	
20	76.1	23,256	6437	
22	123.0	29,690	6808	
12	103.7	13,428	6436	
131.7	111.4	16,065	6432	
14	104.4	18,731	6428	MARCARET'S I ANE
16	87.7	16.601	6453	
13	92.0	16,028	6451	
18	134.7	24,323	6447	
29	25.0	21,951	6445	
29	107.6	30,515	6443	MCCAULEY TERRACE
23	83.6	35,478	6416	
10	100.7	15,213	6411	
۲ ۲	97.5	12,684	6409	
۲ ۲	98.6	13,542	6405	
16	97.6	15,634	6401	
20	98.3	22,378	6400	MCCAULEY CIRCLE
16	131.4	20,794	6320	
18	106.0	18,327	6316	
19	109.0	18,970	6312	
10	102.5	16,416	6313	
23	93.0	17,160	6309	
24	89.1	18,304	6305	
23	107.7	21,771	6301	POST LANE
DEPTH F	WIDTH FT.	AREA SQ.FT.	ADDRESS	STREET NAME

POST LANE	6301	21,771	107.7	236.2
	6305	18,304	89.1	247.3
	6309	17,160	93.0	236.4
	6313	16,416	102.5	154.1
	6312	18,970	109.0	197.2
	6316	18,327	106.0	186.7
	6320	20,794	131.4	169.0
ACCAULEY CIRCLE	6400	22,378	98.3	208.2
	6401	15,634	97.6	160.6
	6405	13,542	98.6	137.0
	6409	12,684	97.5	132.0
	6411	15,213	100.7	155.1
	6416	35,478	83.6	230.6
CAULEY TERRACE	6443	30,515	107.6	291.2
	6445	21,951	25.0	294.6
	6447	24,323	134.7	181.2
	6451	16,028	92.0	139.3
	6453	16,601	87.7	168.0
ARGARET'S LANE	6428	18,731	104.4	149.5
	6432	16,065	111.4	131.7
	6436	13,428	103.7	122.5
	6808	29,690	123.0	228.5
	6437	23,256	76.1	209.3
	6433	26,481	108.9	232.2
	6429	20,101	124.0	162.1
MEAN:		20,154	100.5	190.4
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2-884-5341 Fax 952-884-5 rveyors.com File No. 2022259 W.O. Number 2022259	Y S. JOHNSON CO., INC. D SURVEYORS 9063 Lyndale Avenue South Bloomington, MN. 55437 952-884-5341 Fax 952-884-5	EDINA, MINNESOTA	SITE: A16 McCAULEY CIRCLE	For: PENDRA MAHASETH	500 FOOT EXHIBIT FOR MAHASETH 1ST ADDITION	as E. Hodorff, L.S. sota Reg. No. 23677	by certify that this survey, plan or was prepared by me or under my supervision and that I am a duly ered Land Surveyor under the laws State of Minnesota		REVISIONS	ALE: 1 INCH = 100 FEE	
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6409

LEGEND

. NAME

*OHWL FOR ARROWHEAD LAKE

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LOT MDTH

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ADDRESS

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NAAS LAKE

. MIDPOINT REAR LINE

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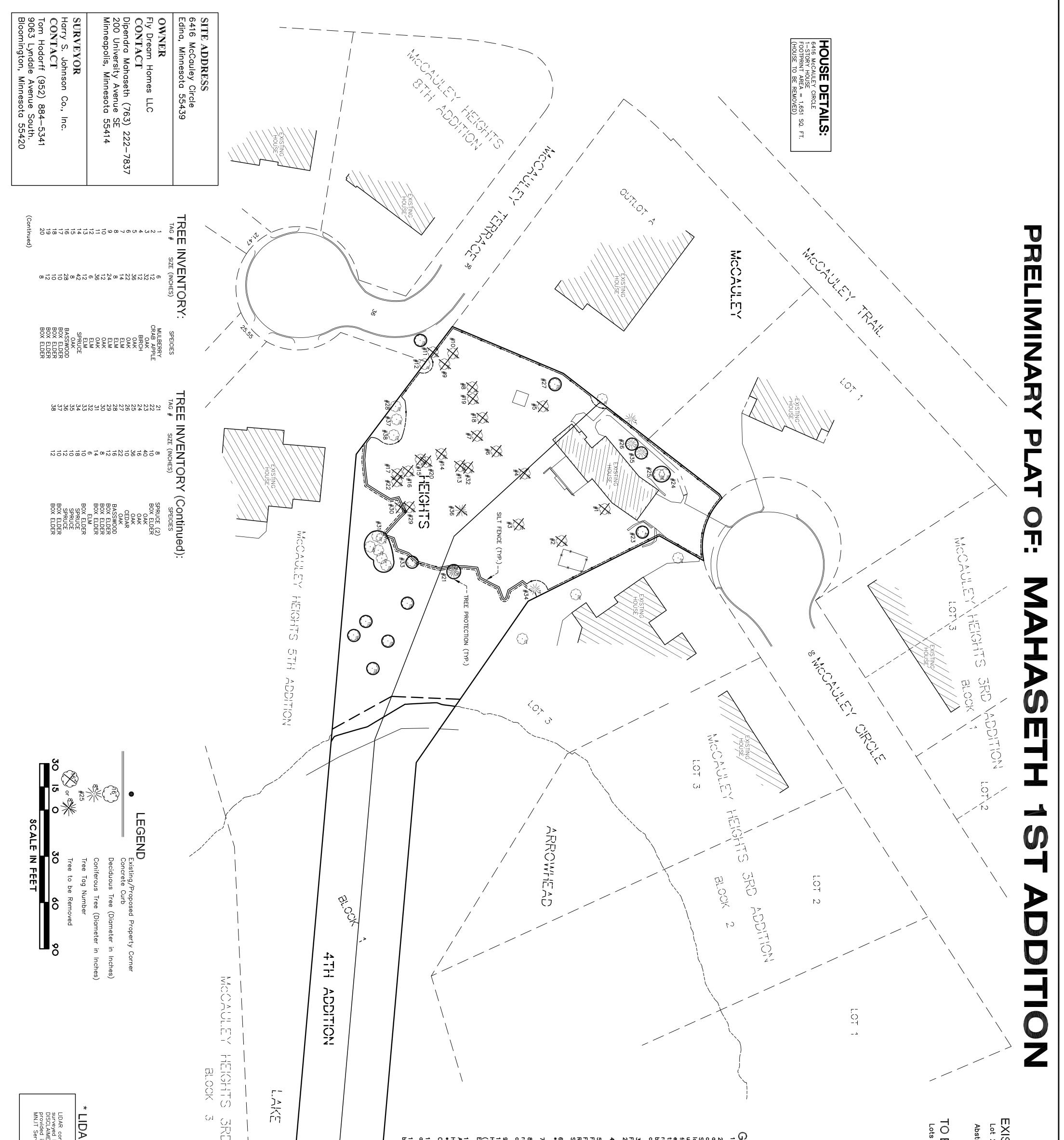
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ota Department of Natural Mn IUPO is a collaborative effort between Resources and MN.IT Services @ MnGeo. e of the

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in the data documentation from MnTOPO. MnTOPO is a collaborative effort bet	lot. ER AND CREDITS: LIDAR data shown and provided is subject to the disclaimers	ntour information was used to show the OHWL line for Arrowhead Lake outside	R NOTE:
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HARRY S. JOHNSON CO., INC. AND SURVEYORS 9063 Lyndale Avenue South Bloomington, MN. 55437 Tele. 952-884-5341 Fax 952-884-5344 www.hsjsurveyors.com File No. 5 CAD Technician CT Sheet No. 10F 1	SITE: 6416 McCAULEY CIRCLE EDINA, MINNESOTA HENNEPIN COUNTY	TREE PLAN for MAHASETH 1ST ADDITION For: DIPENDRA MAHASETH	I hereby certify that this survey, plan or report was prepared by me or under my direct supervision and that I am a duly Registered Land Surveyor under the laws of the State of Minnesota Thomas E. Hodorff, L.S.// Minnesota Reg. No. 23677 Date:	REVISIONS Date: 10-18-22 add tree inventory 10-18-22 add tree inventory	SCALE: 1 INCH = 30 FEET

EXISTING LEGAL DESCRIPTION: Lot 2, Block 1, McCauley Heights 4th Addition, I Abstract Property BE PLATTED AS: 1 and 2, Block 1, MAHASETH 1ST ADDITION, Hennepin 4th Addition, Hennepin County, Minnesota.

County, Minnesota.

SUBDIVISION SUMMARY:

The total number of Lots is equal to 2. The total site area is 56,225 square feet.

PROPOSED AREAS: Site Area Lot 1, Block 1, MAHASETH 1ST ADDITION equals 16,431 square feet = 0.377 acres. Buildable Area: 3,795 square feet (23.10%) Percentage of Area below 18% Slope 27.53% Width: 92.6 feet Depth: 171.4 feet

Site Area Lot 2, Block 1, MAHASETH 1ST ADDITION equals 19,047 square feet = 0.437 acres. Buildable Area: 3,629 square feet (19.05%) Percentage of Area below 18% Slope 33.88% Width: 166.6 feet Depth: 94.8 feet

GENERAL NOTES: 1. The bearing system used is based on Hennepin County Coordinate System.

Utility Notes:

 The location of the underground utilities shown hereon, if any, are approximate only and can not be completely and reliably depicted. PURSUANT TO MSA 216D CONTACT GOPHER STATE ONE CALL AT (612) 454-0002 PRIOR TO ANY EXCAVATION. Gopher State One Call locate requests from surveyors for design may be ignored or result in an incomplete or untimely response. Note to the client and any other user of this survey. Source information from available plans and field markings (public or private) are combined with observed evidence to help develop a view of both above and below ground utilities. Without excavation, the location of any shown underground feature is approximate. Further verification may be required.
 Other underground utilities of which we are unaware may exist. Note: Verify all utilities critical to construction or design.

3. Subject property is identified as being in both "Zone X, Other Areas" AND "Zone X, Other Flood Areas" on Flood Insurance Rate Map No. 27053C0344F, effective date November 4, 2016.

4. Zoning = Presently R-1 (Single Dwelling Unit District) per City of Edina.
5. Setbacks as shown (used to determine buildable areas): Front = 40 feet (McCauley Circle)
Front = 35.6 feet (McCauley Terrace)
Rear = 25 feet
Side = 20 feet

6. Total Site Area = 56,225 square feet = 1.291 acres. *Area Above OHWL = 35,478 square feet = 0.814 acres

This survey was made on the ground.
 No current title work was furnished for the preparation of this survey. Legal description, recorded or unrecorded easements and encumbrances are subject to revision upon receipt of current title work.

9. Elevation datum is based on NAVD 88 data. 100.000 (NAVD 88) = 99.814 (NGVD 29) HSJ Benchmark #1 is located Top of Nail (AS SHOWN ON SURVEY) Elevation = 902.73

Ordinary High Water Level (OHWL) is 875.8 feet (NGVD 29), per MN DNR information for Arrowhead Lake, Hennepin County, Minnesota.
 HSJ converted OHWL elevation: 875.8 (NGVD 29) to 875.99 (NAVD 88).
 * Survey utilizes the 876.00 contour (NAVD 88) as the OHWL and to determine Area above OHWL for the site.

 Proposed Rain Garden, Erosion Control & Water Management Plan as shown on sheets 2 and 3, per Civil Site Group. See Civil Site Group plans for additional information.
 Trees 6 inches in diameter or greater were located for this survey. Tree speicies identified by Family Tree Service LLC on October 15, 2022. speicies identified

August 3rd, 2022

Dear Neighbor,

I am writing to you in regards to the property located at 6416 Mccauley Circle, Edina, MN 55439. I recently purchased this lot with the intent to develop new housing opportunities. I am working with the city of Edina to subdivide the lot and build two single family homes. It is very important to me to gain the approval of the residents in the neighborhood for this project. I do believe this development will be an advantage for the surrounding home values. Given the potential impact on the neighborhood, I had the opportunity to speak to Tom McCauley whose grandfather built the home. He is enthusiastic about the new opportunity taking place on McCauley Circle. I am taking the time to inform the residents in the area of this plan and to get a response before moving forward with applying for the permits.

Please call/text me to 763-222-7837 or email me - dipendra.mahaseth@gmail.com if you object to the subdivision of 6416 Mccauley Circle or if you simply have questions or concerns. Gathering your input now will help me to better plan this project and prevent issues that might arise in the future. By signing below, you approve the subdivision of the address/lot located at 6416 Mccauley Circle, Edina, MN 55439 into two lots with variances. I appreciate your time and support.

Kind Regards, Dipendra Mahaseth

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8/3	Tinflee	6414 McCa	deflir Truk
Date	Name	Address	Signature
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Date <u>8/4/2-2</u>	Name <u>Claire Par</u>	nenter	Address		Signature	
Date	Name Denise Coo		Address 3 01 Post-Ln.		Signature	V V
Date 5.4.22	Name LAWALJ	TT SK	Address	McCul		
Date 3/4/27	Name Adthan Reenz		(🖡	A(L.(/	Signature	-
	Name SING F.L	0 642	Address 8 Margarets La	Ai	Signature PAIS	
8/4/22	Name <u>Kenjuan</u>	Wang 6	Address 429 Marganete	- Ln	Signature	1
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Date Jug_3 ^M	Name Kathy Venagl	Address La GIOGME Call	Signature Left - Kathy Venuglei
Date August 3	Brod Name Preet Anan	Address d 6312-Postlane	Signature
Date 83	Name J. WAN UZY M	Address 21 6400 McCAUS	Signature /
Date/	Name Mille Bloomga	Address 6309 Post Lar	Signature
Date 8/3	Name Jean Zivkovich	Address <u>6404 McCaulayCe</u>	Signature
Date 8/3	Name Ted Wrobel	Address <u>6320 Post Ln</u>	Signature
Date 3	Name Jusephix Oxic	Address Arty 6439 McCa	Signature Leef Josephendreiler
Date 8/4	The file Kat	Address Clyn Holman <u>(A37 Mc/aully</u>	Signature
Date <i>4-4</i>	Name ELIZABETH FE	big 6449 Mc Canley	Signature

Date	Name BYOUN 643	Address	Signature
8/5	1910010 693	3 Margaus M	G
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August 3rd, 2022

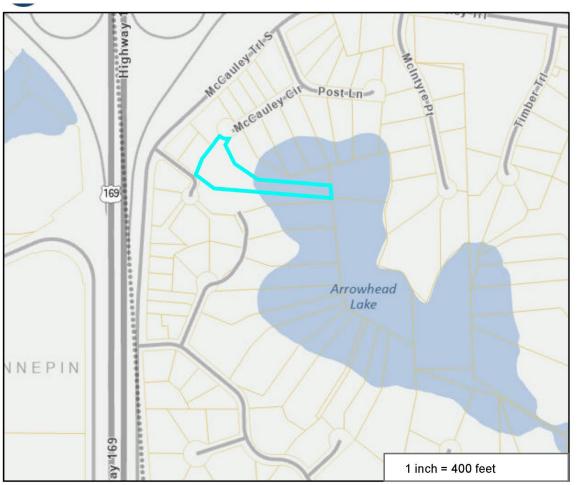
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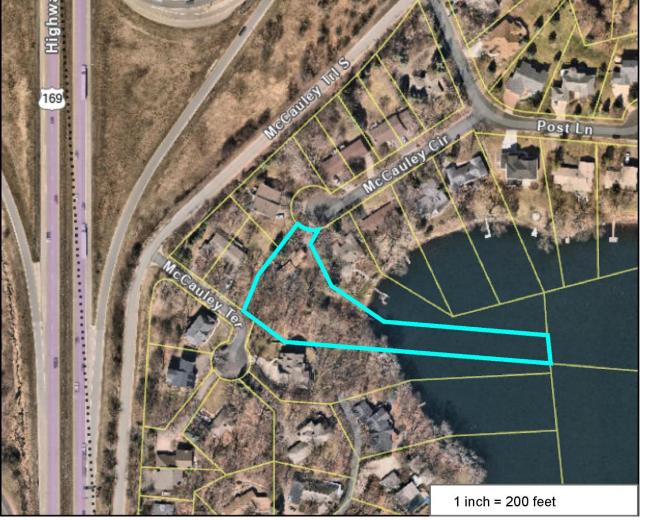
Please call/text me to 763-222-7837 or email me - dipendra.mahaseth@gmail.com if you object to the subdivision of 6416 Mccauley Circle or if you simply have questions or concerns. Gathering your input now will help me to better plan this project and prevent issues that might arise in the future. By signing below, you approve the subdivision of the address/lot located at 6416 Mccauley Circle, Edina, MN 55439 into two lots with variances. I appreciate your time and support.

Kind Regards, Dipendra Mahaseth

Signature Date Name Address 3/5/2022 Tom McCauley 6447 McCauley Terrace Edina, Mu. Melady Ton Signature







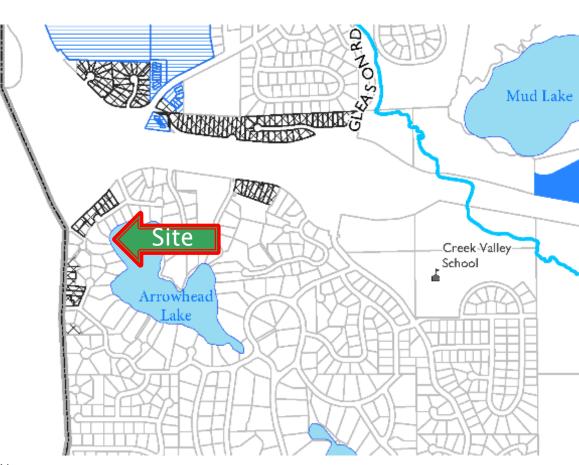


The CITY of **EDINA**





The CITY of **EDINA**



Zoning Map Legend R-I - Single Dwelling Unit **R-2** - Double Dwelling Unit PRD I Planned Residence PRD-2 - Planned Residence PRD-3 - Planned Residence PRD-4 - Planned Residence PRD-5 - Planned Residence PCD-1 - Planned Commercial PCD-2 - Planned Commercial PCD-3 - Planned Commercial PCD-4 - Planned Commercial POD-1 - Planned Office POD-2 - Planned Office RMD - Regional Medical PID - Planned Industrial PUD - Planned Unit Development APD - Automotive Parking PSR-4 - Planned Residence MDD-4 - Mixed Development MDD-5 - Mixed Development

MDD-6 - Mixed Development

Lakes

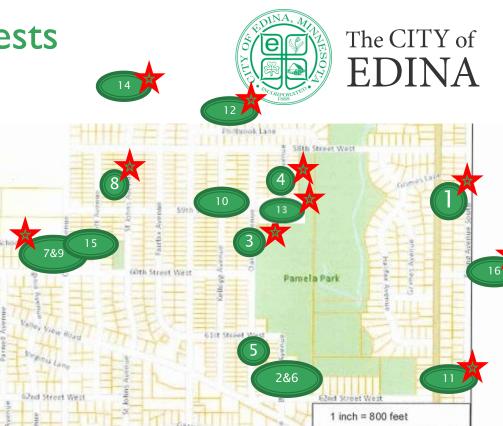
Creeks

EdinaMN.gov

History of Similar Requests

History of Recent Subdivisions with Variances

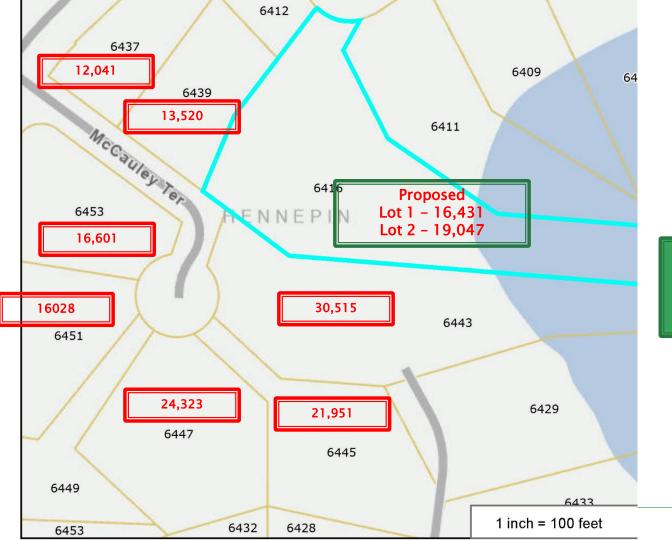
- In 2006, the property at 5901 France Avenue <u>received</u> variances to build four (4) 66-foot wide lots consistent with the area. (Median = 9,269 s.f. & 73 feet wide.)
- In 2008, 6120 Brookview Avenue was proposed to be divided into two (2) 50-foot lots; however, the
 applicant <u>withdrew</u> the request before action was taken. (Median = 6,700 s.f. & 50 feet wide.)
- In 2009, a 100-foot lot at 5920 Oaklawn was <u>granted</u> variances to divide into two (2) 50-foot lots. (Median = 6,699 s.f. & 50 feet wide.)
- In 2011, the property at 5829 Brookview was <u>granted</u> variances to divide into two (2) 50-foot lots. (Median = 6,769 s.f. & 50 feet wide.)
- In 2012, the property at 6109 Oaklawn was <u>denied</u> the request to subdivide the property into two (2) 50foot lots. (Median = 6,701 s.f. & 50 feet wide.)
- 6. In 2012, 6120 Brookview was again proposed for subdivision. That request was <u>denied.</u> (Median = 6,700 s.f. & 50 feet wide.)
- In 2012, 5945 Concord was <u>denied</u> the request to subdivide the property into two (2) 50-foot lots. (Median = 10,028 s.f. & 77 feet wide.)
- In 2015, a 100-foot lot at 5825 Ashcroft Avenue was <u>granted</u> variances to divide into two (2) 50-foot lots. (Median = 6,790 s.f. & 50 feet wide.)
- 9 In 2015, 5945 Concord was <u>approved</u> for a request to subdivide the property into two (2) 50-foot lots. (Median = 10,028 s.f. & 77 feet wide.)
- In 2016, 5845 Kellogg Avenue was <u>denied</u> a request to subdivide the property into two (2) 50-foot lots. (Median = 6,715 s.f. & 50 feet wide.)
- 11. In 2017, 6124 Ewing Avenue was <u>approved</u> for a request to subdivide into two (2) lots with lot width and area variances. (Median = 11,500 s.f. & 80 feet wide.)
- 12. In 2017, 5404 Park Place was <u>approved</u> for a request to subdivide into two (2) lots with lot width variances from 75 feet to 65 feet for each lot; and lot area variances from 9,000 square feet to 8,705 and 8,840 square feet. (Hedian = 8,107 s.f. & width was 60 feet wide.)
- In 2019, 5841 Oaklawn was <u>approved</u> for a request to subdivide into two (2) lots with lot width variances from 75 feet to 50 feet for one lot; and lot area variances from 9,000 square feet to 8,793 and 6,694 square feet.
- In 2019, 4625 Lexington was <u>approved</u> for a request to subdivide into two (2) lots with lot width variances from 75 feet to 60 feet for both lots, and lot area variances from 9,306 square feet to 8,106 and 8,128 square feet. (Median area - 9,306 square feet, median lot depth - 135 feet, and the median lot width 68 feet)
- 15. In 2020, 5928 Ashcroft was <u>denied</u> a request to subdivide into two (2) lots with lot width variances from 75 feet to 50 feet for both lots, and lot area variances from 9,000 square feet to 6,813 and 6,790 square feet. (Median area - 8,841 s,f, median lot depth - 135 and median width was 66 feet.)



16. In 2020, 5928 Abbott Avenue was approved for a two-lot subdivision with lot width variances from 75 feet to 50 feet for both lots, lot area variance from 9,000 square feet to 6,391 and 6,331 square feet, and lot depth variances from 135 feet to 127 feet for both lots. (Median lot area - 6,745 square feet, median lot depth - 129.9 feet, and the median lot width - 50 feet.)

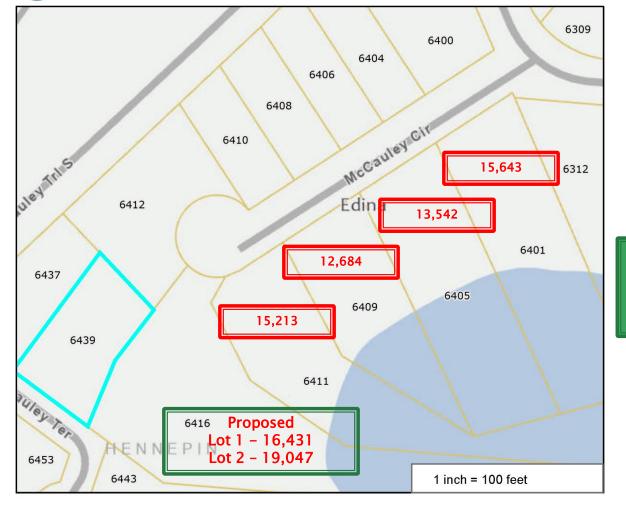


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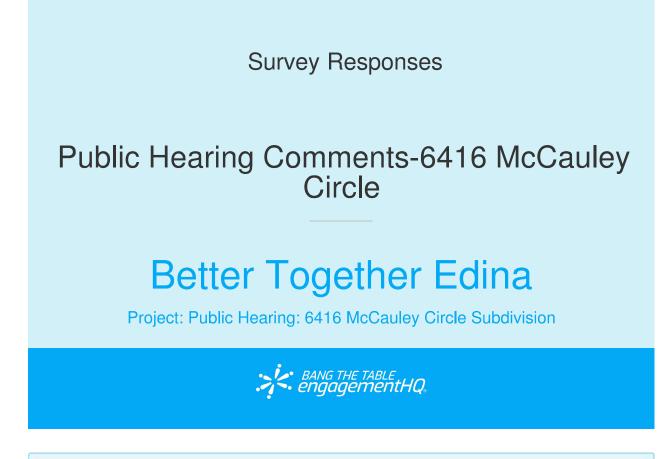


Lot Sizes on McCauley Terrace





Lot Sizes on McCauley Circle





Respondent No: 1 Login: Anonymous Email: n/a	Responded At: Oct 10, 2022 15:19:50 pm Last Seen: Oct 10, 2022 15:19:50 pm IP Address: n/a
Q1. First and Last Name	Safath Zaman
Q2. Address	12718 huntington avenue, savage, mn 55378

This project is good, I support the development. This will make the community look better.

Respondent No: 2 Login: Anonymous Email: n/a	Responded At: Oct 10, 2022 18:15:52 pm Last Seen: Oct 10, 2022 18:15:52 pm IP Address: n/a
Q1. First and Last Name	Jason DiDavide
Q2. Address	816 Portland Ave Minneapolis, MN 55404
Q3. Comment	

Love seeing projects come to life and stand in support of the growth. I absolutely see this as something amazing for the community and what it will bring. This will be the housing of two more wonderful additions to an already wonderful neighborhood.

Respondent No: 3 Login: Anonymous Email: n/a	Responded At: Oct 10, 2022 18:20:43 pm Last Seen: Oct 10, 2022 18:20:43 pm IP Address: n/a
Q1. First and Last Name	Bishal Maharjan
Q2. Address	12889 Oliver Ave S, Burnsville MN 55337

I am sure Dipendra has a plan for maximum use of his land to further develop his property and overall community.

Respondent No: 4 Login: Anonymous Email: n/a	Responded At: Oct 10, 2022 18:32:41 pm Last Seen: Oct 10, 2022 18:32:41 pm IP Address: n/a
Q1. First and Last Name	Nicholas walsh
Q2. Address	15565 Cherry path
Q3. Comment	

Great addition to the neighborhood

Respondent No: 5 Login: Anonymous Email: n/a	Responded At: Oct 10, 2022 20:00:43 pm Last Seen: Oct 10, 2022 20:00:43 pm IP Address: n/a
Q1. First and Last Name	Espinoza
Q2. Address	Gallagher Dr Edina, Mn 55435
Q3. Comment	

Developing two new homes to the community allows for an amazing project to come to life. Let's welcome our new neighbors to the block!

Respondent No: 6 Login: Anonymous Email: n/a	Responded At: Oct 11, 2022 18:20:46 pm Last Seen: Oct 11, 2022 18:20:46 pm IP Address: n/a
Q1. First and Last Name	Bryce Crowley
Q2. Address	5400 Vernon Ave S, Edina, MN
Q3. Comment Looks like a great project!	

Respondent No: 7 Login: Anonymous Email: n/a	Responded At: Oct 11, 2022 18:26:04 pm Last Seen: Oct 11, 2022 18:26:04 pm IP Address: n/a
Q1. First and Last Name	Michael Sauro
Q2. Address	5400 Vernon Ave Edina MN, 55436
Q3. Comment I support this project.	

	Respondent No: 8	Responded At:	Oct 12, 2022 16:57:28 pm
0	Login: Anonymous	Last Seen:	Oct 12, 2022 16:57:28 pm
	Email: n/a	IP Address:	n/a

Q1. First and Last Name

Ayushma Shrestha

Q2. Address

12929 Arbor Lakes Parkway North, Maple Grove, MN, 55369

Q3. Comment

The project looks well planned.

0	Respondent No: 9 Login: Anonymous Email: n/a		Responded At: Last Seen: IP Address:	Oct 12, 2022 20:14:20 pm Oct 12, 2022 20:14:20 pm n/a
Q1. First a	and Last Name	Andrew W		

Q2. Address

2776 Casco Point rd. Wayzata

Q3. Comment

I think this is will be a good addition to the nieghborbood!

	Respondent No: 10 Login: Anonymous Email: n/a	Responded At: Last Seen: IP Address:	Oct 12, 2022 20:24:13 pm Oct 12, 2022 20:24:13 pm n/a
Q1. First ar	nd Last Name	Brianna Olsen	
Q2. Addres	s	315 1st Ave NE Minneapolis mn	
Q3. Comme	ent		

Dipendra is a good friend of mine and I think he has a great vision for this project. I am looking forward to seeing him turn the run-down lot into something beautiful. He has even spoken w/ the Mccauley family and they are on-board! It will be a benefit for the surrounding neighbors as well as the city! Please approve this permit!



Respondent No: 11 Login: Anonymous Email: n/a
 Responded At:
 Oct 13, 2022 09:48:47 am

 Last Seen:
 Oct 13, 2022 09:48:47 am

 IP Address:
 n/a

Q1. First and Last Name

Lindsay Melander

Q2. Address

6453 McCauley Terrace

Q3. Comment

With multiple families on McCauley Terrace that have young children we are extremely concerned about the traffic safety. All of our lots are sloped and thus have minimal yard playing space. Therefore the kids play in the cul de sac. We all know to watch for them but adding another driveway on the curve makes it challenging to see kids zipping down on a bike or running after a ball. Also street parking is already a challenge when a neighbor has guests so this would add to that. Lastly snow removal may become more cumbersome seeing as that area was used as a dumping spot for snow by the city. We now may require more truck snow removal with this addition so it doesn't pile up and trickle down as it melts seeing as we are all on sloped lots. Thank you for your consideration.

	Respondent No: 12 Login: Anonymous Email: n/a	Responded At: Last Seen: IP Address:	Oct 13, 2022 17:17:53 pm Oct 13, 2022 17:17:53 pm n/a
Q1. First a	Ind Last Name	Josephine Ann Parker	
Q2. Addre	SS	6439 McCauley Terrace	
Q3. Comn	nent		

Having reconsider my original approval, I don't believe there is sufficient space for two homes. One very small lot next to my property and the other lot looks okay. The home coming off McCauley Terrace has no place to park other than in their driveway or in front of my property. We have a parking problem as it is and do not need additional cars. The only place for our children to ride their bikes and scooter's is in the circle. We have no sidewalks or trails on McCauley Terrace, McCauley Circle, or McCauley Trail. I believe one house on one lot.

Respondent No: 13 Login: Anonymous Email: n/a	Responded At: Oct 14, 2022 07:54:36 am Last Seen: Oct 14, 2022 07:54:36 am IP Address: n/a
Q1. First and Last Name	Thomas Clapp
Q2. Address	215 Hollander Rd, Wayzata, MN 55391

I support his request to subdivide his property.

Respondent No: 14 Login: Anonymous Email: n/a	Responded At: Oct 16, 2022 12:42:35 pm Last Seen: Oct 16, 2022 12:42:35 pm IP Address: n/a
Q1. First and Last Name	Marinna McDermott
Q2. Address	436 Buchanan St NE Minnespolis, MN 55413

This subdivision is a great plan in a wonderful, sought after location in Edina. Only benefits the city by adding another single family home on the lake!

Respondent No: 15 Login: Anonymous Email: n/a	Responded At: Oct 16, 2022 14:54:18 pm Last Seen: Oct 16, 2022 14:54:18 pm IP Address: n/a
Q1. First and Last Name	Kristen Hallen
Q2. Address	6438 Regency Ln, Eden Prairie, MN 55344

Very logical idea for this wonderful property. I like to see that Mr. Mahaseth is very transparent with the neighbors. Overall great opportunity for the city of Edina.

Respondent No: 16 Login: Anonymous Email: n/a	Responded At: Oct 16, 2022 18:19:34 pm Last Seen: Oct 16, 2022 18:19:34 pm IP Address: n/a
Q1. First and Last Name	Rebecca Ghiselli
Q2. Address	4246 Alden Dr, Edina, MN 55416

I support this new project. We would like to see new buildings in our well-maintained community.



Respondent No: 17 Login: Anonymous Email: n/a

Responded At: Oct 17, 2022 13:49:58 pm Last Seen: Oct 17, 2022 13:49:58 pm IP Address: n/a

Q1. First and Last Name

Claire parmenter

6443 McCauley terrace, Edina

Q3. Comment

Q2. Address

We are very concerned about the proposed devision of the 6416 McCauley circle lot. It is directly next to our home. We are worried about how close the new house would be to ours. The lots in our neighborhood are not huge, but also not small and it would significantly change the look and feel of our house to have another house so close. In addition, it would add increased traffic to our already crowded cul-de-sac (McCauley terrace). We are a small street with only 8 houses, it is our side (not the more populated McCauley circle side) that will be affected by the addition of the driveway and house. We hope our voices will be heard even though there are fewer of us. The lot he is proposing to divide is not large and the division will make two incredibly narrow lots. In fact, the current, very tiny cabin, would not fit on the proposed new lot. This is not something that will improve our neighborhood.

	Respondent No: 18	Responded At:	Oct 17, 2022 20:48:27 pm
	Login: Anonymous	Last Seen:	Oct 17, 2022 20:48:27 pm
	Email: n/a	IP Address:	n/a
Q1. First a	and Last Name	William Schneider	

Q2. Address

6085 Lincoln Dr, Edina, MN, 55436

Q3. Comment

This might help bring more residents to community.

Respondent No: 19 Login: Anonymous Email: n/a	Responded At: Oct 19, 2022 17:02:31 pm Last Seen: Oct 19, 2022 17:02:31 pm IP Address: n/a
Q1. First and Last Name	Michael schrom
Q2. Address	6410 Mccauley circle Edina Mn. 55439
Q3. Comment Live and let live	

Respondent No: 20 Login: Anonymous Email: n/a	Responded At: Oct 19, 2022 19:38:42 pm Last Seen: Oct 19, 2022 19:38:42 pm IP Address: n/a
Q1. First and Last Name	Tom McCauley
Q2. Address	6447 McCauley Terrace
Q3. Comment	

I believe Dipendra Mahaseth's plan to subdivide and build two custom single family homes will enhance the value of the neighborhood.

Respondent No: 21 Login: Anonymous Email: n/a	Responded At: Oct 20, 2022 07:34:19 am Last Seen: Oct 20, 2022 07:34:19 am IP Address: n/a
Q1. First and Last Name	David Nelson
Q2. Address	6411 McCauley Circle
Q3. Comment	

I strongly oppose the subdivision of this property. Way too much house and structure - will change nature of neighborhood and too dense as a neighbor.

Respondent No: 22 Login: Anonymous Email: n/a	Responded At: Oct 20, 2022 07:42:48 am Last Seen: Oct 20, 2022 07:42:48 am IP Address: n/a
Q1. First and Last Name	Samir Bhattarai
Q2. Address	Boston, MA
Q3. Comment	

This will be beautiful properties. I can feel it. Good luck and hope for smooth completion of the project.



Respondent No: 23 Login: Anonymous Email: n/a

Responded At: Oct 20, 2022 08:13:59 am Last Seen: Oct 20, 2022 08:13:59 am IP Address: n/a

Q1. First and Last Name

Stephanie Nelson

6411 McCauley Circle, Edina MN. 55439

Q3. Comment

Q2. Address

I oppose the subdivision request for 6416 McCauley Circle made by Dipendra Mahaseth, as the next door neighbor of this property and resident of 6411 McCauley Circle since 2009. I have been a homeowner and resident of Edina since 1995. I understand that there is a shortage of affordable housing but I do not think that dividing a 83' lot into two +3100 square foot homes qualifies as such. As of October 20, there were nine homes for sale, of new construction and between 3000-4000 square feet, with an average price of \$1,240,010. This is a business plan on the part of Mr. Mahaseth, to request a subdivision and build two homes on a site that can support one home. When Mr. Mahaseth tried to get my husband's support and signature, he was accompanied by a woman he introduced as his "business partner," further evidence that this is a business plan on his part. I oppose the subdivision for the environmental effects it will have on the surrounding area. Construction of two homes will increase run-off into the lake, increase erosion of the steep slope, and remove at least three mature oak trees from the property, reducing shade and increasing energy costs. Building two homes on this property will reduce vegetation and cover used by wildlife, including deer, foxes, coyotes, rabbits, wood ducks, and pileated woodpeckers. Fencing of the property will reduce natural movements of wildlife. The construction entrance on McCauley Circle may cause harm to the mature maple on our property. I also believe that Mr. Mahaseth is misrepresenting the support of the neighborhood both in how the signatures were obtained and in the number of signatures. He wrote in his applicant narrative that it was important to gain the approval of the residents in the neighborhood. I find it questionable that eleven of the 26 signatures were obtained when he showed up at the Neighborhood Night Out Celebration on August 3rd, both at the start of the celebration then after returning later in the evening as the celebration was winding down. Additionally, seven of the 26 of the signatures are from residents outside of the 500 foot property radius indicated on the submitted plat. Two of the 26 signatures are from the rental tenants of 6414 and 6412 McCauley Circle, rather than from the actual owner and landlord of the property. When I met Mr. Mahaseth in August, I welcomed him to the neighborhood and we chatted about his plans for the new house. Mr. Mahaseth stated that he would not be there for at least a year while the building took place. At no point in this conversation did he mention he would be requesting a subdivision nor did he seek to gain my support in this. I also find it curious that the application is missing the signatures of 2 of the four neighboring residents, whose homes will be directly affected by this subdivision. Mr. Mahaseth wrote in his applicant narrative that he had gained the support of Tom McCauley, the grandson of Mr. John McCauley, the original owner of the home. I recall that when Mr. McCauley passed away in 2010, his family also sought a subdivision with variances but were denied. I hope that the Planning Commission will deny this request for a subdivision as well.



Respondent No: 24 Login: Anonymous Email: n/a

Responded At: Oct 20, 2022 09:13:52 am Last Seen: Oct 20, 2022 09:13:52 am IP Address: n/a

Q1. First and Last Name

Louise Segreto

6720 Indian Hills Road, Edina, MN. 55439

Q3. Comment

Q2. Address

I have no objection to the Subdivision as proposed, but would like to pass on one concern to the developer and the City of Edina: Arrowhead Lake is already suffering from a number of issues affecting water quality. Every home that has shoreline on the Lake can help contribute to helping with Lake water quality. Although there is currently no minimum buffer requirement under City Ordinance for maintaining vegetation between the Lake and home landscaping, it sure would be helpful if Mr. Mahaseth would consider not planting grass to the Lake's edge in favor of natural native landscaping that would help with filtration, nutrient runoff and wildlife habitat. Contacting 9 Mile Creek Watershed District would be a great resource for him to contact to get some information and potentially even a grant to explore options.

McCauley Terrace





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McCauley Circle





McCauley Circle





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