

Agenda
Planning Commission
City Of Edina, Minnesota
City Hall, Council Chambers

Wednesday, September 14, 2022
7:00 PM

Participate in Public Hearing(s).
Call 888-504-7949.

Enter Participant Passcode 778177.

Press *1 on your telephone keypad when you would like to get in the queue to speak.
An operator will introduce you when it is your turn.

- I. Call To Order
- II. Roll Call
- III. Approval Of Meeting Agenda
- IV. Approval Of Meeting Minutes
 - A. Draft Minutes of Regular Meeting August 24, 2022
- V. Special Recognitions And Presentations
 - A. Welcome Student Commissioners- Shayaan Gandhi and Sarah Hu
- VI. Community Comment

During "Community Comment," the Board/Commission will invite residents to share relevant issues or concerns. Individuals must limit their comments to three minutes. The Chair may limit the number of speakers on the same issue in the interest of time and topic. Generally speaking, items that are elsewhere on tonight's agenda may not be addressed during Community Comment. Individuals should not expect the Chair or Board/Commission Members to respond to their comments tonight. Instead, the Board/Commission might refer the matter to staff for consideration at a future meeting.
- VII. Public Hearings
 - A. Zoning Ordinance Amendment, to revise the PUD-16 Zoning District to include a new Overall Development Plan and Site Plan Review for 7200 and 7250 France Avenue

VIII. Reports/Recommendations

- A. 2023 Planning Commission Work Plan

IX. Chair And Member Comments

X. Staff Comments

XI. Adjournment

The City of Edina wants all residents to be comfortable being part of the public process. If you need assistance in the way of hearing amplification, an interpreter, large-print documents or something else, please call 952-927-8861 72 hours in advance of the meeting.



CITY OF EDINA

4801 West 50th Street

Edina, MN 55424

www.edinamn.gov

Date: September 14, 2022

Agenda Item #: IV.A.

To: Planning Commission

Item Type:

Minutes

From: Liz Olson, Administrative Support Specialist

Item Activity:

Subject: Draft Minutes of Regular Meeting August 24, 2022

Action

ACTION REQUESTED:

Approve the draft minutes from August 24, 2022.

INTRODUCTION:

ATTACHMENTS:

Draft Minutes August 24, 2022



Minutes
City Of Edina, Minnesota
Planning Commission
Edina City Hall Council Chambers
August 24, 2022

I. Call To Order

Chair Agnew called the meeting to order at 7:02 PM.

II. Roll Call

Answering the roll call were: Commissioners Miranda, Strauss, Barberot, Bennett, Padilla, Smith, and Chair Agnew. Staff Present: Cary Teague, Community Development Director, Kris Aaker, Assistant Planner, Scott Neal, City Manager, Bill Neuendorf, Economic Development Manager, Liz Olson, Administrative Support Specialist.

Absent from the roll call: Commissioner Hayward, Alkire, and Olson.

III. Approval Of Meeting Agenda

Commissioner Padilla moved to approve the August 24, 2022, agenda. Commissioner Miranda seconded the motion. Motion carried unanimously.

Commissioner Bennett arrived at 7:04 p.m.

IV. Approval Of Meeting Minutes

A. Minutes: Planning Commission, July 27, 2022, and August 10, 2022

Commissioner Smith moved to approve the July 27, 2022, and August 10, 2022, meeting minutes. Commissioner Padilla seconded the motion. Motion carried unanimously.

V. Community Comment

None.

VI. Public Hearings

A. B-22-15, Setback Variance at 5900 Zenith Avenue S

Assistant Planner Aaker presented the request of 5900 Zenith Avenue S for a setback variance. Staff recommended approval of the setback variance, as requested subject to the findings and conditions listed in the staff report.

Staff answered Commission questions.

Appearing for the Applicant

Mr. Brian & Mrs. Cassie DeLaitsch, property owners, addressed the Commission.

Public Hearing

None.

Commissioner Strauss moved to close the public hearing. Commissioner Smith seconded the motion. Motion carried unanimously.

The Commission discussed the setback variance.

Motion

Commissioner Smith moved that the Planning Commission recommend approval of the setback variance as outlined in the staff memo subject to the conditions and findings therein. Commissioner Strauss seconded the motion. Motion carried unanimously.

B. Preliminary Rezoning from PID, Planned Industrial District to PUD, Planned Unit Development at 5146 Eden Avenue (Former Public Works Site)

City Manager Neal introduced Economic Development Manager Bill Neuendorf who presented the request for a Preliminary Rezoning to PUD. Staff recommended approval of the preliminary rezoning to PUD, as requested subject to the findings and conditions listed in the staff report.

Director Teague answered Commission questions.

Appearing for the Applicant

Mr. Alex Hall, United Properties; Terry Minarik and Jake Coryell from Think Confluence; Brian Larson, Zac Rosenow, Christine Soma, Kathy Moris and Katherine Gilster from Pope Design; Jordan Brookens, Shea Design; and Erin Jordan, Traffic Engineer at SEH addressed the Commission and answered questions.

The Commission asked questions of the applicants.

Public Hearing

None.

Commissioner Bennett moved to close the public hearing. Commissioner Miranda seconded the motion. Motion carried unanimously.

The Commission discussed the Preliminary Rezoning from PID (Planned Industrial District) to PUD (Planned Unit Development) and provided the following feedback:

- The aspects of the project, in general are great and overall, a good development
- Missing some of the key context of the framework and excitement embedded in it
- This deviates from some of the framework and the framework has the hope for significant change
- There are other creative uses that could have been explored
- Does not want to see something that is repeating other projects
- This is not going to promote the change the City deserves
- There is still a huge missing component in this project
- This is providing the best of what has been planned over the last ten years
- Like the idea of senior cooperative housing with an allocation for low-income housing
- Like to see more consideration of alternatives
- Would like to see this more pedestrian friendly with roundabouts that have bike paths
- Signal monitoring is needed for pedestrian recognition
- Needs more safety and truly be friendly to alternative forms of traffic
- This project fails as a people centered hub for Grandview
- This site is crucial for this neighborhood and does not meet what the area needs
- The City has a huge responsibility and nothing ties this together
- There is no mixed use on the property (no vertical mixed use) and is not a memorable development
- Appreciated seeing the story of the ten years of work done on this space
- The community is running out of space and this is a generational opportunity
- This is not the best project that could be placed on this site

Motion

Commissioner Strauss moved that the Planning Commission recommend approval to the City Council of the Preliminary Rezoning from PID to PUD at 5146 Eden Avenue, as outlined in the staff memo subject to the conditions and findings therein. Commissioner Padilla seconded the motion. 3 Ayes (Strauss, Padilla, Smith), 3 Nays (Miranda, Bennett, Agnew). Motion moved forward with a tie vote.

VII. Reports/Recommendations

A. 2023 Planning Commission Work Plan

Director Teague reviewed the 2023 Planning Commission Work Plan.

Staff answered Commission questions.

VIII. Correspondence and Petitions

Received.

IX. Chair and Member Comments

Received.

X. Staff Comments

Received.

XI. Adjournment

Commissioner Strauss moved to adjourn the August 24, 2022, Meeting of the Edina Planning Commission at 9:29 PM. Commissioner Miranda seconded the motion. Motion carried unanimously.



CITY OF EDINA

4801 West 50th Street

Edina, MN 55424

www.edinamn.gov

Date: September 14, 2022

Agenda Item #: V.A.

To: Planning Commission

Item Type:

Other

From:

Item Activity:

Subject: Welcome Student Commissioners- Shayaan Gandhi
and Sarah Hu

Information

ACTION REQUESTED:

INTRODUCTION:



CITY OF EDINA

4801 West 50th Street

Edina, MN 55424

www.edinamn.gov

Date: September 14, 2022

Agenda Item #: VII.A.

To: Planning Commission

Item Type:
Report and Recommendation

From: Cary Teague, Community Development Director

Item Activity:

Subject: Zoning Ordinance Amendment, to revise the PUD-16
Zoning District to include a new Overall Development
Plan and Site Plan Review for 7200 and 7250 France
Avenue

ACTION REQUESTED:

Recommend the City Council approve the proposal.

INTRODUCTION:

Orion Investment is requesting a Zoning Ordinance Amendment for a completely new redevelopment project for the subject property. This site received a rezoning approval to Planned Unit Development-16 in 2019; however, the project was never constructed. The underlying, or previous zoning on the site is POD, Planned Office District. The existing PUD would allow two six-story buildings with 299 unit of housing and 30,000 square feet of retail/restaurants and 10 owner occupied townhomes. The previous POD, Planned Office District would allow 113,000 square feet of office/medical office use.

The existing office buildings and parking ramp on the sites would be removed. The existing parking ramp is in a very poor state of repair. The project would be developed in two phases. The first phase would be the construction of a five-story 124,620 square foot office building and coffee shop with underground parking at the 7250 France Site. The 7200 site would include a ponding area, sidewalks, green space, landscaping and surface parking where the future building pad would be.

Phase 2 would include a similar sized building in height and square footage, with the future use of housing with retail (150 units). Permanent parking for Phase 1 would be required to be included in the Phase 2 development.

This request would require the following:

A Rezoning/Ordinance Amendment to revise the PUD-16, Planned Unit Development District to establish a new Overall Development Plan and Site Plan review for Phase 1.

ATTACHMENTS:

Staff Report and Memos

Applicant Narrative

AFO Review (Mic Johnson)

Site Location, Zoning, & Comp. Plan

Proposed Plans and Renderings 1 of 4

Proposed Plans and Renderings 2 of 4

Proposed Plans and Renderings 3 of 4

Proposed Plans and Renderings 4 of 4

Better Together Public Hearing Comment Report

Previously Approved Plans and Renderings

Southdale Design Experience Guidelines

Draft Ordinance

Traffic and Parking Study

Staff Presentation

Applicant Presentation



Date: September 14, 2022

To: Planning Commission

From: Cary Teague, Community Development Director

Subject: Zoning Ordinance Amendment, to revise the PUD-16 Zoning District to include a new Overall Development Plan and Site Plan Review for 7200 and 7250 France Avenue

Information / Background:

Orion Investment is requesting a Zoning Ordinance Amendment for a completely new redevelopment project for the subject property. This site received a rezoning approval to Planned Unit Development-16 in 2019; however, the project was never constructed. (See attached approved plans.) The underlying, or previous zoning on the site is POD, Planned Office District. The existing PUD would allow two six-story buildings with 299 unit of housing and 30,000 square feet of retail/restaurants and 10 owner occupied townhomes. The previous POD, Planned Office District would allow 113,000 square feet of office/medical office use.

The existing office buildings and parking ramp on the sites would be removed. The existing parking ramp is in a very poor state of repair. The project would be developed in two phases. The first phase would be the construction of a five-story 124,620 square foot office building and coffee shop with underground parking at the 7250 France Site. The 7200 site would include a ponding area, sidewalks, green space, landscaping and surface parking where the future building pad would be.

Phase 2 would include a similar sized building in height and square footage, with the future use of housing with retail (150 units). Permanent parking for Phase 1 would be required to be included in the Phase 2 development.

This request would require the following:

- A Rezoning/Ordinance Amendment to revise the PUD-16, Planned Unit Development District to establish a new Overall Development Plan and Site Plan review for Phase 1.

Flexibility is requested through the PUD for reduced setbacks building height, floor area ratio, building coverage and parking stalls.

The applicant has gone through the sketch plan process and per the attached applicant narrative have made the following changes to the plans in response:

- Address the West Promenade Street Room typology 1A:
 - incorporated Woonerf in north-south direction
 - bikes and vehicles share Woonerf with striped bike lanes and vegetated median strip.
 - due to extreme grade changes, the Woonerf itself was not determined to be appropriate location for pedestrians to share; pedestrian paths are closer to proposed buildings for safety and accessibility.
 - West 72 ½ street (AKA Danila Street)
 - Developed to become a pedestrian-only public realm and a network of accessible green spaces with art located at intersection with France
- Building scale and fenestration:
 - “75% of building walls to be at setback line to support creation of street room”
 - When considering Danila Street as “street” as defined by the applicable typologies, the proposed buildings meet this requirement.
 - “All building façades are prime (including parking) and must be designed accordingly. There is no back side of a building. “
 - Removed grocery store from program – all facades of proposed building have been carefully considered as prime with mix of human-scaled glazing and natural materials
 - “All facades on the first vertical 60 feet of a building (above grade) shall use natural materials facing the public realm.”
 - Natural materials have been selected for portions of all facades within first 60 feet of building above grade
 - “The landscape of the West Promenade should reinforce the characteristics of the neighborhood: tree lined to make sidewalks pleasant and safe to be on, creating a green vertical street room that is always pleasant to walk down”
 - Danila Street connects the greater Southdale district through this site with a pedestrian-scaled greenway featuring a mix of native landscape, paths with seating, and spaces primed for community events and art installations. This path connects further west to the beautified retention basin nature area, and to the north-south accessible pedestrian path connecting 72nd Street to Gallagher.

SUPPORTING INFORMATION

Surrounding Land Uses

- Northerly: Senior Housing, Sunrise assisted living facility; zoned PSR-4, Planned Senior Citizen District and guided OR, Office Residential.
- Easterly: Macy’s Home Store; zoned PCD-3, Planned Commercial District and guided MXC, Mixed Use Center.
- Southerly: Office building; zoned POD-I, Planned Office District and guided OR, Office Residential.
- Westerly: Apartments and duplexes; zoned PRD, Planned Residential District and guided High Density Residential, and zoned R-2, Double Dwelling Unit District and guided Low Density Residential.

Existing Site Features

The subject property is 5.2 acres in size, contains two existing 4-story office buildings, a parking ramp, surface parking and a wooded area. The parking ramp is in an extremely poor condition.

Planning

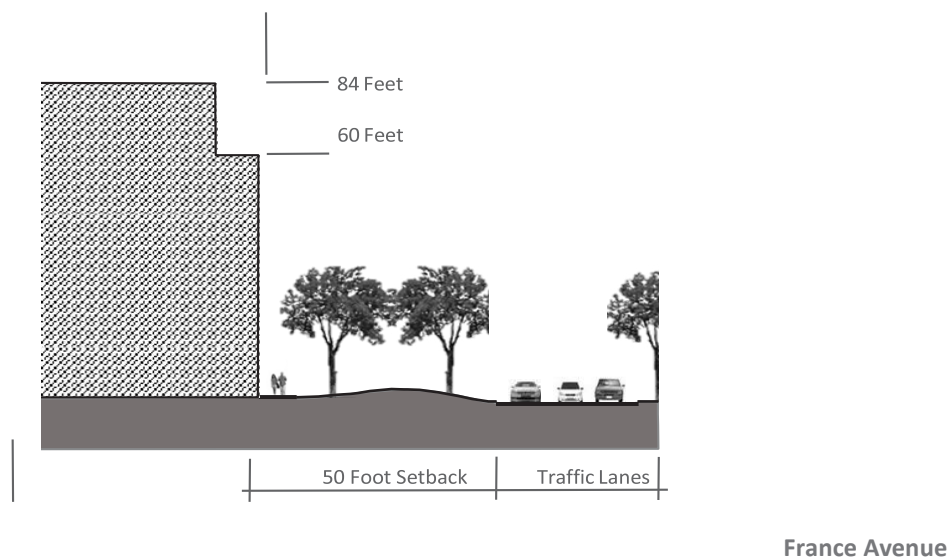
Guide Plan designation: OR, Office Residential.
Zoning: PUD-16, Planned Unit Development -16

Greater Southdale District Design Experience Guidelines - City Goals

The development pattern is generally consistent with the previously approved plans and does attempt to address the Greater Southdale District Design Experience Guidelines. The following remain the highlights the proposal: The division of the property into smaller blocks and the provision of public pedestrian and vehicle access and connections through the site (comprises 19% of the entire site); At final build out, there would be no visible surface parking stalls from France Avenue, and public realm created through the site. Phase I does include a surface parking lot with 83 parking stalls.

The applicant has not followed the street room typology for the east-west street (Typology 4), where the building must step in after the initial 60 feet in height. That is the same for the area along France. Below is the typology that should be followed along France:

Dimensional Characteristics of Street Room Typology 2 *Cornelia Overlay at France Avenue*



At sketch plan review, Mic Johnson, AFO, the City's consultant on the Greater Southdale Area Vision Plan reviewed the revised plans and has provided feedback. (See attached AFO review.) The applicant has attempted to address the AFO review. (See attached applicant narrative.)

Meeting these street room typologies could be made a condition of any approval.

Parking

Based on the City Code requirement, Section 36-1311, (this application was made prior to the adoption of the new parking regulations) Phase I, 124,620 s.f. of office and a 2,200 s.f. coffee shop would require 700 parking stalls (**423 under the new ordinance.**) The project would provide 317 parking stalls, 219 underground stalls and 15 surface stalls on the Phase I site, and 83 parking stalls on the Phase 2 site to the north. At full build out, including the additional 150 units of housing and 10,000 square feet of retail would require 900 parking stalls (**606 under the new ordinance.**) The project would provide 468 parking stalls.

Stantec conducted a parking study to determine if the parking provided would be adequate. The study concludes that there would be adequate parking in both Phase I and Phase 2. (See attached study.) Parking needed for Phase I is 289 stalls (317 provided) and Parking needed for Phase 2 is 356 stalls (468 provided).

Site Circulation/Access/Traffic

Primary access to the proposed development would be off Gallagher Drive and 72nd Street. The access off France Avenue has been eliminated. The result of the elimination of that access allows more green space on the site, as the right turn land is eliminated.

Stantec conducted a traffic study. The study concludes that the existing roadways can be supported by the project. (See attached traffic study.)

Landscaping

Based on the perimeter of the site, 55 overstory trees would be required. The proposed plans show that over 100 overstory trees on site. A full complement of understory shrubs and bushes are also proposed.

Grading/Drainage/Utilities

The city engineer has reviewed the proposed plans and found them to be acceptable subject to the comments and conditions outlined in the attached memo. A site improvement plan agreement would be required to outline public vs. private responsibilities and ownership for private improvement on public property. Any approvals of this project would be subject to review and approval of the Nine Mile Creek Watershed District, as they are the City's review authority over the grading of the site.

Building/Building Material

The building materials would be a combination of brick, stone, architectural metal panels, and glass. The applicant will have a materials board for review at the Planning Commission meeting.

Mechanical Equipment

Any rooftop and/or ground level equipment would have to be screened if visible from adjacent property lines.

Loading Dock/Trash Enclosures

Loading area and trash enclosures would be on the inside of the building/parking ramp.

Building Height

The previously approved project for this site allowed building height up to 67.5 feet. The previous building height requirement for this site was 4 stories and 48 feet. The applicant is requesting flexibility from this standard through the new PUD rezoning of 84 feet.

Within the underlying POD zoning district, the Edina City Code requires that buildings 5-6 stories tall be setback two times the height of the building from the property line of single-family homes. The building height is 84 feet; therefore a 168-foot setback is required. The distance as proposed would be 255 feet, therefore would be code compliant.

Living Streets/Multi-Modal Consideration

Sec. 36-1274. - Sidewalks, trails and bicycle facilities.

- (a) In order to promote and provide safe and effective sidewalks and trails in the City and encourage the use of bicycles for recreation and transportation, the following improvements are required, as a condition of approval, on developments requiring the approval of a final development plan or the issuance of a conditional use permit pursuant to article V of this chapter:
 - (1) It is the policy of the City to require the construction of sidewalks and trails wherever feasible so as to encourage pedestrian and bicycle connectivity throughout the City. Therefore, developments shall provide sidewalks and trails which adjoin the applicant's property:
 - a. In locations shown on the City's sidewalk and trail plan; and
 - b. In other locations where the council finds that the provision of such sidewalks and trails enhance public access to mass transit facilities or connections to other existing or planned sidewalks, trails or public facilities.
 - (2) Developments shall provide sidewalks between building entrances and sidewalks or trails which exist or which will be constructed pursuant to this section.
 - (3) Developments shall provide direct sidewalk and trail connections with adjoining properties where appropriate.
 - (4) Developments must provide direct sidewalk and trail connections to transit stations or transit stops adjoining the property.
 - (5) Design standards for sidewalks and trails shall be prescribed by the engineer.
 - (6) Nonresidential developments having an off-street automobile parking requirement of 20 or more spaces must provide off-street bicycle parking spaces where bicycles may be parked and secured from theft by their owners. The minimum number of bicycle parking spaces required shall be five percent of the automobile parking space requirement. The design and placement of bicycle parking spaces and bicycle racks used to secure bicycles shall be subject to the approval of the city engineer. Whenever possible, bicycle parking spaces shall be located within 50 feet of a public entrance to a principal building.
- (b) The expense of the improvements set forth in subsection (a) of this section shall be borne by the applicant.

The proposal to add 150 units of housing and an office building with retail/commercial uses on France Avenue with extensive pedestrian walkway opportunities through and around the perimeter of the site would enhance the pedestrian experience in the area. The plans are consistent with the vision document for the Greater Southdale Area by providing a 50-foot

setback on France Avenue and 72nd Street. The Setback on Gallagher Drive is proposed at 25 feet, consistent with the previously approved plans.

Planned Unit Development (PUD)

Per Section 36-253 the following are the regulations for a PUD:

- 1. Purpose and Intent. The purpose of the PUD District is to provide comprehensive procedures and standards intended to allow more creativity and flexibility in site plan design than would be possible under a conventional zoning district. The decision to zone property to PUD is a public policy decision for the City Council to make in its legislative capacity. The purpose and intent of a PUD is to include most or all of the following:***
 - a. provide for the establishment of PUD (planned unit development) zoning districts in appropriate settings and situations to create or maintain a development pattern that is consistent with the City's Comprehensive Plan;***
 - b. promote a more creative and efficient approach to land use within the City, while at the same time protecting and promoting the health, safety, comfort, aesthetics, economic viability, and general welfare of the City;***
 - c. provide for variations to the strict application of the land use regulations in order to improve site design and operation, while at the same time incorporate design elements that exceed the City's standards to offset the effect of any variations. Desired design elements may include: sustainable design, greater utilization of new technologies in building design, special construction materials, landscaping, lighting, stormwater management, pedestrian oriented design, and podium height at a street or transition to residential neighborhoods, parks or other sensitive uses;***
 - d. ensure high quality of design and design compatible with surrounding land uses, including both existing and planned;***
 - e. maintain or improve the efficiency of public streets and utilities;***
 - f. preserve and enhance site characteristics including natural features, wetland protection, trees, open space, scenic views, and screening;***
 - g. allow for mixing of land uses within a development;***
 - h. encourage a variety of housing types including affordable housing; and***
 - i. ensure the establishment of appropriate transitions between differing land uses.***
- 2. Applicability/Criteria***
 - a. Uses. All permitted uses, permitted accessory uses, conditional uses, and uses allowed by administrative permit contained in the various zoning districts defined in this Chapter shall be treated as potentially allowable uses within a PUD district, provided they would be allowable on the site under the Comprehensive Plan.***
 - b. Eligibility Standards. To be eligible for a PUD district, all development should be in compliance with the following:***
 - i. where the site of a proposed PUD is designated for more than one (1) land use in the Comprehensive Plan, the City may require that the***

- PUD include all the land uses so designated or such combination of the designated uses as the City Council shall deem appropriate to achieve the purposes of this ordinance and the Comprehensive Plan; any PUD which involves a single land use type or housing type may be permitted provided that it is otherwise consistent with the objectives of this ordinance and the Comprehensive Plan;***
- ii. permitted densities may be specifically stated in the appropriate planned development designation and shall be in general conformance with the Comprehensive Plan; and***
 - iii. the setback regulation, building coverage and floor area ratio of the most closely related conventional zoning district shall be considered presumptively appropriate, but may be departed from to accomplish the purpose and intent described in #1 above.***

The previously approved plans were found to meet the above standards, and therefore, the site was rezoned to PUD. While the plans have been downgraded to make for a more economically feasible project, staff believes the proposal would still meet the purpose and intent of the PUD, as most of the above criteria would be met. The site is guided in the Comprehensive Plan for “Office Residential,” which allows for office, retail and multi-family housing in a density range of up to 75 units per acre. The proposal still is a mixed-use (horizontal mixed-use) development including office, retail, and multi-family residential within the required density range.

The previous project and existing zoning on this site requires affordable housing to be provided within the project. Staff recommends that the multi-family housing development provide 10% of the units within the project for affordable housing.

The proposal would create a more efficient and creative use of the property than existing. The site would be divided up with a public north-south vehicle, bike and pedestrian path as well as an east west pedestrian path. Parking would be primarily enclosed with limited surface parking behind the building, eliminating the surface parking that exists today.

The project will be required to meet the City’s sustainability policy. The sustainability coordinator has reviewed the proposal and submitted comments within the engineering memo. (See attached.)

Comprehensive Plan

The proposed density and uses are all consistent with the City’s 2040 Comprehensive Plan. As mentioned, the site is designated as OR, Office Residential. The description of Land Uses within the Comprehensive Plan is as follows:

- *Transitional areas along major thoroughfares or between higher-intensity districts and residential districts.*
- *Upgrade existing streetscape and building appearance, improve pedestrian and transit environment.*
- *Primary uses are offices, attached or multifamily housing. Secondary uses: Limited retail and service uses (not including “big box” retail).*
- *Encourage structured parking and open space linkages where feasible; emphasize the enhancement of the pedestrian environment.*
- *50%/50% estimated residential/commercial mixed-use.*

Compliance Table

	City Standard (PUD-16) (Measured to the curb)	POD District (Measured to the curb)	Proposed (Measured to the curb)
Front – 72 nd Street Front – France Ave. Front – Gallagher Drive Side – West (north half) Side – West (south half)	35 feet 39 feet 20 feet 90 feet 45 feet	30 feet 50 feet 30 feet 20 feet 20 feet	30 feet 50 feet 25 feet* 150 feet 90 feet
Building Height	6 stories and 67 feet	4 stories and 48 feet	5-6 stories and 84 feet*
Floor Area Ratio (FAR)	1.8	.50	1.0*
Building Coverage		.30	.31*
Parking		Office/retail – 456 spaces Housing – 150 spaces Total – <u>606 spaces</u>	468 spaces*

****Requires Variance from the original zoning requirement***

PRIMARY ISSUES/STAFF RECOMMENDATION

Primary Issues

- **Are the proposed changes to the approved Overall Development Plan reasonable to justify amending the PUD rezoning for this site?**

Yes. Staff does support the revised PUD, for the following reasons:

1. While the plans are more flawed than the previously approved plans, they still do generally address the Greater Southdale District Design Experience Guidelines. Highlights continue to include: The division of the property into smaller blocks and the provision of pedestrian, bicycle and vehicle access and connections through the site both east-west and north-south; increasing the public realm on the site; eliminating most of the surface parking stalls, with none being visible from France Avenue in the Phase 2 build out. The street typology that requires buildings step in 10 feet above 60 feet in height shall be made a condition of approval. Affordable housing should be required within the future apartment project as well as public art located along France Avenue. They will be seeking a LEED certified building in Phase I.
2. The project would meet the following goals of the Comprehensive Plan:
 - Primary uses are offices, attached or multifamily housing. Secondary uses: Limited retail and service uses (not including "big box" retail).
 - Encourage structured parking and open space linkages where feasible; emphasize the enhancement of the pedestrian environment.
 - Locate and orient vehicle parking, vehicular access, service areas and utilities to minimize their visual impact on the property and on adjacent/surrounding properties, without compromising the safety and attractiveness of adjacent streets, parks, and open spaces.
 - Provide sidewalks along primary streets and connections to adjacent neighborhoods along secondary streets or walkways.
 - Limit driveway access from primary streets while encouraging access from secondary streets.
 - Provision of the north-south public vehicle, bike and pedestrian connection through the site.
 - Encourage the development of parking lots or structures so they can be shared by more than one building on the site or by buildings on neighboring sites, and which can transition over time to other uses if parking needs change.
 - Increase mixed-use development where supported by adequate infrastructure to minimize traffic congestion, support transit, and diversify the tax base.
 - Support the development of mixed-use districts that provide a variety of living opportunities within a walkable and livable area.
 - Recognize and support commercial, office, and industrial job centers that draw workers from the city and across the region.
 - Ensure that the city's roads continue to evolve to act as connectors, rather than as barriers. Incorporate amenities and infrastructure into public corridors to make them beautiful, efficient, and multimodal public spaces that contribute to community identity and pride.
 - 50%/50% estimated residential/commercial mixed-use.
3. The proposal meets the City's criteria for PUD zoning. In summary the PUD zoning would:

- Create a more pedestrian-friendly development with the construction of improved sidewalks and connections to the Promenade. The project would bring vibrancy to the area.
 - The building would be of high-quality architectural brick, metal, stone, and glass.
 - Ensure that the buildings proposed in Phase 1 would be the only buildings built on the site unless an amendment to the PUD is approved by City Council. Phase 2 would be required to be multi-family residential with some retail commercial as long as there is adequate parking on the site for all phases.
 - Project would add to the City's affordable housing stock by providing 10% of the units in Phase 2 for affordable housing.
 - Provide for a more creative site design, consistent with goals and policies in the Comprehensive Plan.
 - Enhance green space and landscaping and utilize sustainable concepts.
 - Meet the City's sustainability policy, including seeking a LEED certified building in Phase 1.
 - Provide 19% of the site for public use.
4. The provision of public space still makes up 19% of the site for the interior public sidewalks, driveways, bike space and public realm. If the public space in front of the building on France, Gallagher and 72nd street was included it would make up 38% of the site. (See attached public realm graphic.)
 5. The proposed height is justified. While the proposed buildings would be taller than the previously approved project it would still meet the required setback of buildings 5-6 feet tall to R-1 property. Edina City Code requires that buildings 5-6 stories tall be setback two times the height of the building from the property line of single-family homes. The building height is 84 feet; therefore a 168-foot setback is required. The distance as proposed would be 255 feet.
 6. The existing roadways and proposed parking would support the project. Stantec conducted a traffic and parking impact study and concluded that the proposed development could be supported by the existing roads and proposed parking.

Recommendation

A case can be made for approval and denial of this project. Below provides options for the Planning Commission and City Council to consider:

Denial

Recommend the City Council deny the request for Zoning Ordinance Amendment and new Overall Development Plan for PUD-16.

Denial is based on the following findings:

- I. Does not adequately address the Greater Southdale District Design Experience Guidelines like the originally approved plan did. The streetscape is less active due to more street level parking within the structures. The street typologies have not been adequately followed.

2. The proposed project would not meet the criteria for Rezoning to PUD.
3. The proposed building height is too tall compared to the previously approved project for the site, and there is not an adequate setback to Gallagher Drive.

Approval

Recommend the City Council approve the request for Zoning Ordinance Amendment and new Overall Development Plan for PUD-16.

Approval is based on the following findings:

1. The proposed land uses, and density are consistent with the Comprehensive Plan.
2. The proposal still meets the City's criteria for PUD zoning. The PUD zoning would:
 - a. Create a more pedestrian-friendly development with the construction of improved sidewalks and connections to the Promenade. The project would bring vibrancy to the area.
 - b. The building would be of high-quality architectural brick, metal, stone, and glass.
 - c. Ensure that the buildings proposed in Phase 1 would be the only buildings built on the site unless an amendment to the PUD is approved by City Council. Phase 2 would be required to be multi-family residential with some retail commercial as long as there is adequate parking on the site for all phases.
 - d. Project would add to the City's affordable housing stock by providing 10% of the units in Phase 2 for affordable housing.
 - e. Provide for a more creative site design, consistent with goals and policies in the Comprehensive Plan.
 - f. Enhance green space and landscaping and utilize sustainable concepts.
 - g. Meet the City's sustainability policy, including seeking a LEED certified building in Phase 1.
 - h. Provide 19% of the site for public use.
3. The PUD would ensure that the development proposed would be the only building that would be allowed on the site unless an amendment to the PUD is approved by City Council.
4. The plans address the Greater Southdale District Design Experience Guidelines as follows: The division of the property into smaller blocks and the provision of pedestrian, bicycle and vehicle access and connections through the site both east-west and north-south; increasing the public realm on the site; eliminating most of the surface parking stalls, with none being visible from France Avenue in the Phase 2 build out. Affordable housing should be required within the future apartment project as well as public art located along France Avenue
5. The proposed project would meet the following goals and policies of the Comprehensive Plan:
 - Primary uses are offices, attached or multifamily housing. Secondary uses: Limited retail and service uses (not including "big box" retail).

- Encourage structured parking and open space linkages where feasible; emphasize the enhancement of the pedestrian environment.
 - Locate and orient vehicle parking, vehicular access, service areas and utilities to minimize their visual impact on the property and on adjacent/surrounding properties, without compromising the safety and attractiveness of adjacent streets, parks, and open spaces.
 - Provide sidewalks along primary streets and connections to adjacent neighborhoods along secondary streets or walkways.
 - Limit driveway access from primary streets while encouraging access from secondary streets.
 - Provision of the north-south public vehicle, bike and pedestrian connection through the site.
 - Encourage the development of parking lots or structures so they can be shared by more than one building on the site or by buildings on neighboring sites, and which can transition over time to other uses if parking needs change.
 - Increase mixed-use development where supported by adequate infrastructure to minimize traffic congestion, support transit, and diversify the tax base.
 - Support the development of mixed-use districts that provide a variety of living opportunities within a walkable and livable area.
 - Recognize and support commercial, office, and industrial job centers that draw workers from the city and across the region.
 - Ensure that the city's roads continue to evolve to act as connectors, rather than as barriers. Incorporate amenities and infrastructure into public corridors to make them beautiful, efficient, and multimodal public spaces that contribute to community identity and pride.
 - 50%/50% estimated residential/commercial mixed-use.
6. The existing roadways and proposed parking would support the project. Stantec conducted a traffic and parking impact study and concluded that the proposed development could be supported by the existing roads and proposed parking.

Approval is subject to the following Conditions:

1. The Final Development Plans must be generally consistent with the Preliminary Development Plans dated August 28, 2022.
2. The Final Landscape Plan must meet all minimum landscaping requirements per Chapter 36 of the Zoning Ordinance. A performance bond, letter-of-credit, or cash deposit must be submitted for one and one-half times the cost amount for completing the required landscaping, screening, or erosion control measures at the time of any building permit.
3. Provision of code compliant bike racks for each use near the building entrances.
4. The Final Lighting Plan must meet all minimum requirements per Section 36-1260 of the City Code.
5. Roof-top mechanical equipment shall be screened per Section 36-1459 of the City Code.
6. Submit a copy of the Nine Mile Creek Watershed District permit. The City may require revisions to the approved plans to meet the district's requirements.
7. A Developer's Agreement/Site Improvement Plan Agreement is required at the time of Final

Approval.

8. Phase 2 of the project must include affordable housing within the project, compliant with the City's affordable housing policy. Final determination to be made at final approval for Phase 2.
9. Compliance with the conditions outlined in the director of engineering's memo dated September 1, 2022.
10. Compliance with the Stantec Consulting Traffic & Parking Study recommendations.
11. Subject to the Zoning Ordinance Amendment revising the PUD-16, Planned Unit Development for this site.
12. Public sidewalks must be a minimum of 5 feet in width with a 5-foot boulevard on Gallagher Drive and West 72nd Street, and 8 feet minimum width with an 8-foot boulevard on France Avenue.
13. Dedication of public access easements of the east-west and north-south sidewalks and drive-aisles through the site and sidewalks around the perimeter of the site.
14. Submittal of a construction management plan subject to review and approval of city staff prior to issuance of a building permit. The plan must demonstrate minimal impact to pedestrian and vehicle movement.
15. Hours of construction must be consistent with City Code.
16. The property owner shall be responsible for the maintenance of internal sidewalks and drive aisles. The City would snow plow a 5-foot path of the sidewalk on France. The property owners would be responsible for the remaining areas.
17. Public art should be provided along street frontages and the courtyard.
18. Final Plans shall comply with the street typologies in the Southdale Design Experience Guidelines along France Avenue and the building step in at least ten feet at the 60-foot building height.

Staff Recommendation

Staff recommends approval of the request subject to the findings and conditions listed above.

Deadline for a City decision: November 1, 2022



DATE: 9/1/2022

TO: 7200 – 7250 France Ave, Owner and Development Team

CC: Cary Teague – Community Development Director

FROM: Chad Millner, PE, Director of Engineering
Ross Bintner, PE, Engineering Services Manager
Zuleyka Marquez, PE, Graduate Engineer
Mattias Oddsson, Engineering Services Intern
Grace Hancock, Sustainability Manager

RE: 7200 – 7250 France Ave – Development Review

The Engineering Department has reviewed the subject property for pedestrian facilities, utility connections, grading, flood risk, and storm water. Plans reviewed included civil, landscape, and survey drawings dated 8/1/2022. Additional staff comments on transportation elements are attached.

Review Comment		Required For
General		
1.	Deliver as-build records of public and private utility infrastructure post construction, including xyz coordinates for storm, water, and sanitary infrastructure.	Certificate of Occupancy
2.	Staff has had discussions with the Sunrise Senior Living facility north of the project area. They have concerns with noise and vibrations and the effect on their residents. Communication with them will be required to better understand their concerns.	
Survey		
3.	An existing and proposed site condition survey is required.	Grading/Building Permit
3.1	Show all existing and proposed public and private easements. Update existing private easement regarding drainage as needed. Provide City a copy if updated.	Grading/Building Permit
3.2	Provide easement over Danila St.	Grading/Building Permit
Living Streets		
4.	Design sidewalks to meet ADA requirements.	Grading/Building Permit
5.	Saw cut concrete sidewalk joints on public sidewalks.	Grading/Building Permit
6.	Public sidewalks to be minimum 5' in width with a 5' boulevard on Gallagher Drive and West 72 nd Street and 8' minimum width with 8' boulevard on France Avenue.	Grading/Building Permit



7.	Consider narrowing north-south access road to 12' lanes to create a wider (8'-10") shared-use path on the west side of access road instead of east side across the 7250 France parcel.	General Comment
8.	Increase width of sidewalk to 8' minimum or 10' preferred for west sidewalk across 7200 France to create a shared-use route or a West Promenade.	General Comment
9.	On-site sidewalks are the responsibility of the property owner to maintain, including snow plowing. City will maintain sidewalks adjacent to France Avenue and Gallagher Drive.	General Comment
10.	Consider relocating access road medians to pedestrian crossing to serve as refuge island.	General Comment
11.	Consider covered outdoor bike parking and dedicated bike parking with underground garage.	General Comment

Traffic and Street

12.	Review fire access requirements with fire department. Fire truck turning template attached.	Grading/Building Permit
13.	Provide traffic study and implement City-approved recommendations.	Grading/Building Permit
14.	Driveway entrance permit required for entrance reconstruction. Add pedestrian ramps. Comply with standard plates 410 and 415.	Building Permit
15.	Road patching shall conform to Edina standard plates 540 and 543 on Gallagher Dr and 540 and 545 on 72 nd St W. Comply with County requirements for France Ave S.	Certificate of Occupancy
16.	Proposed France Avenue lighting must remain consistent with Canto light fixtures and be the responsibility of the property owner.	Grading/Building Permit

Sanitary and Water Utilities

17.	Verify fire demand and hydrant locations.	Grading/Building Permit
18.	Domestic water shall be sized by the developer's engineer.	Grading/Building Permit
19.	A looped watermain to the 72 nd street right of way to create a public loop is required. Developer should create plan to serve future 7200 building with private main and City will pay oversize to create 8" consistent with City of Edina Water Supply Plan (SEH 2018) The 8" could be phased with any requirements for developer improvements on 72 nd street, or stubbed to the right of way and finished with a future City lead improvement.	
20.	Domestic sanitary shall be sized by the developer's engineer.	Grading/Building Permit

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21.	Apply for a sewer and water connection permit with Public Works.	Prior to Starting Utility Work
21.1	Meter required for building service line and combined lines. No meter required for fire only service line.	Grading/Building Permit
21.2	Public Works to determine acceptable installation methods.	Grading/Building Permit
22.	Disconnected sanitary and water services to be capped at main.	
23.	A SAC determination will be required by the Metropolitan Council. The SAC determination will be used by the City to calculate sewer and water connection charges	Grading/Building Permit
24.	Single connection from main for fire and domestic, split after main connection.	Grading/Building Permit
25.	Verified well of unknown status located onsite at 7200 France Ave S. Provide well sealing record. Wells not in use must be sealed by a licensed well contractor per MN Rules, Chapter 4725.	Grading/Building Permit
Storm Water Utility		
26.	Provide geotechnical report with soil borings.	Grading/Building Permit
27.	Provide hydraulic and hydrologic report meeting watershed and state construction site permit requirements.	Grading/Building Permit
28.	Submit watershed district permit and copies of private maintenance agreement in favor of watershed.	Grading/Building Permit
29.	Provide high points at/near property lines to verify runoff from both W 72nd St. and France Avenue are eliminated (at permit and with final as-built survey).	Grading/Building Permit
30.	<p>Existing basin LE_20 flood elevation appears to be lowered by this proposal through the creation of additional flood storage. This basin has the potential to have extended inundation of stormwater in flood events.</p> <p>Confirm that building lowest floor elevation will be a minimum of 2' above the 100-year HWL of basin.</p> <p>-or-</p> <p>Applicant to request a site-specific standard with engineer to provide groundwater analysis (or confirm that WD standard is properly used) to confirm that flood exposure does not exist, or inform the vulnerability reducing measures such as floodproofing of the underground parking lot (CWRMP Section 3.1.2.2).</p>	Grading/Building Permit

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31.	<p>Infiltration basins are proposed on a site with MPCA hazardous waste and underground tanks listed. Refer to MNR000018127 and TS0019522 and MNR000017855.</p> <p>Confirm soil remediation meets MPCA/MDH Guidance to allow soil infiltrating features. Nine Mile Creek WD permit review should specifically address this issue.</p>	Grading/Building Permit
32.	<p>These sites are served by a private lift station and the downstream stormwater system has limited capacity. Rate will be limited to 0.5 cfs or existing conditions, whichever is less. Current plan meets this requirement through replacement of private station.</p> <p>Provide city with documentation of ownership and responsible party for maintenance of private lift station.</p>	Grading/Building Permit
33.	<p>The 7200 property provides conveyance of stormwater and flood storage for adjacent properties to the south through three private easements noted on ALTA title survey. Review legal obligations of easement and propose update that are acceptable to each property.</p>	Grading/Building Permit
Grading Erosion and Sediment Control		
34.	<p>A SWPPP consistent with the State General Construction Site Stormwater Permit is required.</p>	Grading/Building Permit
Constructability and Safety		
35.	<p>Construction staging, traffic control, and pedestrian access plans will be required.</p>	Grading/Building Permit
36.	<p>Developer is responsible for maintaining temporary open green space on 7200 France Ave S in Phase 1A.</p>	General
37.	<p>Suggest temporary/permanent railing along sidewalks with steep slopes adjacent.</p>	
38.	<p>Retaining walls over 4-ft in height require design by a structural engineer.</p>	Grading/Building Permit
39.	<p>Provide an existing condition survey (e.g. video or thorough photography) of ROW around the sites.</p>	Grading/Building Permit
Sustainability		
40.	<p>Project must follow Sustainable Buildings Policy</p>	General
Other Agency Coordination		

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41.	MDH, MPCA and MCES permits required as needed.	Grading/Building Permit
42.	Nine Mile Creek Watershed District permit is required.	Grading/Building Permit
43.	Impacts to France Avenue and France Avenue ROW will require Hennepin County permits.	Grading/Building Permit

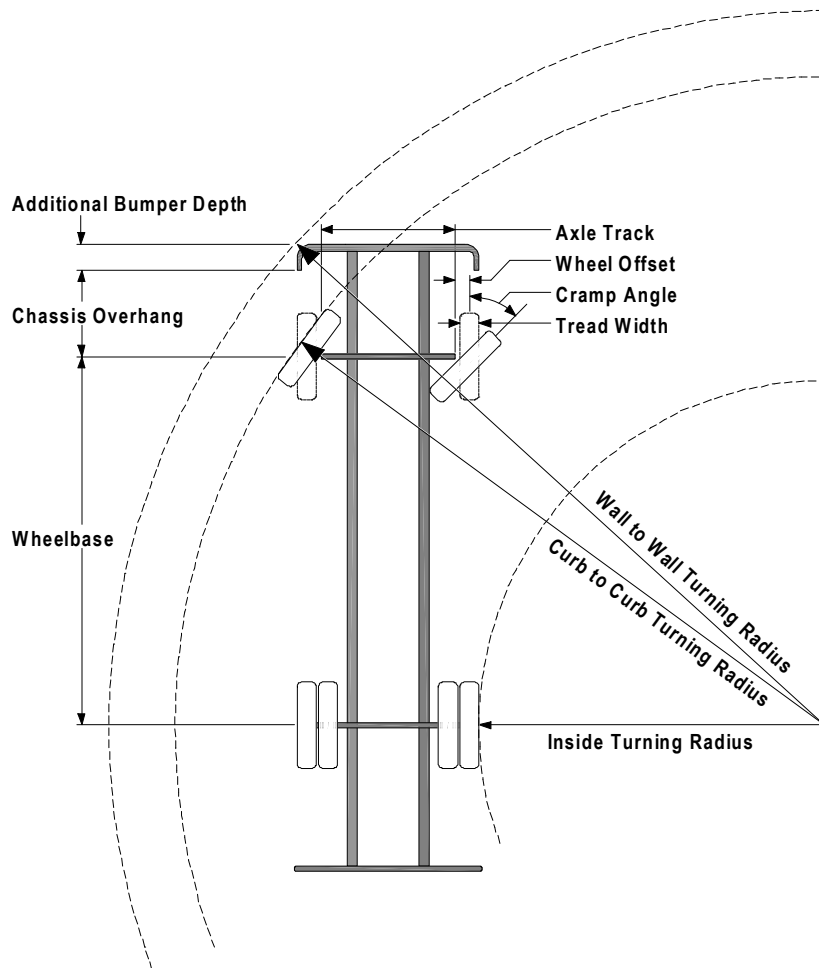
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Turning Performance Analysis

5/1/2013



Parameters:

Inside Cramp Angle:	45.00 °
Axle Track:	81.92 in.
Wheel Offset:	5.25 in.
Tread Width:	16.60 in.
Chassis Overhang:	65.99 in.
Additional Bumper Depth:	19.00 in.
Front Overhang:	84.99 in.
Wheelbase:	258.00 in.

Calculated Turning Radii:

Inside Turn:	20 ft. 4 in.
Curb to Curb:	36 ft. 8 in.
Wall to Wall:	41 ft. 1 in.

Comments:

Truck 12205

Components	PRIDE #	Description
Front Tires	0078244	Tires, Michelin, 425/65R22.50 20 ply XZY 3 tread
Chassis	0070220	Dash-2000, Chassis, PAP/SkyArm/Midmount
Front Bumper	0123625	Bumper, 19" extended, Imp/Vel
Aerial Device	0006900	xxxAerial, 100' Pierce Platform

Notes:

Actual Inside Cramp Angle may be less due to highly specialized options.

Curb to Curb turning radius calculated for a 9.00 inch curb.



Definitions:

Inside Cramp Angle	Maximum turning angle of the front inside tire.
Axle Track	King-pin to king-pin distance of the front axle.
Wheel Offset	Offset from the center-line of the wheel to the king-pin.
Tread Width	Width of the tire tread.
Chassis Overhang	Distance from the center-line of the front axle to the front edge of the cab. This does not include the bumper depth.
Additional Bumper Depth	Depth that the bumper assembly adds to the front overhang.
Wheelbase	Distance between the center lines of the vehicle's front and rear axles.
Inside Turning Radius	Radius of the smallest circle around which the vehicle can turn.
Curb to Curb Turning Radius	Radius of the smallest circle inside of which the vehicle's tires can turn. This measurement assumes a curb height of 9 inches.
Wall to Wall Turning Radius	Radius of the smallest circle inside of which the entire vehicle can turn. This measurement takes into account any front overhang due to the chassis, bumper extensions and/or aerial devices.

- 1 Widen sidewalk to 8' shared-use path to form part of West Promenade
- 2 Narrow north-south street to discourage through traffic
- 3 Remove sharrows, encourage cyclists to use off-street facility
- 4 Remove sidewalk between drive lanes
- 5 Relocate median to serve as pedestrian refuge island
- 6 Consider covered outdoor bike parking
- 7 Consider dedicated bike

- 1 Widen sidewalk to 8' shared-use path to form part of West Promenade
- 2 Narrow north-south street to discourage through traffic
- 3 Remove sharrows, encourage cyclists to use off-street facility
- 4 Remove sidewalk between drive lanes
- 5 Relocate median to serve as pedestrian refuge island
- 6 Consider covered outdoor bike parking
- 7 Consider dedicated bike parking within underground garage

7200 & 7250 France P.U.D. Application Narrative

8/29/2022

Detailed project narrative explaining the proposed plans

This project's goal is to transform a dilapidated and historically problematic site into a thriving center for business and community activity. Development of this site has the potential to create 450-600 jobs when assuming 3-4 per 1,000 sf in jobs

The extreme grade change within the site, paired with the requirement to address the 100-year flood stormwater needs for the adjacent area, provided numerous challenges to the project team. However, we were able to use these challenges to our advantage by working with the existing grade, keeping podium heights human-scaled and at street level, and incorporating ramps and stairs for improved pedestrian circulation while the roadway remains safely shared between bikes and vehicles.

Below is a summary of the proposed project's metrics compared to the previously approved PUD (Ordinance No. 2018-15_PUD-16) on the same site.

- Building Height
 - **Previously approved** maximum PUD building height: 88'-6" feet above Gallagher with 7 stories
 - **Proposed** maximum PUD building height: 84'-0" above Gallagher (see Site Section Comparison diagram for reference).
- Gross Floor Area (GFA)
 - **Previously approved** GFA:
 - Residential: 323,690 (6 floors)
 - Retail: 29,300 (1 floor)
 - Parking: 132,842 (2 floors)
 - **Proposed** GFA of 7250 (calculated per City of Edina's definition):
 - General Office: 124,620 (5 floors)
 - Parking: 97,080 (2 floors below grade)
- Parking Metrics
 - **Previously approved** parking total: 590 stalls
 - **Proposed** parking:
 - 7250 France (Phase 1A)
 - On grade: 15 stalls
 - Below grade: (P1: 110) + (P2: 109) = 219 stalls
 - Total 7250 parking: 234 stalls
 - 7200 France
 - (Phase 1B): 83 temporary parking stalls
 - (Phase 2): approx. 234 enclosed
 - Total 7200 parking: 317 stalls (pending final design)
 - 7200 + 7250 Totals by Phase
 - Total Phase 1A: 234 parking stalls
 - Total Phase 1B: (234 + 83) = 317 parking stalls
 - Total Phase 2: potentially 468 (pending final design for 7200)

The proposed uses

- 7250 France:
 - General Office
 - Retail (approximately 2,000 SF)
- 7200 France:
 - Mixed use to potentially include to following:
 - 150 Dwelling Units
 - 5 Story Building
 - Residential: ~150,000 GFA
 - Retail

Proposed public easements

- France Ave setback becomes pedestrian corridor with landscaped paths, treescapes, and seating areas.
- Stormwater retention basin with landscaped paths and overlooks on west side of site.
- Danila Street becomes pedestrian-only corridor connected by ADA-compliant ramps and paths throughout entirety of site.

How a PUD is justified with this proposal

Per Sec 36-253 of City of Edina Zoning code:

- **sustainable design:** 7250 will pursuing LEED certification; sustainable efforts include energy efficient MEP systems, reduced landscape irrigation needs through use of native/drought tolerant plantings, reduced light pollution, EV charging capacity, among other efforts.
- **Landscaping** design creates attractive public amenities for the surrounding community; Danila Street provides a courtyard that can be utilized for accessible pedestrian access to on-site businesses during the day and then utilized for community events in the evening. The proposed storm-water basin incorporates public viewing and seating areas along with ADA accessible pathways across the site. France Ave setback becomes pedestrian corridor with landscaped paths, treescapes, and seating areas.
- **stormwater management:** new storm-water basin has been designed to address the historically problematic storm water issues on site while also transforming it into a public amenity with overlooks, seating areas, and landscaped paths. The basin public amenity area is sunken below street level to provide a peaceful respite from the streetscape, immersed in a natural setting.
- Total drainage area addressed with this design: 350,232 sf, including 43,451sf of adjacent residential lots, and 104,076sf from 7300 Gallagher.
- **pedestrian-oriented design:** provides a human-scaled street grid by introducing Danila Street at the midpoint of the site, a pedestrian-only path with ADA-compliant ramps, stairs, benches and landscaping from France to the new North-South local street and to the new storm-water basin public nature area on the west side of the site.
- **transition to residential neighborhoods:** by locating the buildings along the east side of site adjacent to France, the density is located away from the adjacent residential buildings. The scale of the proposed buildings is lower or similar to the previously approved PUD; proposed building heights provide a more gradual transition from the residential neighborhoods in the west to the commercial center located to the east across France Ave.

How the phasing will work

- **Phase 1A (baseline assumption)**
 - **7250 France:** Site prep and building construction.
 - **7200 France (west side of site):** will become open greenspace as a temporary condition until design for 7200 development is finalized.
 - **7200 France (east side of site):** will be developed to meet anticipated stormwater needs for both parcels in their developed form with a vegetated stormwater detention basin along the west side of the site. Area designed with careful intention to become a community asset by providing an accessible and beautiful natural area with paths, overlooks, and seating.
 - **New North-South local street:** will create the north-south multi-modal connection providing direct shared vehicle and bike corridor from Gallagher to 72nd street; the north-south pedestrian connection occurs closer to the buildings in order to provide a safer, more welcoming, and ADA-compliant path than would otherwise would have been possible with the extreme grade change of the existing site.
 - **New East-West pedestrian street – Danila St:** will provide ADA-compliant path that connects pedestrian traffic from France Ave across site. Designed to become an extension of the ground level building amenities and retail with entries and seating that spill out onto the public plaza. Which phase?
 - **France Ave frontage:** will provide fully landscaped greenspace and hardscaped pedestrian plaza along the entirety of the 50' setback fronting both 7200 and 7250.
- **Phase 1B**
 - Phase 1A is ongoing.
 - Pending finalization of tenant types at 7250, supplementary parking for that building can be provided on existing surface lot portion of 7200. This would be a temporary condition until 7200 is developed. When the 7200 building is completed, it will provide similar quantities and distribution of parking as the 7250 building. Development of 7200 and the resulting total parking counts will meet the City of Edina's requirements.
- **Phase 2**
 - **7200 France (east side of site):** Site prep and building construction. The building at 7200 will be of similar height and size of the 7250 building. 7200 France will similarly contain enclosed parking so that the majority of parking is not visible from surrounding areas. The remaining areas of the site are to remain as developed in previous phases.

*Bullet point summary of how you have implemented recommendations from the sketch plan review
(what changes have been made to the plans)*

- Address the West Promenade Street Room typology 1A:
 - Incorporated multi-modal local street in north-south direction
 - Bikes and vehicles share this street with striped bike lanes and vegetated median strip.
 - Due to extreme grade changes, the street itself was not determined to be appropriate location for pedestrians to share; pedestrian paths are closer to proposed buildings for safety and accessibility.
 - West 72 ½ street (AKA Danila Street)
 - Developed to become a pedestrian-only public realm and a network of accessible green spaces with art located at intersection with France.
- Building scale and fenestration:
 - “75% of building walls to be at setback line to support creation of street room”
 - When considering Danila Street as “street” as defined by the applicable typologies, the proposed buildings meet this requirement.
 - “All building façades are prime (including parking) and must be designed accordingly. There is no back side of a building. “
 - Removed grocery store from program – all facades of proposed building have been carefully considered as prime with mix of human-scaled glazing and natural materials.
 - “All facades on the first vertical 60 feet of a building (above grade) shall use natural materials facing the public realm.”
 - Natural materials have been selected for portions of all facades within first 60 feet of building above grade.
 - “The landscape of the West Promenade should reinforce the characteristics of the neighborhood: tree lined to make sidewalks pleasant and safe to be on, creating a green vertical street room that is always pleasant to walk down”
 - Danila Street connects the greater Southdale district through this site with a pedestrian-scaled greenway featuring a mix of native landscape, paths with seating, and spaces primed for community events and art installations. This path connects further west to the beautified retention basin nature area, and to the north-south accessible pedestrian path connecting 72nd Street to Gallagher.

Architecture Field Office

2200 Zane Avenue North
Minneapolis, MN 55422

City of Edina

Cary Teague, Community Development Director
4801 W. 50th Street
Edina, MN 55424

To

From

Mic Johnson, FAIA

Date

August 10, 2022

Cary:

At your request, we reviewed the PUD submission for the proposed redevelopment of 7200-7250 France Avenue South based on our experience working with the Greater Southdale Work Group to craft a physical vision for how their guiding principles may translate to the built environment. The resulting vision for development in the district is to create an enhanced human experience along existing major and new connector streets, with overall experience shaped via landscape setbacks, building step backs, a hierarchy of street typologies, transparency at street level, minimizing the impact of the car, and managing storm water as an amenity. The outcome of our collaborations with the Work Group is described in the urban design chapter of the Greater Southdale District Plan and resulted in the Greater Southdale District Design Experience Guidelines (DEG).

The DEG were developed with the understanding that larger sites may yield multiple blocks to be developed, potentially with infill projects that follow the initial phase of development. Every site has a program that is relevant to its own successful operations, and should stay within that footprint to ensure flexibility for future adjacent infill development. On grade parking is strongly discouraged – particularly a full block of on-grade parking. Unless all of a site's parking and broader site infrastructure needs are addressed in the first phase of a PUD, we believe it is in the best interest of both the developer and the City for each block to be independently developed – without program needs for one site being accommodated on an adjacent one. This means that in the future there is not a conflict that would inhibit a site to be fully realized. The proposed phasing of this master plan, especially as it relates to parking, blurs this line and makes us uncertain of the site's future flexibility.

This particular site is unique in that it is influenced by four street typologies (Typology 1: Promenades and Transition Zones; Typology 2: Cornelia Overlay Zone; Typology 3: New Local Streets; Typology 5: Boulevards), which makes it more important that the development of each block be approached separately. Each façade of each block has a different role to play. And, each block has a different role to play in the larger development, the north half of this property has the retention pond; this should be a different consideration than the south half. The master plan should be clear about those distinctions, but in this case the master plan for the PUD is not clear as there is not a clear division between the two blocks.

The woonerf experience illustrated in this proposal offers no transparency into buildings, but rather views of blank walls, lots of stairs, ramping, a parking access door, a typical ramped parking entry, and no pedestrian sidewalk. All we can imagine is that approach would be duplicated in the design framework for Phase 2. These elements are not in the Design Experience Guidelines for a reason.

The kind of space they create is not human-centric, does not promote equity of experience, and does not encourage people to 'come to and stay at' a place. The following comments and questions are intended to guide clarification from the applicant as to why this is the best solution for this site, located at a very important gateway at Gallagher and France and a transition into the neighborhood to the west.

Site Topography and Building Organization

Throughout the report, and by example, the PUD submission itself, it is mentioned that the grades on the site are extreme. The existing topography is 10-20 feet below the level of the surrounding grades. However, when one considers Gallagher Drive (elevation 859' per ESG drawings) and West 72nd Street (elevation 855' per ESG drawings), and the south floor elevation of the proposed new Phase 1 entry (beginning at 857' per ESG drawings), this represents a 3-4 foot difference across the site's north-south axis, which stretches 560 feet—making the experience of walking around the site on West 72nd, France and Gallagher Drive a relatively flat one. The DEG goal was to use the woonerf as the fourth street that connects Gallagher to 72nd (3-4 foot difference in elevation) to make it easy and comfortable for bicycle and pedestrian use. Filling in the empty holes left by the previous buildings with below-grade parking would bring the grade of all of the building footprints up to street level, allowing that to be the starting point for vertical construction. The woonerf would be a true woonerf (ADA compliant) and would be constructed roughly at the same grade as France Avenue. The exterior car ramps, parking garage access doors, stairs, and ADA ramps shown in this proposal all seem unnecessary in a phased master plan for this site.

Transition to Residential Neighborhood

The DEG goal is not to move density away from the adjacent neighborhoods but to create a transition that is framed by the scale of the neighborhood. In this part of the district, one-, two-, or three-story buildings are envisioned to be fronting on a street (West Promenade), defined as a woonerf – meaning combined pedestrians, bicycles and vehicles with pedestrians having the greatest influence. New buildings on the east side of the woonerf would then be scaled to transition from the West Promenade in the form of stepbacks. In this proposal, above the proposed 24-foot first floor at the edge of the woonerf, the building could step back to the proposed final height of the building. This would comply with the spirit of the DEG and meet the characteristics of the woonerf as a transition street within the Cornelia Overlay Zone.

Proposed Danila Street

The DEG recognizes the space between buildings can provide more opportunities for expanding the public realm and in some cases, to introduce pedestrian-oriented streets into sites. There are other options such as woonerfs (reduced thru traffic, pedestrians and bicycles having priority) and an opportunity to create – by linking multiple blocks together – a network of pedestrian-oriented pathways throughout the district. However, in this instance, using Danila as a pedestrian-only space may create limitations in how the street network, with the addition of the West Promenade, may evolve in the future. This is discussed further under Phasing, on the following page. (Woonerfs are ADA compliant by design if they are to accommodate all residents and visitors equitably.)

Proposed Stormwater Retention Basin

It is unclear what the variation in the basin water level will be from day to day, or after a heavy rain event, so as an amenity it might have restrictions that would need to be clearer if it were to become a valued part of community infrastructure. We also note that the use of sunken gardens in urban settings has historically been mixed, with their location below street level leaving many people feeling unsafe for use as part of daily life.

Observations on Phasing

- a. Woonerfs are shared streets with pedestrians having precedence over vehicles. Generally, vehicles move through the street at approximately 5 miles per hour. The City of Edina already has an example of this type of space in the Nolen Mains development at 50th and France. The DEG's use of woonerfs on both the East and West Promenades is to provide a physical redefinition of the street to become pedestrian and bicycle oriented, with vehicular traffic to serve new development. The woonerf becomes the primary element in organizing transition zone spaces that link the entire Southdale district from north to south. New buildings are intended to frame and characterize the woonerf as a safe pedestrian environment. They are intended to house new services that support the neighborhood, along with new commercial programs that create an experience that is shared between building occupants and residents of the adjacent neighborhoods.

The 'woonerf' illustrated in this proposal is essentially a parking garage entry ramp with trees in the middle of the road with no pedestrian access. As it relates to PUD phasing, if Phase 1 of this proposal were to be a self-contained development as described above (i.e. not dependent on any use of the Phase 2 site to meet its program/parking needs), the south half of the woonerf would be constructed as part of Phase 1, with Danila Street completing the woonerf and providing access to below-grade parking. A future Phase 2 would complete the north half of the woonerf and share parking access to below-grade parking from Danila Street with Phase 1.

- b. It is unclear how the future Phase 2 will work if its site is developed initially with a 115-car surface parking lot dedicated to meet the parking requirements/program of Phase 1. Given that the construction duration for new buildings is generally 2-3 years, approximately one-third of Phase 1's parking would be taken out of commission during that time. The DEG recommends no new on-grade parking in the district. It also intends that each building takes care of its own services, parking, etc. The hybrid approach outlined in this submission puts restrictions or a possible future covenant on development of the north parcel based on its proposed initial use as a surface parking lot. Should this 115-car surface lot be necessary to meet the required parking counts for Phase 1, it seems highly unlikely that this important gateway site into the Cornelia neighborhood would ever be developed beyond a surface lot that is more than 10 feet below the level of surrounding sidewalks. Also of note, this surface lot is accessed solely via a ramp down from 72nd Street, not from the "woonerf" roadway shown in the proposal.
- c. It is unclear from the information provided whether the two below-grade parking levels are interconnected within the Phase 1 building or if the access to Phase 1 parking is via the surface lot. Should the latter be the case, the construction of Phase 2 may limit access to one level of Phase 1 parking—which again, could be an impediment to future development of the site.

Other details not addressed:

- a. A building service plan has not been discussed; no building facades or plans show service access point beyond parking access. The Design Experience Guidelines provide considerations into the how the form of buildings and width of streets impact the public realm. As it relates to buildings, important factors influencing the experience in the public realm include the transparency of walls at the ground floor, and the location of drop offs and parking entries, service vehicle access, and on-site utility equipment—all of which, if not taken into consideration at the earliest in the

Architecture Field Office

sketch plan and reflected in all phases of the review process, can become unwanted surprises in the later phases of design and construction.

Additional questions we would encourage the applicant and the Planning Commission/City Council to consider addressing include:

- On grade parking is expressly discouraged in the Design Experience Guidelines. Why does the current plan ignore this important guideline?
- Each building should be able to meet its own internal parking requirements and not depend on on-grade parking or district parking. Why does the current plan use on-grade parking to meet parking needs for a Medical Office Building in Phase 1?
- In this proposal, there is no discussion about the possibility of extending below grade parking or basements under the public realm/streets (the woonerf) as an option to expand the parking footprint without creating a surface lot.
- Has the applicant considered adding a level of parking above the ground floor of Phase 1 to meet all of that building's parking requirements within its site constraints without limiting future flexibility of the north parcel? This is a common way to increase parking on a site when there are limitations to below-grade parking.
- District parking, as the on-grade parking implies, should be reserved for districtwide needs, not those of individual buildings. Is District Parking part of the proposal?
- Why does the north elevation not show the full depth of the surface parking lot below France?
- Why is there extensive investment being made in the North on-grade parking lot, to be removed by a future Phase 2?
- Why are the sites for Phase 1 and Phase 2 connected by a series of 3 ramps that descend 15 feet down from Gallagher into a surface parking lot and access to 2 levels of below grade parking before ascending back out to W. 72nd Street?
- We understand that Phase 2 is not designed, but it is unclear how the applicant would reach 501 parking stalls, unless they envision adding a P3 level to the Phase 2 project? In addition, based on the sections provided, it appears that Phase 2 ground floor would be constructed approximately three feet above grade on 72nd Street.
- Why is the roadway not a woonerf as described in the DEG for the West Promenade?
- Why are there no provisions for pedestrians on the proposed west roadway – only bicycles and cars?
- Is the road there to only provide access to the Phase 1 below grade parking and on grade parking and (potential) Phase 2 below-grade parking?
- What is the strategy for building services – loading dock, garbage and recycling, medical wastes (assuming a medical office building), access from the public realm, etc.?

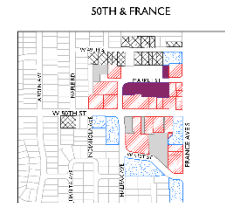
Thank you for the opportunity to review. Please let me know if you have any questions.
Mic



Zoning Map

Legend

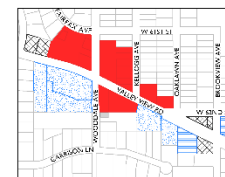
- R-1 - Single Dwelling Unit
- R-2 - Double Dwelling Unit
- PRD-1 - Planned Residence
- PRD-2 - Planned Residence
- PRD-3 - Planned Residence
- PRD-4 - Planned Residence
- PRD-5 - Planned Residence
- PCD-1 - Planned Commercial
- PCD-2 - Planned Commercial
- PCD-3 - Planned Commercial
- PCD-4 - Planned Commercial
- POD-1 - Planned Office
- POD-2 - Planned Office
- RMD - Regional Medical
- PID - Planned Industrial
- PUD - Planned Unit Development
- APD - Automotive Parking
- PSR-4 - Planned Residence
- MDD-4 - Mixed Development
- MDD-5 - Mixed Development
- MDD-6 - Mixed Development
- Lakes
- Creeks
- Edina Boundary
- Railroad
- Public Building
- Public School
- Private School



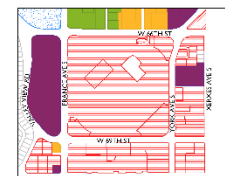
54TH & FRANCE



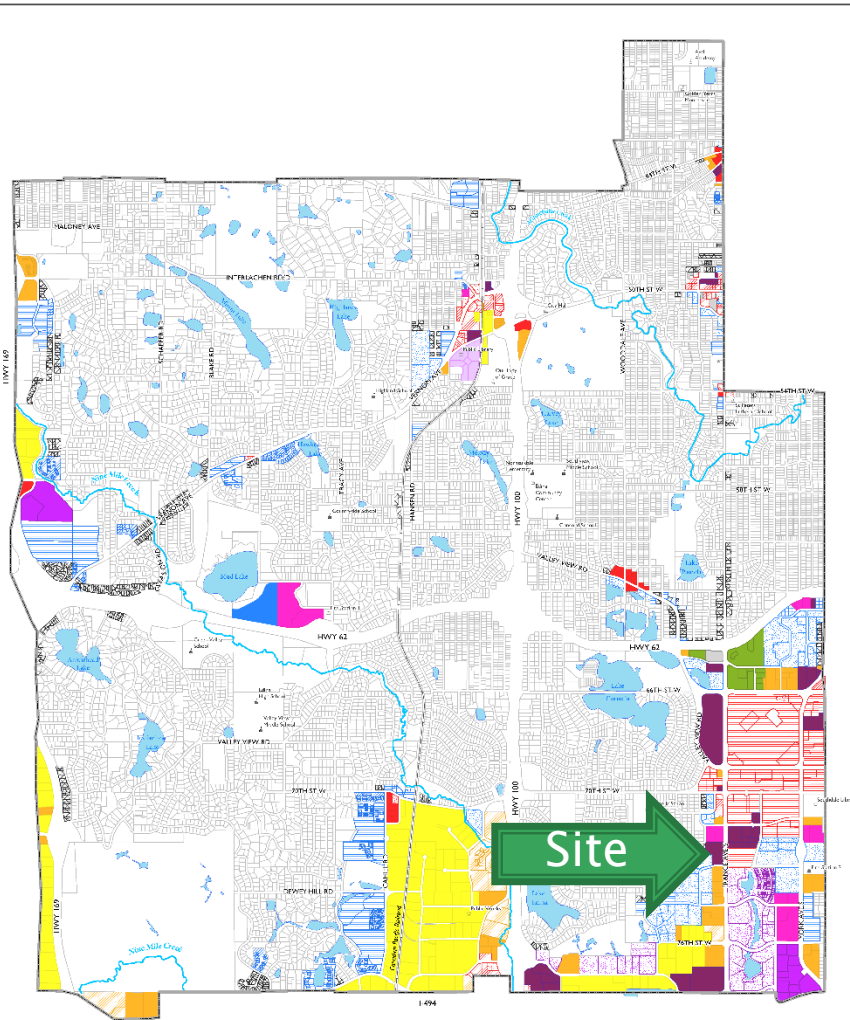
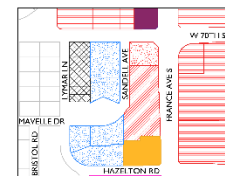
VALLEY VIEW & WOODDALE



SOUTHDALE



70TH & FRANCE



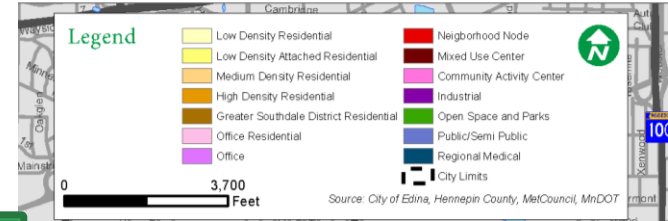
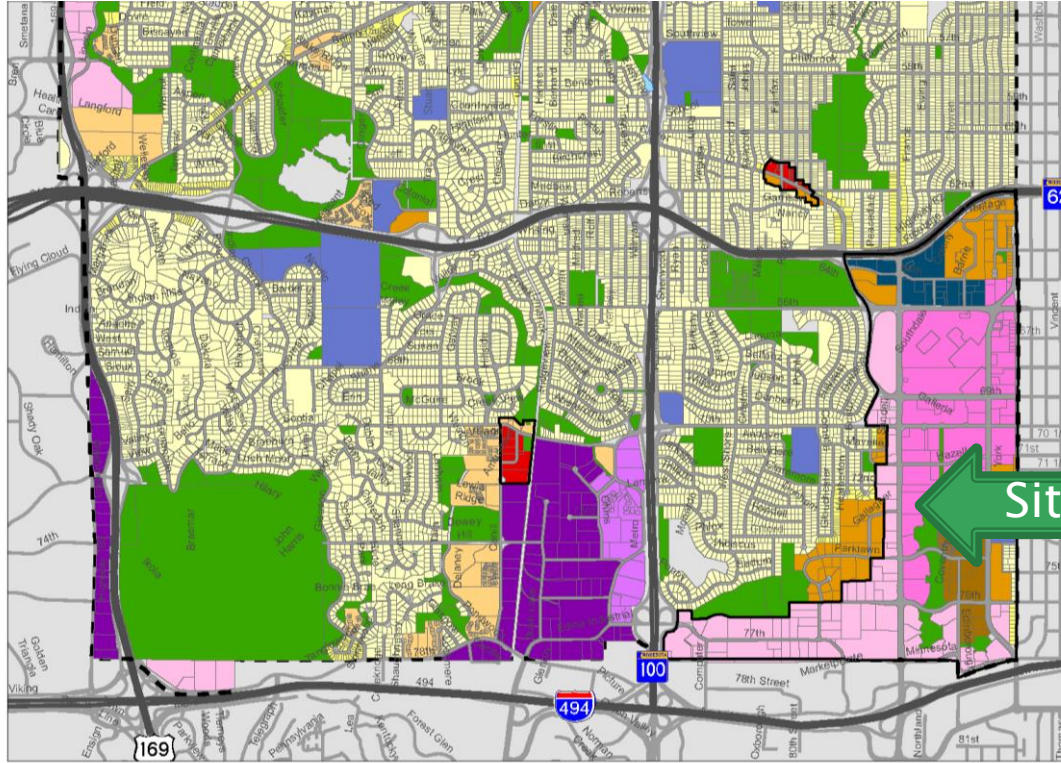


The CITY of EDINA



Edina 2040 Comprehensive Plan

Edina, Minnesota



7200 & 7250 FRANCE AVE | P.U.D.

7200 FRANCE AVE | EDINA, MN 55435



PROJECT OVERVIEW

THIS PROJECT'S GOAL IS TO TRANSFORM A DILAPIDATED AND HISTORICALLY PRESERVABLE SITE INTO A "HUB" CENTER FOR BUSINESS AND COMMUNITY ACTIVITY. DEVELOPMENT OF THIS SITE HAS THE POTENTIAL TO CREATE 480-600 JOBS WHEN ASSIGNED 3-4 FBR, 1000 SF IN JOBS.

- PROPOSED USES:
- 1250
 - MEDICAL OFFICE
 - SMALL RETAIL
 - 7200
 - MIXED USE
 - RETAIL
 - MEDICAL OFFICE
 - HOSPITALITY
 - HOUSING POSSIBLE

- PROPOSED PUBLIC IMPROVEMENTS AND AMENITIES:
- FRANCE AVE SETBACK BECOMES PEDESTRIAN CORRIDOR WITH LANDSCAPED PATHS, TREES/GARDEN, AND SEATING MEAS.
 - STORMWATER RETENTION BASIN WITH LANDSCAPED PATHS AND OVERLOOKS ON WEST SIDE OF SITE
 - DANIELA STREET BECOMES PEDESTRIAN-ONLY CORRIDOR CONNECTED BY ADA-COMPLIANT RAMPS AND PATHS THROUGHOUT ENTIRETY OF SITE

PRELIMINARY BUILDING METRICS

TABLE 1: BUILDING METRICS		
	Nettable Square Foot	Overall Floor Area
Level 1	63,540	0
Level 2	48,540	2
Level 3	37,000	0
Level 4	76,000	26,211
Level 5	25,300	86,831
Level 6	25,300	26,687
Level 7	25,300	26,687
Level 8	25,300	26,687
Level 9	25,300	26,687
Level 10	25,300	26,687
Level 11	25,300	26,687
Level 12	25,300	26,687
Level 13	25,300	26,687
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Level 95	25,300	26,687
Level 96	25,300	26,687
Level 97	25,300	26,687
Level 98	25,300	26,687
Level 99	25,300	26,687
Level 100	25,300	26,687

PARKING COUNT	
Level	Quantity
P1	110
P2	109
P3	11
Total	230

PHASE 1 PARKING

PHASE 1A (7200 SITE): 334 PERMANENT STALLS
PHASE 1B (7250 SITE): 30 TEMPORARY STALLS
TOTAL PHASE 1A + B PARKING: 364 STALLS

187 STALLS / 1000 SF OFFICE ON 7200 ONLY
284 STALLS / 1000 SF OFFICE INCLUDING TEMP PARKING ON 7200

PHASE 2 ANTICIPATED PARKING CORRIDOR

POTENTIALLY 480-600 FUTURE FINAL DESIGN FOR 7200

PROJECT TEAM

OWNER/DEVELOPER:	ORION INVESTMENTS 6500 YORK AVE, SUITE 207 EDINA, MN 55435 PH: 612-812-7788
ARCHITECT:	ELMER SWENSON GRAHAM ARCHITECTS, INC. 500 WASHINGTON AVE. SOUTH, SUITE 1000 MINNEAPOLIS, MN 55415 PH: 612-339-5058
CONTRACTOR:	AMP CONSTRUCTION 5000 WINDY LAKE BLVD, SUITE 600 MINNEAPOLIS, MN 55416 PH: 952-467-4048
CIVIL ENGINEER:	CIVIL SITE GROUP 5000 GLENWOOD AVENUE GOLDEN VALLEY, MN 55422 PH: 612-415-0060
LANDSCAPE ARCHITECT:	CONFLUENCE 330 N. 3RD STREET, SUITE 120 MINNEAPOLIS, MN 55401 PH: 612-339-3702
STRUCTURAL ENGINEER:	MEYER BORGMAN JOHNSON 310 W. WASHINGTON AVENUE SOUTH #900 MINNEAPOLIS, MN 55402 PH: 612-339-0713
MIP ENGINEER:	ENRICHMENT PODS 7700 BUSH LAKE ROAD EDINA, MN 55439 PH: 952-930-0050
PARKING CONSULTANT:	WALSH CONSULTANTS 3800 S. HIGHWAY 100 #505 MINNEAPOLIS, MN 55416 PH: 952-935-6116

PROJECT LOCATION



Vicinity



Site Location

DRAWING INDEX

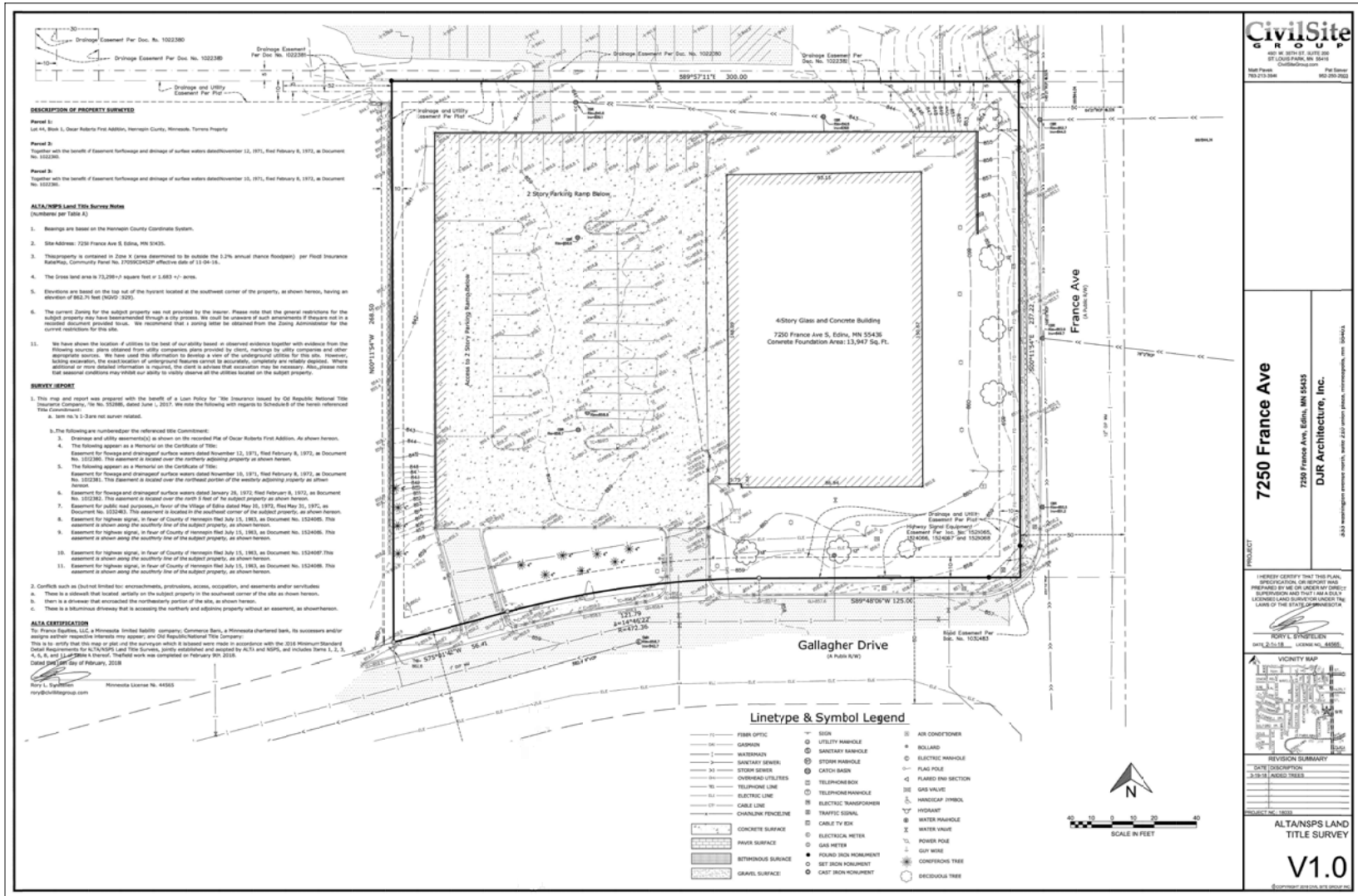
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C2.227	CIVIL DETAILS	*
C2.228	CIVIL DETAILS	*
C2.229	CIVIL DETAILS	*
C2.230	CIVIL DETAILS	*
C2.231	CIVIL DETAILS	*
C2.232	CIVIL DETAILS	*
C2.233	CIVIL DETAILS	*
C2.234	CIVIL DETAILS	*
C2.235	CIVIL DETAILS	*
C2.236	CIVIL DETAILS	*
C2.237	CIVIL DETAILS	*
C2.238	CIVIL DETAILS	*
C2.239	CIVIL DETAILS	*
C2.240	CIVIL DETAILS	*
C2.241	CIVIL DETAILS	*
C2.242	CIVIL DETAILS	*
C2.243	CIVIL DETAILS	*
C2.244	CIVIL DETAILS	*

ISSUED FOR: CITY SUBMITTAL



Know what's **below**.
Call before you dig.

SHEET INDEX		TITLE SHEET	
SHEET NUMBER	SHEET TITLE	DATE	DESCRIPTION
C0.0	TITLE SHEET		
V1.0	SITE SURVEY		
V1.1	SITE SURVEY		
V1.2	SITE SURVEY		
C1.0	PH. 1 REMOVALS PLAN		
C1.1	PH. 2 REMOVALS PLAN		
C2.0 A	PH. 1 (A) SITE PLAN		
C2.0 B	PH. 1 (B) SITE PLAN		
C2.1	PH. 2 SITE PLAN		
C3.0 A	PH. 1 (A) GRADING PLAN		
C3.0 B	PH. 1 (B) GRADING PLAN		
C4.0	PH. 2 GRADING PLAN		
C4.0 A	PH. 1 (A) UTILITY PLAN		
C4.0 B	PH. 1 (B) UTILITY PLAN		
C4.1	PH. 2 UTILITY PLAN		
C5.0	CIVIL DETAILS		
C5.1	CIVIL DETAILS		
C5.2	CIVIL DETAILS		
SWP.0	SWPPP - EXISTING CONDITIONS		
SWP.1	PH. 1 SWPPP - PROPOSED CONDITIONS		
SWP.2	PH. 2 SWPPP - PROPOSED CONDITIONS		
SWP.3	SWPPP - DETAILS		
SWP.4	SWPPP - NARRATIVE		
SWP.5	SWPPP - ATTACHMENTS		
SWP.6	SWPPP - ATTACHMENTS		



ALTA/NSPS LAND TITLE SURVEY

PROPERTY ADDRESS: #7200 FRANCE AVE. SO., EDINA, MN 55435

~for~ Campbell Capital Group LLC

P.I.D.#31-028-24-14-0001

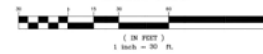
GENERAL NOTES

- Bearings shown herein are on assumed datum.
- Boundary lines of surveyed premises: 198,000 sq. ft. (3,888 acres).
- The survey premises line corners to 72nd St. West & France Avenue South, both publicly dedicated right-of-way.
- Utilities shown herein are observed. Locations were not made during the process of this survey. No statement is made concerning the existence of underground utilities and/or structures. The location of underground utilities and/or structures may vary from locations shown herein and underground utilities and/or structures may be encountered. Contact Capital State One Call Notification Center at (888) 484-0662 for verification of utility types and field location prior to excavation.
- Subsurface and environmental conditions were not examined or considered during the process of this survey. No statement is made concerning the existence of underground or concealed conditions or facilities that may affect the use or development of the surveyed property.
- There is no observable evidence of building construction or building additions within recent months. There is no observable evidence of site use as a solid waste dump, sink or sanitary landfill. There is no observable evidence of recent street or sidewalk repairs within recent months.
- Bearing's shown are on assumed datum.
- MAIL UTILITIES LOCATED AND SHOWN HEREIN PER Gopher State One Call Ticket 14032887. CITY UTILITIES PER CITY PLANS AND FIELD OBSERVATIONS. VERIFY WITH CITY PLANS IN DESIGN PROCESS.

BENCHMARK

BASE FOR ELEVATION: NAVD 83 VIA REAL TIME GPS
MEASUREMENTS (EDINA, MINNESOTA)
DEPARTMENT OF TRANSPORTATION VES NETWORK

GRAPHIC SCALE



LEGEND

- DENOTES IRON MONUMENT FOUND
- DENOTES SANITARY SINKER MANHOLE
- DENOTES STORM SINKER MANHOLE
- DENOTES CATCH BASIN
- DENOTES FIRE HYDRANT
- DENOTES STORM SINKER
- DENOTES CONCRETE
- DENOTES BITUMINOUS
- DENOTES GAS METER/BOX
- DENOTES UTILITY BOX
- DENOTES RETAINING WALL
- DENOTES EXISTING FENCE
- DENOTES UNDERGROUND TELEPHONE/FIBER OPTIC
- DENOTES UNDERGROUND GAS
- DENOTES UNDERGROUND ELECTRIC
- DENOTES WATERMAIN
- DENOTES SANITARY SINKER

PROPERTY DESCRIPTION (Per Title Commitment #8119 Third Supplemental 6-17-2014)

Parcel 1:
The East 1/2 of the South 1/2 of the Northeast Quarter of Section 31, Township 28, Range 24, according to the Government survey thereof, except that pertained within the plat of Oscar Roberts First Addition.

Hennepin County, Minnesota.
Towns: Edina
Towns: Edina
Towns: Edina

Parcel 2:

Together with the benefit of easements for sewage, drainage and storage of surface waters contained in that certain Easement dated January 28, 1972, filed February 8, 1972, as Document No. 1022383.

Parcel 3:

Together with the benefit of the easements for sewage, drainage and storage of surface waters contained in that certain Easement dated January 28, 1972, filed February 8, 1972, as Document No. 1022383.

Parcel 4:

Together with the benefit of the easements for sewage, drainage and storage of surface waters contained in that certain Easement dated January 28, 1972, filed February 8, 1972, as Document No. 1022383.

Schedule B2 (Per Title Commitment #8119 Third Supplemental 6-17-2014)

Item 1.0: Easement for various sewer purposes, in favor of Village of Edina, as created in document dated March 5, 1975, as Document No. 1022383. - AS SHOWN

Item 1.1: Easement for highway purposes, in favor of Village of Edina, as created in document dated July 30, 1987, filed October 5, 1987, as Document No. 581943.

Item 1.2: Highway easement, heirs of rights of access, in favor of Hennepin County, contained in First Certificate dated August 28, 1967 as Document No. 1022383 - 32 feet of roadway No. 1022383, right of access as shown but hereby reserved, see document.

Item 1.3: Terms and conditions of Easement for sewage, drainage and storage of surface waters dated November 13, 1974, filed February 8, 1975, as Document No. 1022383. - AS SHOWN

Item 1.4: Terms and conditions of Easement for sewage, drainage and storage of surface waters dated January 28, 1972, as Document No. 1022383. - AS SHOWN

Item 1.5: Terms and conditions of Easement for sewage, drainage and storage of surface waters dated January 28, 1972, as Document No. 1022383. - AS SHOWN

Item 1.6: Terms and conditions of Easement for sewage, drainage and storage of surface waters dated January 28, 1972, as Document No. 1022383. - AS SHOWN

VICINITY MAP



City of Edina, Minnesota (No Scale)

ZONING & SETBACK INFORMATION

- PROPERTY ZONED R.O.D. #1 (PLANNED OFFICE DISTRICT)
- SETBACK BUILDING
 - FRONT = 30' OR BUILDING HEIGHT, WHICHEVER IS GREATER.
 - INTERIOR = 10' OR BUILDING HEIGHT, WHICHEVER IS GREATER.
- PARKING UTILITIES
 - 20' FROM STREET
 - 20' FROM ADJ. RESIDENTIAL AREA
 - 20' FROM ADJ. AREA

ABOVE INFORMATION PER "CITY TITAGUT" AT CITY PLANNING DEPARTMENT (552426-0460) 07/09/14.

11-Campbell Capital Group LLC, 11000 Corporate L.L.C., 7200 France L.L.C., "North Star, N.A.", a national banking association, its successors and/or assigns, and Old Republic National Title Insurance Company

This is to certify that this map or plat and the survey on which it is based were made in accordance with the 2010 Minimum Standard Data Requirements for ALTA/NSPS Land Title Surveys, and were established and accepted by ALTA and NSPS and include items 2.4, 5, 6, 6.1, 7.1, 8, 9, 11, and 16, of Table A thereof. The fieldwork was completed on 7/19/2014.

Date of Map: 8/15/2014

Revised: 8/15/2014 Name Change



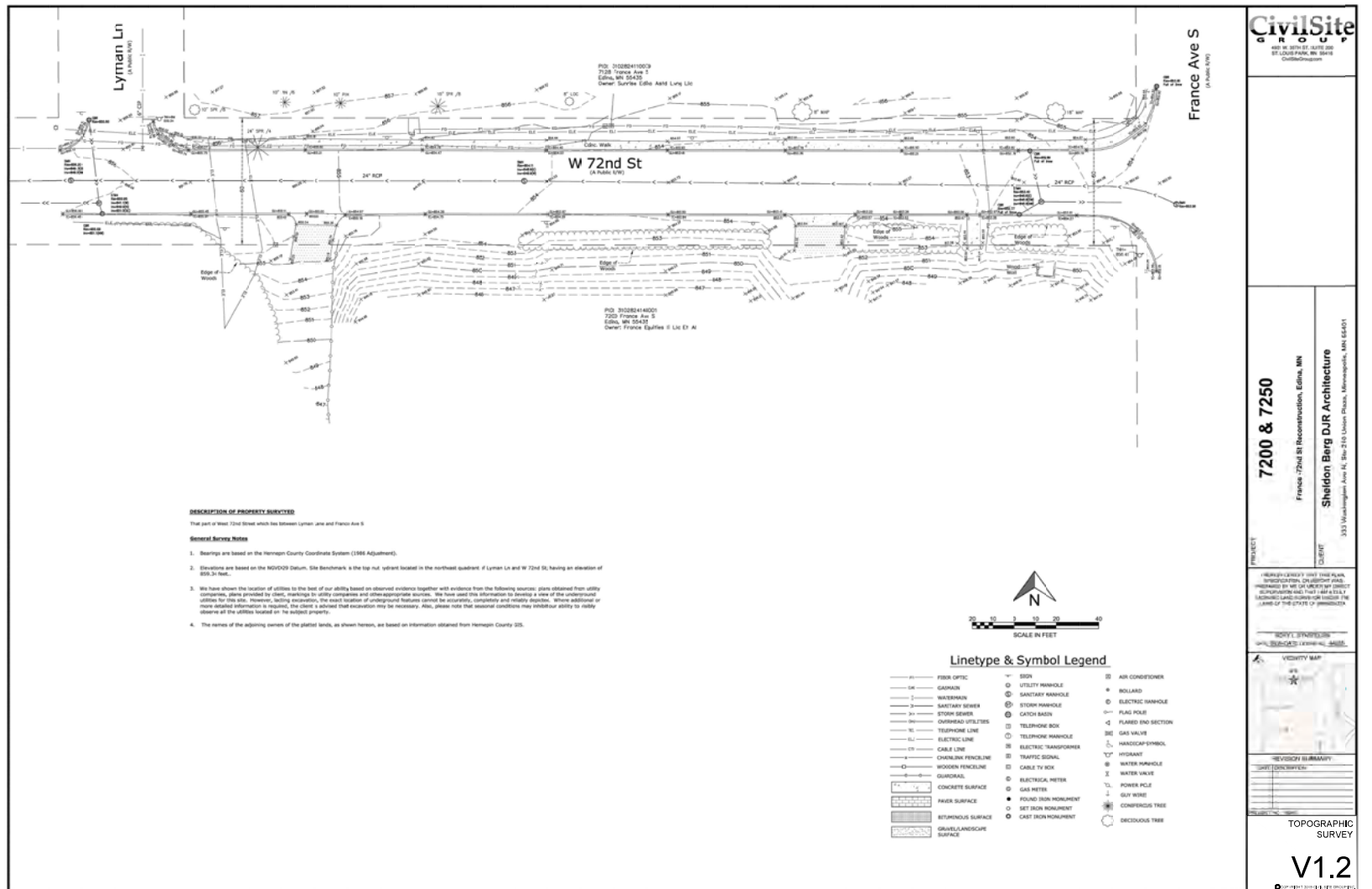
C:\Users\Nina\OneDrive\6409-400\028-24-14-0001\6409-400-01.dwg 8/3/2016 7:56:27 AM CSE

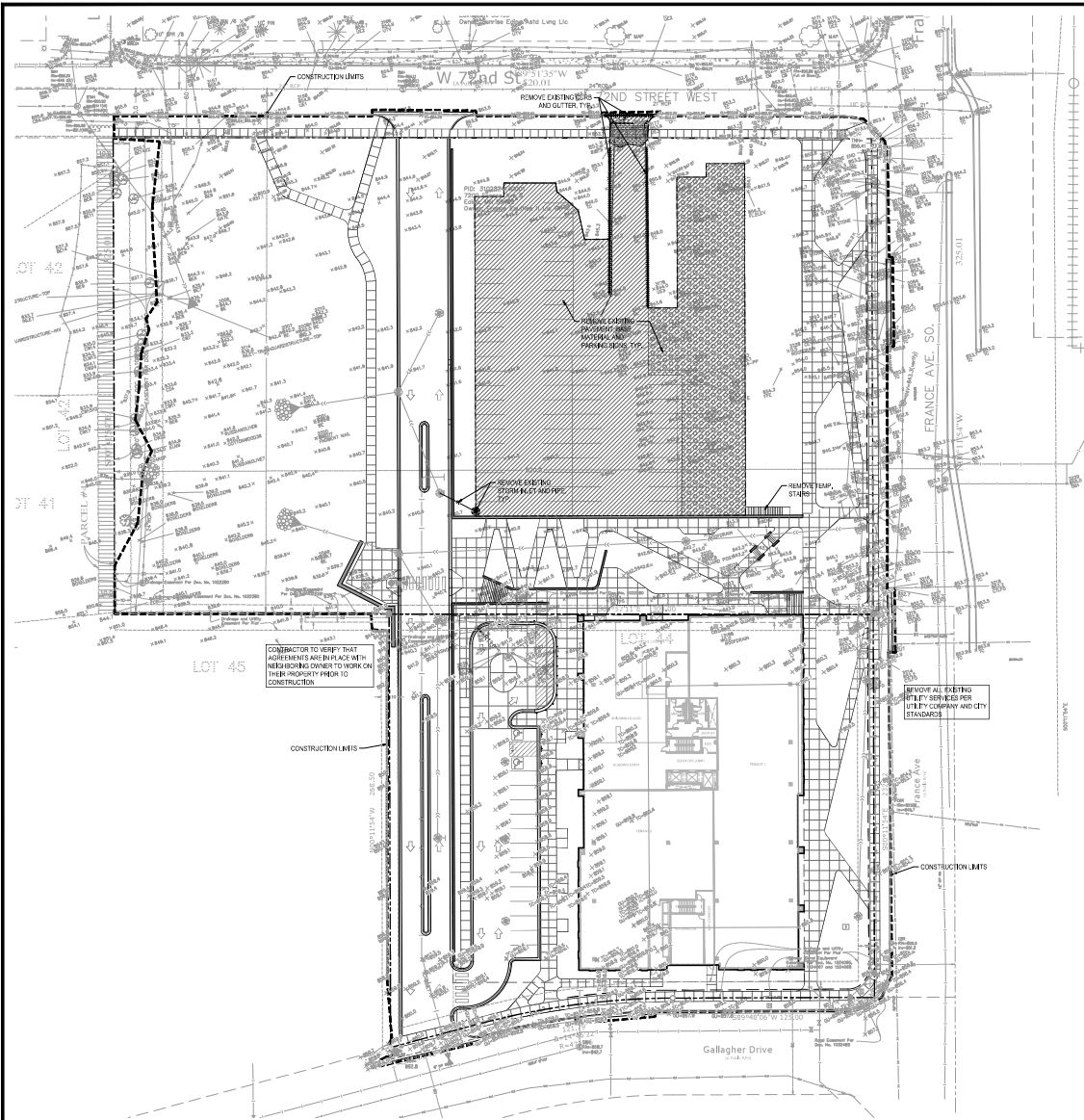
JOB #6409

REVISION SUMMARY	
DATE	DESCRIPTION
08/03/16	REVISED

SITE SURVEY

V1.1





REMOVAL NOTES:

1. ALL EXISTING UTILITY LOCATIONS SHOWN ARE APPROXIMATE. CONTACT 'Gopher State One Call' (800-440-4000) OR MINNESOTA PUBLIC UTILITY LOCATIONS (48 HOURS PRIOR TO CONSTRUCTION). THE CONTRACTOR SHALL REPAIR OR REPLACE ANY UTILITIES THAT ARE DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.
2. SEE STORM WATER POLLUTION PREVENTION PLAN (SWPPP) PLAN FOR CONSTRUCTION STORM WATER MANAGEMENT PLAN.
3. REMOVAL OF MATERIALS NOTED ON THE DRAWINGS SHALL BE IN ACCORDANCE WITH MNDOT, STATE AND LOCAL REGULATIONS.
4. REMOVAL OF PRIVATE UTILITIES SHALL BE COORDINATED WITH UTILITY OWNER PRIOR TO CONSTRUCTION ACTIVITIES.
5. EXISTING PAVEMENTS SHALL BE SAWCUT IN LOCATIONS AS SHOWN ON THE DRAWINGS OR THE NEAREST JOINT FOR PROPOSED PAVEMENT CONNECTIONS.
6. REMOVED MATERIALS SHALL BE DEPOSED OF TO A LEGAL OFF-SITE LOCATION AND IN ACCORDANCE WITH STATE AND LOCAL REGULATIONS.
7. ABANDON, REMOVAL, CONNECTION AND PROTECTION NOTES SHOWN ON THE DRAWINGS ARE APPROXIMATE. COORDINATE WITH PROPOSED PLANS.
8. EXISTING ON-SITE FEATURES NOT NOTED FOR REMOVAL SHALL BE PROTECTED THROUGHOUT THE DURATION OF THE CONTRACT.
9. PROPERTY LINES SHALL BE CONSIDERED GENERAL CONSTRUCTION LIMITS UNLESS OTHERWISE NOTED ON THE DRAWINGS. WORK WITHIN THE GENERAL CONSTRUCTION LIMITS SHALL INCLUDE EXISTING, DEMOLITION AND CLEANUP OPERATIONS AS WELL AS CONSTRUCTION SHOWN ON THE DRAWINGS.
10. WORK OUTSIDE OF THE GENERAL CONSTRUCTION LIMITS SHALL BE ALLOWED AS SHOWN ON THE PLAN AND PER CITY REQUIREMENTS. FOR ANY WORK ON ADJACENT PRIVATE PROPERTY, THE CONTRACTOR SHALL OBTAIN WRITTEN PERMISSION FROM THE ADJACENT PROPERTY OWNER PRIOR TO ANY WORK.
11. DAMAGE BEYOND THE PROPERTY LIMITS CAUSED BY CONSTRUCTION ACTIVITY SHALL BE REPAIRED IN A MANNER APPROVED BY THE ENGINEER/LANDSCAPE ARCHITECT OR IN ACCORDANCE WITH THE CITY.
12. PROPOSED WORK BUILDING AND DRAINAGE SHALL NOT DISTURB EXISTING UTILITIES UNLESS OTHERWISE SHOWN ON THE DRAWINGS AND APPROVED BY THE CITY PRIOR TO CONSTRUCTION.
13. SITE SECURITY MAY BE NECESSARY AND PROVIDED IN A MANNER TO PROHIBIT VANDALISM AND THEFT. DURING AND AFTER NORMAL WORK HOURS, THROUGHOUT THE DURATION OF THE CONTRACT, SECURITY MATERIALS SHALL BE IN ACCORDANCE WITH THE CITY.
14. VEHICULAR ACCESS TO THE SITE SHALL BE MAINTAINED FOR DELIVERY AND INSPECTION ACCESS DURING NORMAL OPERATING HOURS. AT NO POINT THROUGHOUT THE DURATION OF THE CONTRACT SHALL OBSTRUCTION OF ADJACENT STREETS BE BLOCKED WITHOUT APPROVAL BY THE CITY PRIOR TO CONSTRUCTION ACTIVITIES.
15. ALL TRAFFIC CONTROLS SHALL BE PROVIDED AND ESTABLISHED PER THE REQUIREMENTS OF THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES MAINTAINED BY THE CITY. THE SHALL INCLUDE, BUT NOT BE LIMITED TO, BARRIERS, BARRIERS, FLARE, AND T-ADDS. ALL ADJACENT PUBLIC STREETS SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES. NO ROAD CLOSURES SHALL BE PERMITTED WITHOUT APPROVAL BY THE CITY.
16. SHORING FOR BUILDING EXCAVATION MAY BE USED AT THE DISCRETION OF THE CONTRACTOR AND AS APPROVED BY THE OWNER'S REPRESENTATIVE AND THE CITY PRIOR TO CONSTRUCTION ACTIVITIES.
17. EXISTING, DEMOLITION AND CLEANUP AREAS SHALL BE WITHIN THE PROPERTY LIMITS AS SHOWN ON THE DRAWINGS AND MAINTAINED IN A MANNER AS REQUIRED BY THE CITY.
18. ALL EXISTING SITE TRAFFIC REGULATORY SIGNS TO BE INVENTORIED AND IF REMOVED FOR CONSTRUCTION SHALL BE RETURNED TO THE CITY.
19. ALL EXISTING UTILITY LOCATIONS SHOWN ARE APPROXIMATE. CONTACT 'Gopher State One Call' (800-440-4000) OR MINNESOTA PUBLIC UTILITY LOCATIONS (48 HOURS PRIOR TO CONSTRUCTION). THE CONTRACTOR SHALL REPAIR OR REPLACE ANY UTILITIES THAT ARE DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.

CITY OF EDINA REMOVAL NOTES:

1. RESERVED FOR CITY SPECIFIC REMOVAL NOTES.

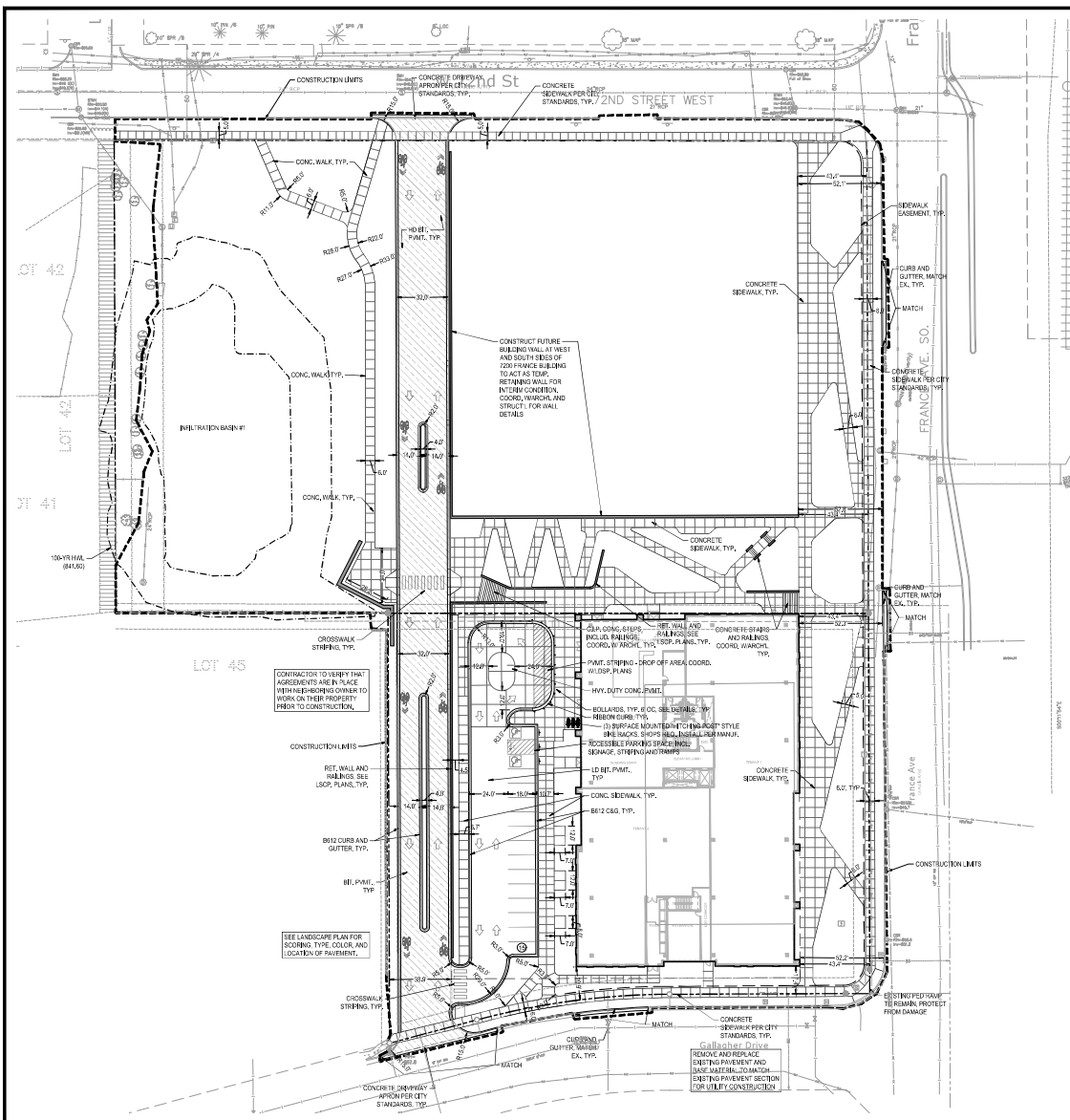
EROSION CONTROL NOTES:

SEE SWPPP ON SHEETS SW1.0 - SW1.8

REMOVALS LEGEND:

- | | |
|------------------|--|
| ----- 1125 ----- | EX. 1' CONTOUR ELEVATION INTERVAL |
| | REMOVAL OF PAVEMENT AND ALL BASE MATERIAL, INCLUDING CURB, CONC. AND DRAINAGE PAVES. |
| | REMOVAL OF STRUCTURE INCLUDING ALL FOOTINGS AND FOUNDATIONS. |
| | REMOVAL CURB AND GUTTER, IF IN RIGHT-OF-WAY, COORDINATE WITH LOCAL GOVERNING BODY. |





OPERATIONAL NOTES:

SNOW REMOVAL	ALL SNOW SHALL BE STORED ON-SITE OUTSIDE PARKING LOT. WHEN FULL, REMOVAL CO. SHALL REMOVE EXCESS ON-SITE.
TRASH REMOVAL	TRASH SHALL BE PLACED INSIDE PARKING GARAGE AND REMOVED BY COMMERCIAL CO. WEEKLY.
DELIVERIES	DELIVERIES SHALL OCCUR AT THE FRONT DOOR VIA STANDARD COMMERCIAL DELIVERY VEHICLES (UPS, FEDEX, USPS).

SITE AREA TABLE:

SITE AREA CALCULATIONS			
7200	BUILDING COVERAGE	EXISTING CONDITION	PHASE I CONDITION
	ALL PAVEMENTS	54,975 SF 41.9%	78,005 SF 56.3%
	ALL NON-PAVEMENTS	64,454 SF 48.0%	60,626 SF 43.7%
TOTAL SITE AREA		138,652 SF 100.0%	138,652 SF 100.0%
7250	BUILDING COVERAGE	EXISTING CONDITION	PHASE I CONDITION
	ALL PAVEMENTS	43,307 SF 29.8%	49,495 SF 35.7%
	ALL NON-PAVEMENTS	36,139 SF 21.7%	8,759 SF 6.3%
TOTAL SITE AREA		79,446 SF 52.8%	58,254 SF 42.0%
COMBINATION			
BUILDING COVERAGE		98,282 SF 71.7%	127,499 SF 92.3%
ALL PAVEMENTS		98,282 SF 71.7%	127,499 SF 92.3%
ALL NON-PAVEMENTS		41,170 SF 29.3%	11,155 SF 8.0%
TOTAL SITE AREA		139,452 SF 100.0%	138,652 SF 100.0%

IMPERVIOUS SURFACE			
7200	EXISTING CONDITION	78,005 SF	56.3%
	PHASE I CONDITION	78,005 SF	56.3%
	DIFFERENCE (EX. VS. PH. I.)	-152 SF	-0.2%
7250	EXISTING CONDITION	43,307 SF	32.3%
	PHASE I CONDITION	49,495 SF	35.7%
	DIFFERENCE (EX. VS. PH. I.)	23,580 SF	15.4%
COMBINATION	EXISTING CONDITION	121,312 SF	87.2%
	PHASE I CONDITION	127,499 SF	92.3%
	DIFFERENCE (EX. VS. PH. I.)	23,580 SF	15.4%

SITE LAYOUT NOTES:

1. ALL EXISTING UTILITY LOCATIONS SHOWN ARE APPROXIMATE. CONTACT "OWNER STATE ONE CALL" (800) 4-A-RECALL OR 800-282-1165 FOR UTILITY LOCATIONS 48 HOURS PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL REPAIR OR REPLACE ANY UTILITIES THAT ARE DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.
2. CONTRACTOR SHALL VERIFY LOCATIONS AND LAYOUT OF ALL SITE ELEMENTS PRIOR TO BEGINNING CONSTRUCTION, INCLUDING BUT NOT LIMITED TO, LOCATIONS OF EXISTING AND PROPOSED PROPERTY LINES, EASEMENTS, SETBACKS, UTILITIES, BUILDINGS AND PAVEMENTS. CONTRACTOR IS RESPONSIBLE FOR FINAL LOCATIONS OF ALL ELEMENTS FOR THE SITE. ANY REVISIONS REQUIRED AFTER COMMENCEMENT OF CONSTRUCTION, DUE TO LOCATION, ADJUSTMENTS SHALL BE CORRECTED AT NO ADDITIONAL COST TO OWNER. ADJUSTMENTS TO THE LAYOUT SHALL BE APPROVED BY THE ENGINEER/LANDSCAPE ARCHITECT PRIOR TO INSTALLATION OF MATERIALS. STONE LAYOUT FOR APPROVAL.
3. THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS PRIOR TO CONSTRUCTION, INCLUDING A RIGHT-OF-WAY AND STREET OPENING PERMIT.
4. THE CONTRACTOR SHALL VERIFY RECOMMENDATIONS NOTED IN THE GEO TECHNICAL REPORT PRIOR TO INSTALLATION OF SITE IMPROVEMENT MATERIALS.
5. CONTRACTOR SHALL FIELD VERIFY COORDINATES AND LOCATION DIMENSIONS & ELEVATIONS OF THE BUILDING AND STAKE FOR REVIEW AND APPROVAL BY THE OWNER REPRESENTATIVE PRIOR TO INSTALLATION OF FOOTING MATERIALS.
6. LOCATIONS OF STRUCTURES, ROADWAY PAVEMENTS, CURBS AND GUTTERS, BOLLARDS, AND WALKS ARE APPROXIMATE AND SHALL BE STAKED IN THE FIELD PRIOR TO INSTALLATION, FOR REVIEW AND APPROVAL BY THE ENGINEER/LANDSCAPE ARCHITECT.
7. CURB EMBLEMINGS SHOWN ARE TO FACE OF CURB. BOLLARD EMBLEMINGS ARE TO FACE OF CONCRETE FOUNDATION. LOCATION OF BUILDINGS IS TO BUILDING FOUNDATION AND SHALL BE AS SHOWN ON THE DRAWINGS.
8. THE CONTRACTOR SHALL SUBMIT SHOP DRAWINGS OR SAMPLES AS SPECIFIED FOR REVIEW AND APPROVAL BY THE ENGINEER/LANDSCAPE ARCHITECT PRIOR TO FABRICATION FOR ALL PREFABRICATED SITE IMPROVEMENT MATERIALS SUCH AS, BUT NOT LIMITED TO, THE FOLLOWING: FURNISHINGS, PAVEMENTS, WALLS, BOLLARDS, BENCHES, FLAGPOLES, LANDING PADS FOR CURB SALES, AND LIGHT AND POLES. THE OWNER RESERVES THE RIGHT TO REJECT INSTALLED MATERIALS NOT PREVIOUSLY APPROVED.
9. PEDESTRIAN CURB RAMP SHALL BE CONSTRUCTED WITH TRUNCATED DOME LANDING AREAS IN ACCORDANCE WITH ADA REQUIREMENTS-SEE DETAIL.
10. CROSSWALK STRIPING SHALL BE 24" WIDE WHITE PAINTED LINE, SPACED AT 8" ON CENTER PERPENDICULAR TO THE FLOW OF TRAFFIC. WIDTH OF CROSSWALK SHALL BE 5' WIDE. ALL OTHER PAVEMENT MARKINGS SHALL BE WHITE IN COLOR UNLESS OTHERWISE NOTED OR REQUIRED BY ADA OR LOCAL GOVERNING BODIES.
11. SEE SITE PLAN FOR CURB AND GUTTER TYPE, DATED BETWEEN CURB TYPE-SIZE DETAIL.
12. ALL CURB RIGID AND BOLLARD IF UNLESS OTHERWISE NOTED.
13. CONTRACTOR SHALL REFER TO FINAL PLAT FOR LOT BOUNDARIES, NUMBERS, AREAS AND DIMENSIONS PRIOR TO SITE IMPROVEMENTS.
14. FIELD VERIFY ALL EXISTING SITE CONDITIONS, DIMENSIONS.
15. PAVING IS TO BE SET PARALLEL OR PERPENDICULAR TO PAVING BUILDING UNLESS NOTED OTHERWISE.
16. ALL PARKING LOT PAINT STRIPING TO BE WHITE, 4" WIDE TYPE.
17. BRUSHWOOD PAVING TO BE "LIGHT DUTY" UNLESS OTHERWISE NOTED. SEE DETAIL SHEETS FOR PAVEMENT SECTIONS.
18. ALL TREES THAT ARE TO REMAIN ARE TO BE PROTECTED FROM DAMAGE WITH A CONSTRUCTION FENCE AT THE EXISTING SEE LANDSCAPE DOCUMENTS.

CITY OF EDINA SITE SPECIFIC NOTES:

1. RESERVED FOR CITY SPECIFIC NOTES.

SITE PLAN LEGEND:

- [Symbol] LIGHT DUTY BRUSHWOOD PAVEMENT IF APPLICABLE. SEE GEOTECHNICAL REPORT FOR AGGREGATE BASE & WEAR COURSE DEPTH. SEE DETAIL.
- [Symbol] HEAVY DUTY BRUSHWOOD PAVEMENT IF APPLICABLE. SEE GEOTECHNICAL REPORT FOR AGGREGATE BASE & WEAR COURSE DEPTH. SEE DETAIL.
- [Symbol] CONCRETE PAVEMENT IF APPLICABLE AS SPECIFIED (PH. I. OR PH. II.) SEE GEOTECHNICAL REPORT FOR AGGREGATE BASE & CONCRETE DEPTHS. METHOD (PH. I. OR PH. II.) SEE DETAIL. WITHIN PRIVATE PROPERTY SEE CDS DETAIL.
- [Symbol] PROPERTY LINE
- [Symbol] CONSTRUCTION LIMITS
- [Symbol] CURB AND GUTTER/PAVEMENT NOTES (T, L, TP) OUT GUTTER WHERE APPLICABLE-SEE PLAN
- [Symbol] TRAFFIC DIRECTIONAL ARROW PAVEMENT MARKINGS
- [Symbol] SIGN AND POST ASSEMBLY, SHOP DRAWINGS REQUIRED, HC - ACCESSIBLE, SIGN UP - NO PARKING FIRE LANE ST - STOP
- [Symbol] ACCESSIBILITY ARROW (IF APPLICABLE) DO NOT PAINT

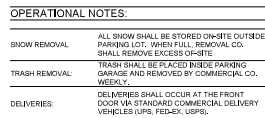


**PRELIMINARY:
NOT FOR
CONSTRUCTION**

7200 & 7250 FRANCE
7200 & 7250 FRANCE AVE S. EDINA, MN 55435
ORION INVESTMENTS
6950 YORK AVE S SUITE 207, EDINA, MN 55435

1. THESESS: CDS BY THE FIELD PLAN, SPECIFICATION, OR REPORT WAS APPROVED BY THE ENGINEER/LANDSCAPE ARCHITECT PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL REPAIR OR REPLACE ANY UTILITIES THAT ARE DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.

David J. Jank
CIVIL ENGINEER
ISSUES/SUBMITTAL SUMMARY
DATE: 10/20/2022
REVISION SUMMARY
DATE DESCRIPTION



SITE AREA CALCULATIONS			
	EXISTING CONDITION	PHASE I CONDITION	
7200 PROPERTY	10,622.37 ± 81%	10,622.37 ± 81%	
BUILDING COVERAGES	84,973.97 ± 46.0%	84,973.97 ± 46.0%	
ALL PAVEMENTS	52,634.29 ± 43.6%	52,634.29 ± 43.6%	
ALL NON-PAVEMENTS			
TOTAL SITE AREA	138,632.57 ± 100.0%	138,632.57 ± 100.0%	
SITE AREA CALCULATIONS			
	EXISTING CONDITION	PHASE I CONDITION	
7200 PROPERTY	43,907.37 ± 29.8%	44,845.37 ± 30.7%	
BUILDING COVERAGES	1,779.19 ± 1.3%	14,973.10 ± 10.8%	
ALL PAVEMENTS	31,325.19 ± 22.7%	8,799.57 ± 6.3%	
ALL NON-PAVEMENTS			
TOTAL SITE AREA	77,011.75 ± 54.8%	77,618.03 ± 52.8%	
SITE AREA CALCULATIONS			
	EXISTING CONDITION	PHASE I CONDITION	
Cumulative	54,543.56 ± 25.7%	54,543.56 ± 23.4%	
ALL PAVEMENTS	31,325.19 ± 21.5%	31,325.19 ± 20.2%	
ALL NON-PAVEMENTS	23,218.37 ± 42.4%	23,218.37 ± 29.7%	

7200 Property	IMPERVIOUS SURFACE		
	EXISTING CONDITION	78,188 SF	56.4%
	PHASE I CONDITION	78,026 SF	56.3%
	DIFFERENCE (EX. VS. PH. I.)	-162 SF	-0.1%
7250 Property	IMPERVIOUS SURFACE		
	EXISTING CONDITION	43,086 SF	31.1%
	PHASE I CONDITION	64,466 SF	46.5%
	DIFFERENCE (EX. VS. PH. I.)	21,380 SF	15.4%
Cumulative	IMPERVIOUS SURFACE		
	EXISTING CONDITION	121,274 SF	57.2%
	PHASE I CONDITION	142,492 SF	67.3%
	DIFFERENCE (EX. VS. PH. I.)	21,218 SF	10.0%

[illegible]

1. RESERVED FOR CITY SPECIFIC NOTES.

LIGHT DUTY SEMI-PERMANENT PAVEMENT (IF APPLICABLE)
USE ELECTRICAL REPORT FOR AGGREGATE BASE &
WEAR COURSE. OPTIM. SEE DETAIL.

HEAVY DUTY SEMI-PERMANENT PAVEMENT (IF APPLICABLE)
USE ELECTRICAL REPORT FOR AGGREGATE BASE &
WEAR COURSE. OPTIM. SEE DETAIL.

CONCRETE PAVED (IF APPLICABLE)
IMPROVED OR WALK-ON ELECTRICAL REPORT FOR AGGREGATE BASE & CONCRETE. OPTIM. WITHIN ROW SEE CSD DETAIL. WITHIN PRIVATE PROPERTY SEE PRIVATE.

PROPERTY LINE

CONSTRUCTION LINES

**CURB AND GUTTER-SEE NOTES (T/L) TIP OUT
GUTTER WHERE APPLICABLE-SEE PLAN**

TRAFFIC DIRECTIONAL ARROW PAVEMENT MARKINGS

**IRON AND NOD TO BE ASSEMBLED, SHOP DRAWINGS REQUIRED,
HP = ACCESSIBLE IRON
S = STOP
ST = STOP
CP = COMPACT CAR PARKING ONLY**

**ACCESSIBILITY ARROW (IF APPLICABLE) DO NOT
PAVE**



PRELIMINARY:
NOT FOR
CONSTRUCTION

7200 & 7250 FRANCE

7200 & 7250 FRANCE AVE S, EDINA, MN 55435

ORION INVESTMENTS

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

David J. Knaeble
DATE 8/12/22 LICENSE NO. 48776

ISSUE/SUBMITTAL SUMMARY	
DATE	DESCRIPTION

0000000	111 9.99 17.41
8100000	203 5.84 17.76

DRAWN BY: AM REVIEWED BY: DK

PROJECT NUMBER: 21419

REVISION SUMMARY	
DATE	DESCRIPTION

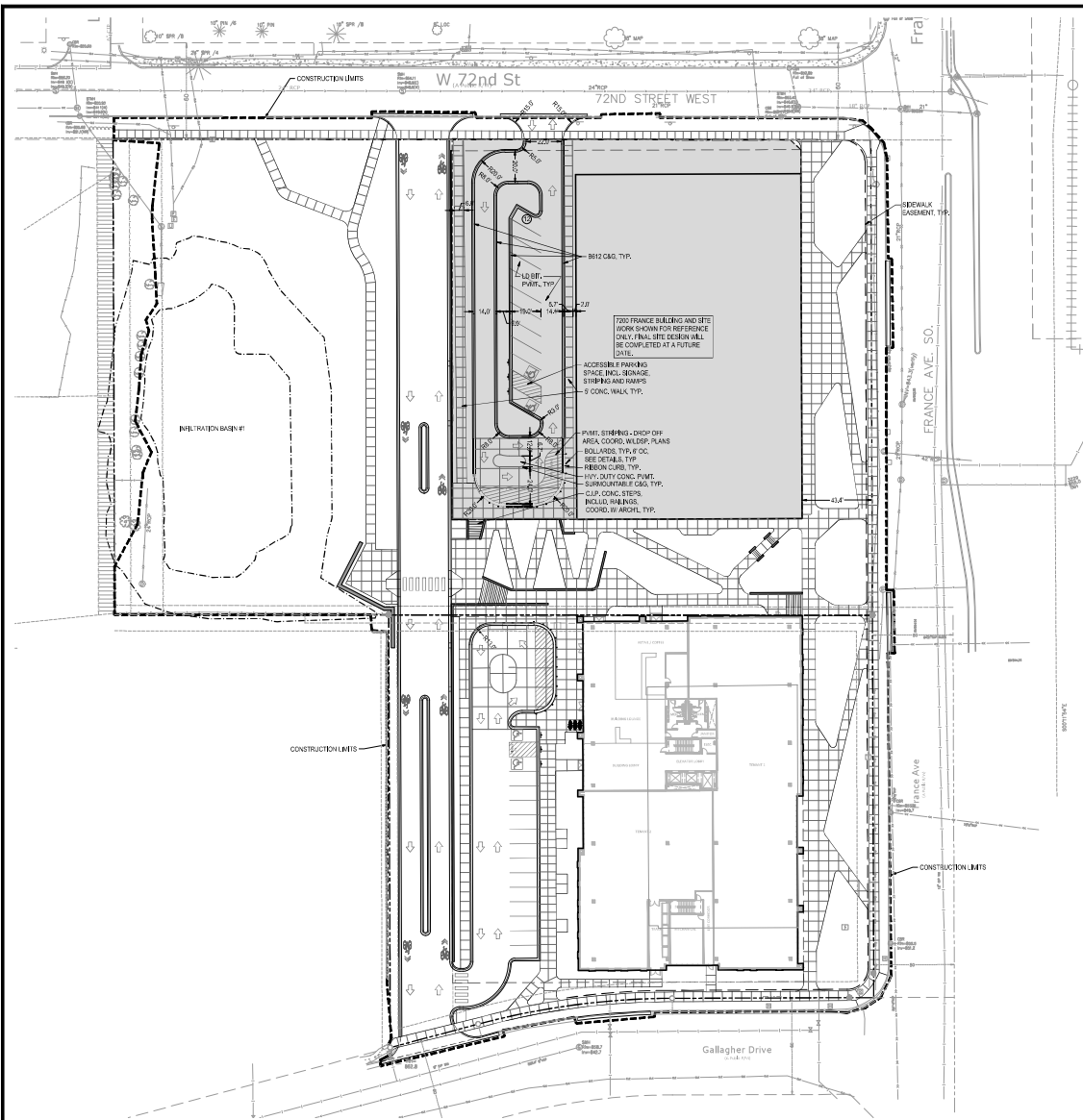
The diagram shows two horizontal parallel lines. The top line is labeled 'a' and the bottom line is labeled 'b'. A vertical line intersects both horizontal lines. The intersection point on line 'a' is labeled 'c' and the intersection point on line 'b' is labeled 'd'.

PH. 1 (B) SITE PLAN

C20D

C2.0 B

DOI: 10.21203/rs.3.rs-2022411/v1



OPERATIONAL NOTES:	
SNOW REMOVAL	ALL SNOW SHALL BE STORED ON-SITE OUTSIDE PARKING LOT. WHEN FULL, REMOVAL CO. SHALL REMOVE EXCESS ON-SITE.
TRASH REMOVAL	TRASH SHALL BE PLACED IN PARKING GARAGE AND REMOVED BY COMMERCIAL CO. WEEKLY.
DELIVERIES	DELIVERIES SHALL OCCUR AT THE FRONT DOOR VIA STANDARD COMMERCIAL DELIVERY VEHICLES (UPS, FEDEX, USPS).

SITE AREA TABLE:

SITE AREA CALCULATIONS			
7200	BUILDING COVERAGE	43,272 SF	8.5%
	ALL PAVEMENTS	64,971 SF	46.1%
	PROPERTY	108,464 SF	46.4%
TOTAL SITE AREA			
7200		108,464 SF	100.0%
7250	BUILDING COVERAGE	46,807 SF	29.4%
	ALL PAVEMENTS	1,279,231 SF	1.9%
	PROPERTY	80,139 SF	23.7%
TOTAL SITE AREA			
7250		81,416 SF	100.0%
TOTAL SITE AREA			
7200 & 7250		189,880 SF	100.0%

IMPERVIOUS SURFACE			
7200	EXISTING CONDITION	76,889 SF	56.8%
	PHASE II CONDITION	87,644 SF	64.0%
	DIFFERENCE (EX. VS PH. II)	10,755 SF	8.2%
7250			
7250	EXISTING CONDITION	41,086 SF	12.3%
	PHASE II CONDITION	44,480 SF	46.5%
	DIFFERENCE (EX. VS PH. II)	3,394 SF	33.4%
TOTAL			
TOTAL	EXISTING CONDITION	117,975 SF	57.2%
	PHASE II CONDITION	132,124 SF	72.7%
	DIFFERENCE (EX. VS PH. II)	14,149 SF	14.6%

SITE LAYOUT NOTES:

- ALL LIVE UTILITY LOCATIONS SHOWN ARE APPROXIMATE. CONTACT "OWNER STATE ONE CALL" (800) 485-5822 OR 800-252-1165 FOR UTILITY LOCATIONS 48 HOURS PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL REPAIR OR REPLACE ANY UTILITIES THAT ARE DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.
- CONTRACTOR SHALL VERIFY LOCATIONS AND LAYOUT OF ALL SITE ELEMENTS PRIOR TO BEGINNING CONSTRUCTION, INCLUDING BUT NOT LIMITED TO, LOCATIONS OF EXISTING AND PROPOSED PROPERTY LINES, EASEMENTS, SETBACKS, UTILITIES, BUILDINGS AND PAVEMENTS. CONTRACTOR IS RESPONSIBLE FOR FINAL LOCATIONS OF ALL ELEMENTS FOR THE SITE. ANY REVISIONS REQUIRED AT OR CONCURRENT WITH CONSTRUCTION, DUE TO LOCATION, ADJUSTMENTS SHALL BE CORRECTED AT NO ADDITIONAL COST TO OWNER. ADJUSTMENTS TO THE LAYOUT SHALL BE APPROVED BY THE ENGINEER/LANDSCAPE ARCHITECT PRIOR TO INSTALLATION OF MATERIALS. STONE LAYOUT FOR APPROVAL.
- THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS PRIOR TO CONSTRUCTION, INCLUDING A RIGHT-OF-WAY AND STREET OPENING PERMIT.
- THE CONTRACTOR SHALL VERIFY RECOMMENDATIONS NOTED IN THE GEO TECHNICAL REPORT PRIOR TO INSTALLATION OF SITE IMPROVEMENT MATERIALS.
- CONTRACTOR SHALL FIELD VERIFY COORDINATES AND LOCATION DIMENSIONS & ELEVATIONS OF THE BUILDING AND STAKE FOR REVIEW AND APPROVAL BY THE OWNER REPRESENTATIVE PRIOR TO INSTALLATION OF FOOTING MATERIALS.
- LOCATIONS OF STRUCTURES, ROADWAY PAVEMENTS, CURBS AND GUTTERS, BOLLARDS, AND WALKS ARE APPROXIMATE AND SHALL BE STAKED IN THE FIELD PRIOR TO INSTALLATION, FOR REVIEW AND APPROVAL BY THE ENGINEER/LANDSCAPE ARCHITECT.
- CURB DIMENSIONS SHOWN ARE TO FACE OF CURB. BOLLARD DIMENSIONS ARE TO FACE OF CONCRETE FOUNDATION. LOCATION OF BUILDINGS TO BUILDING FOUNDATION IS TO BE SHOWN ON THE DRAWINGS.
- THE CONTRACTOR SHALL SUBMIT SHOP DRAWINGS OR SAMPLES AS SPECIFIED FOR REVIEW AND APPROVAL BY THE ENGINEER/LANDSCAPE ARCHITECT PRIOR TO FABRICATION FOR ALL PREFABRICATED SITE IMPROVEMENT MATERIALS SUCH AS, BUT NOT LIMITED TO, THE FOLLOWING: FURNISHING, PAVEMENTS, WALKS AND BOLLARDS, BENCHES, FLAGPOLES, LANDING PADS FOR CURB RAUPS, AND LIGHT AND POLES. THE OWNER RESERVES THE RIGHT TO REJECT INSTALLED MATERIALS NOT PREVIOUSLY APPROVED.
- PEDESTRIAN CURB RAUPS SHALL BE CONSTRUCTED WITH TRUNCATED DOME LANDING AREAS IN ACCORDANCE WITH ADA REQUIREMENTS-SEE DETAIL.
- CROSSWALK STRIPING SHALL BE 2' WIDE WHITE PAINTED LINE, SPACED 4' ON CENTER PERPENDICULAR TO THE FLOW OF TRAFFIC. WIDTH OF CROSSWALK SHALL BE 5' WIDE. ALL OTHER PAVEMENT MARKINGS SHALL BE WHITE IN COLOR UNLESS OTHERWISE NOTED OR REQUIRED BY ADA OR LOCAL GOVERNING BODIES.
- SEE SITE PLAN FOR CURB AND GUTTER TYPE, TAPER BETWEEN CURB TYPES-SEE DETAIL.
- ALL CURB RISES AND SLOPES SHALL BE AS SPECIFIED OTHERWISE NOTED.
- CONTRACTOR SHALL REFER TO FINAL PLAT FOR LOT BOUNDARIES, NUMBERS, AREAS AND DIMENSIONS PRIOR TO SITE IMPROVEMENTS.
- FIELD VERIFY ALL EXISTING SITE CONDITIONS, DIMENSIONS.
- PAVING IS TO BE SET PARALLEL TO PERPENDICULAR TO PAINTING BUILDING UNLESS NOTED OTHERWISE.
- ALL PARKING LOT PAINT STRIPING TO BE WHITE, WHITE TYPE.
- GRAVINOUS PAVING TO BE "LIGHT" DUTY UNLESS OTHERWISE NOTED. SEE DETAIL SHEETS FOR PAVEMENT SECTIONS.
- ALL TREES THAT ARE TO REMAIN ARE TO BE PROTECTED FROM DAMAGE WITH A CONSTRUCTION FENCE AT THE DRAINAGE. SEE LANDSCAPE DOCUMENTS.

CITY OF EDINA SITE SPECIFIC NOTES:

- RESERVED FOR CITY SPECIFIC NOTES.


SITE PLAN LEGEND

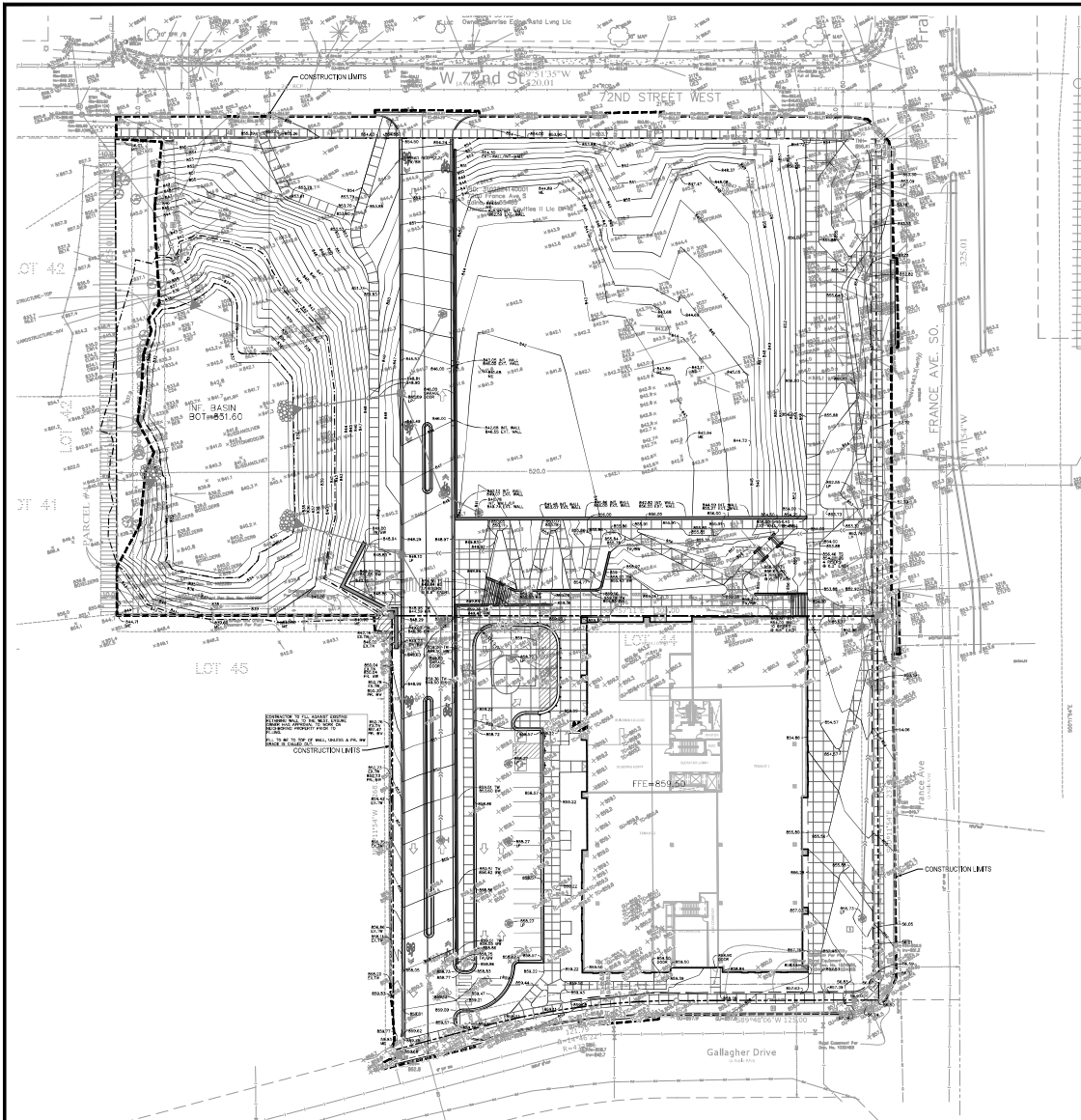
	PHASE II CONSTRUCTION LIMITS
	LIGHT DUTY BITUMINOUS PAVEMENT IF APPLICABLE. SEE GEOTECHNICAL REPORT FOR AGGREGATE BASE & WEAR COURSE DEPTH. SEE DETAIL.
	HEAVY DUTY BITUMINOUS PAVEMENT IF APPLICABLE. SEE GEOTECHNICAL REPORT FOR AGGREGATE BASE & WEAR COURSE DEPTH. SEE DETAIL.
	CONCRETE PAVEMENT IF APPLICABLE AT SPECIFIED PAVEMENTS. SEE GEOTECHNICAL REPORT FOR AGGREGATE BASE & CONCRETE DEPTHS. WITHIN ROW SEE CITY DETAIL. WITHIN PRIVATE PROPERTY SEE CUS DETAIL.
	PROPERTY LINE
	CONSTRUCTION LIMITS
	CURB AND GUTTER/PAVEMENT NOTES (1:1) 10' OUT GUTTER WHERE APPLICABLE-SEE PLAN
	TRAFFIC DIRECTIONAL ARROW PAVEMENT MARKINGS
	STOP AND POST ASSEMBLY, SHOP DRAWINGS REQUIRED.
	HC - ACCESSIBLE STOP
	ST - STOP
	CP - COMPACT CAR PARKING ONLY
	ACCESSIBILITY ARROW (IF APPLICABLE) DO NOT PAINT.



PRELIMINARY:
NOT FOR
CONSTRUCTION

7200 & 7250 FRANCE
7200 & 7250 FRANCE AVE S. EDINA, MN 55445
ORION INVESTMENTS
6950 YORK AVE S SUITE 207, EDINA, MN 55435

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.	
	
David J. Kneitel	
CITY OF EDINA	EDINA, MN 55427
ISSUES/SUBMITTAL SUMMARY	
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GENERAL GRADING NOTES:

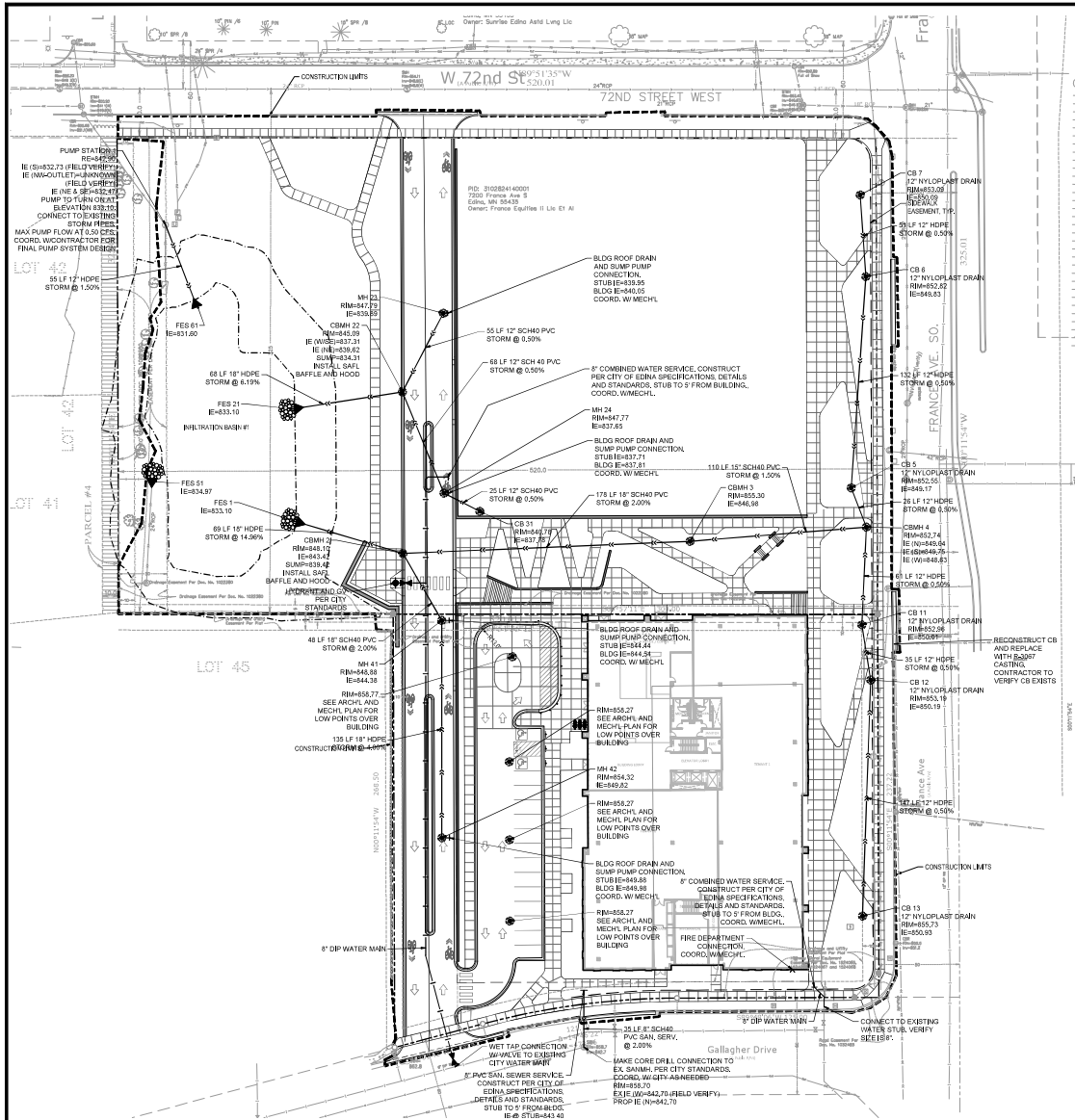
1. CONTRACTOR SHALL VERIFY ALL BUILDING ELEVATIONS, (P.E. LVL, GRD) PRIOR TO CONSTRUCTION BY CROSS CHECKING WITH ARCHITECTURAL, STRUCTURAL, AND CIVIL ELEVATIONS FOR EQUIVALENT "TOP" ELEVATIONS. THIS MUST BE DONE PRIOR TO EXCAVATION AND INSTALLATION OF ANY FOOTING MATERIALS. VERIFICATION OF THE COORDINATION SHALL BE COMPLETED IN WRITING BY CME, SUPERVISOR, ARCHITECTURAL, STRUCTURAL AND CONTRACTOR PRIOR TO CONSTRUCTION.
2. ALL EXISTING UTILITY LOCATIONS SHOWN ARE APPROXIMATE. CONTACT "COPPER STATE ONE CALL" (866)464-0002 OR (866)252-1166 FOR UTILITY LOCATIONS. 48 HOURS PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL REPAIR OR REPLACE ANY UTILITIES THAT ARE DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.
3. SEE SITE PLAN FOR HORIZONTAL LAYOUT AND GENERAL GRADING NOTES.
4. THE CONTRACTOR SHALL COMPLY WITH THE SITE GRADING CONSTRUCTION INCLUDING BUT NOT LIMITED TO SITE PREPARATION, SOIL CORRECTION, EXCAVATION, EMBANKMENT, ETC. IN ACCORDANCE WITH THE REQUIREMENTS OF THE OWNER'S SOILS ENGINEER. ALL SOIL TESTING SHALL BE COMPLETED BY THE OWNER'S SOILS ENGINEER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL REQUIRED SOIL TESTS AND INSPECTIONS WITH THE SOILS ENGINEER.
5. ANY ELEMENTS OF AN EARTH RETENTION SYSTEM AND RELATED EXCAVATIONS THAT FALL WITHIN THE PUBLIC RIGHT-OF-WAY WILL REQUIRE A RIGHT-OF-WAY EXCAVATION PERMIT. CONTRACTOR IS RESPONSIBLE FOR ACQUIRING THIS PERMIT PRIOR TO CONSTRUCTION IF APPLICABLE.
6. ALL EXISTING UTILITY LOCATIONS SHOWN ARE APPROXIMATE. CONTACT "COPPER STATE ONE CALL" (866)464-0002 OR (866)252-1166 FOR UTILITY LOCATIONS. 48 HOURS PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL REPAIR OR REPLACE ANY UTILITIES THAT ARE DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.
7. GRADING AND EXCAVATION ACTIVITIES SHALL BE PERFORMED IN ACCORDANCE WITH THE NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT REQUIREMENTS A PERMIT REQUIREMENTS OF THE CITY.
8. PROPOSED SPOT GRADES ARE FLOW-TO FINISHED GRADE ELEVATIONS, UNLESS OTHERWISE NOTED.
9. GRADES OF WALKS SHALL BE INSTALLED WITH A MAX. LONGITUDINAL SLOPE AND 1% MIN. AND 2% MAX. CROSS SLOPE, UNLESS OTHERWISE NOTED.
10. PROPOSED SLOPES SHALL NOT EXCEED 3:1 UNLESS INDICATED OTHERWISE ON THE DRAWINGS. MAXIMUM SLOPES IN MAINTAINED AREAS 4:1.
11. PROPOSED RETAINING WALLS, FREESTANDING WALLS, OR COMBINATION OF WALL TYPES GREATER THAN 4' IN HEIGHT SHALL BE DESIGNED AND ENGINEERED BY A REGISTERED RETAINING WALL ENGINEER. DESIGN DRAWINGS SHALL BE SUBMITTED FOR REVIEW AND APPROVAL PRIOR TO CONSTRUCTION.
12. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTENANCE OF GRADE STAKES THROUGHOUT THE DURATION OF CONSTRUCTION TO ESTABLISH PROPER GRADES. THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR A FINAL FIELD CHECK OF FINISHED GRADES ACCEPTABLE TO THE ENGINEER AND CME ARCHITECT PRIOR TO TOPSOIL AND GRASSING ACTIVITIES.
13. IF EXCESS OR SHORTAGE OF SOIL MATERIAL EXISTS, THE CONTRACTOR SHALL TRANSPORT ALL EXCESS SOIL MATERIAL OFF THE SITE TO AN AREA SELECTED BY THE CONTRACTOR, OR IMPORT SUITABLE MATERIAL TO THE SITE.
14. EXCAVATE TOPSOIL FROM AREAS TO BE FURTHER EXCAVATED OR REGRADED AND STOCKPILE IN AREAS DESIGNATED ON THE SITE. THE CONTRACTOR SHALL WAIVE EROSION TOPSOIL, FOR REPAIRING ON THE SITE AS PREPARED, EXCESS TOPSOIL, SHALL BE PLACED IN EMBANKMENT AREAS, OUTSIDE OF BUILDING PADS, ROADWAYS AND PARKING AREAS. THE CONTRACTOR SHALL SUBMIT CUT AREAS, WHERE TURF IS TO BE ESTABLISHED, TO A DEPTH OF 6 INCHES, REPAIR TOPSOIL IN AREAS WHERE TURF IS TO BE ESTABLISHED TO A MINIMUM DEPTH OF 6 INCHES.
15. FINISHED GRADING SHALL BE COMPLETED. THE CONTRACTOR SHALL UNIFORMLY GRADE AREAS WITHIN LIMITS OF GRADING, INCLUDING ADJACENT TRANSITION AREAS, PROVIDE A SMOOTH FINISHED SURFACE WITHIN SPECIFIED TOLERANCES, WITH UNIFORM LEVELS OR SLOPES BETWEEN POINTS WHERE ELEVATIONS ARE SHOWN, ON BETWEEN CHECK POINTS AND EXISTING GRADES. AREAS THAT HAVE BEEN FINISHED GRADING SHALL BE PROTECTED FROM SUBSEQUENT CONSTRUCTION OPERATIONS, TRAFFIC AND PRODIGAL REPAIR. AREAS THAT HAVE BECOME RUPTED BY TRAFFIC OR ERODED BY WATER OR HAS SETTLED BELOW THE CORRECT GRADE, ALL AREAS DETURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED TO EQUAL OR BETTER THAN ORIGINAL CONDITION OR TO THE REQUIREMENTS OF THE NEW WORK.
16. PRIOR TO PLACEMENT OF THE AGGREGATE BASE, A TEST HOLE WILL BE REQUIRED ON THE STREET AND/OR PARKING AREA SUBGRADE. THE CONTRACTOR SHALL PROVIDE A LOADED TANDEN AXLE TRUCK WITH A GROSS WEIGHT OF 20 TONS. THE TEST HOLE SHALL BE AT THE DISCRETION OF THE SOILS ENGINEER AND SHALL BE COMPLETED IN AREAS AS DIRECTED BY THE SOILS ENGINEER. THE SOILS ENGINEER SHALL DETERMINE WHICH SECTIONS OF THE STREET OR PARKING AREA ARE UNSATISFACTORY. CORRECTION OF THE SUBGRADE SOILS SHALL BE COMPLETED IN ACCORDANCE WITH THE REQUIREMENTS OF THE SOILS ENGINEER. NO TEST HOLE SHALL OCCUR WITHIN 10' OF ANY UNDERGROUND STORM RETENTION/RETENTION SYSTEMS.
17. TOLERANCES
- 18.1. THE BUILDING SUBGRADE FINISHED SURFACE ELEVATION SHALL NOT VARY BY MORE THAN 0.30 FOOT ABOVE OR 0.30 FOOT BELOW, THE PRESCRIBED ELEVATION AT ANY POINT WHERE MEASUREMENT IS MADE.
- 18.2. THE STREET OR PARKING AREA SUBGRADE FINISHED SURFACE ELEVATION SHALL NOT VARY BY MORE THAN 0.35 FOOT ABOVE OR 0.35 FOOT BELOW, THE PRESCRIBED ELEVATION OF ANY POINT WHERE MEASUREMENT IS MADE.
- 18.3. AREAS WHICH ARE TO RECEIVE TOPSOIL, SHALL BE GRADED TO WITHIN 0.30 FOOT ABOVE OR BELOW THE REQUIRED ELEVATION, UNLESS DIRECTED OTHERWISE BY THE ENGINEER.
- 18.4. TOPSOIL SHALL BE GRADED TO PLUS OR MINUS 1/2 INCH OF THE SPECIFIED THICKNESS.
19. MAINTENANCE
- 19.1. THE CONTRACTOR SHALL PROTECT NEWLY GRADED AREAS FROM TRAFFIC AND EROSION, AND KEEP AREA FREE OF TRASH AND DEBRIS.
- 19.2. CONTRACTOR SHALL REPAIR AND REESTABLISH GRADING IN SETTLED, ERODED AND RUTTED AREAS TO SPECIFIED TOLERANCES, DURING THE CONSTRUCTION IF REQUIRED, AND DURING THE WARRANTY PERIOD. ERODED AREAS WHERE TURF IS TO BE ESTABLISHED SHALL BE RESEEDING AND MULCHING.
- 19.3. WHERE COVERED COMPACTED AREAS ARE DETURBED BY SUBSEQUENT CONSTRUCTION OPERATIONS OR ADVERSE WEATHER, CONTRACTOR SHALL SCARP, SURFACE, RESEED, AND COMPACT TO REQUIRED DENSITY PRIOR TO FURTHER CONSTRUCTION.

CITY OF EDINA GRADING NOTES:

1. RESERVED FOR CITY SPECIFIC GRADING NOTES.

EROSION CONTROL NOTES:

SEE SHEET 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CITY OF EDINA UTILITY NOTES:

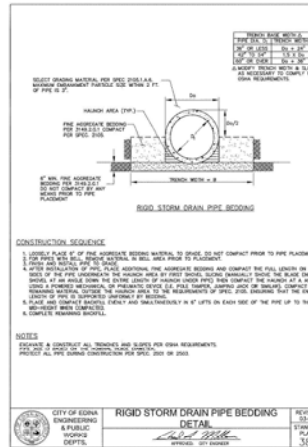
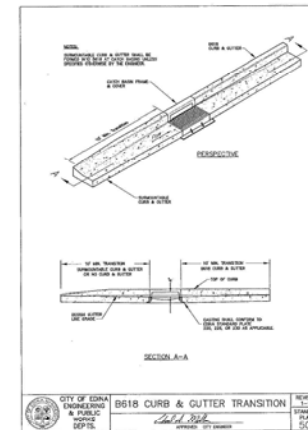
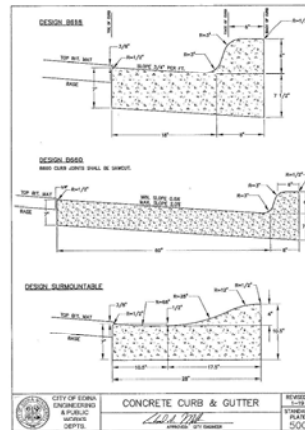
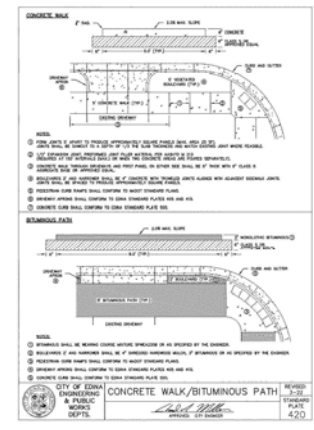
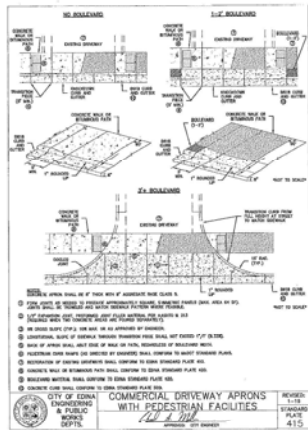
- RESERVED FOR CITY SPECIFIC UTILITY NOTES.

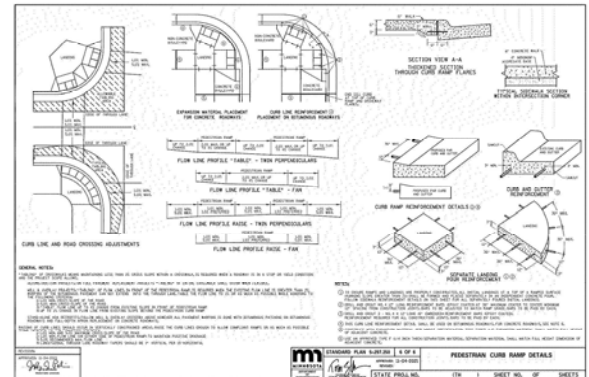
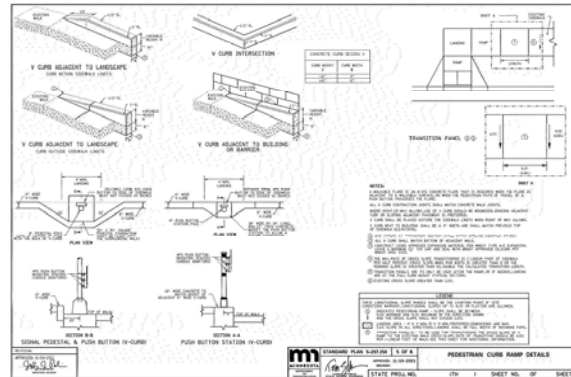
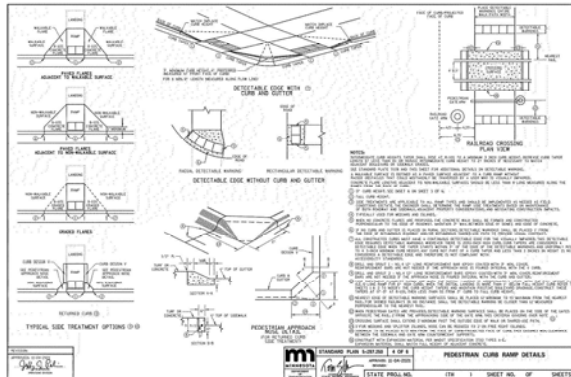
UTILITY LEGEND:





1. RESERVED FOR CITY SPECIFIC UTILITY NOTE





00 & 7250 FRANCE
7200 & 7250 FRANCE AVE S, EDINA, MN 55435

ORION INVESTMENTS
8550 YORK AVE S SUITE 207, EDINA, MN 55435

ISSUE/SUBMITTAL SUMMARY	
DATE	DESCRIPTION
REVISION	1. 2. 3. 4. 5. 6. 7. 8. 9. 10. 11. 12. 13. 14. 15. 16. 17. 18. 19. 20. 21. 22. 23. 24. 25. 26. 27. 28. 29. 30. 31. 32. 33. 34. 35. 36. 37. 38. 39. 40. 41. 42. 43. 44. 45. 46. 47. 48. 49. 50. 51. 52. 53. 54. 55. 56. 57. 58. 59. 60. 61. 62. 63. 64. 65. 66. 67. 68. 69. 70. 71. 72. 73. 74. 75. 76. 77. 78. 79. 80. 81. 82. 83. 84. 85. 86. 87. 88. 89. 90. 91. 92. 93. 94. 95. 96. 97. 98. 99. 100. 101. 102. 103. 104. 105. 106. 107. 108. 109. 110. 111. 112. 113. 114. 115. 116. 117. 118. 119. 120. 121. 122. 123. 124. 125. 126. 127. 128. 129. 130. 131. 132. 133. 134. 135. 136. 137. 138. 139. 140. 141. 142. 143. 144. 145. 146. 147. 148. 149. 150. 151. 152. 153. 154. 155. 156. 157. 158. 159. 160. 161. 162. 163. 164. 165. 166. 167. 168. 169. 170. 171. 172. 173. 174. 175. 176. 177. 178. 179. 180. 181. 182. 183. 184. 185. 186. 187. 188. 189. 190. 191. 192. 193. 194. 195. 196. 197. 198. 199. 200. 201. 202. 203. 204. 205. 206. 207. 208. 209. 210. 211. 212. 213. 214. 215. 216. 217. 218. 219. 220. 221. 222. 223. 224. 225. 226. 227. 228. 229. 230. 231. 232. 233. 234. 235. 236. 237. 238. 239. 240. 241. 242. 243. 244. 245. 246. 247. 248. 249. 250. 251. 252. 253. 254. 255. 256. 257. 258. 259. 260. 261. 262. 263. 264. 265. 266. 267. 268. 269. 270. 271. 272. 273. 274. 275. 276. 277. 278. 279. 280. 281. 282. 283. 284. 285. 286. 287. 288. 289. 290. 291. 292. 293. 294. 295. 296. 297. 298. 299. 300. 301. 302. 303. 304. 305. 306. 307. 308. 309. 310. 311. 312. 313. 314. 315. 316. 317. 318. 319. 320. 321. 322. 323. 324. 325. 326. 327. 328. 329. 330. 331. 332. 333. 334. 335. 336. 337. 338. 339. 340. 341. 342. 343. 344. 345. 346. 347. 348. 349. 350. 351. 352. 353. 354. 355. 356. 357. 358. 359. 360. 361. 362. 363. 364. 365. 366. 367. 368. 369. 370. 371. 372. 373. 374. 375. 376. 377. 378. 379. 380. 381. 382. 383. 384. 385. 386. 387. 388. 389. 390. 391. 392. 393. 394. 395. 396. 397. 398. 399. 400. 401. 402. 403. 404. 405. 406. 407. 408. 409. 410. 411. 412. 413. 414. 415. 416. 417. 418. 419. 420. 421. 422. 423. 424. 425. 426. 427. 428. 429. 430. 431. 432. 433. 434. 435. 436. 437. 438. 439. 440. 441. 442. 443. 444. 445. 446. 447. 448. 449. 450. 451. 452. 453. 454. 455. 456. 457. 458. 459. 460. 461. 462. 463. 464. 465. 466. 467. 468. 469. 470. 471. 472. 473. 474. 475. 476. 477. 478. 479. 480. 481. 482. 483. 484. 485. 486. 487. 488. 489. 490. 491. 492. 493. 494. 495. 496. 497. 498. 499. 500. 501. 502. 503. 504. 505. 506. 507. 508. 509. 510. 511. 512. 513. 514. 515. 516. 517. 518. 519. 520. 521. 522. 523. 524. 525. 526. 527. 528. 529. 530. 531. 532. 533. 534. 535. 536. 537. 538. 539. 540. 541. 542. 543. 544. 545. 546. 547. 548. 549. 550. 551. 552. 553. 554. 555. 556. 557. 558. 559. 560. 561. 562. 563. 564. 565. 566. 567. 568. 569. 570. 571. 572. 573. 574. 575. 576. 577. 578. 579. 580. 581. 582. 583. 584. 585. 586. 587. 588. 589. 590. 591. 592. 593. 594. 595. 596. 597. 598. 599. 600. 601. 602. 603. 604. 605. 606. 607. 608. 609. 610. 611. 612. 613. 614. 615. 616. 617. 618. 619. 620. 621. 622. 623. 624. 625. 626. 627. 628. 629. 630. 631. 632. 633. 634. 635. 636. 637. 638. 639. 640. 641. 642. 643. 644. 645. 646. 647. 648. 649. 650. 651. 652. 653. 654. 655. 656. 657. 658. 659. 660. 661. 662. 663. 664. 665. 666. 667. 668. 669. 670. 671. 672. 673. 674. 675. 676. 677. 678. 679. 680. 681. 682. 683. 684. 685. 686. 687. 688. 689. 690. 691. 692. 693. 694. 695. 696. 697. 698. 699. 700. 701. 702. 703. 704. 705. 706. 707. 708. 709. 710. 711. 712. 713. 714. 715. 716. 717. 718. 719. 720. 721. 722. 723. 724. 725. 726. 727. 728. 729. 730. 731. 732. 733. 734. 735. 736. 737. 738. 739. 740. 741. 742. 743. 744. 745. 746. 747. 748. 749. 750. 751. 752. 753. 754. 755. 756. 757. 758. 759. 760. 761. 762. 763. 764. 765. 766. 767. 768. 769. 770. 771. 772. 773. 774. 775. 776. 777. 778. 779. 780. 781. 782. 783. 784. 785. 786. 787. 788. 789. 790. 791. 792. 793. 794. 795. 796. 797. 798. 799. 800. 801. 802. 803. 804. 805. 806. 807. 808. 809. 810. 811. 812. 813. 814. 815. 816. 817. 818. 819. 820. 821. 822. 823. 824. 825. 826.

C5.2



SWPPP NOTES:

1. ALL EXISTING UTILITY LOCATIONS SHOWN ARE APPROXIMATE. CONTACT "GOSHER STATE ONE CALL" (615-454-0002 OR 800-255-1166) FOR UTILITY LOCATIONS, 48 HOURS PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL REPAIR OR REPLACE ANY UTILITIES THAT ARE DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.
2. THIS PROJECT IS MORE THAN ONE ACRE AND WILL REQUIRE AN NPDES PERMIT. CONTRACTOR IS RESPONSIBLE FOR OBTAINING ANY EROSION CONTROL MEASURES REQUIRED BY THIS SPECIFICATION.
3. SEE SHEETS SW1.0 - SW1.6 FOR ALL EROSION CONTROL NOTES, DESCRIPTIONS, AND PRACTICES.
4. SEE GRADING PLAN FOR ADDITIONAL GRADING AND EROSION CONTROL NOTES.
5. CONTRACTOR IS RESPONSIBLE FOR SWPPP IMPLEMENTATION, INSPECTIONS, AND COMPLIANCE WITH NPDES PERMIT.

CITY OF EDINA EROSION CONTROL NOTES:

1. RESERVED FOR CITY SPECIFIC EROSION CONTROL NOTES.

ALL SPECIFIED EROSION AND SEDIMENT CONTROL PRACTICES, AND MEASURES CONTAINED IN THIS SWPPP ARE THE MINIMUM REQUIREMENTS. ADDITIONAL PRACTICES MAY BE REQUIRED DURING THE COURSE OF CONSTRUCTION.

CivilSite
S E S U E

Civil Engineering • Surveying • Landscape Architecture
5000 Glenwood Avenue
Golden Valley, MN 55422
civilsitegroup.com 612-615-0000

PRELIMINARY:
NOT FOR
CONSTRUCTION

7200 & 7250 FRANCE

7200 & 7250 FRANCE AVE S, EDINA, MN 55435

ORION INVESTMENTS

35550 YORK AVE S SUITE 207, EDINA, MN 55435

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

David J. Knaeble
DATE: 8/12/22 LICENSE NO. 48776

ISSUE/SUBMITTAL SUMMARY	
DATE	DESCRIPTION
06/09/2007	1ST SUBMITTAL
07/09/2007	2ND SUBMITTAL

DRAWN BY: AM REVIEWED BY: CK
PROJECT NUMBER: 21419

REVISION SUMMARY	
DATE	DESCRIPTION

PH. 2 SWPPP -
PROPOSED

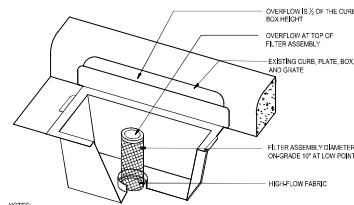
CONDITIONS
SW1.2

SVV 1.2
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LEGEND:

PHASE II CONSTRUCTION LIMITS
EX 1" CONTOUR ELEVATION INTERVAL
1/2" CONTOUR ELEVATION INTERVAL
DRAINAGE ARROW
SILT FENCE / BIOEROD. / GRADING LIMIT
INLET PROTECTION
STABILIZED CONSTRUCTION ENTRANCE
EROSION CONTROL BLANKET

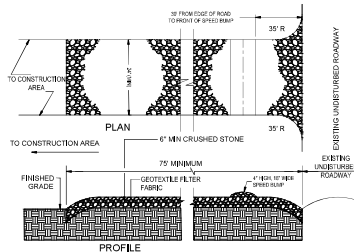




- NOTES:
1. REPLACE INLET GRATE UPON COMPLETE INSTALLATION OF INLET PROTECTION FABRIC.
2. CONTRACTOR SHALL REMOVE ALL ACCUMULATED DEBRIS AND GRASS FROM THE SURFACE OF THE SYSTEM AFTER EACH STORM EVENT AND AT THE COMPLETION OF THE CONTRACT.
3. REFERENCE APPLICABLE STANDARD PLATE BOXING.

1 CURB INLET FILTER

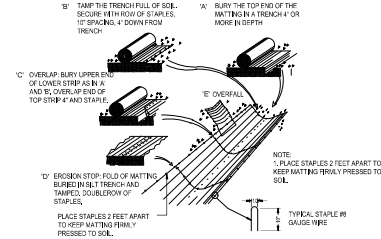
N T S



- NOTES:
1. PROVIDE APPROPRIATE TRANSITION BETWEEN STABILIZED CONSTRUCTION ENTRANCE AND UNDISTURBED ROADWAY.
2. THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO UNDISTURBED ROADWAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE OR ADDING STONE TO THE LENGTH OF THE ENTRANCE.
3. REPAIR AND CLEANOUT MEASURES USED TO TRAP SEDIMENT.
4. ALL SEDIMENT SPILLS, UNWASHED WHEELS, OR TRACKS ONTO UNDISTURBED ROADWAY SHALL BE REMOVED AS DIRECTED BY THE ENGINEER.
5. FINAL LOCATION AND INSTALLATION SHALL BE COORDINATED WITH THE CITY PRIOR TO CONSTRUCTION ACTIVITIES.
6. CRUSHED STONE SHALL BE 1/2" DIA. CLOSE GRADED, AND IN ACCORDANCE TO MNDOT SECTION 2116.

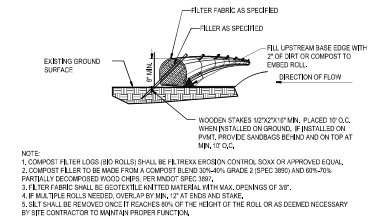
2 STABILIZED CONSTRUCTION ACCESS

N T S



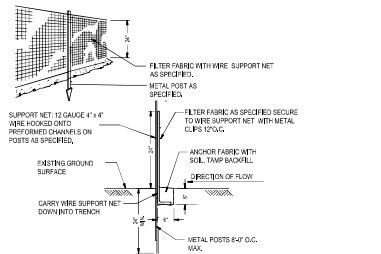
3 EROSION BLANKET

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4 SEDIMENT BIO-ROLL / COMPOST FILTER LOG

N T S



5 SEDIMENT FENCE

N T S

PRELIMINARY:
NOT FOR
CONSTRUCTION

PROJECT
7200 & 7250 FRANCE
7200 & 7250 FRANCE AVE S. EDINA, MN 55435
ORION INVESTMENTS
6950 YORK AVE S SUITE 207, EDINA, MN 55435

DESIGNED BY: CIVIL SITE
CHECKED BY: CIVIL SITE
DATE: 01/20/2022
BY: CIVIL SITE

ISSUE/SUBMITTAL SUMMARY
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SWPPP - DETAILS

SW1.3

ATTACHMENT B: SWPPP INSPECTION FORM

NOTE: THIS INSPECTION REPORT DOES NOT ADDRESS ALL ASPECTS OF THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM/STATE DISPOSAL SYSTEM (NPDES/SDS), CONSTRUCTION STORMWATER PERMIT (PERMIT) ISSUED ON AUGUST 1, 2016. THE COMPLETION OF THIS CHECKLIST DOES NOT GUARANTEE THAT ALL PERMIT REQUIREMENTS ARE IN COMPLIANCE. IT IS THE RESPONSIBILITY OF THE PERMITTEE(S) TO READ AND UNDERSTAND THE PERMIT REQUIREMENTS.

FACILITY INFORMATION

SITE NAME: _____
SITE ADDRESS: _____ CITY: _____ STATE: _____ PERMIT NUMBER: _____
CITY: _____ STATE: _____ CIP CODE: _____

INSPECTION INFORMATION

INSPECTOR NAME: _____ PHONE NUMBER: _____
ORGANIZATION/CITY/STATE: _____

DATE (MM/DD/YYYY): _____ TIME: _____ AM / PM
IS THE INSPECTION CARRIED IN SEDIMENT EROSION CONTROL AND BILT DOCUMENTED IN THE STORMWATER POLLUTION PREVENTION PLAN (SWPPP)? ☐ Y ☐ N
IS THE INSPECTION ROUTINE OR IN RESPONSE TO A STORM EVENT?
☐ 7 DAY ☐ 30 DAY

RAINFALL AMOUNT (IF APPLICABLE): _____
SITE WITHIN ONE MILE OF SPECIAL OR HAZARDOUS WASTE TREATMENT PLANT (STP) OR POTENTIALLY RECEIVE DISCHARGE FROM THE SITE? ☐ Y ☐ N
IF YES, FOLLOW SECTION 2 AND OTHER APPLICABLE PERMIT REQUIREMENTS

NOTE: IF N/A IS SELECTED AT ANY TIME, SPECIFY WHY IN THE COMMENT AREA FOR THAT SECTION.

EROSION CONTROL REQUIREMENTS (SECTION 1)

1. ARE SLOES STABILIZED WHERE NO CONSTRUCTION ACTIVITY HAS OCCURRED FOR 14 DAYS (INCLUDING STOCKPILES)? (IF DAYS WHERE APPLICABLE, OR 24 HOURS DURING MINNESOTA DEPARTMENT OF NATURAL RESOURCES (DNR) FISH SPawning RESTRICTIONS)	Y	N	N/A
2. HAS THE NEED TO STABILIZE STEEP SLOPES BEEN MINIMIZED?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
3. IF STEEP SLOPES ARE DETOURED, ARE STABILIZATION PRACTICES DESIGNED FOR STEEP SLOPES USED?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
4. ALL SLOPES/AREAS STABILIZED 200' BACK FROM POINT OF DISCHARGE OR PROPERTY EDGE WITHIN 24 HOURS (MULCH, HYDROMULCH, TACKLER, OR SIMILAR BEST MANAGEMENT PRACTICES (BMPs) ARE NOT ACCEPTABLE IN SLOPES/AREAS IF THE SLOPE IS GREATER THAN FOUR DEGREES APPROPRIATE BMPs INSTALLED PROTECTING SLOPES/AREAS)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
5. DO PIPE OUTLETS HAVE ENERGY DISSIPATION (WITHIN 24 HOURS OF CONNECTION)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
6. IS CONSTRUCTION PROHIBITED FROM OCCURRING IN DISCHARGE AREAS WITH THE SWPPP?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
7. ARE AREAS NOT TO BE DISTURBED MARKED OFF (FENCES, SIGNS, ETC.)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

COMMENTS:

SEDIMENT CONTROL REQUIREMENTS (SECTION 1)

1. ARE PERIMETER SEDIMENT CONTROLS INSTALLED PROPERLY ON ALL DOWN-DRAINAGE PERIMETERS?	Y	N	N/A
2. ARE APPROPRIATE BMPs SELECTED PROTECTING SLOPES, CATCH BASINS, AND OUTLETS?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
3. IS A 50' FOOT NATURAL BUFFER PRESERVED AROUND ALL SURFACE WATERS DURING CONSTRUCTION?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
4. IF NO 50' FOOT NATURAL BUFFER, ARE BUFFER CONTROLS INSTALLED?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
5. DO ALL PROBLETS STOCKPILES HAVE EROSION CONTROL IN PLACE?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
6. IS THERE A TEMPORARY SEDIMENT BARRI ON SITE AND IF NOT, IS A BMP REQUIRED IN SECTION 14 OF THE PERMIT?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
7. IS SOIL COMPACTION BEING MONITORED WHERE NOT DESIGNED FOR COMPACTION?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
8. IS TOPSOIL BEING PRESERVED AND REUSABLE?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
9. IF CHEMICAL FLOCCULANTS ARE USED, IS THERE A CHEMICAL FLOCCULANT PLAN IN PLACE?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

COMMENTS:

MAINTENANCE AND INSPECTIONS (SECTION 1)

1. ARE ALL PREVIOUSLY STABILIZED AREAS MAINTAINING GROUND COVER?	Y	N	N/A
2. ARE PERIMETER CONTROLS MAINTAINED AND FUNCTIONING PROPERLY? SEDIMENT REMOVED WHEN ORIGINALLY FULLY?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
3. ARE BUILT PROTECTION SYSTEMS MAINTAINED AND ADEQUATELY PROTECTING BUILT UP?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
4. ARE THE TEMPORARY SEDIMENT BARRIERS MAINTAINED AND FUNCTIONING PROPERLY?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
5. ARE SLOPES, TRACKING BMPs, BILT, ETC. IN PLACE AND MAINTAINED AND FUNCTIONING PROPERLY?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
6. ARE ALL SLOPES/AREAS STABILIZED WITHIN 24 HOURS OF CONSTRUCTION?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
7. HAVE ALL SURFACE WATERS, STITCHES, CONDUITS, AND DISCHARGE POINTS BEEN INSPECTED?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
8. HAVE ALL DISCHARGES BEEN MONITORED TO ENSURE THAT CLEAN WATER IS LEAVING THE SITE AND THE DISCHARGE IS NOT CAUSING EROSION OR SCOUR?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

IF YES, RECORD THE LOCATION OF ALL POINTS OF DISCHARGE, PHOTOGRAPH AND DESCRIBE THE DISCHARGE (SIZE, COLOR, ODOOR, FOAM, OIL, SHEEN, TIME, ETC.). DESCRIBE HOW THE DISCHARGE WILL BE ADDRESSED, WAS THE DISCHARGE A SEDIMENT DELTA? IF YES, WILL THE DELTA BE RECOVERED WITHIN SEVEN DAYS AND IN ACCORDANCE WITH PERMIT 11 OF THE PERMIT?

COMMENTS:

POLLUTION PREVENTION (SECTION 1)

1. ARE ALL CONSTRUCTION MATERIALS THAT CAN LEACH POLLUTANTS UNDER COVER OR PROTECTED?	Y	N	N/A
2. ARE HAZARDOUS MATERIALS BEING PROPERLY STORED?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
3. ARE APPROPRIATE BMPs BEING USED TO PREVENT DISCHARGES ASSOCIATED WITH FUELING AND MAINTENANCE OF EQUIPMENT OR VEHICLES?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
4. ARE ALL SOLID WASTES BEING PROPERLY CONTAINED AND DISPOSED OFF?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
5. IS THERE A CONCRETE/OTHER MATERIAL WASHOUT AREA ON SITE AND BILT BEING USED?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
6. IS THE CONCRETE WASHOUT AREA MAINTAINED?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
7. ARE THE CONCRETE/OTHER MATERIAL WASHOUT AREAS PROPERLY MAINTAINED?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

COMMENTS:

OTHER

1. IS A COPY OF THE SWPPP, INSPECTION RECORDS, AND TRAINING DOCUMENTATION LOCATED ON THE CONSTRUCTION SITE OR CAN IT BE MADE AVAILABLE WITHIN 72 HOURS?	Y	N	N/A
2. HAS THE SWPPP BEEN FOLLOWED AND NOT LIMITED ON SITE AND AMENDED AS NEEDED?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
3. IS ANY DISTURBANCE OCCURRING ON SITE?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
4. IF YES, WHY? BMPs ARE BEING USED TO ENSURE THAT CLEAN WATER IS LEAVING THE SITE AND THE DISCHARGE IS NOT CAUSING EROSION OR SCOUR?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

4. WILL A PERMANENT STORMWATER MANAGEMENT SYSTEM BE CREATED FOR THIS PROJECT IF REQUIRED AND IN ACCORDANCE WITH SECTION 15 OF THE PERMIT (IF ADDING AN ACRES OR MORE OF NEW IMPERVIOUS SURFACES)? IF YES, DESCRIBE:	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
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5. IF INFILTRATION/FILTRATION SYSTEMS ARE BEING CONSTRUCTED, ARE THEY MARKED AND PROTECTED FROM COMPACTION AND SEDIMENTATION?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
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6. DESCRIPTION OF AREAS OF NON-COMPLIANCE NOTED DURING THE INSPECTION, REQUIRED CORRECTIVE ACTIONS, AND RECOMMENDED DATE OF COMPLETION OF CORRECTIVE ACTIONS:	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
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7. PROPOSED AMENDMENTS TO THE SWPPP:	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
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8. POTENTIAL AREAS OF FUTURE CONCERN:	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
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9. ADDITIONAL COMMENTS:	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
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DISCLOSURES:
• AFTER DISCOVERY, THE PERMIT REQUIRES MANY OF THE DEFICIENCIES THAT MAY BE FOUND ON SITE BE CORRECTED WITHIN A SPECIFIED PERIOD OF TIME. SEE PERMIT FOR MORE DETAILS.
• THE PERMITTEE(S) IS/ARE RESPONSIBLE FOR THE INSPECTION AND MAINTENANCE OF TEMPORARY AND PERMANENT WATER QUALITY MANAGEMENT BMPs AS WELL AS EROSION PREVENTION AND SEDIMENT CONTROL BMPs UNTIL ANOTHER PERMITTEE HAS OBTAINED COVERAGE UNDER THE PERMIT ACCORDING TO SECTION 1, OR THE PROJECT HAS MET THE TERMINATION CONDITIONS OF THE PERMIT AND A NOTICE OF TERMINATION HAS BEEN SUBMITTED TO THE MINNESOTA POLLUTION CONTROL AGENCY.

ATTACHMENT C: MAINTENANCE PLAN FOR PERMANENT STORM WATER TREATMENT SYSTEM

ATTACHMENT C: MAINTENANCE
FACILITY MANAGEMENT SCHEDULE

1. ALL STORMWATER RETENTION, DETENTION, AND TREATMENT BASINS MUST BE INSPECTED AT LEAST ONCE A YEAR TO DETERMINE THAT BASIN RETENTION AND TREATMENT CHARACTERISTICS ARE ADEQUATE. A STORAGE TREATMENT BASIN WILL BE CONSIDERED INADEQUATE IF SEDIMENT HAS DECREASED THE NET STORAGE VOLUME BY 50 PERCENT OR DRY STORAGE VOLUME BY 25 PERCENT OF ITS ORIGINAL DESIGN VOLUME. BASED ON THIS INSPECTION, IF A STORMWATER BASIN REQUIRES SEDIMENT CLEANOUT, THE BASIN WILL BE RESTORED TO ITS ORIGINAL DESIGN CAPACITY AND VEGETATED STATE WITHIN ONE YEAR OF THE INSPECTION DATE.
2. ALL OUTLET STRUCTURES, CULVERTS, CAPITAL STRUCTURES AND OTHER STORMWATER FACILITIES FOR WHICH MAINTENANCE REQUIREMENTS ARE NOT OTHERWISE SPECIFIED HEREIN MUST BE INSPECTED DURING THE SPRING, SUMMER, AND FALL OF EACH YEAR. WITHIN 30 DAYS OF THE INSPECTION DATE, ALL ACCUMULATED SEDIMENT AND DEBRIS MUST BE REMOVED SUCH THAT EACH STORMWATER FACILITY OPERATES AS DESIGNED AND INTENDED. CONTRIBUTING DRAINAGE AREAS MUST BE KEPT CLEAR OF LITTER AND VEGETATIVE DEBRIS, AND LOW SPES AND OVERFLOW OR LWAYS KEPT CLEAR. BUILT AREAS KEPT CLEAN, AND UNDESIRABLE VEGETATION REMOVED. DEBRIS REMOVED, THE FUNCTION OR INTEGRITY OF THE FACILITIES, IF ANY, WILL BE CORRECTED, AND ANY STRUCTURAL DAMAGE REPAIRING OR THREATENING TO IMPAIR THE FUNCTION OF THE FACILITIES MUST BE REPAIRED.
3. VOLUME CONTROL FACILITIES AND CONTRIBUTING DRAINAGE AREAS MUST BE INSPECTED EVERY THREE MONTHS DURING THE OPERATIONAL PERIOD BETWEEN SPRING, SUMMER, AND FALL. SHORT-TERM FLOODING MUST BE MONITORED AFTER RAINFALL EVENTS OF 1 INCH OR MORE TO ENSURE THAT THE CONTRIBUTING DRAINAGE AREA IS CLEAR OF LITTER AND DEBRIS, AND LOW SPES AND OVERFLOW OR LWAYS ARE CLEAR. INLET AREAS ARE CLEAR, UNDESIRABLE VEGETATION IS REMOVED, AND THERE IS NO EVIDENCE OF IMPAIRED OR THREATENING TO IMPAIR THE FUNCTION OF A FACILITY. IF SEDIMENT HAS ACCUMULATED IN A DISTRIBUTION FEATURE, WITHIN 30 DAYS OF INSPECTION DEPOSITED SEDIMENTS MUST BE REMOVED. THE DISTRIBUTION CAPACITY OF THE UNDERLYING SOIL MUST BE RESTORED, AND ANY SURFACE DISTURBANCE MUST BE STABILIZED. INSPECTION MUST ENSURE THAT SEDIMENT TRAPS AND FOREBAYS ARE TRAPPING SEDIMENT AND THAT MORE THAN 50 PERCENT OF THE STORAGE VOLUME REMAINS. THE CONTRIBUTING DRAINAGE AREA IS STABLE, ALL NO BROKEN OR OBSTRUCTED, AND DEETS AND OUTLET (OVERFLOW OR LWAYS ARE IN GOOD CONDITION WITH NO DEBRIS). MAINTENANCE TECHNIQUES USED MUST PROTECT THE DISTRIBUTION CAPACITY OF THE PRACTICE BY LIMITING SOIL COMPACTION TO THE GREATEST EXTENT POSSIBLE (E.G., BY USING LOW-IMPACT EARTH-MOVING EQUIPMENT).

CivilSite

4000 Hennepin Avenue
Golden Valley, MN 55427
612.447.4400

PRELIMINARY:
NOT FOR
CONSTRUCTION

7200 & 7250 FRANCE
7200 & 7250 FRANCE AVE S. EDINA, MN 55435
ORION INVESTMENTS
6950 YORK AVE S SUITE 207, EDINA, MN 55435

DESIGNED BY: J. J. Kunkle
CIVIL SITE
DATE: 10/10/2022
PROJECT NUMBER: 1243

ISSUES/SUBMITTAL SUMMARY
DATE DESCRIPTION
BY: J. J. Kunkle

REVISION SUMMARY
DATE DESCRIPTION
BY: J. J. Kunkle

DRAWN BY: J. J. Kunkle
PROJECT NUMBER: 1243

REVISION SUMMARY
DATE DESCRIPTION
BY: J. J. Kunkle

SWPPP - ATTACHMENTS

SW1.6



7200-7250

7200-7250 France Avenue
South, Edina, MN

CONFLUENCE

LANDSCAPE ARCHITECT
500 N THIRD ST. SUITE 120
MINNEAPOLIS, MN 55402
(763) 835-3742 FAX (763) 835-288-4397
www.confluencemn.com

esc
ARCHITECTURE & DESIGN

505 Washington Avenue South, Suite 1000
Minneapolis, MN 55408
(612) 291-5081 / (612) 291-5382
www.escaustin.com

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report was prepared by me or under my direct
supervision and that I am a duly licensed architect
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Signature _____

Typed or Printed Name _____

License # _____ Date _____

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P.U.D.
SUBMISSION
08/12/2022

ORIGINAL ISSUE:

REVISIONS:

1.000 - Check & Update - 11/10/22

22113

PROJECT NUMBER

JC

DESIGNED BY

REV IN JAB

TM

OWNERS BY

LANDSCAPE SITE PLAN
RENDERED "PHASE 1B"

L1.1B

8/10/2022 11:00:00 AM

01
L1.1

LANDSCAPE SITE PLAN RENDERED

SCALE 1"=10'

1"=10'





7200-7250

7200-7250 France Avenue
South, Edina, MN

CONFLUENCE

LANDSCAPE ARCHITECT
550 N THIRD ST, SUITE 120
MINNEAPOLIS, MN 55402
(763) 435-3742 FAX (763) 435-4000
www.confluencemn.com

esc
ARCHITECTURE & DESIGN

555 Washington Avenue South, Suite 1000
Minneapolis, MN 55408
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SUBMISSION
08/12/2022

ORIGINAL ISSUE:

REVISIONS:
No. Description Date

22113

PROJECT NUMBER

IC

DESIGNED BY

TM

CHECKED BY

REV IN JAN

LANDSCAPE SITE PLAN
RENDERED "PHASE 2"

L1.2

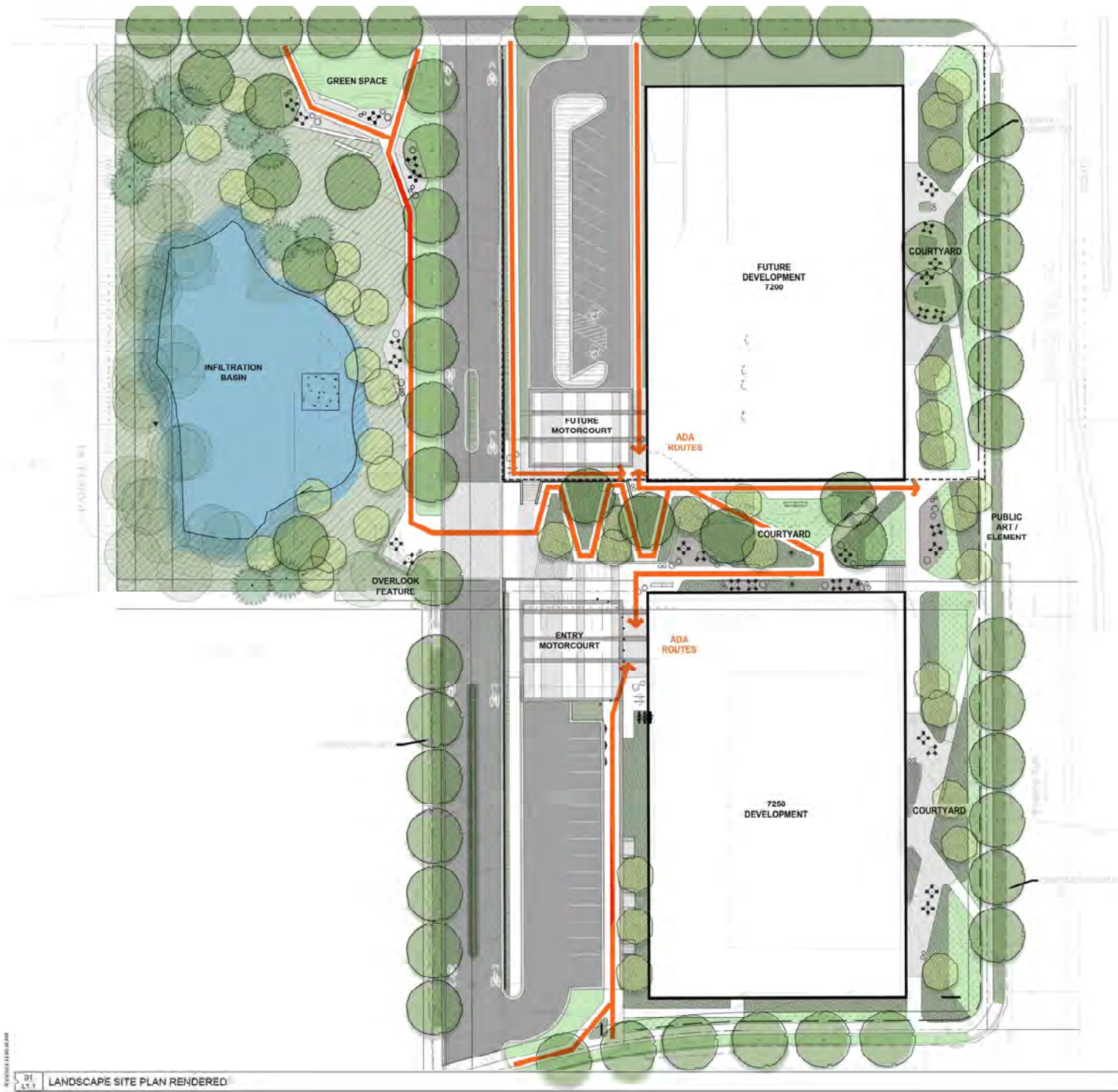
RENDERED 08/12/2022

B1
L1.2

LANDSCAPE SITE PLAN RENDERED

SCALE: 1"=10'





7200-7250

7200-7250 France Avenue
South, Edina, MN

CONFLUENCE

LANDSCAPE ARCHITECT
550 N THIRD ST, SUITE 120
MINNEAPOLIS, MN 55402
(763) 420-3742 FAX (763) 420-4000
www.confluencemn.com

esc
ARCHITECTURE & DESIGN

555 Washington Avenue South, Suite 1000
Minneapolis, MN 55408
(612) 291-5084 / (612) 291-5382
www.escdesign.com

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Typed or Printed Name

License # Date

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SUBMISSION
08/12/2022

ORIGINAL ISSUE:

REVISIONS:
No. Description Date

22113

PROJECT NUMBER

IC

DESIGNED BY

KEY PLAN

TM

CONSTRUCTED BY

DATE

LANDSCAPE SITE PLAN
RENDERED

L1.3

REVISED 11/08/2024

01

LANDSCAPE SITE PLAN RENDERED

SCALE: 1"=10'





7200 & 7250 FRANCE AVENUE - OVERALL EXISTING SITE AERIALS



7200 FRANCE AVENUE - EXISTING SITE AERIALS & PHOTOS



7250 FRANCE AVENUE - EXISTING SITE AERIALS & PHOTOS

**7200 & 7250
FRANCE AVE**
7200 & 7250 FRANCE AVE, EDINA,
MN 55435



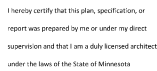
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Typed or Printed Name _____
License # _____ Date _____

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P.U.D. SUBMISSION 8/12/2022		
ORIGINAL ISSUE: 08/09/22		
REVISIONS:		
No.	Description	Date
222702 PROJECT NUMBER		
ESG DRAWN BY	ESG CHECKED BY	
KEY PLAN		
EXISTING SITE PHOTOS (7200 & 7250)		
A0.1		

7200 & 7250 FRANCE AVE, EDINA,
MN 55435



Signature _____

Typed or Printed Name _____

License # _____ Date _____

NOT FOR CONSTRUCTION

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SUBMISSION
8/12/2022

06/30/22

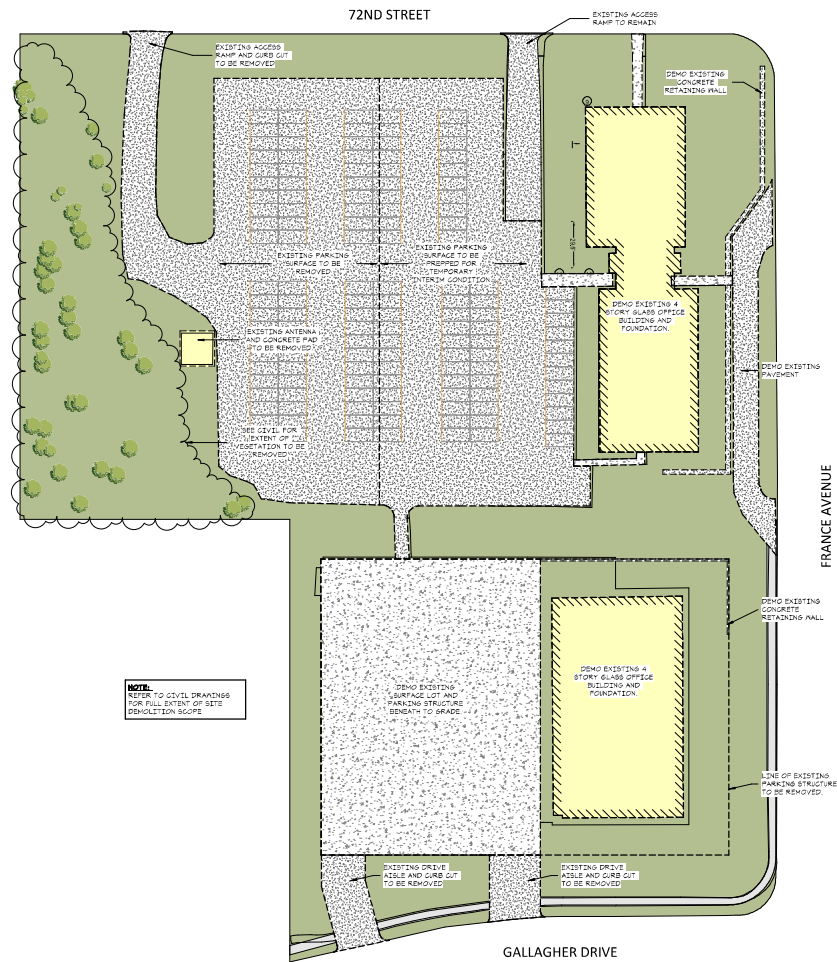
No.	Description	Date
1	---	TBD

PROJECT NUMBER _____

DRAWN BY _____ CHECKED BY _____

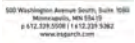
KEY PLAN

A0.2



NOTE:
REFER TO CIVIL DRAWINGS
FOR FULL EXTENT OF SITE
DEMOLITION SCOPE

7200 & 7250 FRANCE AVE, EDINA,
MN 55435



Signature _____

Typed or Printed Name _____

cense #	Date
---------	------

1 **LEVEL 1 SITE PLAN - PHASE 1B**
AQ.3B 1" = 30'-0"

ORIGINAL ISSUE:

DEFINITIONS:

REVISIONS:

No.	Description	Date
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222702

ESG _____
DRAWN BY

ESG _____
CHECKED BY

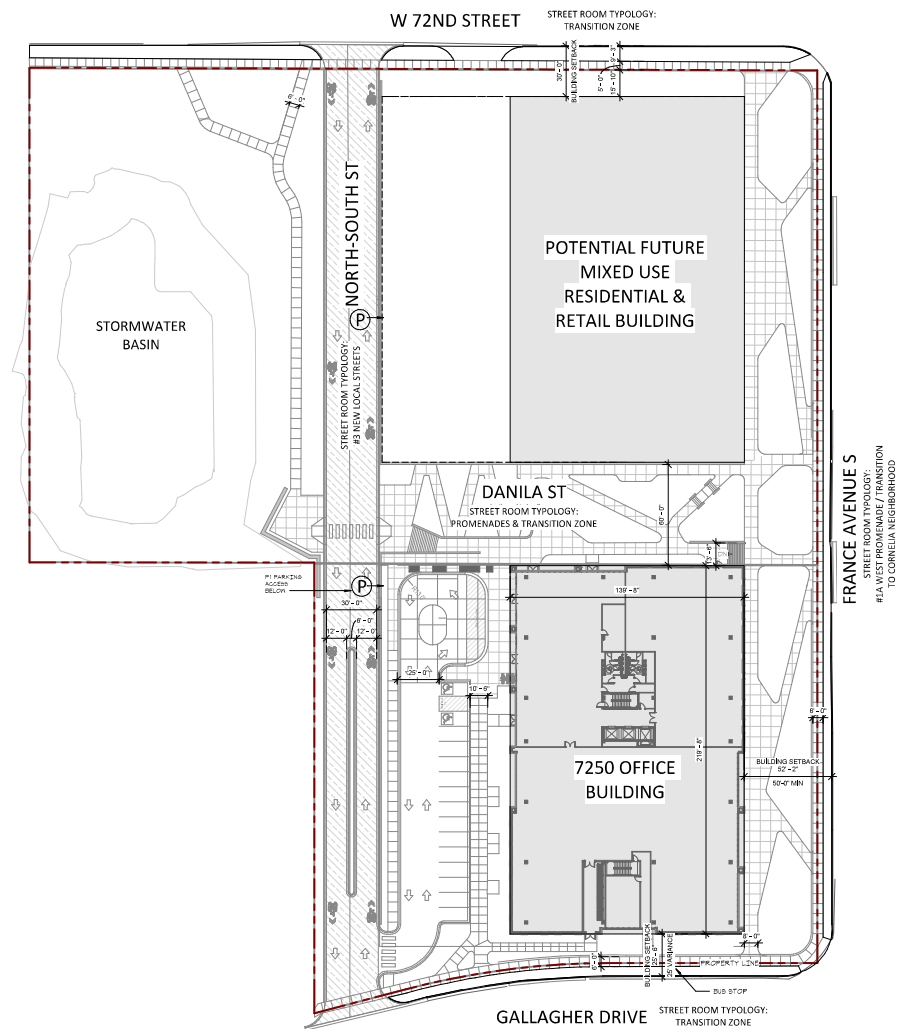
KEY PLAN

A0.3B

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Typed or Printed Name _____
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LEVEL 1 SITE PLAN - PHASE 2
1" = 30'-0"

**P.U.D.
SUBMISSION
8/12/2022**

ORIGINAL ISSUE:
08/01/22
REVISIONS:
No. Description Date

222702
PROJECT NUMBER
ESG ESG
DRAWN BY CHECKED BY

KEY PLAN

SITE PLAN - PHASE 2
(7200 & 7250)

A0.4

esg
ARCHITECTURE & DESIGN

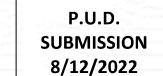
500 Washington Avenue South, Suite 1000
Minneapolis, MN 55415
p 612.329.5508 / f 612.329.5262
www.esgarch.com

Signature _____

Typed or Printed Name _____

License # _____ Date _____

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ORIGINAL ISSUE:
06/30/22

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No.	Description	Date
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222702
PROJECT NUMBER

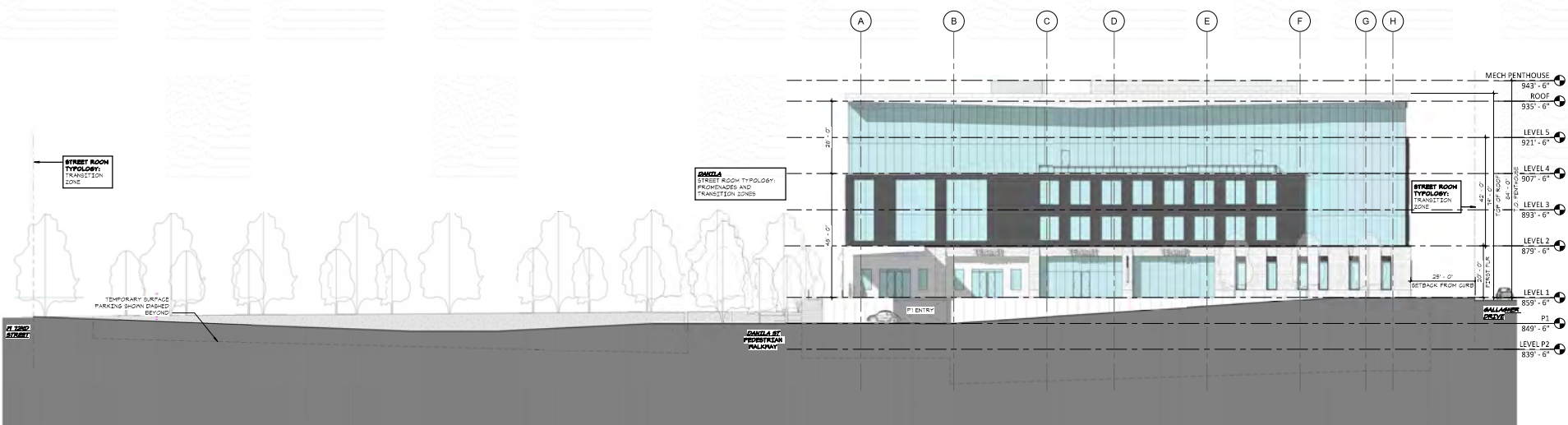
ESG
DRAWN BY

ESG
CHECKED BY

KEY PLAN

SITE SECTION DIAGRAMS
- PHASE 1 (7200 & 7250)

A0.5



1 PHASE 1 - NORTH / SOUTH SITE SECTION
30.5 1/16" = 1'-0"



2 PHASE 1 - EAST / WEST SITE SECTION AT DANILA
A0.5 1/16" = 1'-0"

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SUBMISSION
8/12/2022**

ORIGINAL ISSUE:
08/09/22

REVISIONS:
No. Description Date

222702
PROJECT NUMBER

ESG ESG
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KEY PLAN

SITE SECTION DIAGRAMS
- PHASE 2 (7200 & 7250)

A0.6



1
A0.6
PHASE 2 - NORTH / SOUTH SITE SECTION
1/16" = 1'-0"



PHASE 1A SITE RENDERING - ASSUMED TEMPORARY GREEN SPACE



PHASE 1B SITE RENDERING - TEMPORARY SURFACE PARKING CONDITION



PHASE 2 - SITE RENDERING W/ POTENTIAL BUILDING @ 7250

**P.U.D.
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8/12/2022**

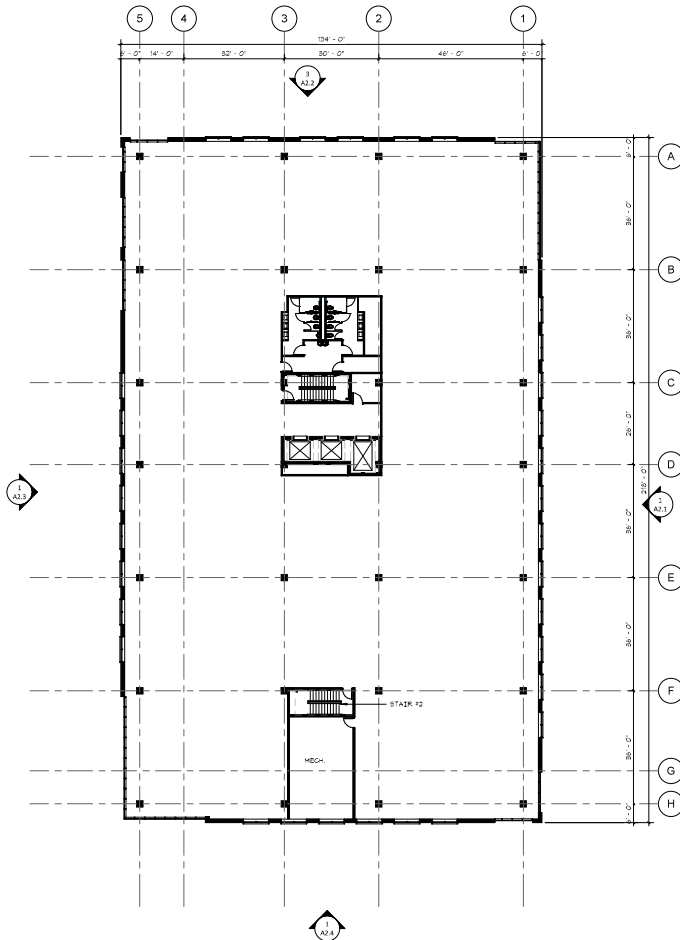
ORIGINAL ISSUE:
08/26/22
REVISIONS:
No. Description Date

222702
PROJECT NUMBER
ESG ESG
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KEY PLAN

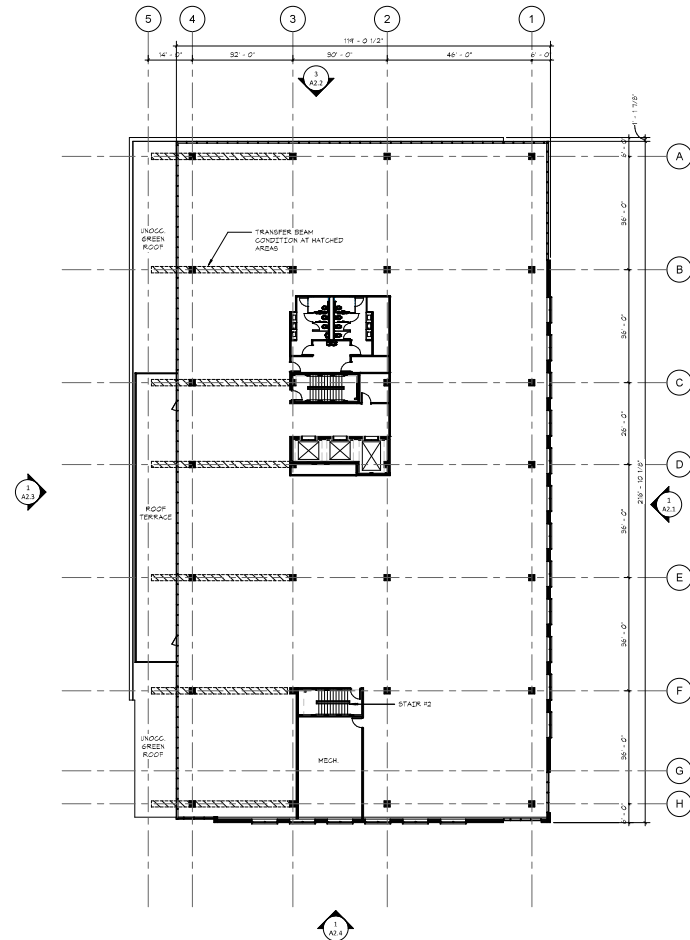
PHASE 1 AND 2 SITE
RENDERINGS
A0.7

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P.U.D. SUBMISSION 8/12/2022	
ORIGINAL ISSUE: 08/09/22	
REVISIONS: No. Description Date	
222702 PROJECT NUMBER	
ESG DRAWN BY	ESG CHECKED BY
KEY PLAN	
7250 FLOOR PLANS	
A1.3	



1 LEVEL 3 FLOOR PLAN
A1.3 1/16" = 1'-0"



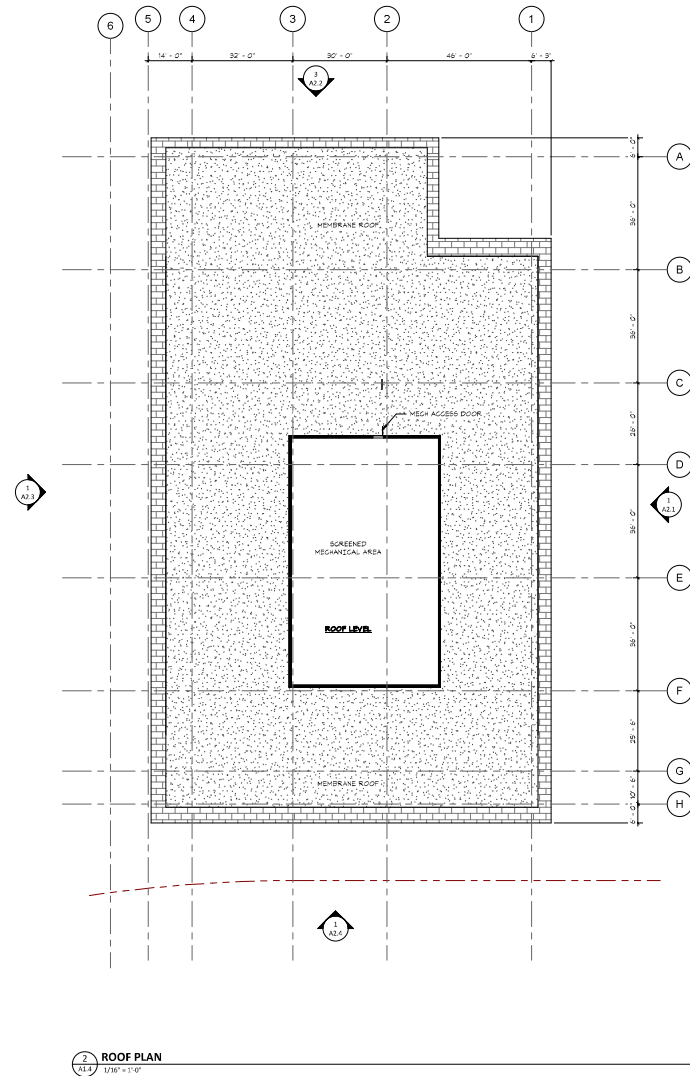
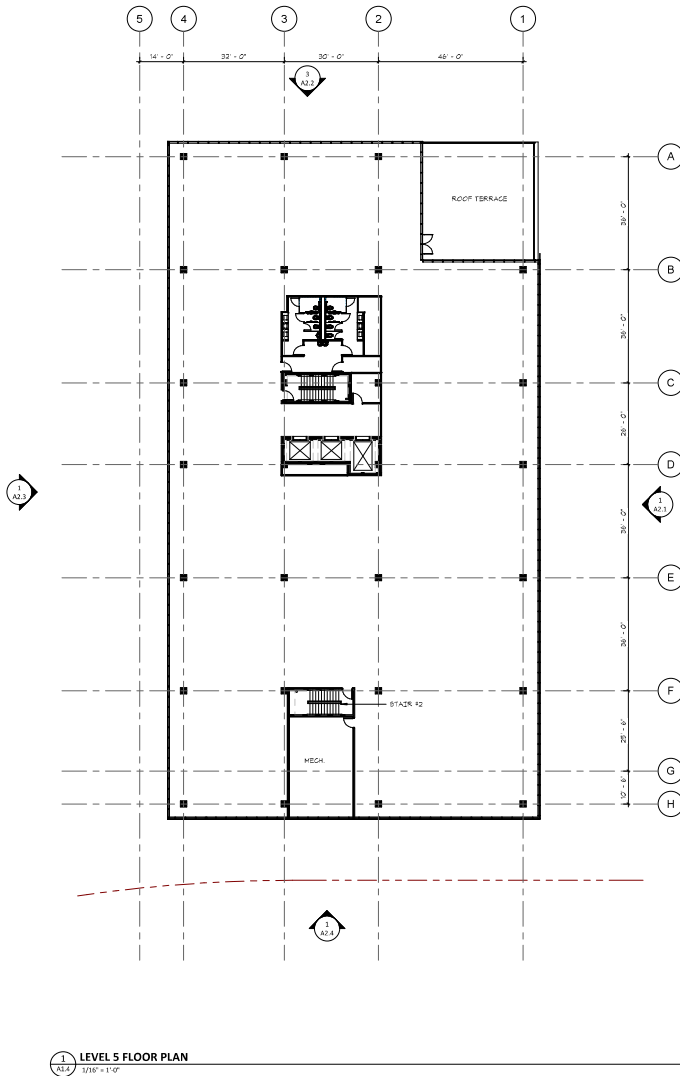
2 LEVEL 4 FLOOR PLAN
A1.3 1/16" = 1'-0"

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ORIGINAL ISSUE: 08/02/22		
REVISIONS: No. Description Date		
222702 PROJECT NUMBER		
ESG DRAWN BY	ESG CHECKED BY	
KEY PLAN		
7250 FLOOR PLANS		
A1.4		



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SUBMISSION
8/12/2022**

ORIGINAL ISSUE:
08/02/22
REVISIONS:
No. Description Date

222702
PROJECT NUMBER
ESG DRAWN BY ESG
CHECKED BY
KEY PLAN

7250 EXTERIOR
ELEVATION
A2.2

MATERIAL COUNTS - 7250 FRANCE									
	NORTH		EAST		SOUTH		WEST		TOTAL
GLAZED WALL SYSTEM / PUNCHED WINDOW	6,273	53%	9,634	51%	5,441	46%	11,263	60%	32,633 53.2%
METAL PANEL	1,679	14%	1,863	10%	1,616	14%	2,843	15%	8,072 13.2%
ARCHITECTURAL STONE / BRICK	3,869	33%	7,501	39%	4,609	39%	4,679	25%	20,658 33.7%
TOTAL SF	11,811	100%	19,007	100%	11,724	100%	18,786	100%	61,348 100.00%



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CONSTRUCTION

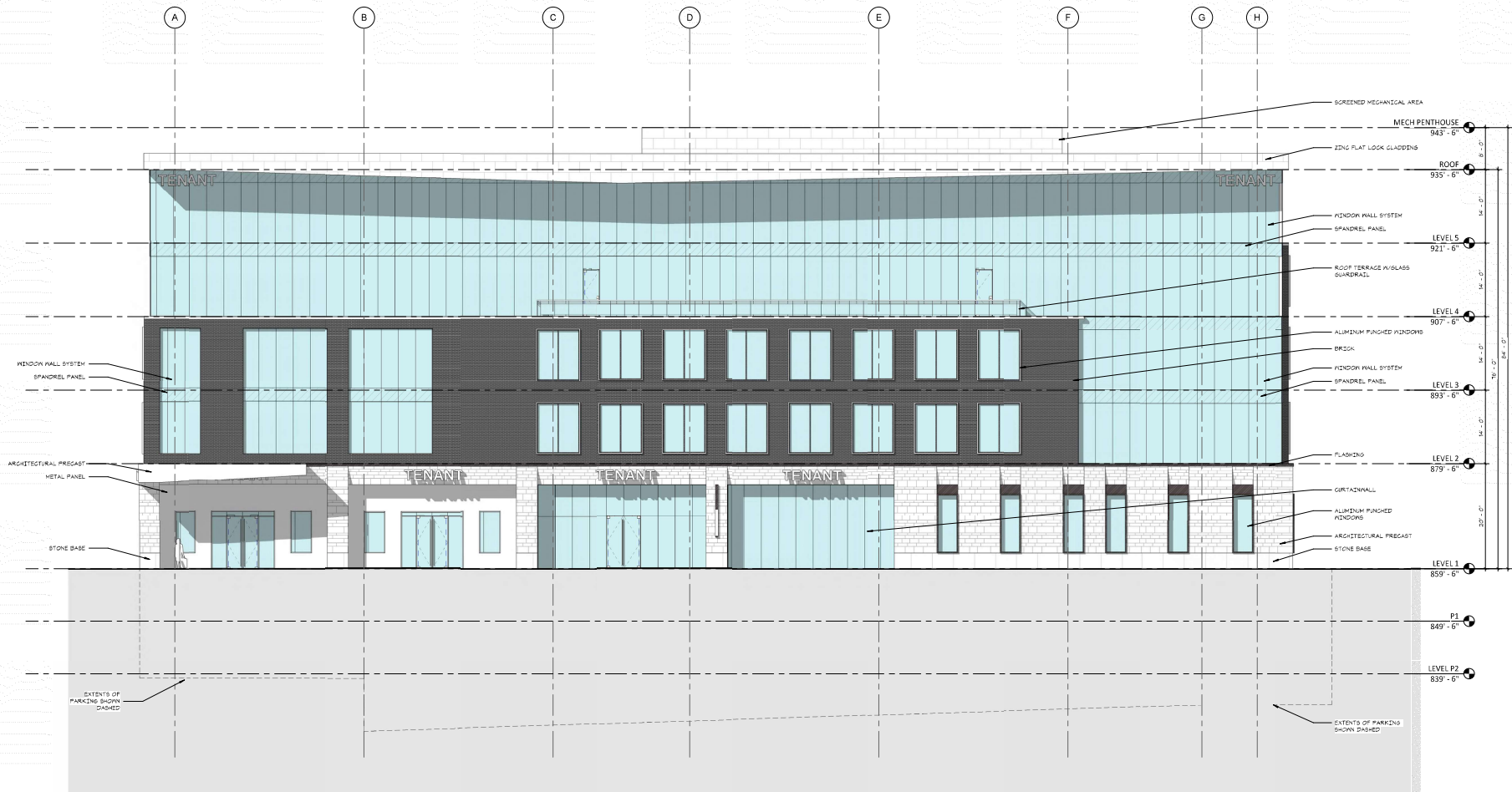
P.U.D.
SUBMISSION
8/12/2022

ORIGINAL ISSUE:
08/08/22
REVISIONS:
No. Description Date

222702
PROJECT NUMBER
ESG DRAWN BY ESG
CHECKED BY
KEY PLAN

7250 EXTERIOR
ELEVATION
A2.3

MATERIAL COUNTS - 7250 FRANCE						
	NORTH	EAST	SOUTH	WEST	TOTAL	
GLAZED WALL SYSTEM / PUNCHED WINDOW	6,272	538	9,654	521	5,442	86%
METAL PANEL	1,174	14%	1,853	10%	1,678	14%
ARCHITECTURAL STONE / BRICK	3,869	31%	2,502	13%	4,609	39%
TOTAL SF	11,816	100%	18,037	100%	11,724	100%



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SUBMISSION
8/12/2022**

ORIGINAL ISSUE:

06/06/22

REVISIONS:

No. Description Date

222702

PROJECT NUMBER

ESG

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KEY PLAN

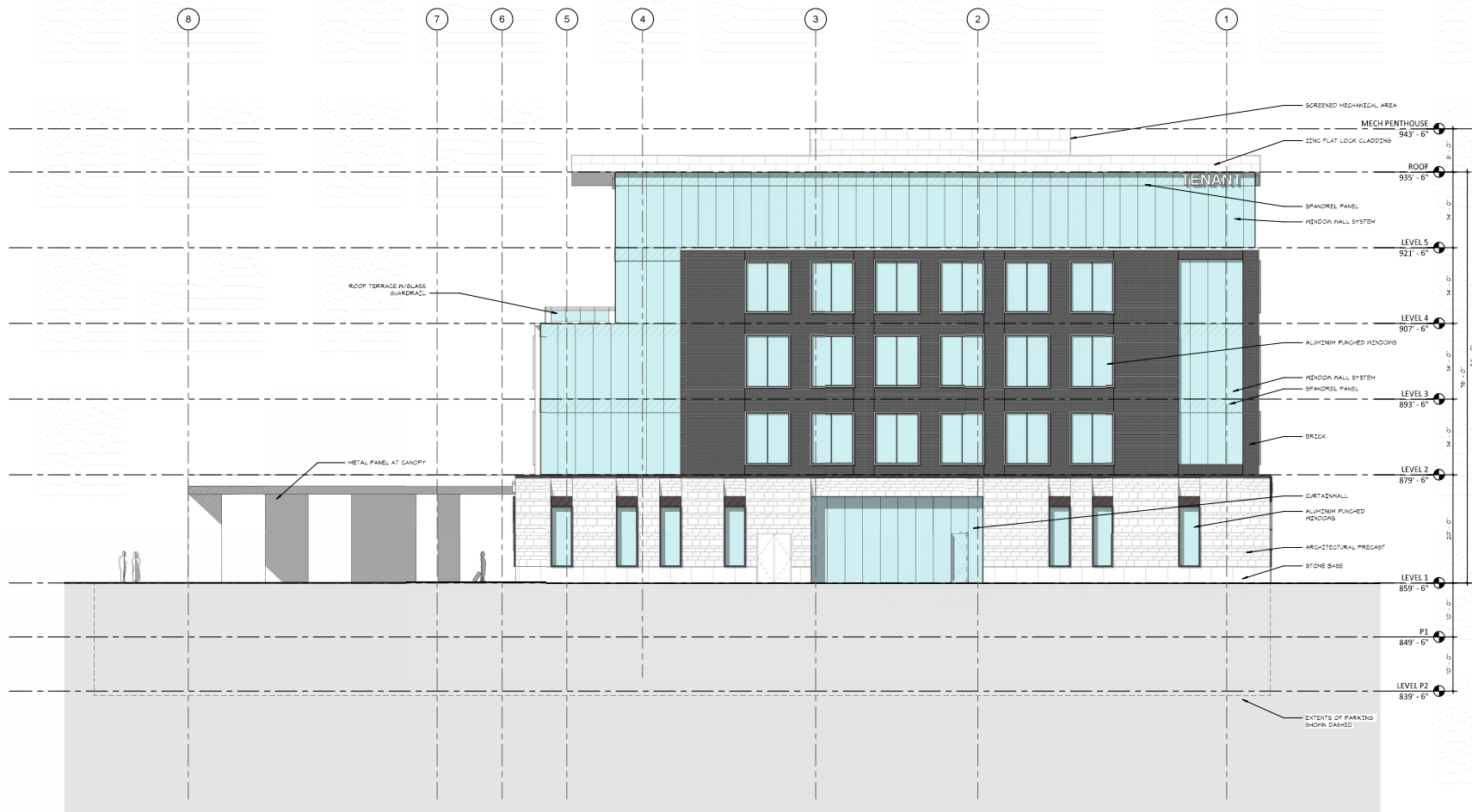
ESG

CHECKED BY

7250 EXTERIOR
ELEVATION

A2.4

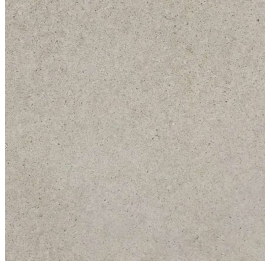
MATERIAL COUNTS - 7250 FRANCE						
	NORTH	EAST	SOUTH	WEST	TOTAL	
GLAZED WALL SYSTEM / PUNCHED WINDOW	6,272	539	9,654	501	5,441	86%
METAL PANEL	1,174	144	1,853	104	1,676	14%
ARCHITECTURAL STONE / BRICK	3,869	316	2,502	394	4,609	39%
TOTAL SF	11,816	100%	18,037	100%	11,724	100%
					18,788	100%
					61,363	100.00%



LEVEL 1 MATERIALS



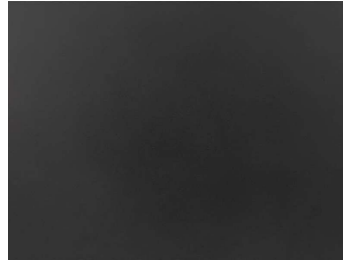
HYBRID WINDOW
WALL - SSG SYSTEM



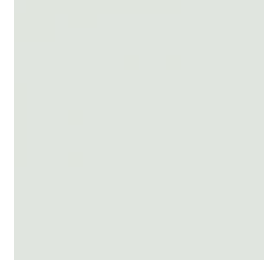
ARCHITECTURAL
PRECAST - COLOR 1



STONE BASE



METAL PANEL - COLOR 1



METAL PANEL - COLOR 2

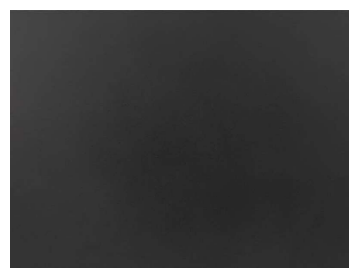
LEVELS 2-5 MATERIALS



HYBRID WINDOW
WALL - SSG SYSTEM



ARCHITECTURAL BRICK



METAL PANEL - COLOR 1

ROOF MATERIALS



FLAT LOCK ZINC PANEL

**7200 & 7250
FRANCE AVE**
7200 & 7250 FRANCE AVE, EDINA,
MN 55435

esg
ARCHITECTURE & DESIGN

500 Washington Avenue South, Suite 1000
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SUBMISSION
8/12/2022**

ORIGINAL ISSUE:
08/02/22

REVISIONS:
No. Description Date

222702
PROJECT NUMBER

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KEY PLAN

7250 EXTERIOR
MATERIALS

A2.5

Survey Responses

Public Hearing Comments-7200 and 7250
France

Better Together Edina

Project: Public Hearing: 7200 and 7250 France Avenue



VISITORS					
30					
CONTRIBUTORS			RESPONSES		
5			5		
0	0	5	0	0	5
Registered	Unverified	Anonymous	Registered	Unverified	Anonymous



Respondent No: 1

Login: Anonymous

Email: n/a

Responded At: Aug 22, 2022 14:42:55 pm

Last Seen: Aug 22, 2022 14:42:55 pm

IP Address: n/a

Q1. **First and Last Name**

Melinda Emerson

Q2. **Address**

7124 Heatherton Trl

Q3. **Comment**

I don't understand why they are putting surface area parking at 7200 if it won't be there long term and there is ramp parking available. We should be avoiding traffic on 72nd. Make it a wild flower garden for now.



Respondent No: 2

Login: Anonymous

Email: n/a

Responded At: Aug 29, 2022 09:00:38 am

Last Seen: Aug 29, 2022 09:00:38 am

IP Address: n/a

Q1. **First and Last Name**

robert and linda carlson

Q2. **Address**

5250 grandview sq. unit 2105 edina mn 55436

Q3. **Comment**

This proposal is a great use of the property -. It provides much needed residential and office needs in this location.. It is proposed by a quality developer who has recently completed other successful projects within the City. He has proven his word is his bond. His developments have todays look and quality above the norm. Suggest this be expedited in a timely manner. Consternation of any kind should be limited. Thanks for the opportunity to respond. Live in Edina for 12 years.



Respondent No: 3

Login: Anonymous

Email: n/a

Responded At: Sep 01, 2022 06:57:40 am

Last Seen: Sep 01, 2022 06:57:40 am

IP Address: n/a

Q1. **First and Last Name**

Linda Carlson

Q2. **Address**

5250 Grandview Square, #2105, Edina. 55436

Q3. **Comment**

I think this proposal is perfect for that location. The developer has an excellent history in our wonderful city and can be relied upon to do an outstanding job on a timely basis. We would hope the city grants approval without much consternation. The sooner that building is completed the better.



Respondent No: 4

Login: Anonymous

Email: n/a

Responded At: Sep 12, 2022 09:00:02 am

Last Seen: Sep 12, 2022 09:00:02 am

IP Address: n/a

Q1. **First and Last Name**

Lorenzo Bassi

Q2. **Address**

6337 Mildred Avenue, Edina, MN 55439

Q3. **Comment**

This looks like a great project, and one that would help the area thrive. I believe the idea of having both an office building and a quality apartment/retail complex in that space is a very good one. I look forward to seeing this project come to life.
Regards, Lorenzo Bassi



Respondent No: 5

Login: Anonymous

Email: n/a

Responded At: Sep 12, 2022 11:30:26 am

Last Seen: Sep 12, 2022 11:30:26 am

IP Address: n/a

Q1. **First and Last Name**

Angela Chapman

Q2. **Address**

5509 Parkwood lane

Q3. **Comment**

I am very excited to hear more about the building project at 7200 and 7250 France Ave EDINA. From what I have seen so far I think it will be an amazing improvement for that area! I am definitely for job creation and supporting ways to improve EDINA. I cant wait for the new buildings!



7200 - 7250 France Avenue

Edina, MN

December 4, 2018

17-124.01

DJR
ARCHITECTURE INC.



Height Diagram - Current

7200 - 7250 France Avenue

Edina, MN

Height Diagram Comparison

17-124.01

December 4, 2018



CITY COUNCIL MEETING, DECEMBER 4TH

- 6th Floor @ 67'-5"
- Full step back on 6th floor
- Stone on street level
- Brick added on upper levels

7200 - 7250 France Avenue

Edina, MN

North East View

17-124.01

December 4, 2018

DJR
ARCHITECTURE INC.



CITY COUNCIL MEETING, DECEMBER 4TH

- 6th Floor @ 67'-5"
- Full step back on 6th floor
- Stone on street level
- Brick added on upper levels

7200 - 7250 France Avenue

Edina, MN

South East View

17-124.01

December 4, 2018

DJR
ARCHITECTURE INC.



CITY COUNCIL MEETING, DECEMBER 4TH

- 6th Floor @ 67'-5"
- Full step back on 6th floor
- Stone on street level
- partial Brick on upper levels

7200 - 7250 France Avenue

Edina, MN

South West View

17-124.01

December 4, 2018

DJR
ARCHITECTURE INC.



CITY COUNCIL MEETING, DECEMBER 4TH

- 6th Floor @ 67'-5"
- Full step back on 6th floor
- Stone on street level
- Brick added on upper levels

7200 - 7250 France Avenue

Edina, MN

North West View

17-124.01

December 4, 2018



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South East View

17-124.01

December 4, 2018

DJR
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Greater Southdale District **Design Experience Guidelines**

March 5, 2019



Preface

What are Design Experience Guidelines?

A. Preface

What are Design Experience Guidelines?

For nearly four years, a Work Group comprised of Edina residents and business leaders contemplated the future evolution of the Greater Southdale District, using the notion of “experience” as the foundational element from which to shape the district over the next 50 years. The first phase of the Work Group’s efforts consisted of developing Working Principles that would guide the group’s efforts into subsequent stages as well as suggest a dialog for considering new introductions within the district. Phase Two translated these principles into an urban design framework plan that proposed enhanced connections across existing major streets, introduced a new block framework to encourage walkability, and posed design strategies to create an improved public realm and promote higher quality, pedestrian-oriented development.

The broader experience the urban design framework strives to achieve is the creation of a Greater Southdale District that connects seamlessly between the existing urban and residential fabric, maximizes the development potential of each site, enhances walkability and livability, and encourages interaction among current and future residents, businesses, and institutions. The urban design framework is the foundation to the Greater Southdale District Plan, a part of the City of Edina 2018 Comprehensive Plan. The Greater Southdale District Design Experience Guidelines is a companion to those documents, outlining high-level planning and design strategies for public realm, site, and building design supporting the desired experience outcomes.

The Design Experience Guidelines sets baseline guidance for developers, designers, City of Edina staff, Planning Commissioners and members of the City Council when proposing, designing or evaluating proposed introductions within the Greater Southdale District.

Use of the Design Experience Guidelines

The Design Experience Guidelines apply to proposed development within the Greater Southdale District and former Pentagon Park, and should be referred to when embarking on new development or redevelopment. The Design Experience Guidelines also offer direction for new public realm features and the reimagining of existing public infrastructure. The document does not stand alone; it must be considered as part of a larger set of district goals, urban design framework and patterns, and policy guidance outlined in the Greater Southdale District Plan and the 2018 City of Edina Comprehensive Plan. Together, these documents set forth

City expectations for the future form of the Greater Southdale District, and inform all potential projects within the district.

The Design Experience Guidelines are not a substitute for City of Edina zoning codes and ordinances, but instead provide substantial background for developers and design teams and a framework from which to approach proposed projects within the district. The Design Experience Guidelines clarify the dimensional characteristics of eight different Street Room typologies that together form the experience within the district. The physical qualities of each street room's height and width, and shaped by the architecture of the district, informs the design of the public realm on streets that connect and bind the human experience of the district. These physical characteristics also shape the experience of transition – transitions from existing single-family neighborhoods and transitions into and out of the district, outlining gradual changes in building scale within these critical transition areas to bridge between one- and two-story residences and the greater intensity of the Greater Southdale District.

In conjunction with the Urban Design chapter of the Greater Southdale District Plan, this document provides both a philosophical and practical framework to facilitate discussion among the City, development teams, and the community when considering proposals for change within the district. During the recommended Sketch Plan review process (described on page 70 of the Greater Southdale District Plan), the Design Experience Guidelines are intended to facilitate dialog about broader district goals, patterns and connections, building massing, programmatic opportunities and shared public realm connectivity. Dialog at this point in the review of a proposal requires imagination, looking beyond the immediate site to imagine the creation of a consistently positive human experience, requiring a proposal to recognize the ways in which it influences that experience on adjacent and nearby sites. Once a proposal progresses beyond the Sketch Plan, the document is used as a test of outcomes and touchstone to measure how every proposal meets the desired district experience.

Further specific design details related to landscaping, curb and intersection design, stormwater management and daylighting, lighting standards, street furniture, and a host of other factors related to the experience of the district will be described in future versions of this or a similar document as the need for direction related to those features becomes better understood.

2

The Vision

Aspirations for the Greater Southdale District

2. The Vision

The Greater Southdale District Experience

Background and Context

Collectively, for nearly four years, the Greater Southdale Area Work Group has focused on building upon our community's history of innovation, engagement and community commitment to create a vision for the possibilities of the Greater Southdale District. This work has culminated in the Greater Southdale District Plan (adopted December 2018) and the Greater Southdale District Design Experience Guidelines.

Change is inevitable within the district. In the past five years alone, over 1,500 new housing units have been constructed or are underway, along with myriad other commercial projects. This is a tremendous opportunity in time, and in the broader evolution of our community. We can transform the physical environment of the Greater Southdale District from a traditional car-oriented suburban commercial district with its sea of surface parking lots and 'superblocks,' into a vibrant place whose character is neither urban nor suburban, but blends the best characteristics of both to create a place that is uniquely of Edina. This new model will support all modes of transportation (cars, buses, bikes, scooters, and pedestrians) and serve future generations of Edina residents, employers, and visitors. It will complement, not compete with, the single family neighborhoods that have historically defined much of the community's fabric.

Within the broader 750+ acre Greater Southdale District, and the former Pentagon Park, there exist a remarkable variety of assets. This plan connects those assets with a new street grid that overlays a human scale and allows access via a variety of transportation options. The plan sets forth a strategy to bridge between adjacent single family neighborhoods and the more commercially focused areas of the district. And, it uses public realm infrastructure—including parks, plazas, green streets, woonerfs, and waterways that manage stormwater—as the connective tissue that gives the district its unique identity. Together, these attributes will set the stage for a remarkable daily experience for those who live, work and play within the Greater Southdale District.

Aspirations for the Future Greater Southdale District

The Greater Southdale Work Group summarized the overall districtwide vision and land use strategies as part of the District Plan. These aspirations, which follow, are at the foundation of design policy parameters and the overall experience we are striving to shape as the Greater Southdale Area evolves over the next 50 years.

1. Imagine Greater Southdale District evolution organized around human activity, with vibrant pedestrian-focused streets, beautiful parks and public spaces, and endearing and enduring buildings where:

- A sense of invention is expected from new introductions, both public and private, that build on the district's spirit of innovation.
- Its role as regional and local center for living, shopping, working, learning, entertainment, hospitality, and medical services is enhanced.
- Other Edina neighborhoods, near and distant, benefit from investment in the district and the evolution of each parcel.
- Investment in the public realm is reflected by a commensurate investment as private parcels evolve.
- Public and civic services accommodate a growing and diverse district and community population.
- Transitions at the district's edges recognize compatible use and scale and neighboring uses are perpetuated on their terms.

2. Make the Greater Southdale District the model of healthy urban living where:

- The district's form encourages healthy living habits, particularly through walking.
- The design of buildings and spaces, both public and private, attract the widest possible range of the district's population.
- Storm water is a valued resource by making it part of the experience of the district.
- Emissions and pollutants are mitigated through the introduction of significant tree canopy and limiting idling vehicles on streets, creating a more inviting walking experience along the district's streets.
- Sustainable solutions result in a stock of healthy buildings that compel healthy activity for their occupants.
- Public features mitigate impacts of non-local infrastructure, especially to contain the ill effects of adjacent highways.

3. Invent sustainable infrastructure matching the district's sense of innovation where:

- Mutually-supportive and forward-looking infrastructure is the norm, looking beyond baseline utilitarian functions of a single site to create a broadly supportive district infrastructure.
- Infrastructure aligns with the creation of public space in the district, sharing space and resources that result in compelling, attractive and high-functioning civic spaces.
- Care for and perpetuation of public infrastructure anticipates daily human activity in all seasons.

4. Create neighborhoods of activity within the broader mixed-use patterns of the district where:

- Logical boundaries based on reasonable walking distances are established, with major streets as seams binding the activity of each side into an inviting and accessible public space.
- Focal points of public activity are found within each neighborhood.
- Key community services and facilities are present and help define the fabric of the District.
- Core services are delivered within each neighborhood or in an adjacent neighborhood.
- Neighborhoods are linked along street and park corridors highlighted by visible human activity.

5. Offer a spectrum of living opportunities integrated through the district where:

- Housing orients to a variety of income levels and household types.
- Ownership options constitute a significant portion of the living opportunities in the area.
- "Missing Middle" living opportunities (duplexes, triplexes, side-by-side or stacked townhouses, rowhouses with multiple units, and small buildings with four to six apartments) allow a broader range of Edina residents, workforce members and others to consider relocating to the District.
- Buildings for living strongly orient to the public spaces of each neighborhood within the District.

6. Expand significantly the number and extent of parks and public spaces where:

- Opportunities for the introduction of another large signature public space complement the programming and activities available at Centennial Lakes.
- An extension of the Promenade to Strachauer Park links neighbors and activity to the district.
- New promenades on the East and West edges of the District create movement corridors for pedestrians and bicyclists and serve as vital places for a transition between neighborhoods and the District.
- Parks and publicly accessible spaces are clearly visible and directly accessible from the public realm.
- Spaces for visible human activity and occupation, either public or publicly accessible, occur on every block.

7. Encourage district evolution based on incremental change and the creation of a great pedestrian experience where:

- A basic framework of streets and blocks encourages pedestrian activity and well-considered buildings.
- A rich variety of public or publicly accessible spaces are woven into the experience of the district.
- Sites and buildings support a pedestrian experience first, with storage of cars not a focus.
- Development on each site links to adjacent streets and to neighboring sites to create continuous, safe, and inviting pedestrian experiences.

8. Build (or plan for) a street network encouraging pedestrian movement across and through the district where:

- Walkable block lengths (200 feet) are the baseline framework for development.
- Enhanced and more frequent street crossings facilitates pedestrian movement.
- Wide landscaped boulevards encourage pedestrian activity and create a distinct district signature.
- Community corridors within and extending well beyond the district enhance bicycle and pedestrian access while accommodating vehicle traffic on pedestrian terms.

9. Imagine transportation in the district where:

- Cars are not the focus and streets accommodate more than vehicles.
- Major streets balance access and mobility.
- Some streets serve as community corridors, linking to other community destinations with features that allow for movement in addition to cars.
- Transit is a baseline service, both within the district and to non-Edina locations.
- Transportation recognizes trends, including autonomous vehicles and a time when parking structures aren't needed for public parking

10. Expect the delivery of high quality, well-designed buildings and sites where:

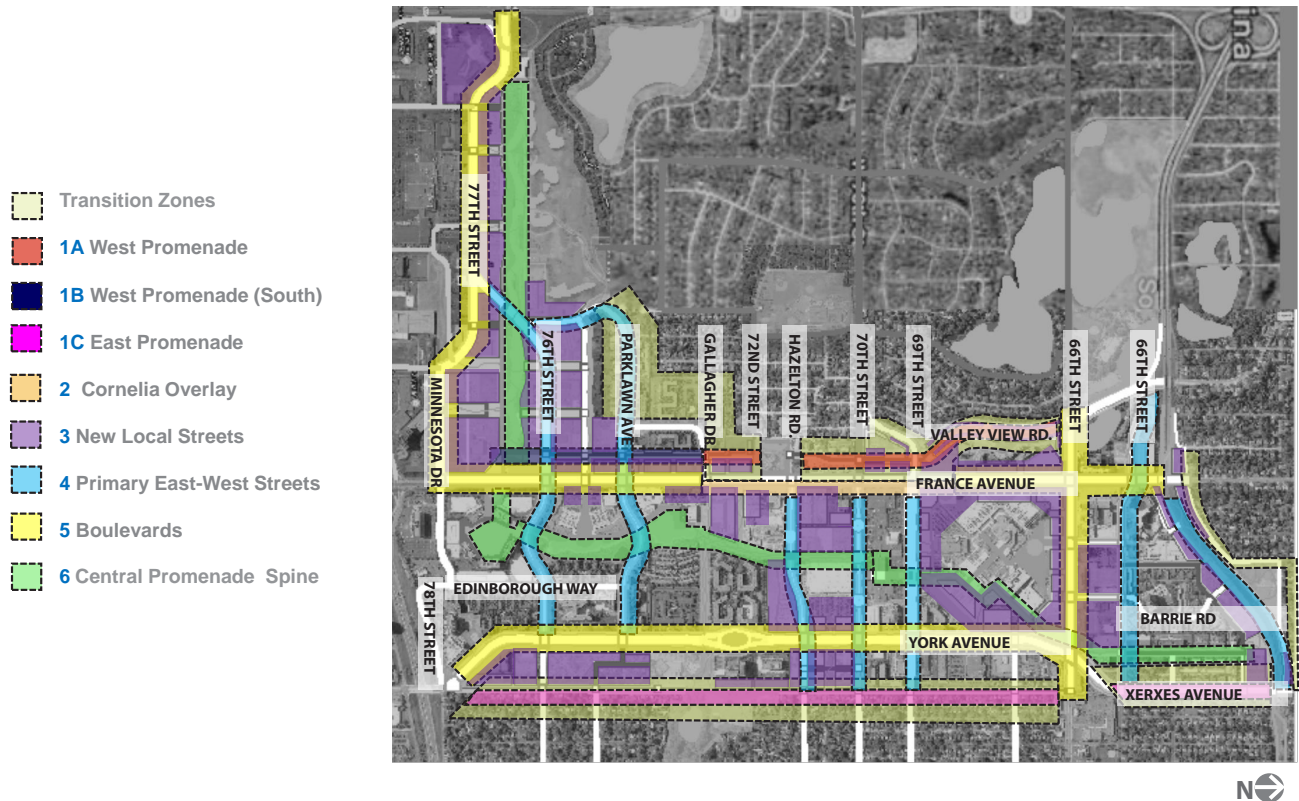
- Spaces on sites are considered for people first, including connections between sites; then the ways structures are placed; and then places to store cars are found.
- Visible human activity is prominent and integrated at every site.
- People are brought to the streets via major building entries oriented to major streets.
- Storm water remains visible as an amenity, allowing it to become a central part of the experience of each site.

11. Frame development guidance for evolution where:

- Development review includes the desired experience, not solely quantitative thresholds.
- Accommodation of adjacent and near parcels are considered in the evolution of a single parcel.
- Early reviews focus on ideas, patterns and relationships, not specific and engineered plans, with that part of the review process based in dialog, not presentation and reaction.
- Demonstrations of quality and especially quality from a long-term perspective are baseline considerations.
- Collaboration leads to a superior result, with the community's expectations clearly framed as part of the deliberation.
- Flexibility is not a right, but rather the natural by-product of a fair exchange for benefits, collaboration, and quality in development.

STREET ROOM TYPOLOGIES

A hierarchy of streets and pathways within the district is the framework for public realm development and related building form. Each street across the district has a role in how it serves pedestrians, bicycles and vehicles in connecting sub-districts, adjacent single family neighborhoods, and the overall Greater Southdale District and creating a unified sense of place. It is the intent that street typologies define the public realm experience: the space between buildings, dimensions of building setbacks from the street, heights of facades at the building face at the street and building step backs, where the façade of the building steps back from the volume of the street room.



Street Room Typology 1: Promenades and Transition Zones

Promenades are new woonerf-type streets on the west side of France Avenue and on the east side of York Avenue. Within this typology, there are several different variations for new building development in these important transition zones, responding to and respecting the context of adjacent single family neighborhoods.

Street Room Typology 2: Cornelia Overlay Zone

With the understanding that there is special sensitivity related to new building development near the Cornelia neighborhood, this is a special zone governing the design of the public realm/street room on the east side of France Avenue between the north side of 69th Street and Gallagher Drive. Buildings within this zone will be expected to maintain the east side of the France Avenue street room, but will be of a lower overall scale than new

Street Room Typology 1A

West Promenade / Transition to Cornelia Neighborhood

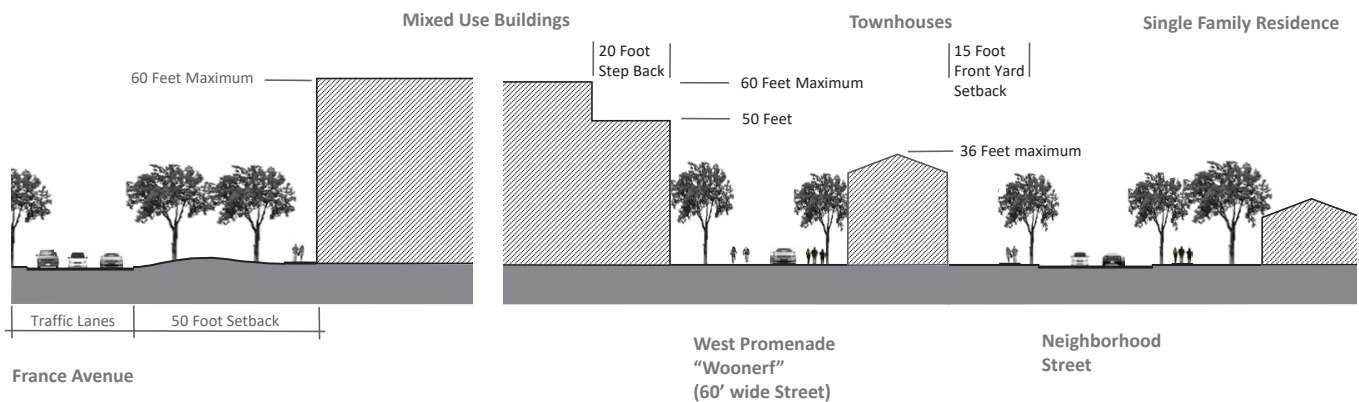


The sub-district to the west side of France between 69th and Parklawn is a unique transition zone within the Greater Southdale District. Any proposed developments within this zone should be approached with special sensitivity. This means that the street experience within that zone should be perceived as connected to the Cornelia neighborhood through landscaping and trees, and buildings that gradually transition in both height and function and use between lower intensity neighborhoods to the more commercially-focused district on the east side of France Avenue.

The West Promenade, a new north-south pedestrian, bike and vehicle street/woonerf that accommodates service access, is envisioned between France Avenue and the Cornelia neighborhood. This new shared street is intended to provide service access to new developments along France (keeping vehicular traffic out of single family neighborhoods), and providing a new framework to support the transition from townhouses and lower-scale residential buildings on the west side of the West Promenade, to slightly taller buildings on the east side of the West Promenade.

Dimensional Characteristics of Street Room Typology 1A

West Promenade / Transition to Cornelia Neighborhood



As illustrated in the section above, within this transition zone, building heights will step up incrementally, from those that are adjacent to single family homes to those that are facing France Avenue to provide a more gradual transition from the residential neighborhood to the more commercially-oriented Southdale District.

The street room experience within Typology 1A will be shaped by the following experience guidelines:

- New buildings that are adjacent to single family residential properties, on the west side of the West Promenade, should not exceed 36 feet in height. Townhomes are the preferred residential typology in this area of the transition zone.
- All ground level space east of the West Promenade should have 20-foot floor-to-floor height. This dimension allows for flexibility to accommodate one level of retail space along the street, or two-story townhomes facing the West Promenade.
- All parking, other than short-term retail or guest parking, and building services need to be located below grade or hidden within the building. If on ground level or above, parking and/or building services must be surrounded on all sides by program space such as commercial or housing.
- On the east side of the West Promenade, building faces should not exceed 50' in height. Any height above that limit should step back 20 feet from the facade of the building.
- On France Avenue, a 50 foot setback is required from curb to face of building with a maximum building height of 60 feet.
- On individual developments, should the City choose to permit height above the 60-foot height limit, it is recommended that additional height above 60 feet step back from the face of the building by a minimum of 10 feet in depth and 12 feet in height.

Street Room Typology 1B

West Promenade Between Parklawn and Minnesota Drive

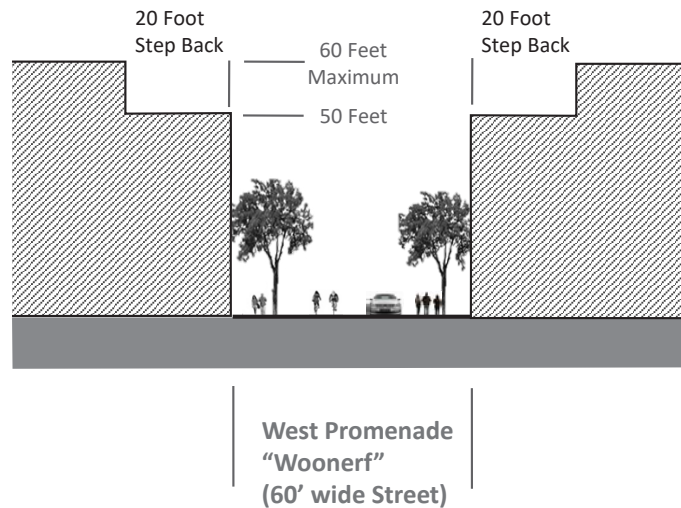


South of the zone designated as Street Room Typology 1A (where the Cornelia transition area immediately abuts single family neighborhoods), the Street Room experience shifts to respond to its changing context. It becomes one that is more commercial in nature and in concert with the expected higher intensity in the southwest quadrant the Greater Southdale District.

As the West Promenade extends south into Typology 1B, it continues to serve pedestrian, vehicular and service access. Uniform building heights on each side of the West Promenade are intended to support the transition from existing multi-family housing to taller buildings at the south end of France Avenue near the gateway from 494.

Dimensional Characteristics of Street Room Typology 1B

West Side of France Between Parklawn and Minnesota Drive



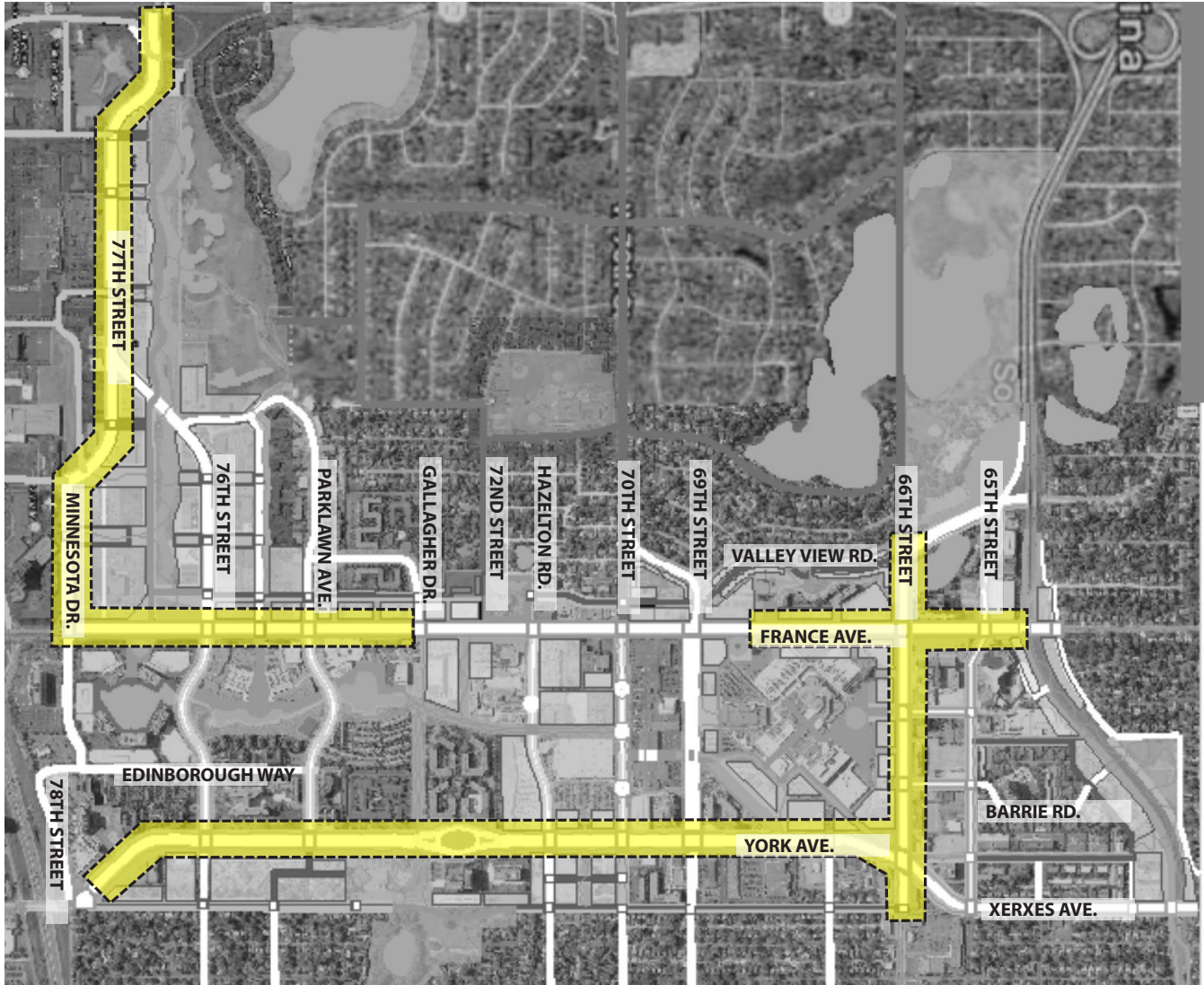
Within this zone, existing buildings are more commercial in nature and do not immediately abut single family neighborhoods. This unique condition lends itself to new development along the West Promenade that is still lower in scale, without the preference for townhomes or smaller scale buildings along one edge, as in Typology 1A.

The street room experience within Typology 1B will be shaped by the following experience guidelines:

- Building faces at the West Promenade within Typology 1B shall not exceed 50 feet in height. Any height above that limit should step back 20 feet from the facade of the building.
- All street level space shall be 20 feet, floor-to-floor in height. This dimension allows for flexibility for retail space (on France) and two-story townhomes facing the West Promenade.
- All parking, other than short-term retail or guest parking, and building services need to be located below grade or hidden within the building. If on ground level or above, parking and/or building services must be surrounded on all sides by program space such as commercial or housing.
- Building faces on the east side of the West Promenade are intended to provide continuity in scale and experience from Street Room Typology 1A and from one side of the street to another.

Street Room Typology 5

The Boulevards



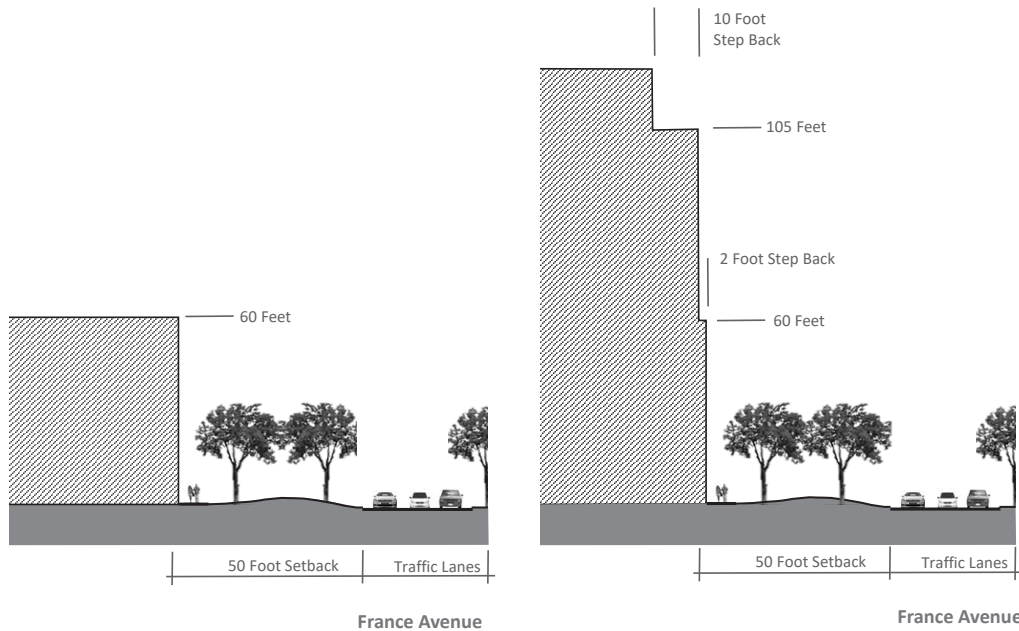
Streets that are included in this typology include the primary district boulevards such as France Avenue, York Avenue, W 66th Street and W 77th Street. In addition to being the widest streets in the district, they also currently carry a high volume of vehicular traffic. The intent of this typology is to create streets that connect the Greater Southdale District to the larger Edina community. These commercially-focused streets will reinforce the district's unique role in serving Edina's neighborhoods, while at the same time, recognizing that the district has a role in the broader metropolitan

region—providing employment, health, retail, entertainment, and a wide range of housing options.

The streets that fall into Typology 5 will have the greatest impact in conveying the overall identity of the district, with wide, multi-use streetscapes lined with a double row of trees within a consistent 50-foot setback. Medians may also be present in the boulevard streetscape to accommodate plantings and/or mass transit lines and stations. In many cases, boulevards will be adjacent to the tallest buildings in the district and will be the locations for transit stops.

Dimensional Characteristics of Street Room Typology 5

The Boulevards



On these wide streets, a sense of scale is maintained by creating a uniform street wall of 60 feet, with taller structures stepping back from this 60-foot datum. This consistency in building heights along the street will form the edge of the street room—bridging between lower intensity and transitional areas, and the higher intensity zones within the Greater Southdale District.

The street room experience within Typology 5 will be shaped by the following experience guidelines:

- On France Avenue, a 50 foot setback is required from curb to face of building with a minimum building height of 60 feet (diagram at left). Above that 60 foot height, the building face should step back two feet to create a cornice line, and can then extend to 105 feet. Above 105 feet, building faces must step back an additional 10 feet (as illustrated in diagram at right, above.)
- Building podiums along these streets need to maintain as closely as possible the 60-foot height limit while still adhering to the guidance of 75% of building face at the setback line to create the fundamental experience of the street room.
- All parking, other than short-term retail or guest parking, and building services need to be located below grade or hidden within the building. If on ground level or above, parking and/or building services must be surrounded on all sides by program space such as commercial or housing.
- Parking and building services should not be accessed via these streets.
- Incorporate 10- to 12-foot wide sidewalks that create opportunities for gathering, outdoor cafes, pavilions, etc.
- Within the 50-foot setback, trees should be planted in a double row to add a strong canopy for pedestrian activity.

5

Implementing and Measuring the Guidance

Ten Things to Remember

5. Implementing and Measuring the Guidance

Ten Things to Remember

1. Every new development begins with the 200' x 200' block, or some variation based on context.
2. Every block or building in a development will need streets to connect between buildings. Not all of these streets will need to accommodate vehicles, providing the opportunity for parks, plazas or courtyards—important parts of the public realm.
3. Buildings will not be greater than 200 feet in length, thereby minimizing the negative impact continuous walls can have on a comfortable pedestrian experience.
4. All streets are not equal. The plan outlines a hierarchy that is driven by the kind of experiences that are expected on these streets and how they facilitate an enlivened public realm.
5. Designated transition zones are about maintaining the quality of life in these areas without restricting growth in other parts of the district.
6. Promenades and East-West Streets are the bridge between single family neighborhoods, such as the Cornelia neighborhood of Edina and the west side of Richfield, to more intense parts of the district.
7. Street Rooms will intersect and overlap each other in many circumstances. At these intersections, lower building heights should prevail, giving the smaller scaled building precedence over larger scale buildings.
8. Building footprints above 60 feet in height are limited to 12,000 SF for residential uses, and 24,000 SF for commercial.
9. Within the first 60 vertical feet of a building, primary materials systems that are more traditional like brick, stone, glass wall systems are preferred. Above 60 feet, other materials such as metal wall systems within a larger curtainwall system, can be introduced. These baseline parameters should not be a deterrent to architectural innovation but rather are intended to serve as a measure of quality and continuity throughout the district
10. Transparency at the ground level facing the public realm is key to the individual experience and is a catalyst for how to activate and maintain a community-based approach to daily life and experience.

ORDINANCE NO. 2012-__
AN ORDINANCE AMENDING THE ZONING ORDINANCE
TO REVISE THE PUD-16, PLANNED UNIT DEVELOPMENT-16 ZONING DISTRICT

The City Of Edina Ordains:

Section 1. Chapter 36, Article VIII, Division 4 is hereby amended to add the following:

Sec. 36-507 Planned Unit Development District-16 (PUD-16)

(a) *Legal description:*

Lot 44, Block I, Oscar Roberts 1st Addition, Hennepin County.
The North 325 Feet of the East 520 Feet of the Southeast 1/4 of the Northeast 1/4 Except Road. (7200 & 7250 France Avenue)

(b) *Approved Plans.* Incorporated herein by reference are the re-development plans, including the master development plan for the site received by the City on February 27, 2019 except as amended by City Council Resolution No. 2019-23 on file in the Office of the Planning Department.

(c) *Principal Uses:*

All uses allowed in the PCD-2 Zoning District
Multi-family Apartments/Townhomes/Condos.

(d) *Accessory Uses:*

All accessory uses allowed in the PCD-2 Zoning District.

(e) *Conditional Uses:*

All conditional uses allowed in the PCD-2 Zoning District.

(f) *Development Standards.* In addition to the development standards per the PCD-2 Zoning District, the following shall apply:

	Required
<u>Building Setbacks</u>	
Front – France	39 feet
Front – Gallagher	20 feet 25 feet
Front – 72nd Street (apartments)	35 feet
Front – 72nd Street (townhomes)	20 feet
Side – West (townhomes)	90 feet
Side – West (apartments)	45 feet

Existing text – XXXX

Stricken text – ~~XXXX~~

Added text – XXXX

Building Height	6 stories and 67 feet 84 feet (Building to be constructed per the approved plans)
Maximum Floor Area Ratio (FAR)	1.8%
Parking Stalls	590 stalls required (Per the approved plans) Phase 1 – 317 stalls Phase 2 – 468 stalls
Parking Stall Size	8.5 x 18'
Drive Aisle Width	24 feet

- (g) Signs shall be regulated per the PCD Zoning District based on the use.
- (h) ~~Twenty percent (20%) of the dwelling units in the building shall be dedicated for affordable housing at 60% or less of area median gross income, for a minimum of 25 years from the date of certificate of occupancy.~~
- (h) Affordable housing units must be included in the project with the development of housing in Phase 2 per the City's affordable housing policy at the time of final approval for Phase 2.**

Section 2. This ordinance is effective upon approval by the Metropolitan Council of the Comprehensive Plan Amendment.

First Reading:

Second Reading:

Published:

Attest:

Sharon Allison, City Clerk

James B. Hovland, Mayor

Please publish in the Edina Sun Current on:

Send two affidavits of publication.

Bill to Edina City Clerk

Existing text – XXXX

Stricken text – ~~XXXX~~

Added text – XXXX

DRAFT

Traffic and Parking Study for 7200 and 7250 France Avenue in Edina, MN

Prepared for:
City of Edina

4801 W. 50th Street
Edina, MN 55424



Prepared by:

**Stantec Consulting
Services Inc.**

1800 Pioneer Creek Center
Maple Plain, MN 55359
Phone: 7963-479-4200
Fax: 763-479-4242

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I hereby certify that this report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

Edward F. Terhaar
License No. 24441

DATE: September 8, 2022

1.0 Executive Summary

The purpose of this Traffic and Parking Study is to evaluate the traffic impacts of the proposed new mixed use development located at 7200 and 7250 France Avenue in Edina, MN. The project site is located on the west side of France Avenue between Gallagher Drive and 72nd Street. The proposed project location is currently occupied by two vacant office buildings, a parking structure, and surface parking.

This study examined weekday a.m. and p.m. peak hour traffic impacts of the proposed redevelopment at the following intersections:

- France Avenue/70th Street
- France Avenue/Hazelton Road
- France Avenue/72nd Street
- France Avenue/Gallagher Drive
- France Avenue/Parklawn Avenue
- Parklawn Avenue/Gallagher Drive
- Gallagher Drive/proposed access
- 72nd Street/proposed access

The proposed project will involve removal of the existing office buildings and constructing two new buildings. The project includes 468 total on-site parking spaces, with 234 on the 7200 site and 234 on the 7250 site. The entire project is expected to be completed by 2027.

The land uses and sizes assumed for this study are shown below:

- 7250 Building – 124,620 square feet of general office and a 2,200 square foot coffee shop.
- 7200 Building – 150 apartment units and 10,000 square feet of general retail space.

As shown in the site plan, the project includes access on Gallagher Drive and 72nd Street.

The project will be constructed in phases, with the 7250 building constructed first and the 7200 building constructed at a later date.

The conclusions drawn from the information and analyses presented in this report are as follows:

- The proposed development is expected to generate 372 trips during the weekday a.m. peak hour, 340 trips during the weekday p.m. peak hour, and 3,214 weekday daily trips.
- The traffic generated by the proposed development has minimal impact on the intersection operations at the study intersections. No improvements are needed at these intersections to accommodate the proposed project.

- Traffic volumes on 72nd Street west of Lynmar Lane are expected to increase by 57 trips during the a.m. peak hour and 48 trips during the p.m. peak hour.
- The proposed project is designed to provide both pedestrian and bicycle connections to the surrounding infrastructure. Space for pedestrian amenities is provided along all streets surrounding the project. Access to the Nine Mile Creek Regional Trail is provided at the Gallagher Drive crosswalk.
- Both short-term and long-term bicycle spaces should be provided in order to accommodate employees, customers, and residents. The short-term spaces should be located near building entrances and provide facilities to securely park each bicycle. Long-term spaces for residents should be provided in the parking ramp or in a separate room within the building. The provision of a bicycle maintenance station would help encourage bicycle use by all site users.
- The proposed number of parking spaces can accommodate the expected peak parking demand based on Institute of Transportation Engineers (ITE) data for both Phase 1 and full development of the site.
- Edina City code requires 1.0 parking space per apartment unit and 1 space per 300 square feet for the office, retail, and coffee shop uses. This equates to 422 total spaces for Phase 1 and 605 total spaces for the full development.
- Per City requirements, a Travel Demand Management (TDM) plan is required for this project. The goal of the TDM plan is to reduce vehicular trips during peak hours and carbon emissions from vehicles. TDM strategies for this site include:
 - Providing maps that show the area bus routes and schedules.
 - Providing maps of bicycle and pedestrian facilities.
 - Providing information on starting and joining commuter programs.
 - Providing long-term and short-term bicycle parking spaces for site users.
 - Offering a pre-paid Metro Transit Go-To Card to all new residents and employees.

2.0 Purpose and Background

The purpose of this Traffic and Parking Study is to evaluate the traffic impacts of the proposed new mixed use development located at 7200 and 7250 France Avenue in Edina, MN. The project site is located on the west side of France Avenue between Gallagher Drive and 72nd Street. The proposed project location is currently occupied by two vacant office buildings, a parking structure, and surface parking. The project location is shown in **Figure 1**.

This study examined weekday a.m. and p.m. peak hour traffic impacts of the proposed redevelopment at the following intersections:

- France Avenue/70th Street
- France Avenue/Hazelton Road
- France Avenue/72nd Street
- France Avenue/Gallagher Drive
- France Avenue/Parklawn Avenue
- Parklawn Avenue/Gallagher Drive
- Gallagher Drive/proposed access
- 72nd Street/proposed access

Proposed Development Characteristics

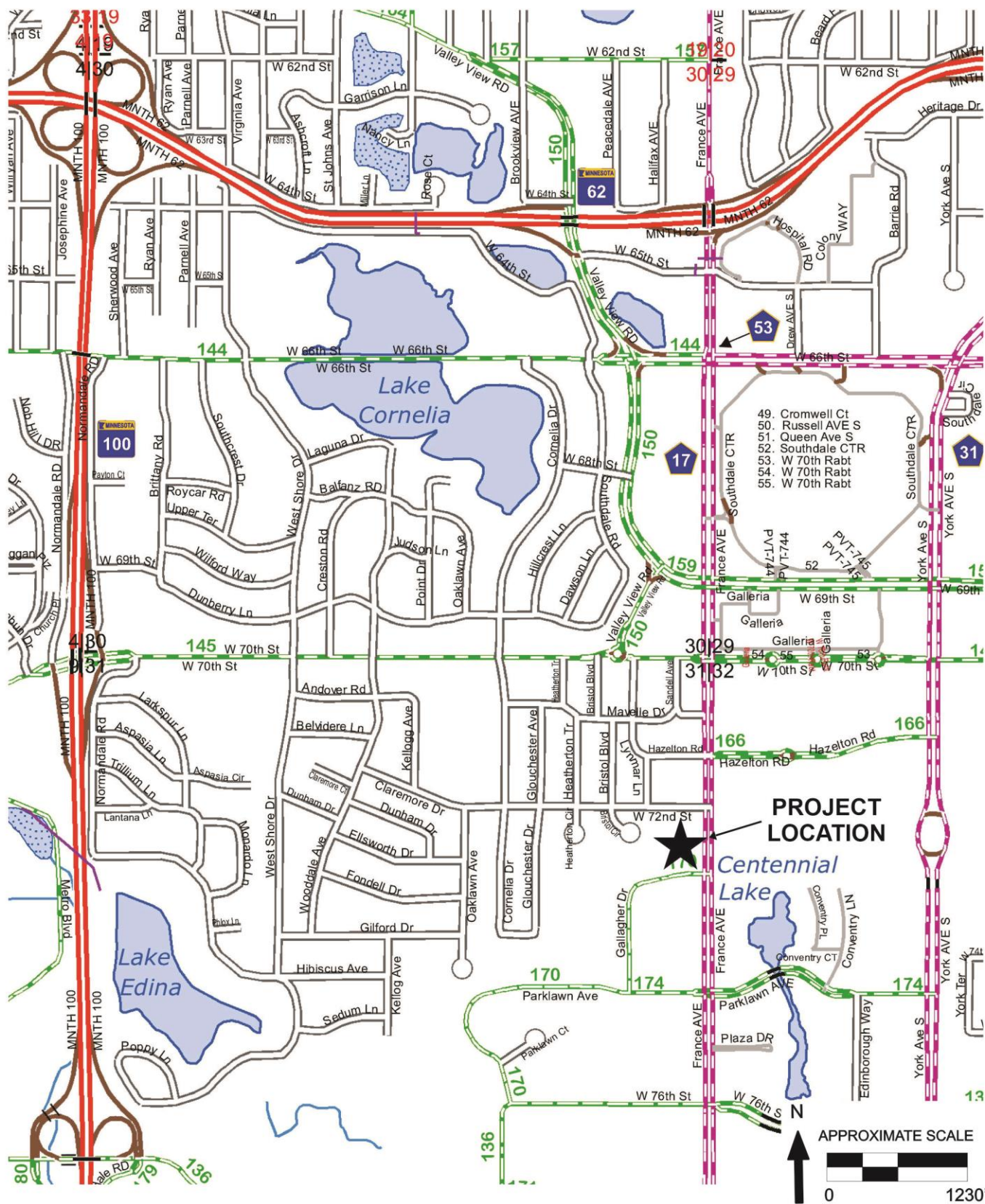
The proposed project will involve removal of the existing office buildings and constructing two new buildings. The project includes 468 total on-site parking spaces, with 234 on the 7200 site and 234 on the 7250 site. The entire project is expected to be completed by 2027. The current site plan is shown in **Figure 2**.

The land uses and sizes assumed for this study are shown below:

- 7250 Building – 124,620 square feet of general office and a 2,200 square foot coffee shop.
- 7200 Building – 150 apartment units and 10,000 square feet of general retail space.

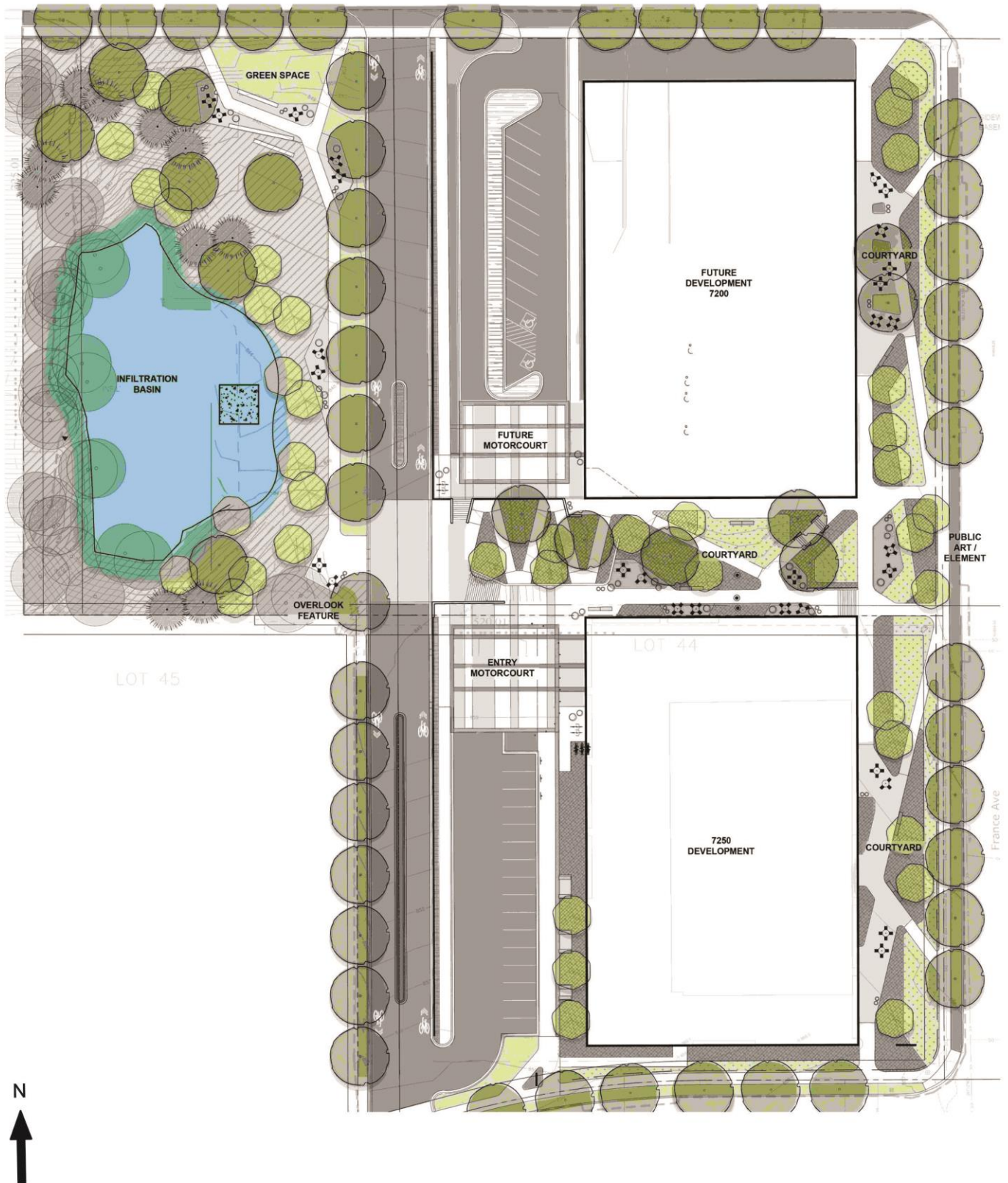
As shown in the site plan, the project includes access on Gallagher Drive and 72nd Street.

The project will be constructed in phases, with the 7250 building constructed first and the 7200 building constructed at a later date.



TRAFFIC AND PARKING STUDY
FOR 7200 AND 7250
FRANCE AVENUE
IN EDINA, MN

FIGURE 1
PROJECT LOCATION



TRAFFIC AND PARKING STUDY
FOR 7200 AND 7250
FRANCE AVENUE
IN EDINA, MN

FIGURE 2
SITE PLAN

DRAFT

3.0 Existing Conditions

The proposed site is currently occupied by two vacant office buildings, a parking structure, and surface parking. The site is bounded by France Avenue to the east, Gallagher Drive to the south, 72nd Street to the north, and residential areas to the west.

Near the site location, France Avenue is a six-lane divided roadway with turn lanes at major intersections. Gallagher Drive and 72nd Street are a two-lane roadways. Existing conditions at intersections near the proposed project location are shown in **Figure 3**.

France Avenue/70th Street (traffic signal control)

This intersection has four approaches and is controlled with a traffic signal. The eastbound and westbound approaches provide one left turn lane, one through lane, and one right turn lane. The northbound and southbound approaches provides one left turn lane, two through lanes, and one through/right turn lane.

France Avenue/Hazelton Road (traffic signal control)

This intersection has four approaches and is controlled with a traffic signal. The westbound approach provides one left turn lane, one through lane, and one right turn lane. The eastbound approach provides one left turn and one through/right turn lane. The southbound approach provides one left turn lane, two through lanes, and one through/right turn lane. The northbound approach provides one left turn lane, three through lanes, and one right turn lane.

France Avenue/72nd Street (minor street stop sign control)

This intersection has three approaches and is controlled with a stop sign on the eastbound 72nd Street approach. The eastbound approach provides one right turn lane. The northbound approach provides one left turn lane and three through lanes. The southbound approach provides two through lanes and one through/right turn lane. Left turns are not allowed from 72nd Street onto France Avenue.

France Avenue/Gallagher Drive (traffic signal control)

This intersection has four approaches and is controlled with a traffic signal. The eastbound and westbound approaches provide one left turn lane and one through/right turn lane. The northbound and southbound approaches provide one left turn lane, two through lanes, and one through/right turn lane.

France Avenue/Parklawn Avenue (traffic signal control)

This intersection has four approaches and is controlled with a traffic signal. The eastbound and westbound approaches provide one left turn lane, one through lane, and one right turn lane. The southbound approach provides one left turn lane, two through lanes, and one through/right turn lane. The northbound approach provides one left turn lane, three through lanes, and one right turn lane.

Parklawn Avenue/Gallagher Drive (minor street stop sign control)

This intersection has three approaches and is controlled with a stop sign on the southbound Gallagher Drive approach. The eastbound approach provides one left turn/through lane. The westbound approach provides one through/right turn lane. The southbound approach provides one left/right turn lane.

72nd Street/Cornelia Drive (all-way stop sign control)

This intersection has four approaches and is controlled with stop signs on all approaches. All approaches provide one left turn/through/right turn lane.

Turn movement data for the intersections was collected during the weekday a.m. (7:00 - 9:00 a.m.) and p.m. (4:00 - 6:00 p.m.) peak periods in July 2022.

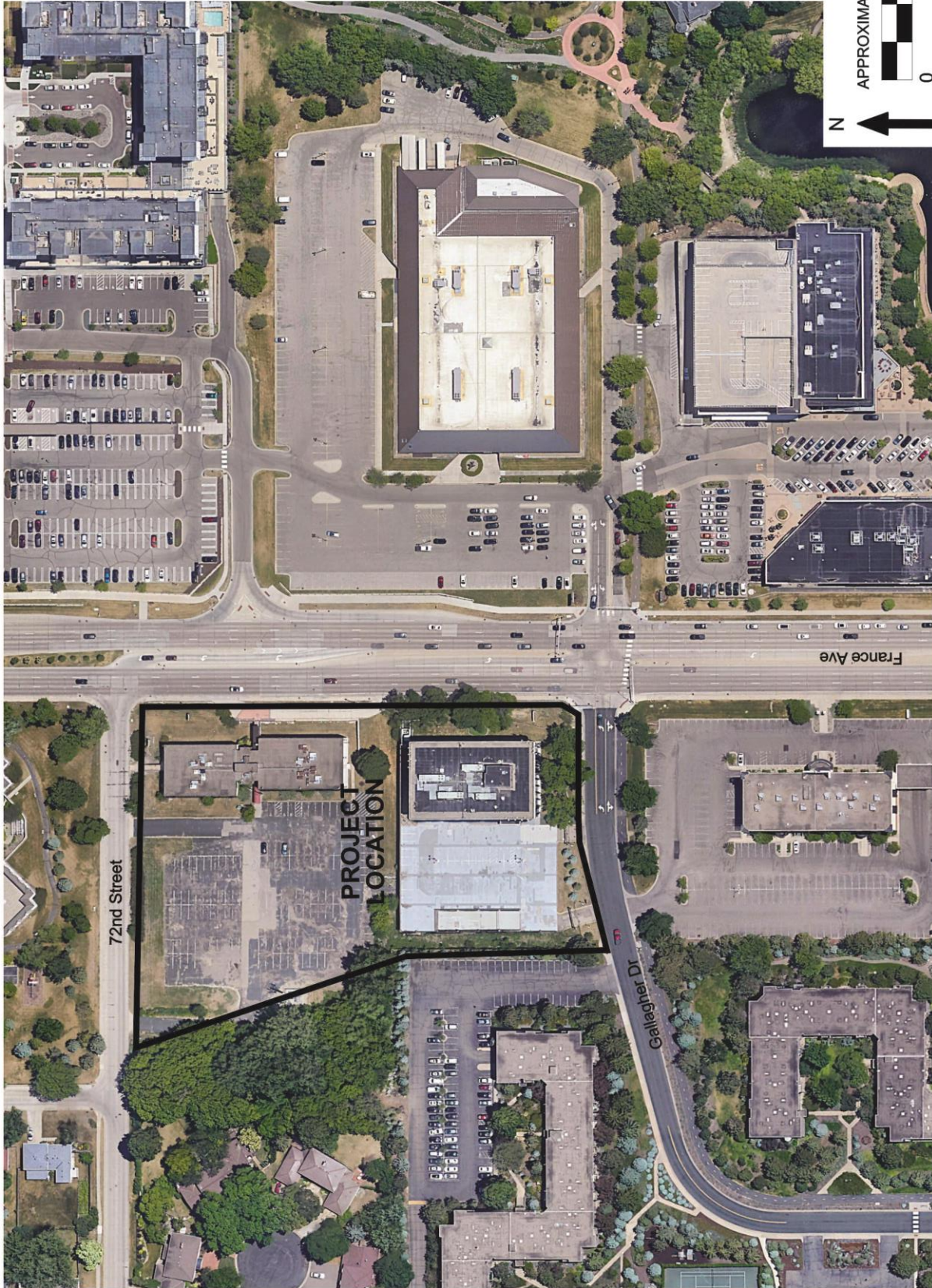


FIGURE 3
EXISTING CONDITIONS

TRAFFIC AND PARKING STUDY
FOR 7200 AND 7250
FRANCE AVENUE
IN EDINA, MN



4.0 Traffic Forecasts

Traffic Forecast Scenarios

To adequately address the impacts of the proposed project, forecasts and analyses were completed for the year 2027. Specifically, weekday a.m. and p.m. peak hour traffic forecasts were completed for the following scenarios:

- *2022 Existing.* Existing volumes were determined through traffic counts at the subject intersections. The existing volume information includes trips generated by the uses near the project site.
- *2027 No-Build.* Existing volumes at the subject intersections were increased by 0.5 percent per year to determine 2027 No-Build volumes. The 0.5 percent per year growth rate was calculated based on both recent growth experienced near the site and projected growth in the area.
- *2027 Build.* Trips generated by the proposed development were added to the 2027 No-Build volumes to determine 2027 Build volumes.

Trip Generation

Weekday a.m. and p.m. peak hour trip generation for the proposed development were calculated based on data presented in the eleventh edition of *Trip Generation*, published by the Institute of Transportation Engineers (ITE). Trips generated by the existing uses were based on the traffic count data. The resultant trip generation estimates are shown in **Table 4-1.**

Table 4-1
Trip Generation for Proposed Project

Land Use	Size	Weekday AM Peak Hour			Weekday PM Peak Hour			Weekday Daily
		In	Out	Total	In	Out	Total	Total
7200 Building								
Apartments	150 DU	13	43	56	36	23	59	681
General Retail	10,000 SF	14	10	24	33	33	66	545
Subtotal		27	53	80	69	56	125	1226
7250 Building								
General Office	124,620 SF	166	23	189	30	149	179	1351
Coffee Shop	2,200 SF	52	51	103	18	18	36	637
Subtotal		218	74	292	48	167	215	1988
Totals		245	127	372	117	223	340	3214

DU=dwelling unit, SF=square feet

The coffee shop is expected to be utilized by residents, employees, and visitors as well as the general public. The trip totals for the coffee shop have been reduced to account for residents, employees, and visitors who will already be on-site.

Trip Distribution Percentages

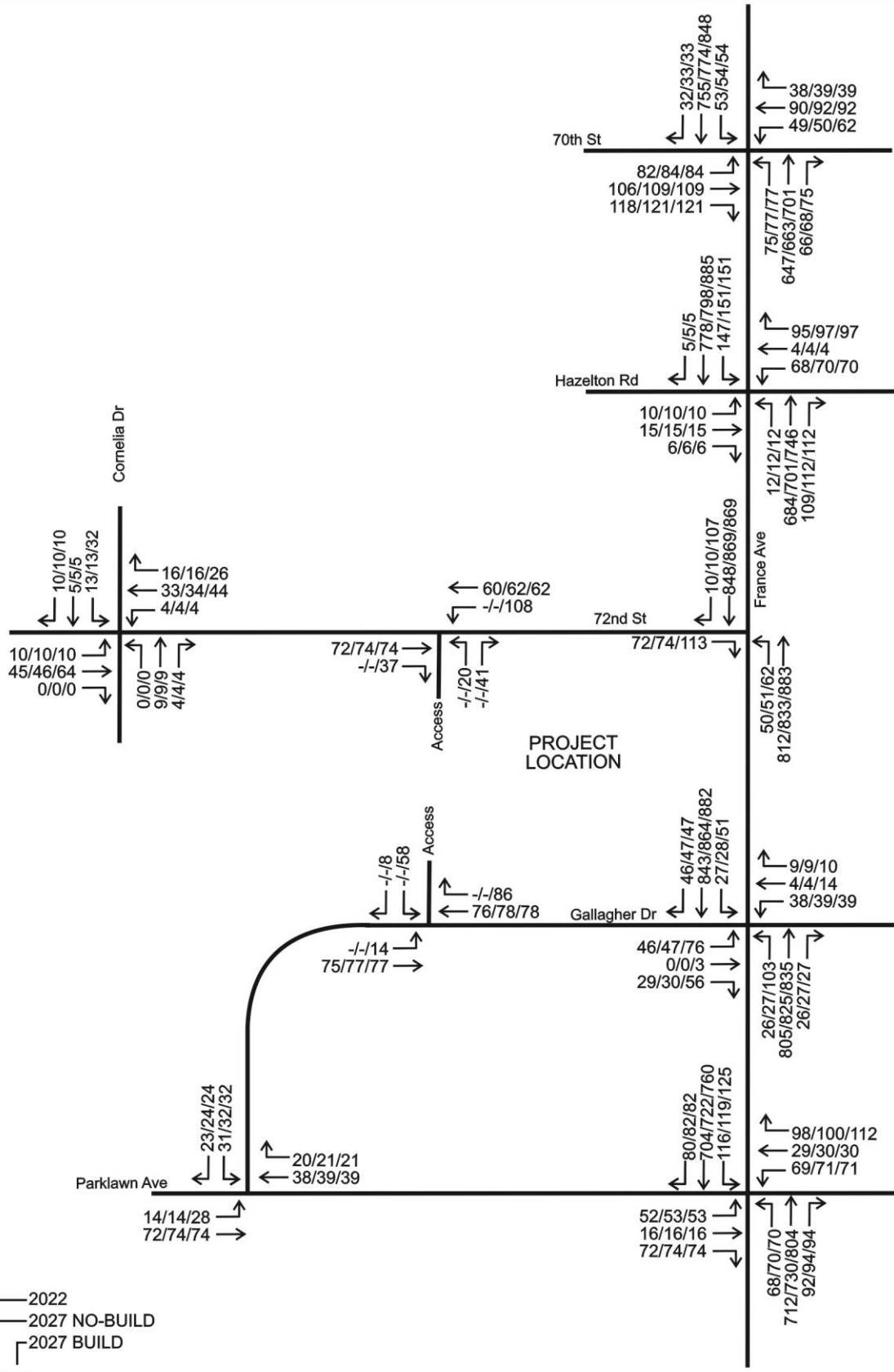
Trip distribution percentages for the subject development trips were established based on the nearby roadway network, existing and expected future traffic patterns, and location of the subject development in relation to major attractions and population concentrations.

The distribution percentages for trips generated by the proposed development are as follows:

- 30 percent to/from the north on France Avenue
- 7 percent to/from the west on 72nd Street
- 8 percent to/from the north of Cornelia Drive
- 5 percent to/from the east on 70th Street
- 5 percent to/from the east on Hazelton Road
- 5 percent to/from the east on Gallagher Drive
- 5 percent to/from the west on Parklawn Avenue
- 5 percent to/from the east on Parklawn Avenue
- 30 percent to/from the south on France Avenue

Traffic Volumes

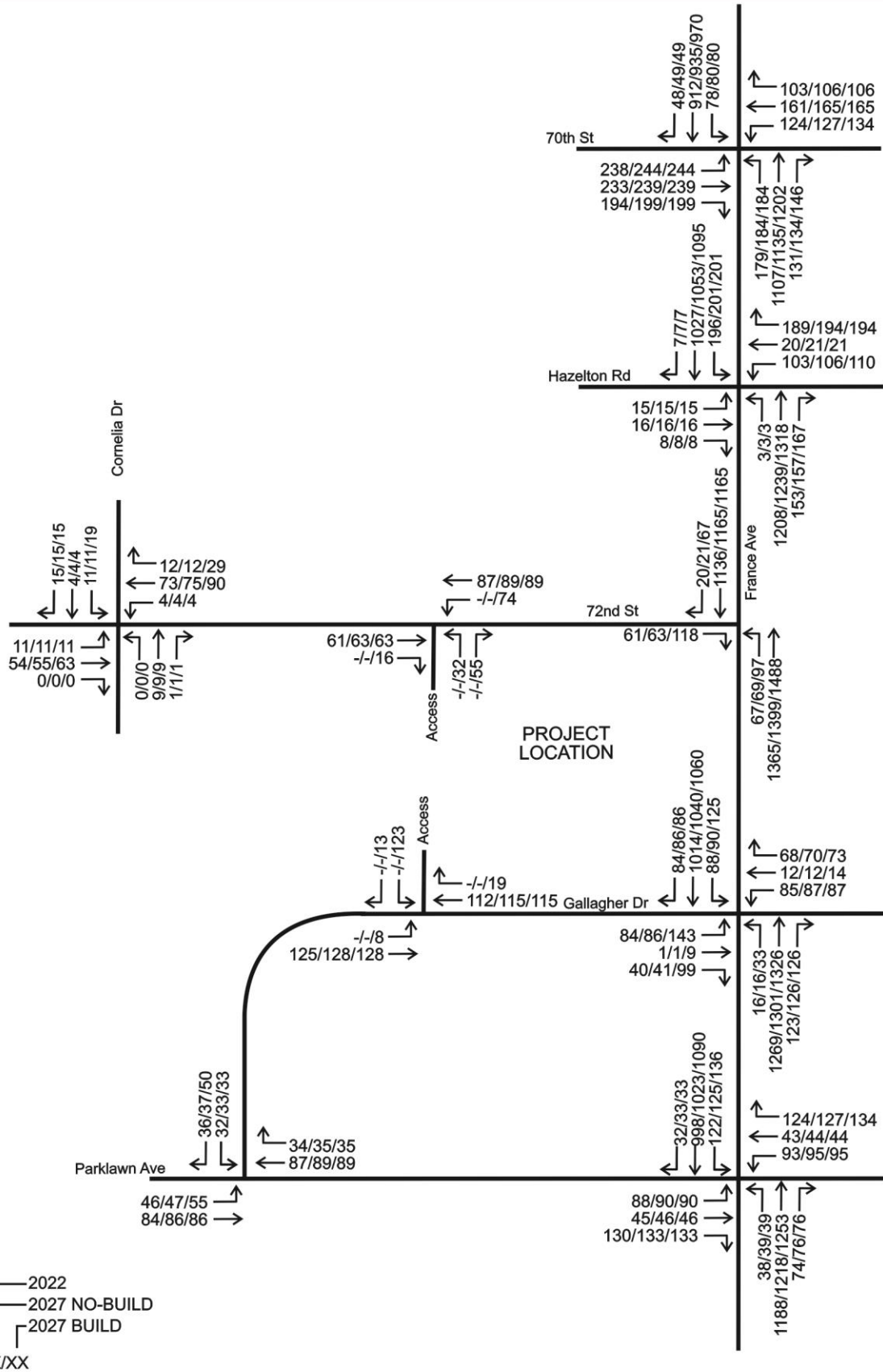
Development trips were assigned to the surrounding roadway network using the preceding trip distribution percentages. Traffic volumes were established for all the forecasting scenarios described earlier during the weekday a.m. and p.m. peak hours. The resultant traffic volumes are presented in **Figures 4 and 5**.



TRAFFIC AND PARKING STUDY
FOR 7200 AND 7250
FRANCE AVENUE
IN EDINA, MN

FIGURE 4

**WEEKDAY A.M. PEAK
HOUR VOLUMES**



TRAFFIC AND PARKING STUDY
FOR 7200 AND 7250
FRANCE AVENUE
IN EDINA, MN

FIGURE 5

**WEEKDAY P.M. PEAK
HOUR VOLUMES**

5.0 Traffic Analysis

Intersection Level of Service Analysis

Traffic analyses were completed for the subject intersections for all scenarios described earlier during the weekday a.m. and p.m. peak hours using Synchro software. Initial analysis was completed using existing geometrics and intersection control.

Capacity analysis results are presented in terms of level of service (LOS), which is defined in terms of traffic delay at the intersection. LOS ranges from A to F. LOS A represents the best intersection operation, with little delay for each vehicle using the intersection. LOS F represents the worst intersection operation with excessive delay. The following is a detailed description of the conditions described by each LOS designation:

- Level of service A corresponds to a free flow condition with motorists virtually unaffected by the intersection control mechanism. For a signalized or an unsignalized intersection, the average delay per vehicle would be approximately 10 seconds or less.
- Level of service B represents stable flow with a high degree of freedom, but with some influence from the intersection control device and the traffic volumes. For a signalized intersection, the average delay ranges from 10 to 20 seconds. An unsignalized intersection would have delays ranging from 10 to 15 seconds for this level.
- Level of service C depicts a restricted flow which remains stable, but with significant influence from the intersection control device and the traffic volumes. The general level of comfort and convenience changes noticeably at this level. The delay ranges from 20 to 35 seconds for a signalized intersection and from 15 to 25 seconds for an unsignalized intersection at this level.
- Level of service D corresponds to high-density flow in which speed and freedom are significantly restricted. Though traffic flow remains stable, reductions in comfort and convenience are experienced. The control delay for this level is 35 to 55 seconds for a signalized intersection and 25 to 35 seconds for an unsignalized intersection.
- Level of service E represents unstable flow of traffic at or near the capacity of the intersection with poor levels of comfort and convenience. The delay ranges from 55 to 80 seconds for a signalized intersection and from 35 to 50 seconds for an unsignalized intersection at this level.
- Level of service F represents forced flow in which the volume of traffic approaching the intersection exceeds the volume that can be served. Characteristics often experienced include long queues, stop-and-go waves, poor travel times, low comfort and convenience, and increased accident exposure. Delays over 80 seconds for a signalized intersection and over 50 seconds for an unsignalized intersection correspond to this level of service.

The LOS results for the study intersections are presented in **Figures 6 and 7** and discussed below.

France Avenue/70th Street (traffic signal control) - During the a.m. peak hour under existing, 2027 No-Build, and 2027 Build conditions, all movements operate at LOS D or better. The overall intersection operates at LOS D for all scenarios.

During the p.m. peak hour under existing, 2027 No-Build, and 2027 Build conditions, all movements operate at LOS E or better. The overall intersection operates at LOS D for all scenarios.

France Avenue/Hazelton Road (traffic signal control) - During the a.m. peak hour under existing, 2027 No-Build, and 2027 Build conditions, all movements operate at LOS D or better. The overall intersection operates at LOS C for all scenarios.

During the p.m. peak hour under existing, 2027 No-Build, and 2027 Build conditions, all movements operate at LOS E or better. The overall intersection operates at LOS D for all scenarios.

France Avenue/72nd Street (minor street stop sign control) - During the a.m. peak hour under existing, 2027 No-Build, and 2027 Build conditions, all movements operate at LOS C or better. The overall intersection operates at LOS A for all scenarios.

During the p.m. peak hour under existing, 2027 No-Build, and 2027 Build conditions, all movements operate at LOS C or better. The overall intersection operates at LOS A for all scenarios.

France Avenue/Gallagher Drive (traffic signal control) - During the a.m. peak hour under existing, 2027 No-Build, and 2027 Build conditions, all movements operate at LOS D or better. The overall intersection operates at LOS D for all scenarios.

During the p.m. peak hour under existing, 2027 No-Build, and 2027 Build conditions, all movements operate at LOS E or better. The overall intersection operates at LOS C for all scenarios.

France Avenue/Parklawn Avenue (traffic signal control) - During the a.m. peak hour under existing, 2027 No-Build, and 2027 Build conditions, all movements operate at LOS E or better. The overall intersection operates at LOS D for all scenarios.

During the p.m. peak hour under existing, 2027 No-Build, and 2027 Build conditions, all movements operate at LOS E or better. The overall intersection operates at LOS C for all scenarios.

Parklawn Avenue/Gallagher Drive (minor street stop sign control) - During the a.m. peak hour under existing, 2027 No-Build, and 2027 Build conditions, all movements operate at LOS A. The overall intersection operates at LOS A for all scenarios.

During the p.m. peak hour under existing, 2027 No-Build, and 2027 Build conditions, all movements operate at LOS B or better. The overall intersection operates at LOS A for all scenarios.

72nd Street/Cornelia Drive (all-way stop sign control) - During the a.m. peak hour under existing, 2027 No-Build, and 2027 Build conditions, all movements operate at LOS A. The overall intersection operates at LOS A for all scenarios.

During the p.m. peak hour under existing, 2027 No-Build, and 2027 Build conditions, all movements operate at LOS A. The overall intersection operates at LOS A for all scenarios.

Gallagher Drive/proposed access (minor street stop control) - During the a.m. peak hour under 2027 Build conditions, all movements operate at LOS B or better. The overall intersection operates at LOS A for all scenarios.

During the p.m. peak hour under 2027 Build conditions, all movements operate at LOS B or better. The overall intersection operates at LOS A for all scenarios.

72nd Street/proposed access (minor street stop control) - During the a.m. peak hour under 2027 Build conditions, all movements operate at LOS B or better. The overall intersection operates at LOS A for all scenarios.

During the p.m. peak hour under 2027 Build conditions, all movements operate at LOS A. The overall intersection operates at LOS A for all scenarios.

Overall Traffic Impacts

The traffic generated by the proposed development has minimal impact on the intersection operations at the study intersections. No improvements are needed at these intersections to accommodate the proposed project.

72nd Street Impacts

72nd Street extends west from France Avenue and provides connections to residential areas west of the project site. 72nd Street is also used to access 70th Street via connections to Cornelia Drive and Wooddale Avenue. Observations indicate that traffic utilizing 72nd Street is a mixture of local trips and trips accessing 70th Street.

Under existing conditions, the office building has two access points on 72nd Street. Through observations and data collected at the access locations for previous studies completed for this site, approximately 15% of existing site trips used 72nd Street west of Lynmar Lane to access the site. Under future conditions, this equates to 57 trips during the a.m. peak hour and 48 trips during the p.m. peak hour.

Bicycle and Pedestrian Facilities

Under existing conditions, sidewalk is provided on both sides of France Avenue, on the north/west side of Gallagher Drive west of France Avenue, and on the north side of 72nd Street west of France Avenue. The Nine Mile Creek Regional Trail is located on the south/east side of Gallagher Drive. Striped crosswalks and pedestrian signal heads are provided at all signalized intersections along France Avenue.

The proposed project is designed to provide both pedestrian and bicycle connections to the surrounding infrastructure. Space for pedestrian amenities is provided along all streets surrounding the project. Access to the Nine Mile Creek Regional Trail is provided at the Gallagher Drive crosswalk.

The site plan does not specifically indicate the location or number of bicycle parking spaces. Both short-term and long-term bicycle spaces should be provided in order to accommodate employees, customers, and residents. The short-term spaces should be located near building entrances and provide facilities to securely park each bicycle. Long-term spaces for residents should be provided in the parking ramp or in a separate room within the building. The provision of a bicycle maintenance station would help encourage bicycle use by all site users.

Transit Facilities

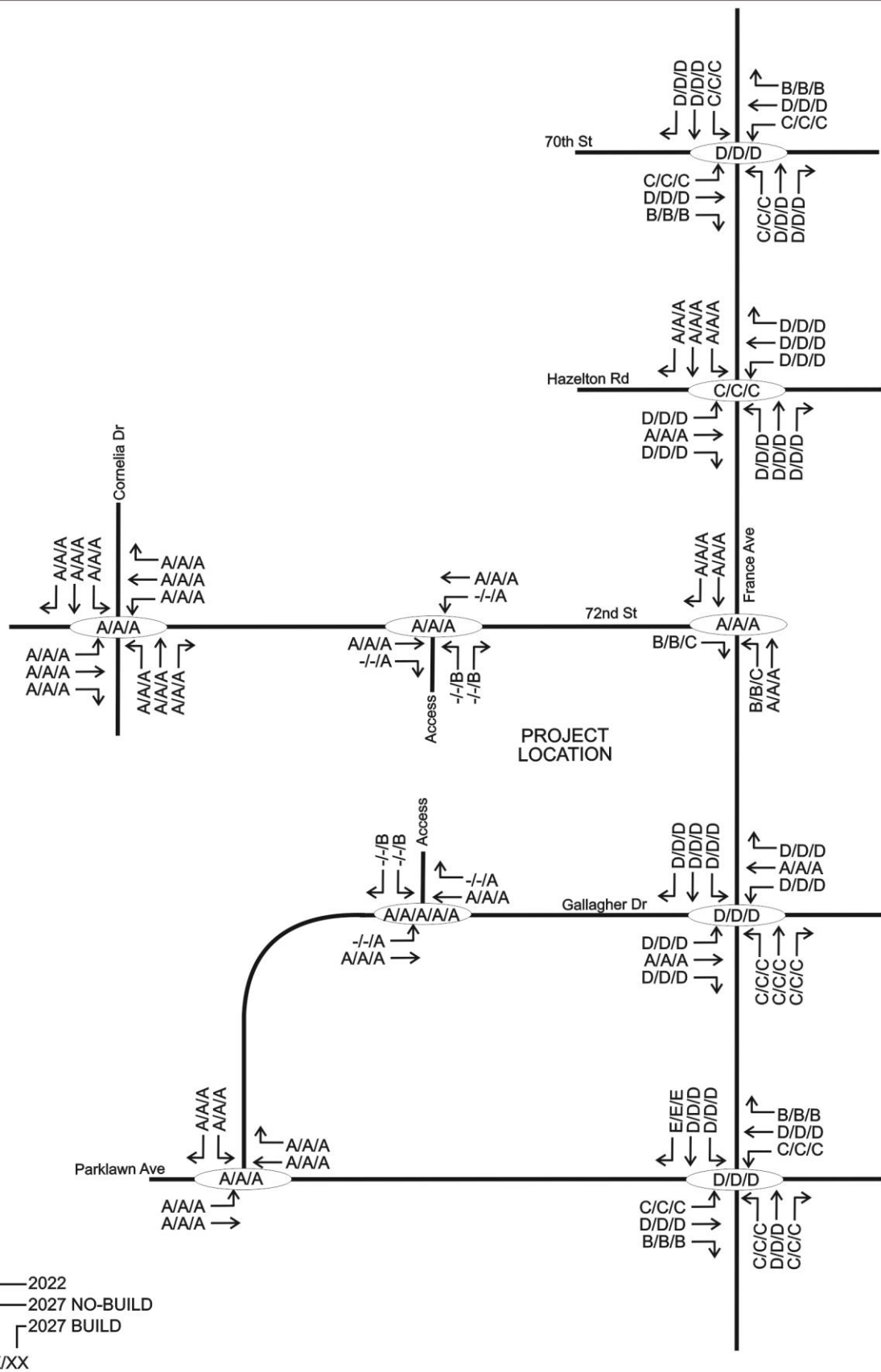
The subject site presently is served by the Metro Transit bus route 6. Bus stops exist on France Avenue, Gallagher Drive, Parklawn Avenue, and Hazelton Road.

Travel Demand Management Plan (TDM)

Per City requirements, a Travel Demand Management (TDM) plan is required for this project. The goal of the TDM plan is to reduce vehicular trips during peak hours and carbon emissions from vehicles. TDM strategies for this site include:

- Providing maps that show the area bus routes and schedules.
- Providing maps of bicycle and pedestrian facilities.
- Providing information on starting and joining commuter programs.
- Providing long-term and short-term bicycle parking spaces for site users.
- Offering a pre-paid Metro Transit Go-To Card to all new residents and employees.

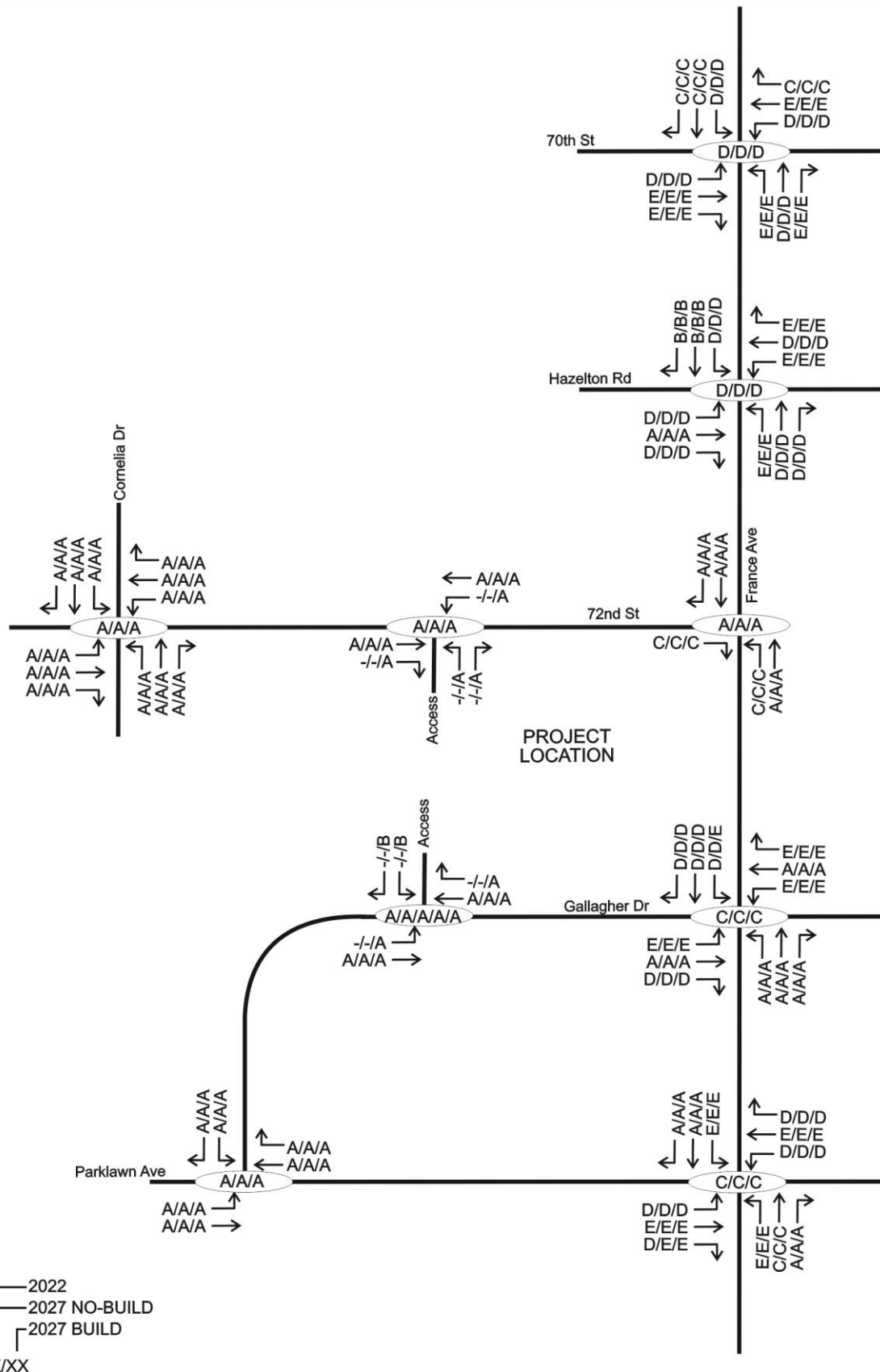
The goal of the TDM plan is a 10-20 percent reduction in single occupant vehicle trips. The TDM plan strategies should be implemented at the time the project is complete and fully operational. The overall cost of the strategies is estimated at \$2,000.



TRAFFIC AND PARKING STUDY
FOR 7200 AND 7250
FRANCE AVENUE
IN EDINA, MN

FIGURE 6

**WEEKDAY A.M. PEAK
HOUR LOS RESULTS**



TRAFFIC AND PARKING STUDY
FOR 7200 AND 7250
FRANCE AVENUE
IN EDINA, MN

FIGURE 7

**WEEKDAY P.M. PEAK
HOUR LOS RESULTS**

6.0 Parking Analysis

The entire project will ultimately include 468 on-site parking spaces, with 234 on the 7200 site and 234 on the 7250 site. The first phase of the project will be construction of the 7250 building, which will include 219 underground stalls, 15 surface stalls, and 83 temporary stalls on the 7200 site. Therefore the 7250 building will have access to 317 stalls until the 7200 building is constructed at a later date. The total number of parking stalls for each phase of the project is shown below:

- Phase 1 (7250 building only) – 317 stalls
- Full build (7250 and 7200 buildings) – 468 stalls

Parking data from the Institute of Transportation Engineers (ITE) was used to determine the expected parking demand for the proposed land uses. Data provided in the ITE publication *Parking Generation*, 5th Edition, indicates the various proposed uses peak at different times during the day. The ITE data was adjusted to account for the expected modal split for the site.

Based on the ITE data, the peak weekday parking demand for Phase 1 is 289 spaces, which occurs between 8 am and 11 am. The 317 spaces provided for Phase 1 can accommodate the expected peak parking demand. The peak parking demand for the entire development is 356 spaces, which occurs between 8 am and 11 am. The 468 spaces provided for the entire development can accommodate the expected peak parking demand.

If the retail and apartment for the 7200 building change from those currently assumed, the parking calculations should be updated to ensure adequate on-site parking.

Edina City code requires 1.0 parking space per apartment unit and 1 space per 300 square feet for the office, retail, and coffee shop uses. This equates to 422 total spaces for Phase 1 and 605 total spaces for the full development.

7.0 Conclusions and Recommendations

The conclusions drawn from the information and analyses presented in this report are as follows:

- The proposed development is expected to generate 372 trips during the weekday a.m. peak hour, 340 trips during the weekday p.m. peak hour, and 3,214 weekday daily trips.
- The traffic generated by the proposed development has minimal impact on the intersection operations at the study intersections. No improvements are needed at these intersections to accommodate the proposed project.
- Traffic volumes on 72nd Street west of Lynmar Lane are expected to increase by 57 trips during the a.m. peak hour and 48 trips during the p.m. peak hour.
- The proposed project is designed to provide both pedestrian and bicycle connections to the surrounding infrastructure. Space for pedestrian amenities is provided along all streets surrounding the project. Access to the Nine Mile Creek Regional Trail is provided at the Gallagher Drive crosswalk.
- Both short-term and long-term bicycle spaces should be provided in order to accommodate employees, customers, and residents. The short-term spaces should be located near building entrances and provide facilities to securely park each bicycle. Long-term spaces for residents should be provided in the parking ramp or in a separate room within the building. The provision of a bicycle maintenance station would help encourage bicycle use by all site users.
- The proposed number of parking spaces can accommodate the expected peak parking demand based on Institute of Transportation Engineers (ITE) data for both Phase 1 and full development of the site.
- Edina City code requires 1.0 parking space per apartment unit and 1 space per 300 square feet for the office, retail, and coffee shop uses. This equates to 422 total spaces for Phase 1 and 605 total spaces for the full development.
- Per City requirements, a Travel Demand Management (TDM) plan is required for this project. The goal of the TDM plan is to reduce vehicular trips during peak hours and carbon emissions from vehicles. TDM strategies for this site include:
 - Providing maps that show the area bus routes and schedules.
 - Providing maps of bicycle and pedestrian facilities.
 - Providing information on starting and joining commuter programs.
 - Providing long-term and short-term bicycle parking spaces for site users.
 - Offering a pre-paid Metro Transit Go-To Card to all new residents and employees.

8.0 Appendix

- Level of Service Worksheets



The CITY of
EDINA

Zoning Ordinance Amendment to Revise PUD- 16 – 7200 & 7250 France Avenue

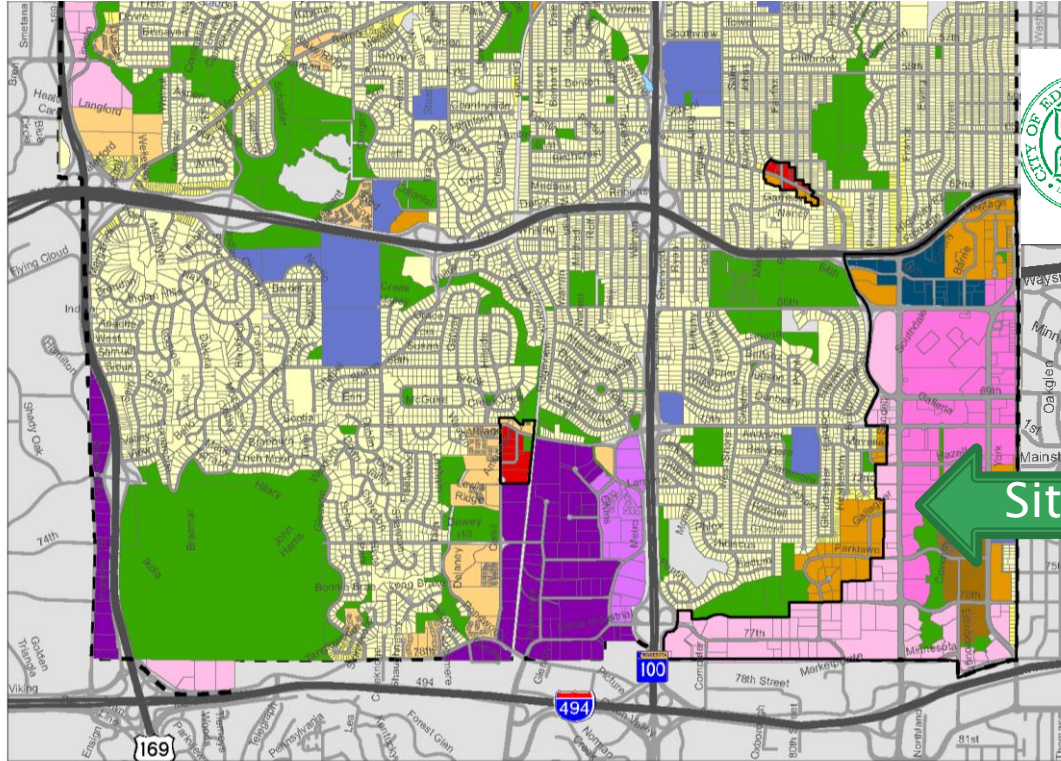


The CITY of EDINA



Edina 2040 Comprehensive Plan

Edina, Minnesota



Legend

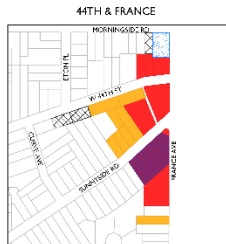
- | | |
|--|---------------------------|
| Low Density Residential | Neighborhood Node |
| Low Density Attached Residential | Mixed Use Center |
| Medium Density Residential | Community Activity Center |
| High Density Residential | Industrial |
| Greater Southdale District Residential | Open Space and Parks |
| Office Residential | Public/Semi Public |
| Office | Regional Medical |
| | City Limits |



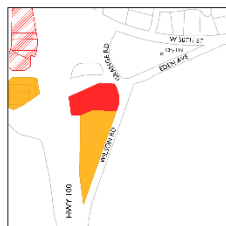
Site

3,700
Feet

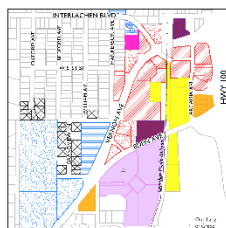
Source: City of Edina, Hennepin County, MetCouncil, MnDOT



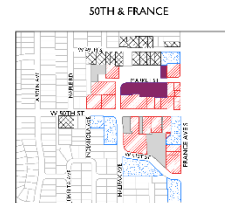
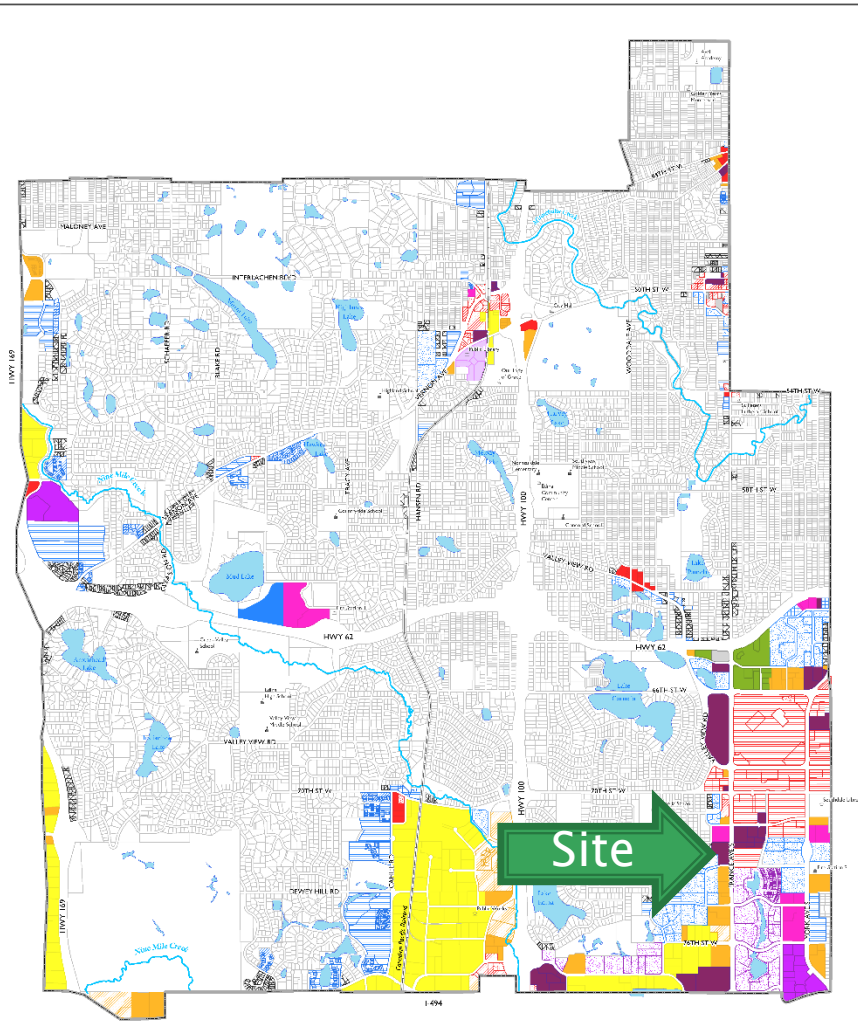
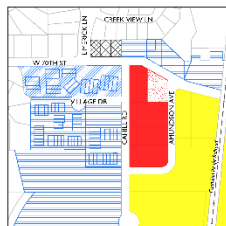
WILSON & EDEN



GRANDVIEW



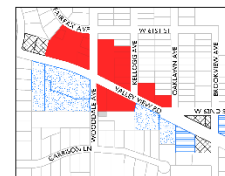
CAHILL & 70TH



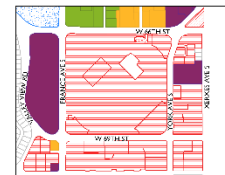
54TH & FRANCE



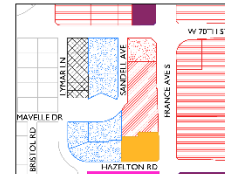
VALLEY VIEW & WOODDALE



SOUTHDALÉ



70TH & FRANCE



Zoning Map

Legend

- R-1 - Single Dwelling Unit
 PRD-1 - Planned Residence
 PRD-2 - Planned Residence
 PRD-3 - Planned Residence
 PRD-4 - Planned Residence
 PRD-5 - Planned Residence
 PCD-1 - Planned Commercial
 PCD-2 - Planned Commercial
 PCD-3 - Planned Commercial
 PCD-4 - Planned Commercial
 POD-1 - Planned Office
 POD-2 - Planned Office
 RMD - Regional Medical
 PID - Planned Industrial
 PUD - Planned Unit Development
 APD - Automotive Parking
 PSR-4 - Planned Residence
 MDD-4 - Mixed Development
 MDD-5 - Mixed Development
 MDD-6 - Mixed Development
 Lakes
 Creeks
 Edina Boundary
 Railroad
 Public Building
 Public School
 Private School





The CITY of
EDINA



CITY COUNCIL MEETING, DECEMBER 4TH

2019 Approved Overall
Development Plan

2019 Approved Overall Development Plan



The CITY of
EDINA



CITY COUNCIL MEETING, DECEMBER 4TH

- 6th Floor @ 67'-5"
- Full step back on 6th floor
- Stone on street level
- partial Brick on upper levels

7200 - 7250 France Avenue

Edina, MN

South West View

37-124-01

December 4, 2018

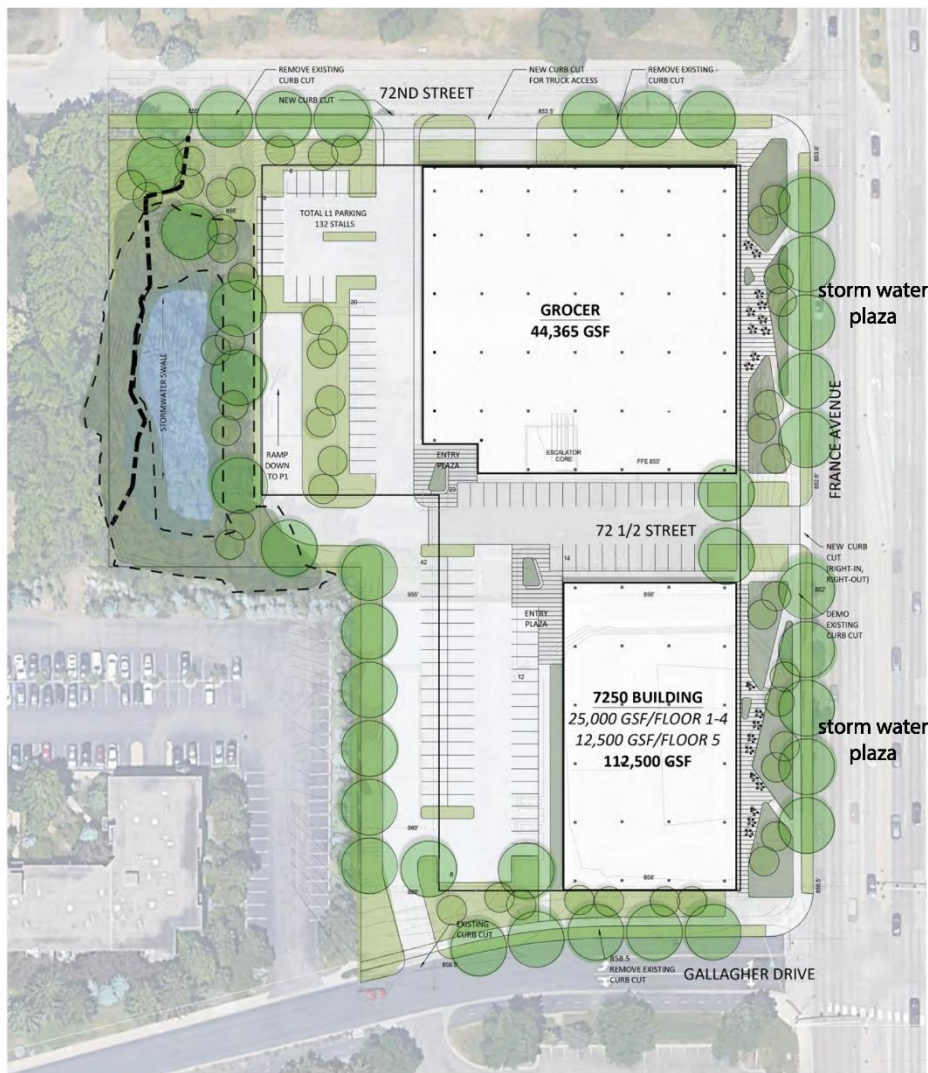
DJR
ARCHITECTURE INC.



Cleason Mortgage Corporation



The CITY of EDINA



Sketch Plan Proposal



The CITY of
EDINA

Revisions from Sketch Plan:

- Eliminated the retail/grocery store
- Enhanced the north-south drive/bike/pedestrian experience
- Enhanced architecture (applicant to detail)
- Eliminated the drive entrance off France.



The CITY of
EDINA

Phase 1



Phase 2





esg
ARCHITECTURE & DESIGN

500 Washington Avenue South, Suite 1000
Minneapolis, MN 55415
P: 612.201.1500 F: 612.201.5102
www.esgarch.com

I hereby certify that all calculations, specifications, and reports were prepared by me or under my direct supervision and that I am a duly licensed professional engineer in the state of Minnesota.

Signature _____
Printed Name _____
Date _____

**NOT FOR
CONSTRUCTION**



**P.U.D.
SUBMISSION
8/12/2022**

ORIGINAL (SHEET):
08/10/22

REVISIONS:
No. Description Date

222702
PROJECT NUMBER
Author: [Signature]
Checker: [Signature]
DATE: [Signature]



esg
ARCHITECTURE & DESIGN

500 Washington Avenue South, Suite 1000
Minneapolis, MN 55401
p 612.281.0000 f 612.281.0382
www.esg-ny.com

This rendering, including any photographs or
report was prepared by me or under my direct
supervision and that I am a duly licensed architect
under the laws of the State of Minnesota.

Signature

Type or Printed Name

License #

Date

**NOT FOR
CONSTRUCTION**



**P.U.D.
SUBMISSION
8/12/2022**

ORIGINAL SCALE:

1/8" = 1'-0"

REV'S ONLY:

No. Description Date

222702

DATE: 11/08/2021

ESG

ESG

DRAWN BY: CHC-DE-BY

KEY PLAN



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505 Washington Avenue South, Suite 1000
Minneapolis, MN 55415
p 612.291.1088 f 612.291.2882
www.esgarch.com

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Project Name: _____
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Client: _____

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SUBMISSION
8/12/2022**

ORIGINAL ISSUE: _____
REVISED: _____
BY: _____ DATE: _____

2222022
PROJECT: _____
ESG: _____
CHECKED BY: _____
KEY PLAN: _____

This Request Requires:



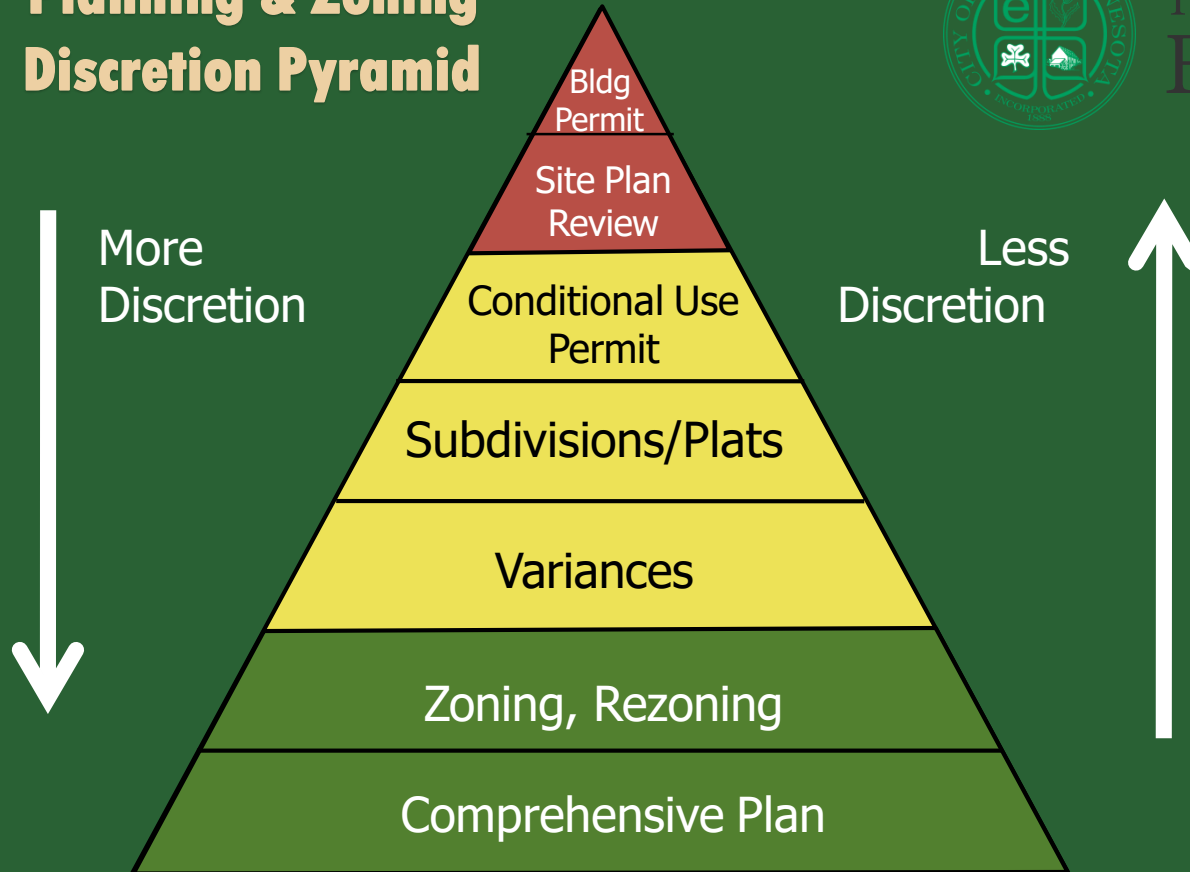
The CITY of
EDINA

- A Rezoning/Ordinance Amendment to revise the PUD-16, Planned Unit Development District to establish a new Overall Development Plan and Site Plan review for Phase I.

Planning & Zoning Discretion Pyramid



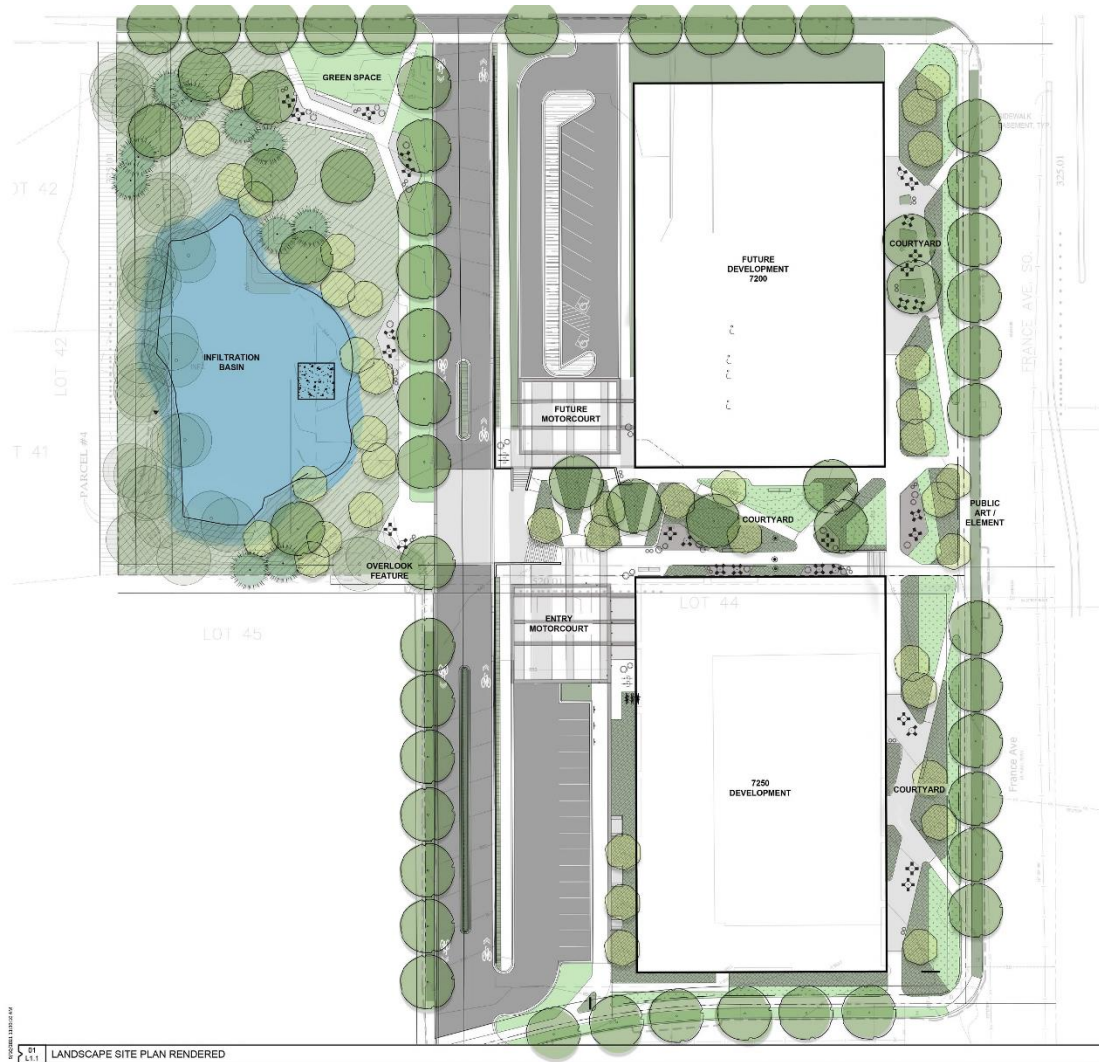
The CITY of
EDINA



Review of the Site Plan



The CITY of
EDINA



The CITY of
EDINA



The CITY of EDINA



11/11/2020 10:00 AM

01 LANDSCAPE SITE PLAN RENDERED

SCALE

1" = 10'

LEVEL 1 MATERIALS



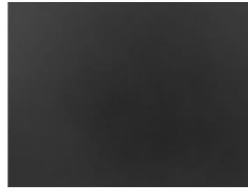
HYBRID WINDOW
WALL - SSG SYSTEM



ARCHITECTURAL
PRECAST - COLOR 1



STONE BASE



METAL PANEL - COLOR 1



METAL PANEL - COLOR 2

LEVELS 2-5 MATERIALS



HYBRID WINDOW
WALL - SSG SYSTEM



ARCHITECTURAL BRICK



METAL PANEL - COLOR 1

ROOF MATERIALS



FLAT LOCK ZINC PANEL

7200 & 7250
FRANCE AVE
7200 & 7250 - FRANCE AVE, LOMA
VILLAS

esc
ARCHITECTURE & DESIGN

300 West Main Avenue, Suite 100
Bloomington, MN 55403
p 612.291.0800 f 612.291.0802
www.escarch.com

7200 & 7250 - FRANCE AVE
7200 & 7250 - FRANCE AVE
7200 & 7250 - FRANCE AVE
7200 & 7250 - FRANCE AVE

7200 & 7250
7200 & 7250
7200 & 7250

NOT FOR
CONSTRUCTION

P.U.D.
SUBMISSION
8/12/2022

DATE: 8/12/2022
BY: [Signature]
REVISED: [Signature]

222702
PROJECT NUMBER
ESC ESC
STATION STATION
DATE DATE

7250 EXTERIOR
MATERIALS

A2.5



The CITY of EDINA

	City Standard (PUD-16) (Measured to the curb)	POD District (Measured to the curb)	Proposed (Measured to the curb)
Front – 72 nd Street Front – France Ave. Front – Gallagher Drive Side – West (north half) Side – West (south half)	35 feet 39 feet 20 feet 90 feet 45 feet	30 feet 50 feet 30 feet 20 feet 20 feet	30 feet 50 feet 25 feet* 150 feet 90 feet
Building Height	6 stories and 67 feet	4 stories and 48 feet	5-6 stories and 84 feet*
Floor Area Ratio (FAR)	1.8	.50	1.0*
Building Coverage		.30	.31*
Parking		Office/retail – 456 spaces Housing – 150 spaces Total – <u>606 spaces</u>	468 spaces*

Primary Issue



The CITY of
EDINA

- Are the proposed changes to the approved Overall Development Plan reasonable to justify amending the PUD rezoning for this site?

Yes. Staff does support the revised PUD, for the following reasons:

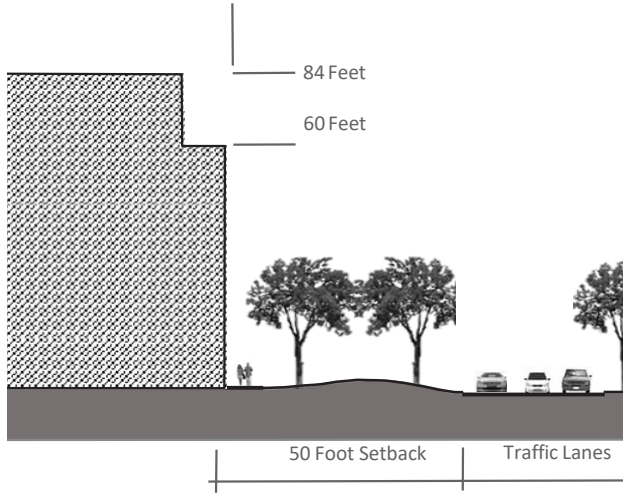
- I. While the plans are more flawed than the previously approved plans, they still do generally address the Greater Southdale District Design Experience Guidelines. Highlights continue to include: The division of the property into smaller blocks and the provision of pedestrian, bicycle and vehicle access and connections through the site both east-west and north-south; increasing the public realm on the site; eliminating most of the surface parking stalls, with none being visible from France Avenue in the Phase 2 build out. The street typology that requires buildings step in 10 feet above 60 feet in height shall be made a condition of approval. Affordable housing should be required within the future apartment project as well as public art located along France Avenue. They will be seeking a LEED certified building in Phase I.



The CITY of
EDINA

Dimensional Characteristics of Street Room Typology 2

Cornelia Overlay at France Avenue





Primary Issue

- Are the proposed changes to the approved Overall Development Plan reasonable to justify amending the PUD rezoning for this site?

2. The project would meet the following goals of the Comprehensive Plan:

- Encourage structured parking and open space linkages where feasible; emphasize the enhancement of the pedestrian environment.
- Locate and orient vehicle parking, vehicular access, service areas and utilities to minimize their visual impact on the property and on adjacent/surrounding properties, without compromising the safety and attractiveness of adjacent streets, parks, and open spaces.
- Provide sidewalks along primary streets and connections to adjacent neighborhoods along secondary streets or walkways.
- Limit driveway access from primary streets while encouraging access from secondary streets.
- Provision of the north-south public vehicle, bike and pedestrian connection through the site.
- Increase mixed-use development where supported by adequate infrastructure to minimize traffic congestion, support transit, and diversify the tax base.
- Support the development of mixed-use districts that provide a variety of living opportunities within a walkable and livable area.
- Recognize and support commercial, office, and industrial job centers that draw workers from the city and across the region.

Primary Issue



The CITY of
EDINA

- Are the proposed changes to the approved Overall Development Plan reasonable to justify amending the PUD rezoning for this site?

3. The proposal meets the City's criteria for PUD zoning. In summary the PUD zoning would:
 - Create a more pedestrian-friendly development with the construction of improved sidewalks and connections to the Promenade. The project would bring vibrancy to the area.
 - The building would be of high-quality architectural brick, metal, stone, and glass.
 - Ensure that the buildings proposed in Phase 1 would be the only buildings built on the site unless an amendment to the PUD is approved by City Council. Phase 2 would be required to be multi-family residential with some retail commercial as long as there is adequate parking on the site for all phases.
 - Project would add to the City's affordable housing stock by providing 10% of the units in Phase 2 for affordable housing.
 - Provide for a more creative site design, consistent with goals and policies in the Comprehensive Plan.
 - Meet the City's sustainability policy, including seeking a LEED certified building in Phase 1.
 - Provide 19% of the site for public use.

Primary Issue



The CITY of
EDINA

- Are the proposed changes to the approved Overall Development Plan reasonable to justify amending the PUD rezoning for this site?
 - 4. The provision of public space still makes up 19% of the site for the interior public sidewalks, driveways, bike space and public realm. If the public space in front of the building on France, Gallagher and 72nd street was included it would make up 38% of the site. (See attached public realm graphic.)
 - 5. The proposed height is justified. While the proposed buildings would be taller than the previously approved project it would still meet the required setback of buildings 5-6 feet tall to R-1 property. Edina City Code requires that buildings 5-6 stories tall be setback two times the height of the building from the property line of single-family homes. The building height is 84 feet; therefore a 168-foot setback is required. The distance as proposed would be 255 feet.
 - 6. The existing roadways and proposed parking would support the project. Stantec conducted a traffic and parking impact study and concluded that the proposed development could be supported by the existing roads and proposed parking.



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Better Together Edina



The CITY of
EDINA

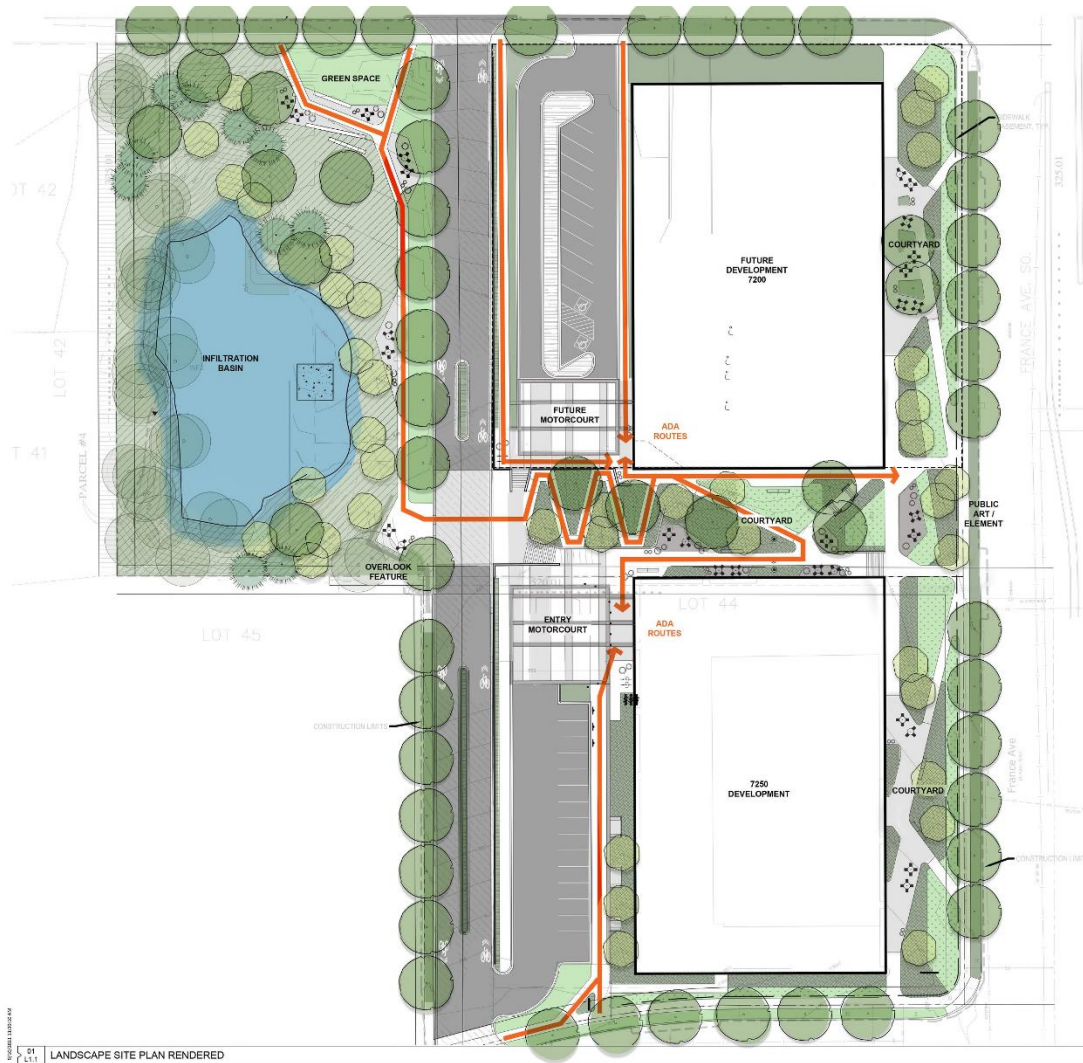
Recommendation



The CITY of
EDINA



The CITY of
EDINA



The CITY of
EDINA



escg

 **ORION**
INVESTMENTS

P.U.D. SUBMISSION
7200 & 7250 FRANCE
EDINA, MN

- A. | SITE CONTEXT
- B. | PROJECT OVERVIEW + GUIDELINES
- C. | SITE PLANS + PHASING
- D. | SITE FORCES + IMPROVEMENTS
- E. | CONCEPTUAL RENDERINGS + VISUALS

PROJECT DATES

MARCH 8, 2022	COMMUNITY & NEIGHBORHOOD INPUT MEETING
APRIL 13, 2022	PLANNING COMMISSION - SKETCH PLAN PRESENTATION
APRIL 19, 2022	CITY COUNCIL - SKETCH PLAN PRESENTATION
JULY 21, 2022	COMMUNITY & NEIGHBORHOOD INPUT MEETING
SEPT 14, 2022	PLANNING COMMISSION - P.U.D. HEARING & PRESENTATION
SEPT 20, 2022	CITY COUNCIL - P.U.D. PRESENTATION
OCT 6, 2022	CITY COUNCIL FINAL ACTION (IF NEEDED)











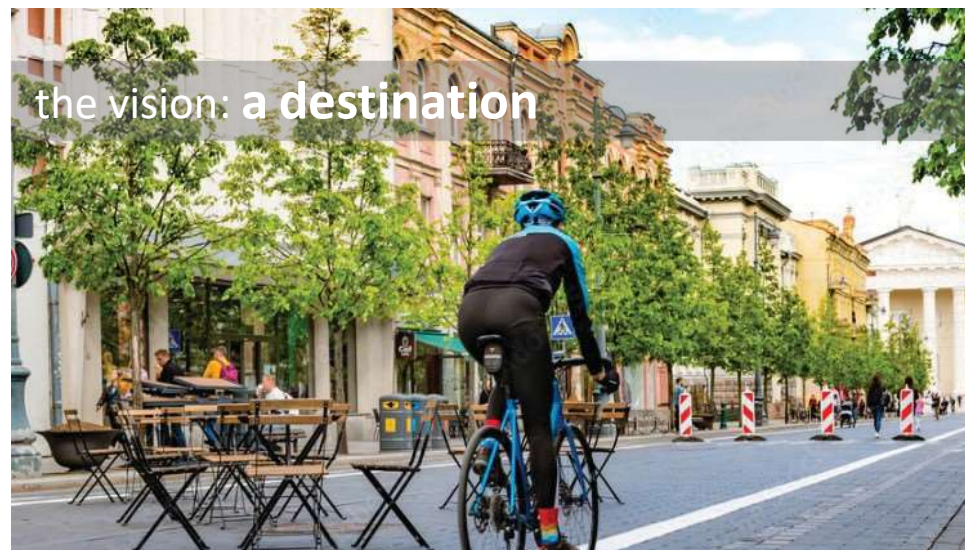
PROJECT OVERVIEW:

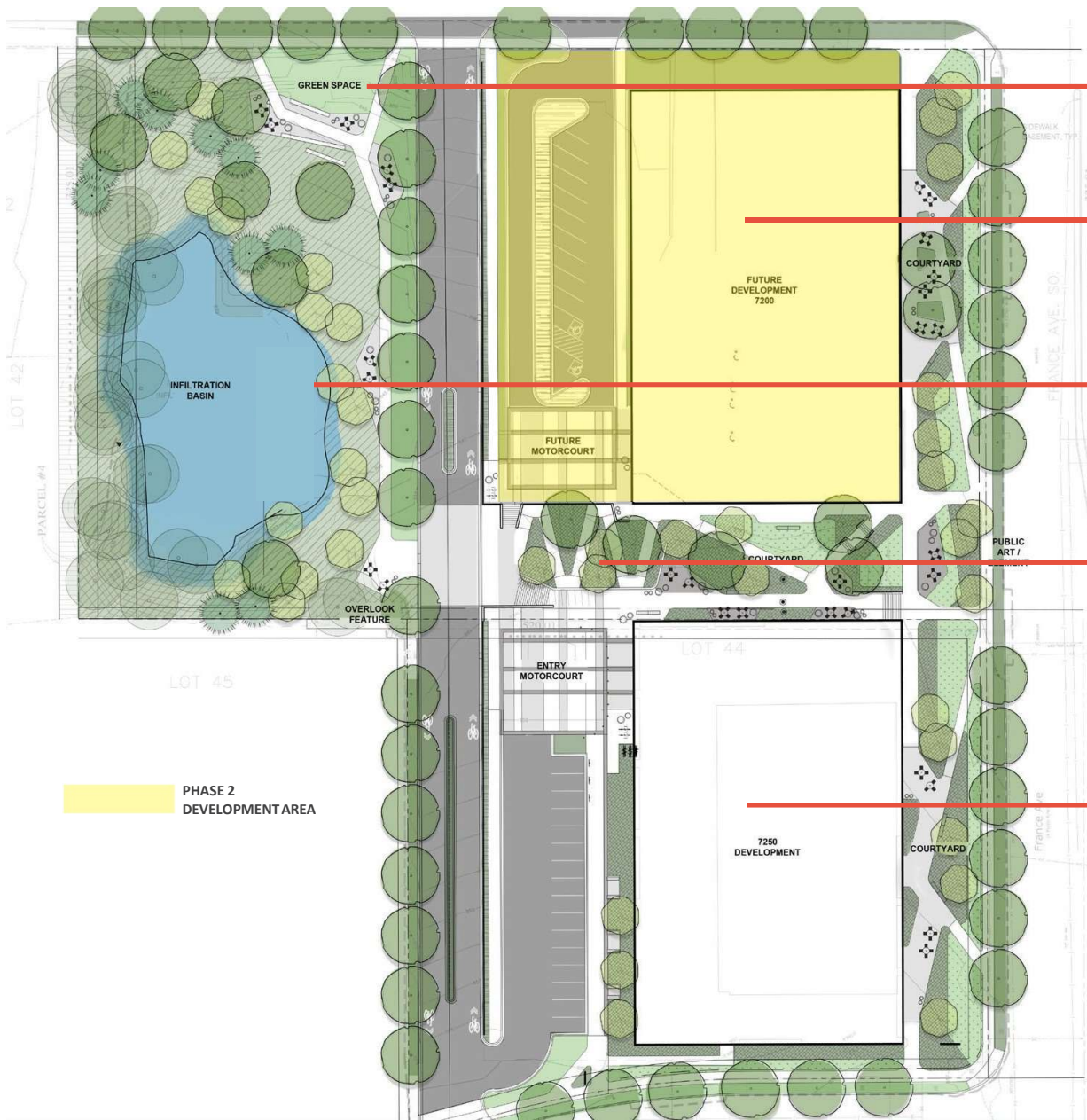
The team envisions this site as an opportunity to create an iconic development along the France corridor that will be a catalyst for growth.

- Project Team includes Orion Investments, ESG, and others to advance the development, entitlement and design of our redevelopment concept
- The Greater Southdale District Plan and District Design Experience Guidelines serve as the main planning and design criteria for this development.

PROJECT PRINCIPLES:

- Consistent with the City of Edina's vision to create a street grid system
- Excavate site to create two levels of parking below newly-established grade. Upon completion, **94%** of the parking will be below grade.
- Stormwater retention pond and rain garden included to manage local rainfall on site and provide public amenity when not active.
- 500+ jobs created.
- Environmental remediation of site.
- Bicycle and pedestrian connectivity to neighboring infrastructure.
- Sustainability // LEED Certified.





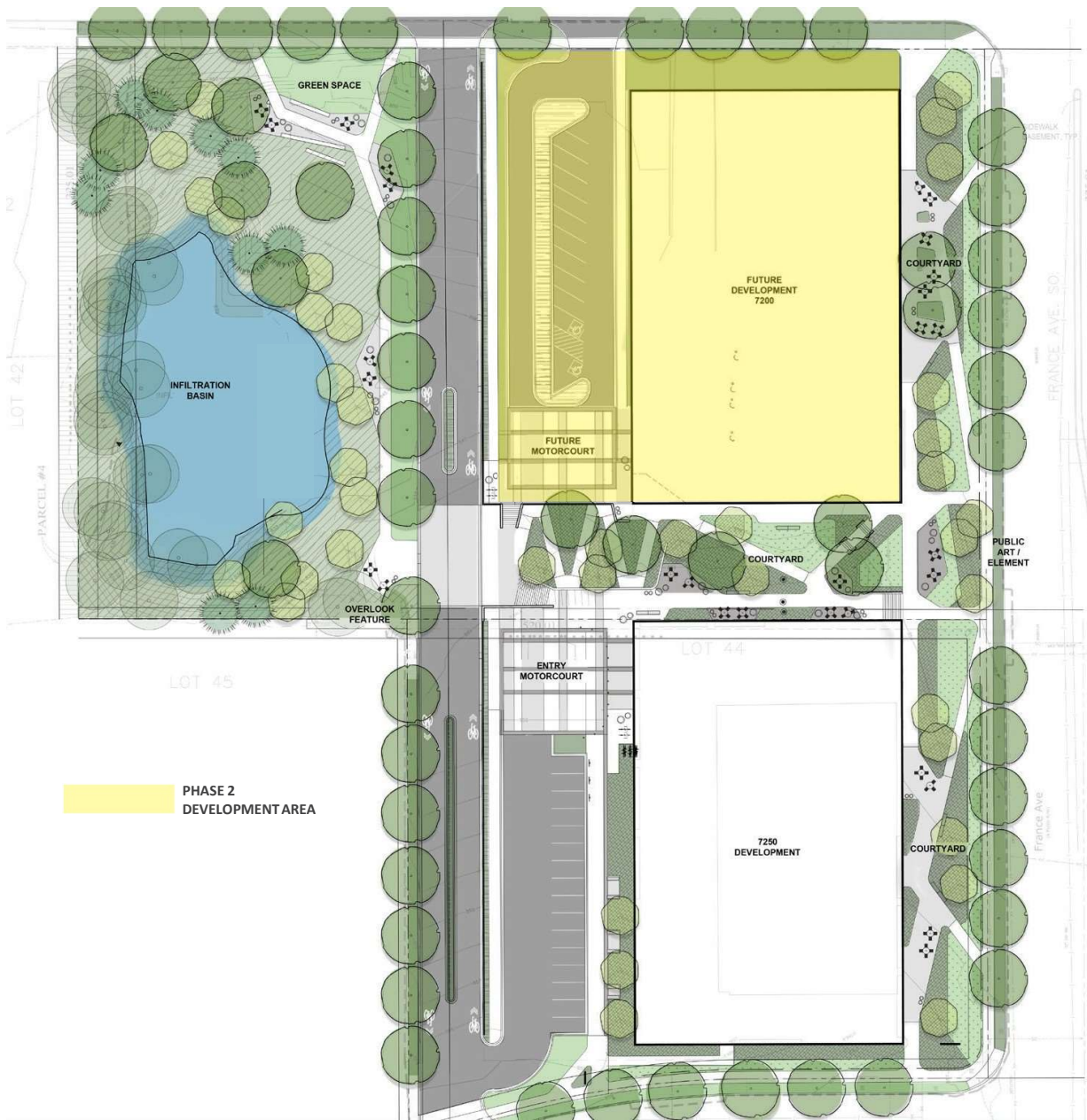
ACCESSIBLE SCENIC WALKWAY AND GREEN SPACE

TEMPORARY GREENSPACE (PHASE 1A) OR INTERIM PARKING (PHASE 1B); FUTURE BUILDING SITE (PHASE 2)
 - POTENTIALLY 124,620 GFA
 - 83 TEMPORARY PARKING STALLS (PHASE 1B)

STORMWATER RETENTION BASIN

DANILA ST - PEDESTRIAN WALKWAY
 - EXTENSION OF BUILDING AMENITY SPACE
 ~ 12,110 SF

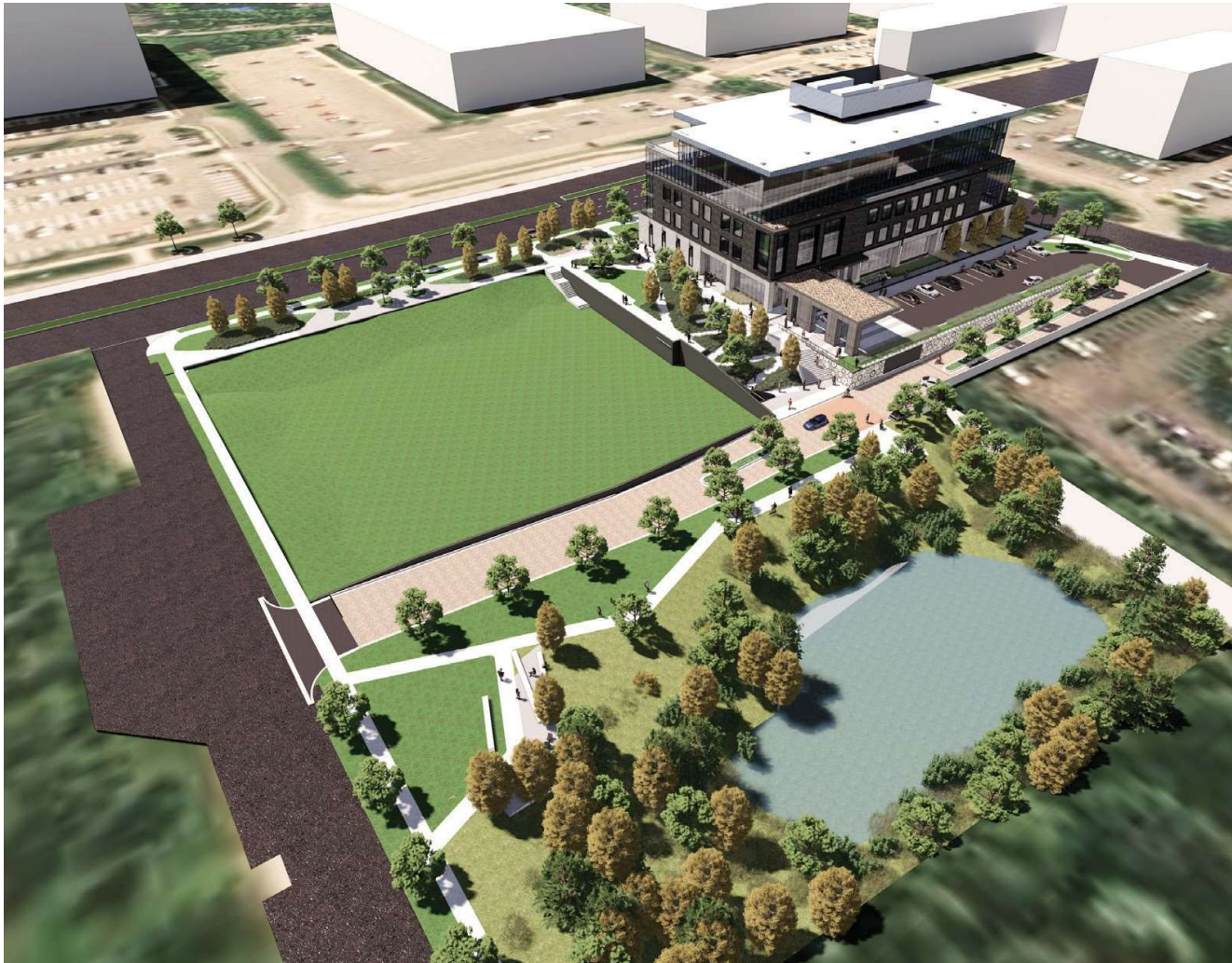
7250 OFFICE BUILDING
 ~124,620 GROSS FLOOR AREA
 - 234 COMMERCIAL PARKING STALLS



SITE PLAN:

- Site design creates attractive public amenities for the surrounding community
- France Ave setback becomes pedestrian corridor with landscaped paths, treescapes, and seating areas
- Stormwater retention basin nature area with landscaped paths and overlooks on west side of site
- Introduction of Danila Street to create a pedestrian-only corridor connected by ADA-compliant ramps and paths throughout entirety of site; can be utilized for accessible pedestrian access to on-site businesses during the day and then utilized for community events in the evening
- Human-scaled, comfortable pedestrian experience as well as improved bike access to connect to nearby bike path infrastructure
- Native and low maintenance plantings at stormwater retention pond
- Eliminate 2 curb cuts on France Ave





PHASE 1A:

- **Office Building on SE corner**
- All public amenities and infrastructure completed in Phase 1.
- **15** Surface parking stalls on parcel; **219** below-grade parking stalls across two parking levels to achieve **94%** below-grade parking.
- Planted setbacks and hardscape paths with seating off of 72nd and France improve pedestrian experience
- Primary Building entry points are along internal streets
- Phase 1 includes stormwater basin nature area as well as Danila Street and new internal north-south street



PHASE 1B:

- Office Building on SE corner
- **IF NEEDED: 115** temporary parking stalls on NE portion of lot



PHASE 2:

- Potential Mixed Use Building on NE corner
- Multi-family // Hospitality // Retail Mix
- Approximately 150 Units.
- Majority enclosed parking.

SUSTAINABLE DESIGN

BROWNFIELD DEVELOPMENT

- Transforming an existing parking lot
- Dramatically improving the physical environment

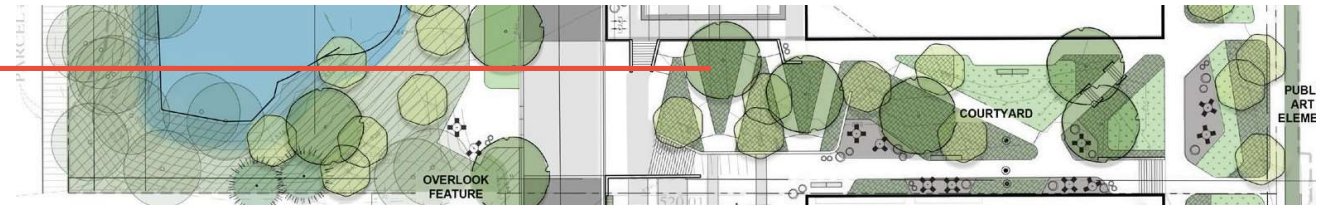
ADDING DENSITY WITH STRONG PUBLIC REALM

- Density supporting current and future transit
- Best Land Use Practices
- Promotes multiple transportation options



PUBLIC GREEN SPACE

- Supports healthy outdoor human activity
- Strong urban tree canopy throughout the site



STORMWATER

- Improvement from existing surface asphalt conditions
- Holding capacity for large precipitation events
- Improved quality prior to returning to groundwater



NATIVE LANDSCAPING

- Supporting the natural ecosystem

ELECTRIC VEHICLE CHARGING & CAR SHARING

SOLAR READY

ORGANICS RECYCLING

HEALTH & WELLNESS IN BUILDING DESIGN





10 AM - DECEMBER 21



12 PM - DECEMBER 21



3 PM - DECEMBER 21



10 AM - MARCH / SEPTEMBER 21



12 PM - MARCH / SEPTEMBER 21



3 PM - MARCH / SEPTEMBER 21



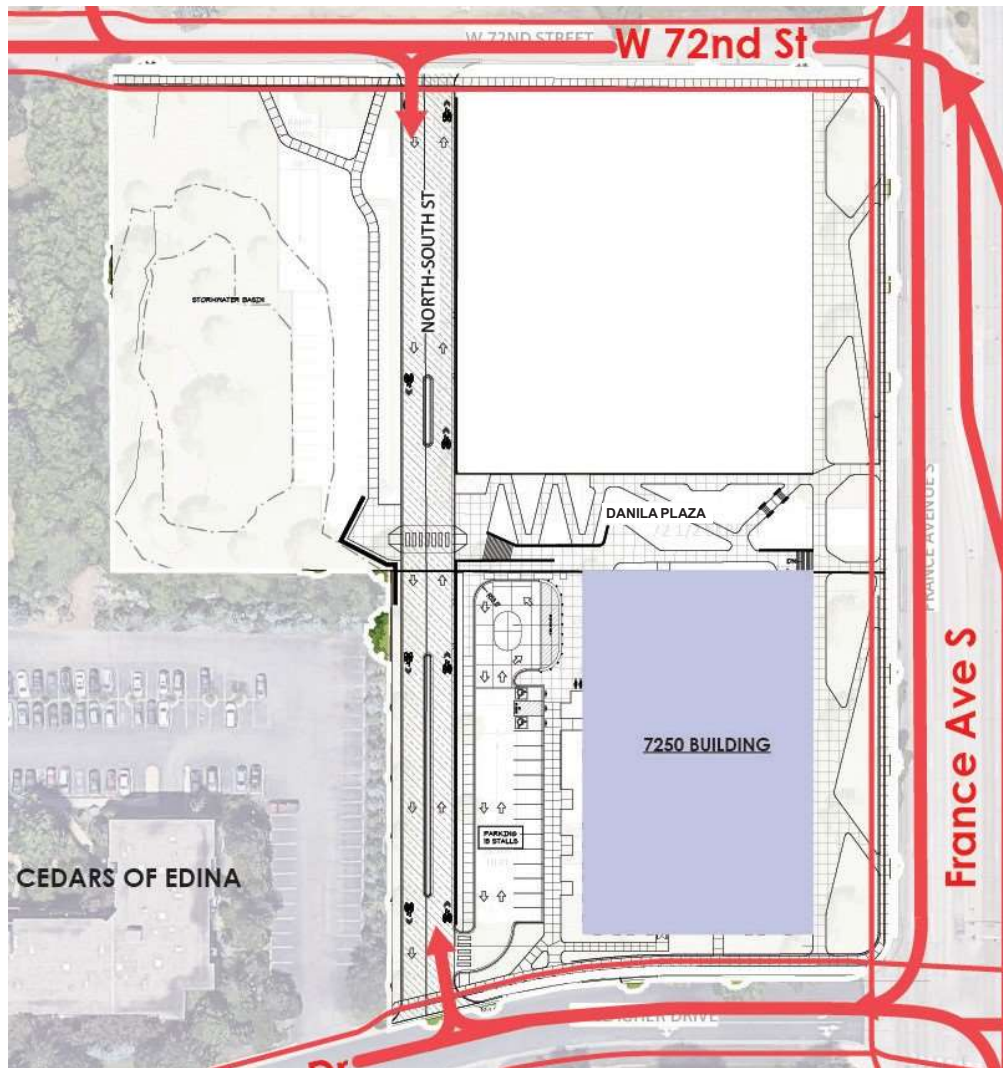
10 AM - JUNE 21



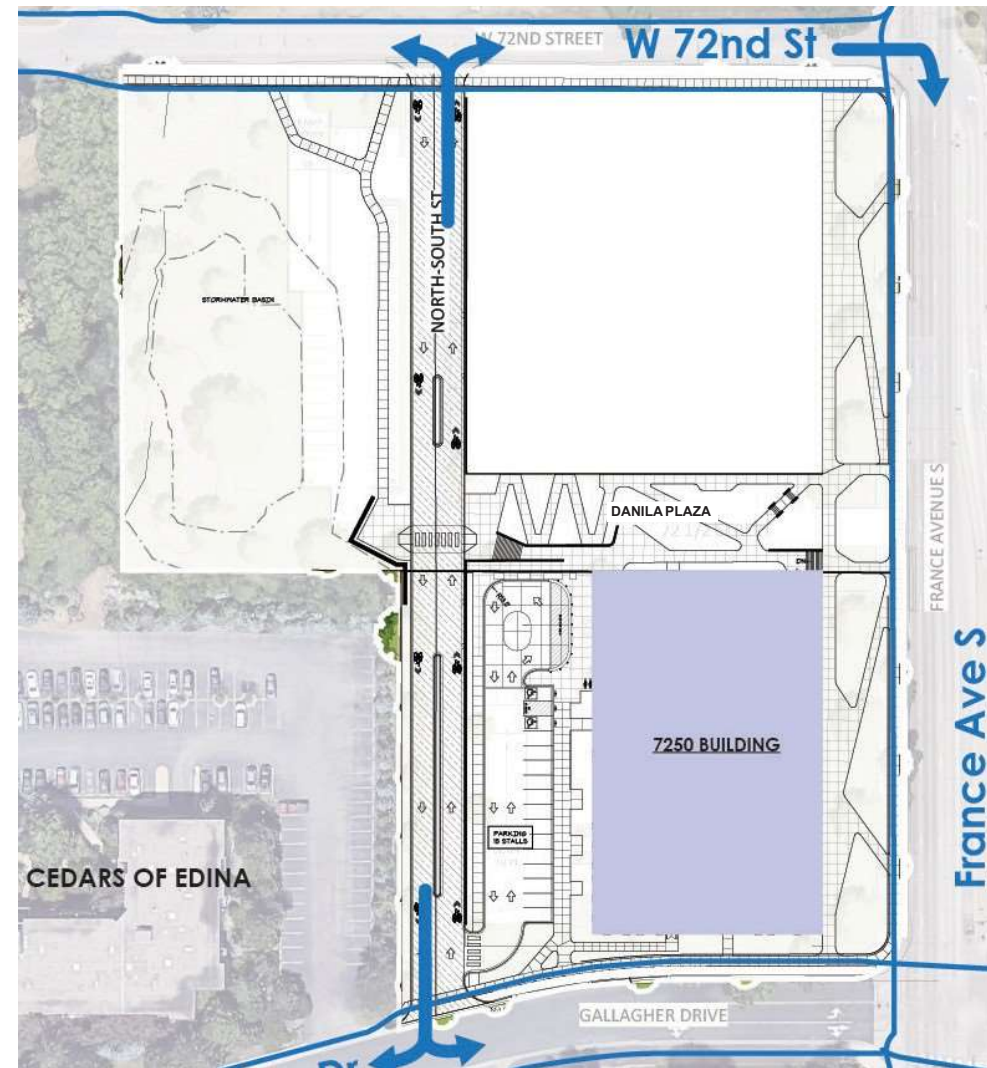
12 PM - JUNE 21



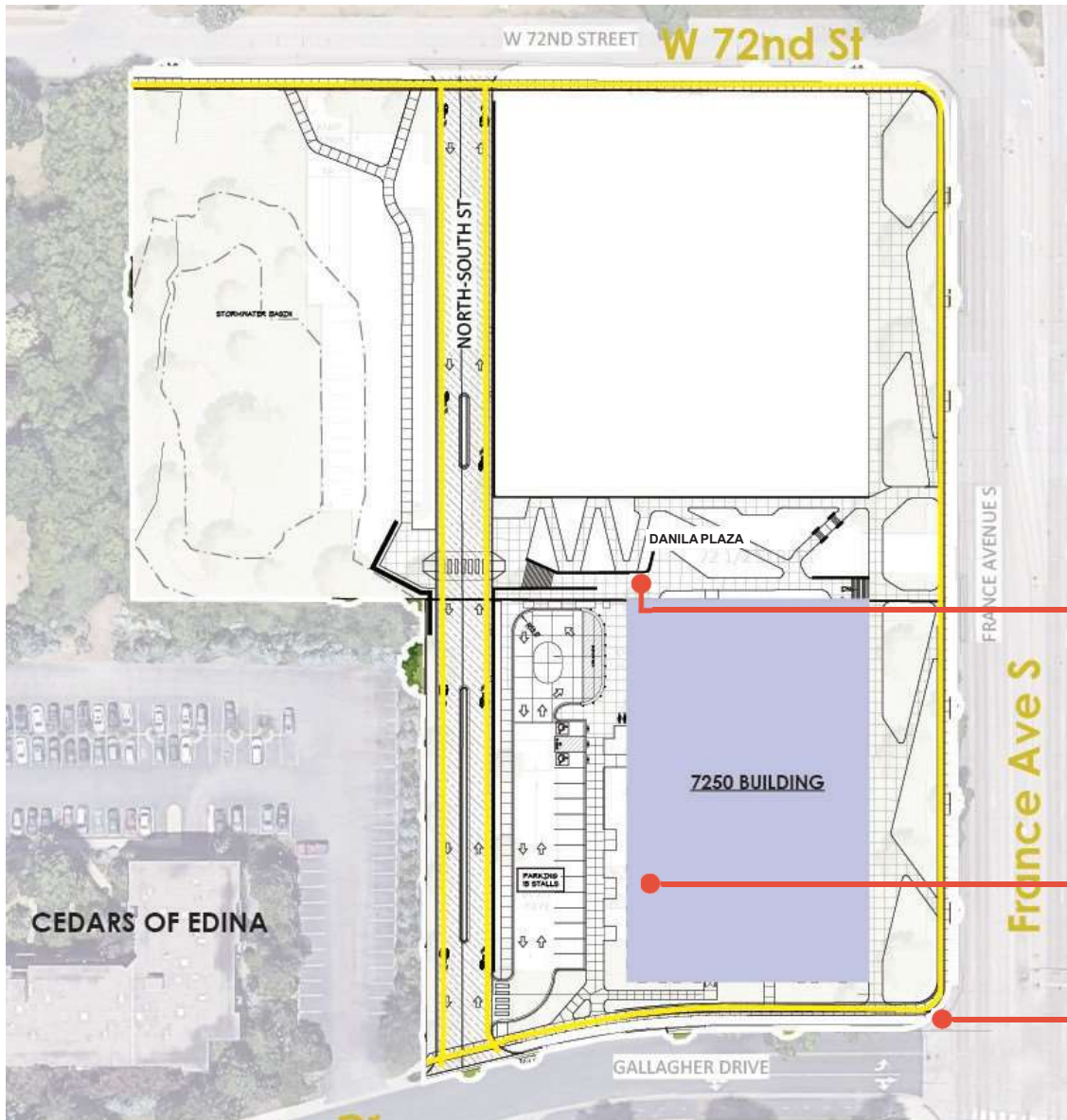
3 PM - JUNE 21



SITE ENTRANCE



SITE EGRESS

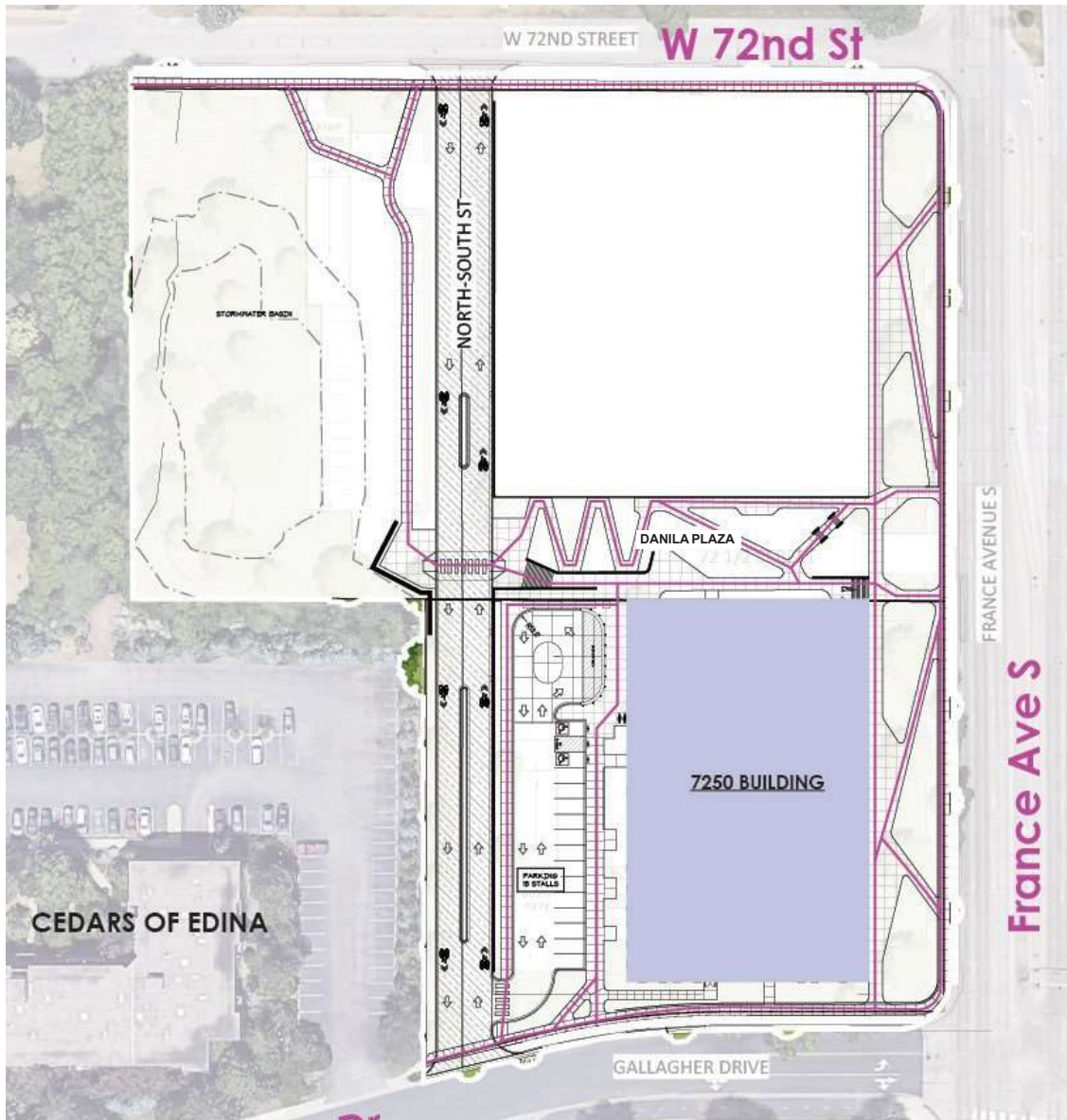


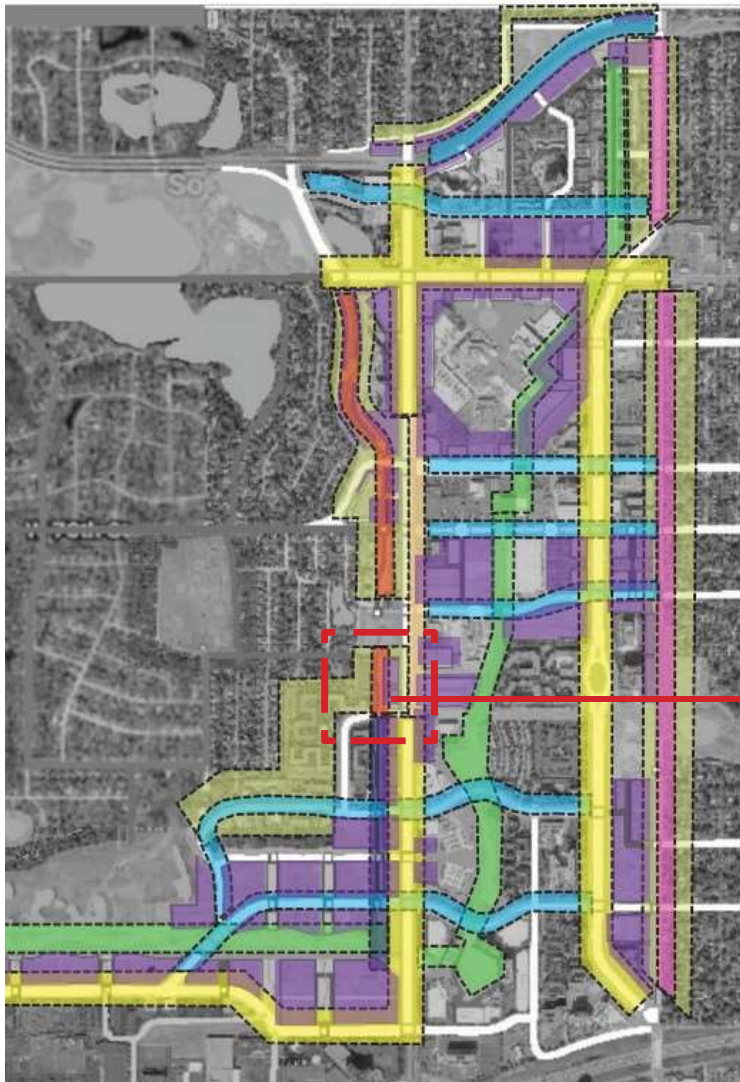
DANILA PLAZA - 6 BICYCLE
PARKING STALLS



7250 BUILDING - 24 BICYCLE
PARKING STALLS

ACCESS AND CONNECTION
TO NINE MILE CREEK
REGIONAL TRAIL





- Transition Zones
- 1A West Promenade
- 1B West Promenade (South)
- 1C East Promenade
- 2 Cornelia Overlay
- 3 New Local Streets
- 4 Primary East-West Streets
- 5 Boulevards
- 6 Central Promenade Spine

SITE:
7200 & 7250

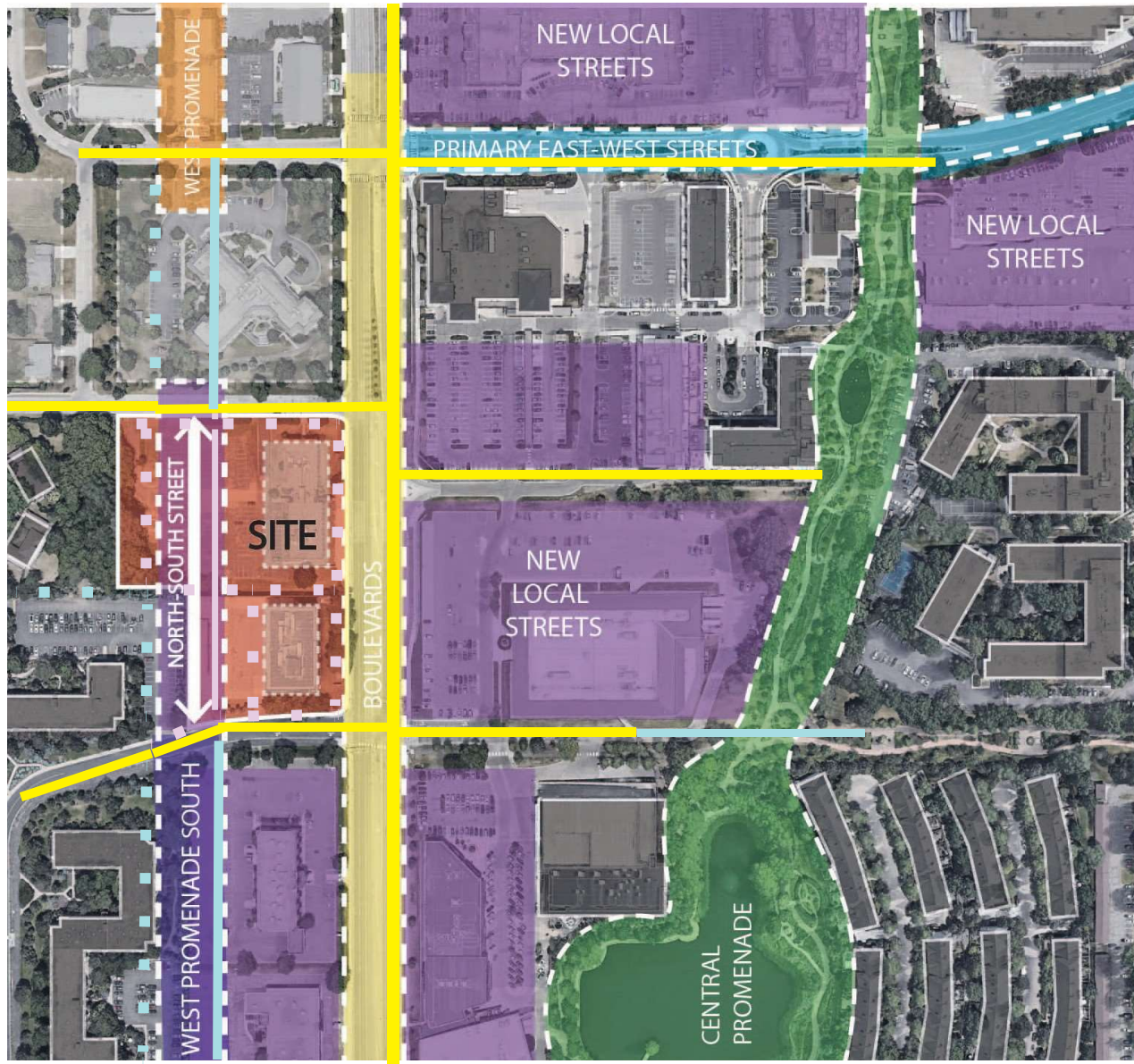
APPLICABLE STREET ROOM TYPOLOGIES:

STREET ROOM TYPOLOGY 1A:
WEST PROMENADE / TRANSITION TO CORNELIA
NEIGHBORHOOD

STREET ROOM TYPOLOGY:
TRANSITION ZONE

STREET ROOM TYPOLOGY 3:
NEW LOCAL STREETS

SOUTHDALE DESIGN GUIDELINES DISTRICTS



- EXISTING STREET GRID
- CURRENTLY PROPOSED STREET GRID
- POTENTIAL FUTURE STREET
- PROPOSED PEDESTRIAN CONNECTIONS
- POTENTIAL FUTURE PEDESTRIAN CONNECTION

NINE THINGS TO REMEMBER:

1. Every new development begins with the 200' x 200' block, or some variation based on context.

The existing property has been broken down into three smaller blocks by new internal streets. The arrangement of blocks will provide a density appropriate for a transitional area in proximity to residential areas, while providing a mixed-use environment with walkable streetscapes and attractive greenspaces.

2. Every block or building in a development will need streets to connect between buildings. Not all of these streets will need to accommodate vehicles, providing the opportunity for parks, plazas or courtyards—important parts of the public realm.

Danila Plaza is proposed as a pedestrian-only internal street within the redevelopment. This redevelopment creates small blocks with pleasantly-arranged urban public and private spaces while enhancing connections within the broader district. Danila Plaza extends beyond the North-South street to provide additional access to the greenspace amenity surrounding new stormwater basin along the west side of the site.

3. Buildings will not be greater than 200 feet in length, thereby minimizing the negative impact continuous walls can have on a comfortable pedestrian experience.

Buildings designed for 7200 and 7250 will comply with the intent of the 200 ft maximum requirement. Given the importance of providing enclosed parking within the district, the project proposes enclosed parking at both buildings. The proposed building at 7250 is 140 feet wide and 7200 is intended to be of similar massing. In order to maximize efficiency of the floor plan and to keep the parking podium at 7250 below grade, the length of the building is roughly 220 feet. This length is interrupted by a 90 degree jog in the floor plate at the building's mid point to break up the expression of the elevation. The footprints of the upper levels of office are about 134 feet by 218 feet to allow for a floorplate of approximately 29,100 SF.

Careful attention and thought have been paid to the step-backs, footprint, and orientation of the 7250 building. Placing the buildings along the east side of the development assures that building shadows won't negatively impact the neighboring residential properties.

The building massing has been developed in close coordination with the Southdale street typology guidance to ensure that setbacks of building bulk meet the intent of this pedestrian-focused document. A mixture of material, architectural detailing highlighting entry locations, and transparency through glazing will be leveraged to break down the opaqueness of the buildings at the pedestrian level.

4. All streets are not equal. The plan outlines a hierarchy that is driven by the kind of experiences that are expected on these streets and how they facilitate an enlivened public realm.

France Avenue is a major street artery that lines the site to the east. Access to the project is primarily from 72nd Street and Gallagher Drive via a new internal North-South Street. Additionally, a new East-West pedestrian-only street (Danila) provides an accessible means of accessing both blocks at the mid-point of the site. This pedestrian walkway will be a highly landscaped, walkable, sustainably focused site feature that provides pedestrian scale entry elements to each of the mixed-use portions of the project as well as a flexible indoor / outdoor spaces in various places for seasonal programming. Refer to the typology diagrams throughout this document and other illustrations that depict this hierarchy and the experience of each street typology.



NINE THINGS TO REMEMBER:

5. Designated transition zones are about maintaining the quality of life in these areas without restricting growth in other parts of the district.

Transition zones are critical to neighborhood building, especially when transforming a suburban environment to an urban environment over time with phased development projects. The creation of the dedicated pedestrian street and greenspace surrounding the stormwater basin within this project will create a graceful transition from the residential neighborhoods located to the west, to the commercial areas located east of France. By introducing a new local street grid via the new internal North-South street, this development may catalyze future growth to the north and south of the site by extending street in these directions.

6. Promenades and East-West Streets are the bridge between single family neighborhoods, such as the Cornelia neighborhood of Edina and the west side of Richfield, to more intense parts of the district.

A dedicated pedestrian east-west link (Danila Plaza) from France Avenue to the new natural area will be provided. The master plan provides a positive pedestrian experience throughout the site, connecting pedestrian movement to green space, the pedestrian plaza, and providing a clear pedestrian hierarchy to movement in and around the site. The project provides the beginning of a strong neighborhood pedestrian circulation network that connects to existing and future development in all directions.

7. Street Rooms will intersect and overlap each other in many circumstances. At these intersections, lower building heights should prevail, giving the smaller scaled building precedence over larger scale buildings.

The pedestrian street (Danila Plaza) is strategically located to provide a connector between all 3 sectors of the site, and ultimately acts as a large street room gathering place for neighbors, tenants, and visitors. This area will be the gathering focal point for the many pedestrian connections, acting as a collector to bring people together.

8. Within the first 60 vertical feet of a building, primary materials systems that are more traditional like brick, stone, glass wall systems are preferred. Above 60 feet, other materials such as metal wall systems within a larger curtainwall system, can be introduced. These baseline parameters should not be a deterrent to architectural innovation but rather are intended to serve as a measure of quality and continuity throughout the district.

The primary exterior materials proposed for the 7250 building are architectural precast with stone texture and glass at the ground level. The upper levels are clad in glass, architectural brick, and metal panel accents.

9. Transparency at the ground level facing the public realm is key to the individual experience and is a catalyst for how to activate and maintain a community-based approach to daily life and experience.

At the pedestrian level along France Avenue, the commercial space is articulated by bays of glass book-ended with stone-look architectural precast panels. Full height glazing within these bays allow unobstructed views of activity within the building and define points of entry. The primary entry and lobby will be located on the north end of the west facade adjacent to a limited amount of convenience surface parking and awning offering protection from inclement weather. This lobby also provides access to the shared elevators for the below grade parking and office as well as shared amenities for the office building such as conferencing, and a coffee/food kiosk.

ADA-accessible paths allow for pedestrians to move freely across the site to engage with all areas of the site including the public natural area surrounding the stormwater basin as well as the future building at 7200.

RELEVANT STREET ROOM TYPOLOGIES:

Street Room Typology 1: WEST PROMENADE / TRANSITION TO CORNELIA NEIGHBORHOOD

- On France Avenue, a 50-foot setback is required from curb to face of building with a building podium height of 60 feet. Above the 60-foot height limit, additional height should step back 10 feet from the face of the building, to a maximum height of 84 feet.

Building meets the required 50'-0" setback along France. Additionally, the building facade changes materiality at roughly 62 ft above street level and then steps back 37 ft at the north-east corner Level 5 Amenity deck. The building height to top of mechanical penthouse is 84'-0".

- On the east side of the West Promenade, building faces should not exceed 50' in height. Any height above that limit should step back 20 feet from the facade of the building.

The west building facade rises 48'-0" and then steps back 18'-6".

- All parking, other than short-term retail or guest parking, and building services need to be located below grade or hidden within the building. If on ground level or above, parking and/or building services must be surrounded on all sides by program space such as commercial or housing.

Proposed parking is enclosed within buildings with the exception small surface parking lot provided for enhanced pedestrian safety and ease of access.

Street Room Typology 3: NEW LOCAL STREETS

- On France Avenue, a 50-foot setback is required from curb to face of building with a minimum building height of 60 feet (diagram at left). Above that 60-foot height, the building face should step back two feet to create a cornice line and can then extend to 105 feet. Above 105 feet, building faces must step back an additional 10 feet (as illustrated in diagram at right, above.)

Building exceeds required 50'-0" setback along France. Additionally, the building facade changes materiality at roughly 62 ft above street level and then steps back 37 ft at the north-east corner Level 5 Amenity deck. The building height to top of mechanical penthouse is 84'-0".

- Building podiums along these streets need to maintain as closely as possible the 60-foot height limit while still adhering to the guidance of 75% of building face at the setback line to create the fundamental experience of the street room.

Building is located on or within 2'-0" of the setback line on France and Gallagher.

- All parking, other than short-term retail or guest parking, and building services need to be located below grade or hidden within the building. If on ground level or above, parking and/or building services must be surrounded on all sides by program space such as commercial or housing.

Parking is enclosed and primarily located below grade. Small surface lot near main entrance of 7250 provides ease of access.

- Parking and building services should not be accessed via these streets.

No additional curb cuts are proposed along France Avenue. No building access for vehicles or loading is proposed on France Avenue.

- Incorporate 10- to 12-foot wide sidewalks that create opportunities for gathering, outdoor cafes, pavilions, etc. **Sidewalks along France Avenue are ample at greater than 15 ft in overall width (7-8 ft each) as it is a double sidewalk proposal allowing for plaza space and outdoor seating. Pedestrian street Danila Plaza is 60'-0" wide and incorporates pedestrian paths, stairs and ADA-compliant ramps as well as a series of seating areas. Pedestrian path along west side of the North-South street provides seating areas and an overlook feature to enjoy the preserved natural area. For other sidewalk dimensions see diagram in this document.**

- Within the 50-foot setback, trees should be planted in a double row to add a strong canopy for pedestrian activity. **Trees are planted in double rows on France and 70th, see site plan for arrangement.**

THE PUBLIC REALM EXPERIENCE:

Connections

- The overall strategy is to connect intersections, incorporate street typologies, and incorporate green systems to add value to the experience of the district.
- The public realm is to be connected continuously north-south from Centennial Lakes, the Promenade, the Galleria, Southdale Center, Fairview Southdale, to Strachauer Park. All new development shall support that goal.
- The public realm should be connected east-west from Edinborough Park to Centennial Lakes, west of France to Pentagon Park and Fred Richards Park to Highway 100 on the west. This will set up future connections to districts to the west—such as 70th and Cahill—supporting an overall vision of a more connected and integrated Edina community.
- The district must be connected continuously east-west from the Cornelia neighborhood to Yorktown Park.
- New north-south promenades should be created on the west side of France and east side of Xerxes as part of the broader strategy to sensitively transition to single family residential neighborhoods.
- Expand Centennial Lakes Park to France Avenue... celebrate this important public amenity by making it more visible as a gateway into the district.
- Create a dynamic landscape that includes water, especially stormwater expressed as part of landscape, to create public amenity spaces.
- Streets within the superblocks, East and West Promenades, and extension of the Promenade north to Strachauer Park should be surfaced with pavers to promote a dominance of pedestrians and bikes over vehicles.
- New parks and plazas shall be either public or publicly-accessible, not private, in nature.
- Increase number of sidewalks, pathways, and smaller parks/gardens to better address mobility. Incorporate places to sit throughout the district.
- New trees should provide continuity of the street room experience with canopies that are consistent with the Street Room Typology to enhance the continuity of pedestrian experience.

The landscape and hardscape elements are woven through the site to create a human-scaled, enticing, yet comfortable pedestrian experience. Native plantings throughout the site create a low maintenance, attractive greenspace. Planting beds runs parallel with France Ave and act as both a buffer from the adjacent traffic and a functional means of managing on-site storm water. The native planting gardens also provide an educational opportunity to highlight the City of Edina's laudable goals for environmental stewardship. Landscaped areas with benches and public art elements create publicly accessible gathering areas while also serving to activate the building's street-facing façades.

The development team has paid close attention to how this site interfaces with the adjacent properties to the west by ensuring that building scale, landscaping and proposed uses respect the adjacent neighborhood. We've also incorporated multi-modal street grids designed to connect with existing and future bike, pedestrian, and vehicle infrastructure.

Intersections

- Street Room Typologies overlay each other, unifying the overall district experience through the recognition of unique conditions that evoke unique design responses based upon location.
- Street Room Typologies connect intersections throughout the district, linking experiences together from one neighborhood to the next.
- Street Room Typologies with lower façade heights take president over those with higher façade heights at these points of intersection.
- The architecture of a façade of one block making up an intersection should be conceived as part of all corners of the intersection.
- Crosswalks at intersections need to be an integral part of the public realm and continue the overall street room experience from one block to another.
- The hierarchy of intersections will change based on an evolving context and investment in the intersection experience.

The intersection of Gallagher & France is an important point of connection for vehicles, bikes, and pedestrians alike. A robust entry plaza with decorative paving, landscape and lighting will identify the building entry. A highly visible building corner at street level will encourage activity in around the building and act as a welcoming sign to the traffic moving by sharing the character of the development and activity within.

Street Room Form

- Building setbacks are to be considered as a part of the overall landscape and public amenities, and should be designed to create a continuous pedestrian experience along major corridors to support “pools of human activity.”
- Every new development should connect all publicly-accessible spaces such as pocket parks, courtyards and plazas to the street room typology.

The site design provides the beginning of a strong neighborhood pedestrian circulation network that connects to existing and future development in all directions. We provide a safe and enhanced pedestrian and bike movement along France avenue separating and setting back the pedestrian from France moving the sidewalk closer to the building and buffering a lush green infrastructure landscape that will be both function and educational. Provided a human-scaled street grid by introducing Danila Plaza at the midpoint of the site: a pedestrian-only path with ADA-compliant ramps, stairs, benches and landscaping from France to the new North-South local street and to the new storm-water basin public nature area on the west side of the site.

- Along all major corridors, seventy five percent (75%) of face of building walls need to be at the setback line to support the creation of a ‘street room.’
- All new building façades in the district must have seventy five percent (75%) transparency at the ground level.

Project aims to comply with this stipulation. Please see exterior elevations for this submittal. Design of building facades aim to strike a balance that adequately meets both Design Experience Guidelines and the City of Edina's Sustainable Building Policy.

- All building façades are prime (including parking) and must be designed accordingly. There is no back side of a building.

We have carefully considered all facades as active and human-scaled. Majority of parking is located below grade, with one small convenience surface lot on west side of building for improved accessibility and activation of primary entrance.

- All facades on the first vertical 60 feet of a building (above grade) shall use natural materials facing the public realm.

The building uses natural materials facing the public realm in this way. Please see exterior elevations for this submittal.

- Above 50-60 feet, glass, precast panels with brick/tile are the preferred material palette. Metal panel can be used as a secondary part of a wall system.

Please see exterior elevations for this submittal.

- No building façade can be longer than 200' without changing direction by a minimum of 90 degrees.
- The longest building facade is 220'; however, this elevation breaks at the approximate mid-point of block with an inset portion of the street-level facade. Please see site plans for this submittal.**

THE PUBLIC REALM EXPERIENCE:

Building Form

- Ground floors should have a minimum ceiling height of 20' for flexibility. This floor-to-floor height will allow the space to accommodate commercial, two floors of parking, or two-story townhouses.

First floor building height is 20 ft tall at 7250

- Above-grade parking structures should be designed with flat floorplates to allow for future conversion and lined with programmable public realm space to minimize the visual impact of car storage.

Parking is enclosed and primarily located below grade. Small surface lot near main entrance of 7250 provides ease of access.

- Within 50-60 feet of the ground, it is preferred that rooftops be programmed to accommodate residential or public user activities (e.g. a restaurant or terrace).

Rooftop located with 50-65 ft of the ground level are programmed to offer rooftop terraces and green spaces. On the street level, the pedestrian plaza (Danila) is designed to create flexibility for a variety of uses, including providing additional outdoor seating area for the office coffee kiosk and building amenity conferencing center.

- All development services, including rooftop mechanical systems, should be located within buildings and should not be visible from the public realm, or semi-private and private areas of the development. The exception are rooftop-mounted solar panels, which should be located on the highest point of the buildings.

The development has all rooftop mechanical systems planned as screened from the public realm, or semi-private and private areas. The tallest mechanical equipment (i.e. cooling tower) is located on top level and projects through roof in order to reduce overall building height.

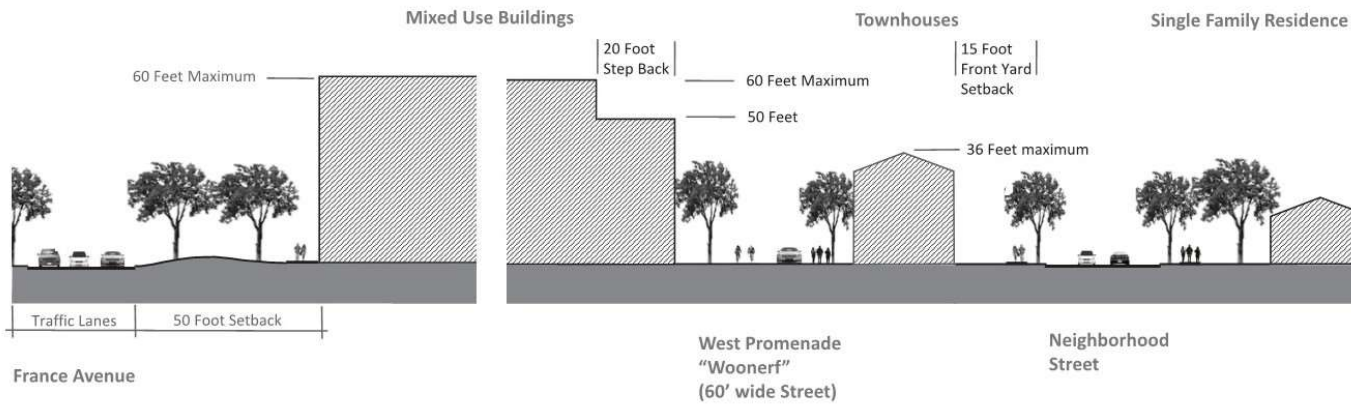
- Building footprints above 60 feet should be no greater than 12,000 SF for residential use and 24,000 SF for commercial space.

Our office floorplates vary from 21,745 - 26,200 GFA. These floorplate sizes are a result of careful planning around utility consumption, natural light entering space, creating connections within space, and maximizing efficiencies.

- Design buildings for flexibility and adaptability in the future, including use of structural systems that will allow a building's function to fundamentally change.

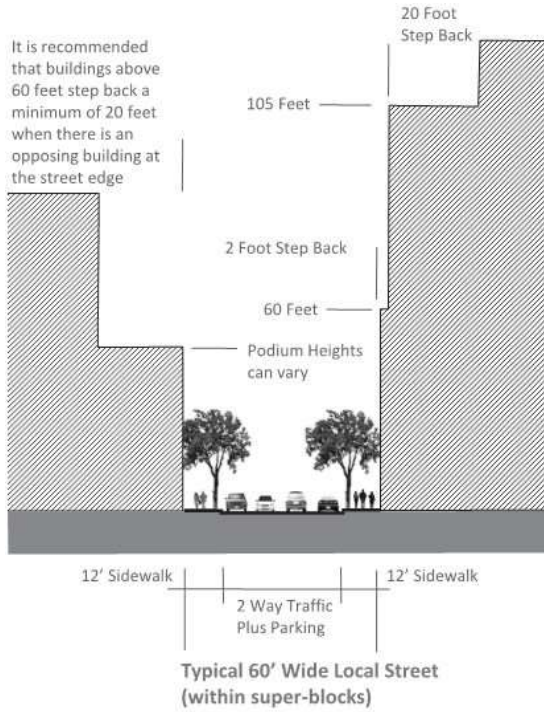
The development team has planned to accommodate a variety of users on the first floor, including but not limited to office, small commercial, and small retail. Allowing for future flexibility has been a cornerstone of our design ideas and is reflected in our structural bay sizing and construction type selection.





STREET ROOM TYPOLOGY 1A:
WEST PROMENADE / TRANSITION TO CORNELIA
NEIGHBORHOOD





STREET ROOM TYPOLOGY 3: NEW LOCAL STREETS



DRAWING SET (SUBMITTED AS SEPARATE DOCUMENT)

		08/12/22 - P.U.D.
DRAWING NUMBER	DRAWING NAME	
ARCHITECTURAL		
T1.1	TITLE SHEET	•
CIVIL		
C0.0	TITLE SHEET	•
C1.0	PH.1 REMOVALS PLAN	•
C1.1	PH.2 REMOVALS PLAN	•
C2.0	PH. 1 SITE PLAN	•
C2.0A	P1(A) SITE PLAN	•
C2.0B	P1(B) SITE PLAN	•
C2.1	PH. 2 SITE PLAN	•
C3.0A	PH. 1 (A) GRADING PLAN	•
C3.0B	PH 1 (B) GRADING PLAN	•
C3.1	PH. 2 GRADING PLAN	•
C4.0A	PH. 1 (A) UTILITY PLAN	•
C4.0B	PH 1(B) UTILITY PLAN	•
C4.1	PH. 2 UTILITY PLAN	•
C5.0	CIVIL DETAILS	•
C5.1	CIVIL DETAILS	•
C5.2	CIVIL DETAILS	•
SW1.0	SWPPP - EXISTING CONDITIONS	•
SW1.1	PH. 1 SWPPP - PROPOSED CONDITIONS	•
SW1.2	PH. 2 SWPPP - PROPOSED CONDITIONS	•
SW1.3	SWPPP - DETAILS	•
SW1.4	SWPPP - NARRATIVE	•
SW1.5	SWPPP - ATTACHMENTS	•
SW1.6	SWPPP - ATTACHMENTS	•
V1.0	SITE SURVEY	•
V1.1	SITE SURVEY	•
LANDSCAPE		
L1.1A	LANDSCAPE SITE PLAN - PHASE 1A	•
L1.1B	LANDSCAPE SITE PLAN - PHASE 1B	•
L1.2	LANDSCAPE SITE PLAN - PHASE 2	•
L1.3	LANDSCAPE SITE PLAN - ADA ROUTES	•
L2.0	LANDSCAPE PLANTING PLAN	•
ARCHITECTURAL		
A0.1	EXISTING SITE PHOTOS (7200 & 7250)	•
A0.2	SITE PLAN - DEMOLITION (7200 & 7250)	•
A0.3A	SITE PLAN - PHASE 1A	•
A0.3B	SITE PLAN - PHASE 1B (7200 & 7250)	•
A0.4	SITE PLAN - PHASE 2 (7200 & 7250)	•
A0.5	SITE SECTION DIAGRAMS - PHASE 1 (7200 & 7250)	•
A0.6	SITE SECTION DIAGRAMS - PHASE 2 (7200 & 7250)	•
A0.7	PHASE 1 AND 2 SITE RENDERINGS	•
A0.8	EXTERIOR RENDERINGS (7200 & 7250)	•
A0.9	EXTERIOR RENDERINGS (7200 & 7250)	•
A0.10	EXTERIOR RENDERINGS (7200 & 7250)	•
A1.1	7250 FLOOR PLANS	•
A1.2	7250 FLOOR PLANS	•
A1.3	7250 FLOOR PLANS	•
A1.4	7250 FLOOR PLANS	•
A2.1	7250 EXTERIOR ELEVATION	•
A2.2	7250 EXTERIOR ELEVATION	•
A2.3	7250 EXTERIOR ELEVATION	•
A2.4	7250 EXTERIOR ELEVATION	•
A2.5	7250 EXTERIOR MATERIALS	•
ELECTRICAL		
E0.1P	ELECTRICAL SITE PHOTOMETRIC PLAN	•
E0.2P	ELECTRICAL SITE PHOTOMETRIC PLAN	•
E0.3P	ELECTRICAL SITE PHOTOMETRIC DETAIL	•



SKETCH PLAN FEEDBACK & RESPONSES

- April 13th Planning Commission Meeting
- April 19th City Council Meeting

Sketch Plan Feedback/Comments Major Points:

· Address the West Promenade Street Room typology 1A:

o Incorporated multi-modal local street in north-south direction

- Bikes and vehicles share this street with striped bike lanes and vegetated median strip.
- Due to significant grade changes, the street itself was not determined to be appropriate location for pedestrians to share; pedestrian paths are closer to proposed buildings for safety and accessibility.

o West 72 ½ street (AKA Danila Plaza)

- Developed to become a pedestrian-only public realm and a network of accessible green spaces with art located at intersection with France.

· Building scale and fenestration:

o “75% of building walls to be at setback line to support creation of street room”

- When considering Danila Plaza as “street” as defined by the applicable typologies, the proposed buildings meet this requirement.

o “All building façades are prime (including parking) and must be designed accordingly. There is no back side of a building. “

- Removed grocery store from program – all facades of proposed building have been carefully considered as prime with mix of human-scaled glazing and natural materials.

o “All facades on the first vertical 60 feet of a building (above grade) shall use natural materials facing the public realm.”

- Natural materials have been selected for portions of all facades within first 60 feet of building above grade.

o “The landscape of the West Promenade should reinforce the characteristics of the neighborhood: tree lined to make sidewalks pleasant and safe to be on, creating a green vertical street room that is always pleasant to walk down”

- Danila Plaza connects the greater Southdale district through this site with a pedestrian-scaled greenway featuring a mix of native landscape, paths with seating, and spaces primed for community events and art installations. This path connects further west to the beautified retention basin nature area, and to the north-south accessible pedestrian path connecting 72nd Street to Gallagher.

LANDSCAPE & PUBLIC REALM:

As the Greater Southdale Area Plan states, “a framework emerges for how streets and the public realm will be structured, the relationship of open space to buildings, and how together the designed environment will support the desired experience outcome.”

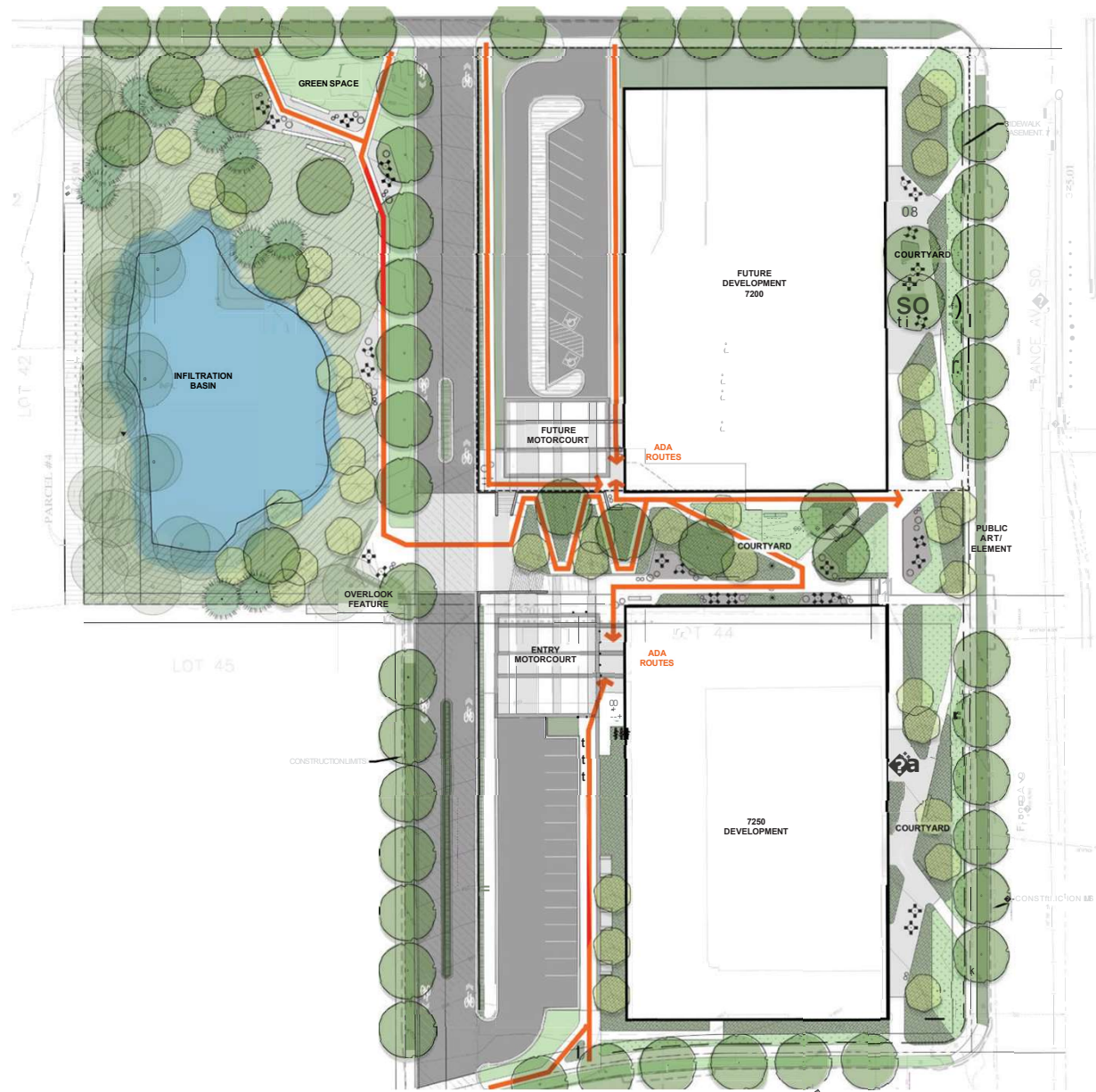
The landscape design put forth as part of this development establishes a public realm that responds to the districts design guidelines. Developing street rooms, creating community space as a focal point, strengthening connections, and addressing the human scale through aesthetic quality, safety, and promotion of social interaction.

The design is organized around human activity, with vibrant pedestrian-focused streets, beautiful public spaces, and highlights storm water as a valued resource by making it part of the experience of the development. A more inviting walking experience along the streets is created through wide sidewalks and dedicated pedestrian street (Danila Plaza), with seating areas interspersed along the ADA accessible ramp that allows pedestrians to easily move throughout entirety of site.

These spaces between the buildings establish a network of green spaces that support the health and wellbeing of the community. The desired “Street Room Typologies” connect the development, linking experiences together from one end of the site to the other. This Street Room form create a continuous, safe, and inviting pedestrian experience along corridors and support “pools of human activity” balancing access and mobility. The streetscape is comprised of planting beds with shrubs, perennials and trees will establish the buffer along all public sidewalks between street and walk increasing permeable surface area, providing a shade canopy, and reducing solar heat gain.

Plants selected for the Street Rooms are species that will tolerate the harsh conditions of their environment. Trees and plants within these areas are salt-tolerant which help to improve their longevity. Several of the tree species selected for the rest of the site are either native to Minnesota or are adapted species of natives that are better suited for their location or condition.

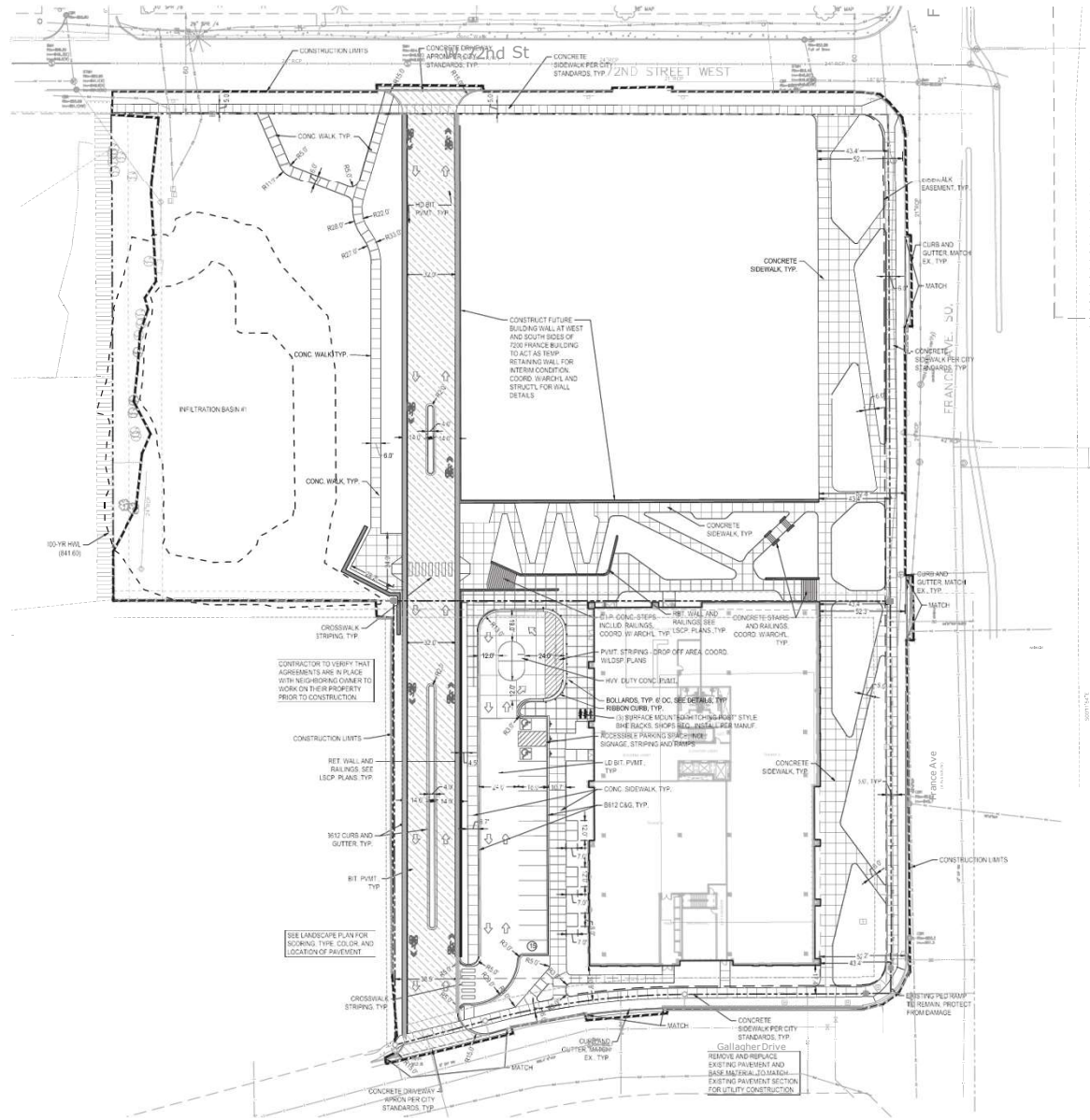


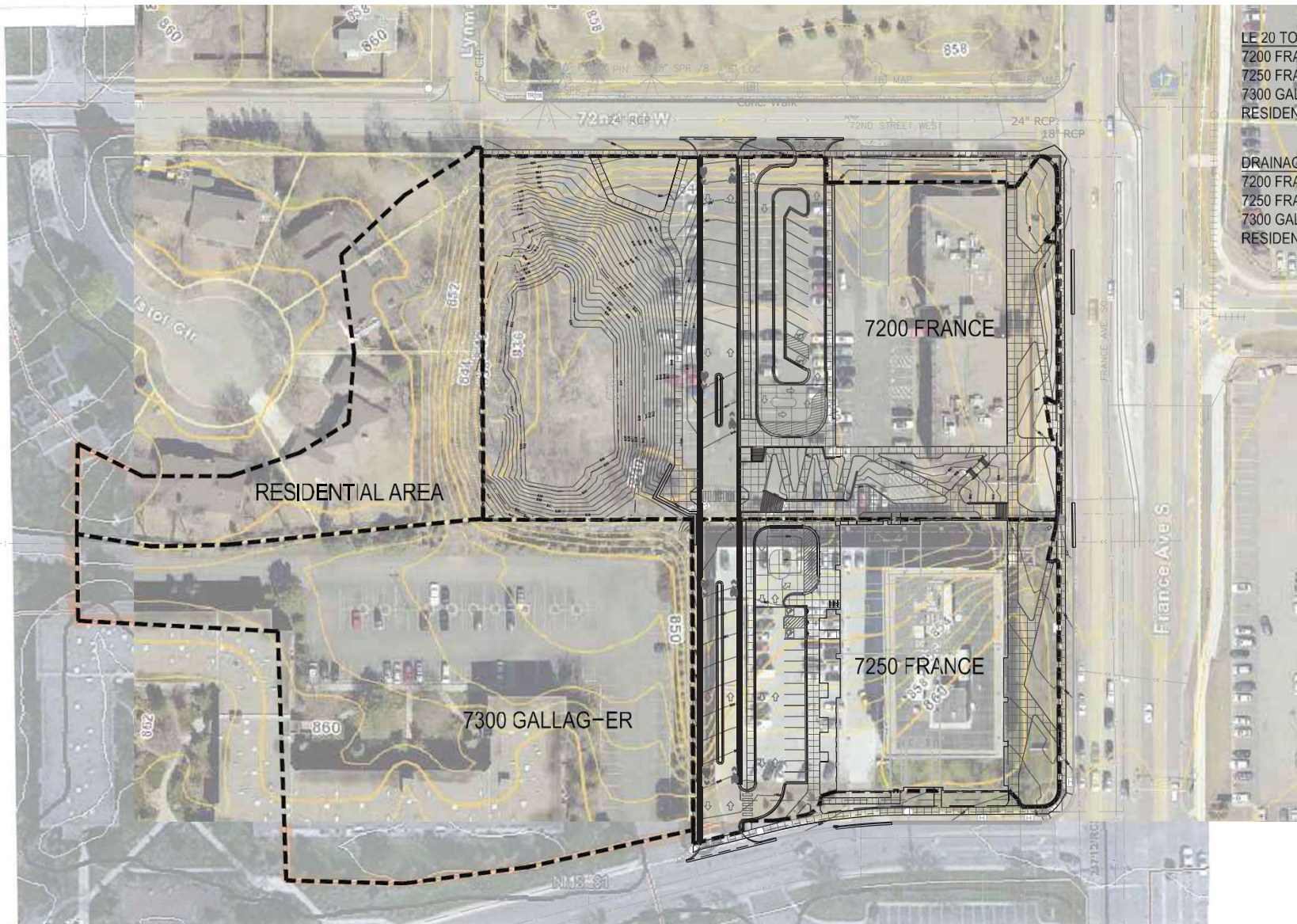


The proposed site layout includes placement of one building separated from a future building pad by a pedestrian-only connection. On-street parking, lined with landscaping will allow access and passage for pedestrians and vehicles to easily access the front entry. Crosswalks and signage will allow for controlled crossing of pedestrians with vehicles and connect to surrounding trails and public pathways. Parking for the proposed building will be met with underground parking and limited surface parking.

Grading around the site will match in with existing grades along the perimeter. All sidewalks and plazas within the site will be graded to ADA standards. Surface water will be directed to the on-site stormwater infiltration/detention basin.

Sanitary and water services for the 7250 building will connect to the site along the south property line from Gallagher Drive. A proposed water line will be brought through the site from Gallagher Drive to provide the required fire hydrants and the water service for the 7200 building. The proposed sanitary service for the 7200 building will come from the north off of 72nd St. Stormwater runoff from the buildings and impervious surfaces will be directed to the on-site stormwater infiltration/detention basin. The stormwater basin is designed to provide the required water abstraction, rate control and water quality control. Large rainfall events will be pumped to the existing storm sewer system in 72nd St. from the stormwater basin.





LE 20 TOTAL DRAINAGE AREA = 350,232 SF
 7200 FRANCE AREA = 133,980 SF (38%)
 7250 FRANCE AREA = 68,725 SF (20%)
 7300 GALLAGHER AREA = 104,076 SF (30%)
 RESIDENTIAL AREA = 43,451 SF (12%)

DRAINAGE AREA IMPERVIOUS = 214,335 SF
 7200 FRANCE AREA = 71,990 SF (34%)
 7250 FRANCE AREA = 60,772 SF (28%)
 7300 GALLAGHER AREA = 68,990 SF (32%)
 RESIDENTIAL AREA = 12,583 SF (6%)

**42% OF DRAINAGE
AREA SERVES
NEIGHBORING
PROPERTY**



1" = 80'-0"
 40'-0" 0 80'-0"

CivilSite
 GROUP

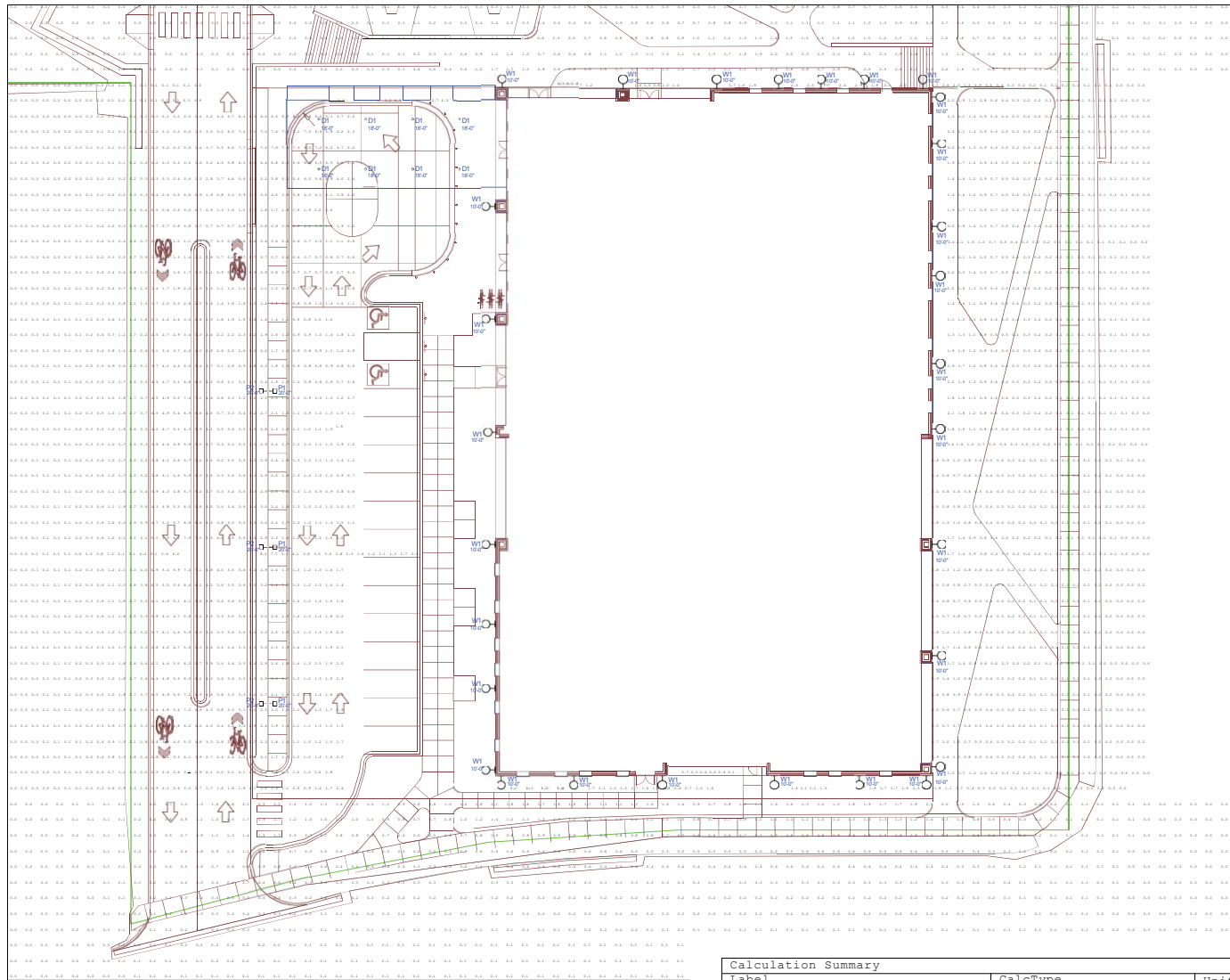
4931 W. 35TH ST., #200
 ST. LOUIS PARK, MN 55416
 952.250.2003 / 763.213.394
 www.CivilSiteGroup.com

7200-7250 FRANCE - EDINA, MN

FLOOD ZONE - PROPOSED DRAINAGE MAP

Project Number:	21419	Revision Number:	1
Issue Date:	6/30/22	Revision Date:	9/7/22

DA4



1 ELECTRICAL SITE PHOTOMETRIC PLAN
SCALE: 1"=20'

Calculation Summary						
Label	CalcType	Units	Avg	Max	Min	Avg/Min
EAST PROPERTY LINE	Illuminance	Fc	0.09	0.2	0.0	N.A.
NORTH PROPERTY LINE	Illuminance	Fc	0.00	0.0	0.0	N.A.
Site Calc	Illuminance	Fc	0.95	8.2	0.0	N.A.
SOUTH PROPERTY LINE	Illuminance	Fc	0.56	1.0	0.1	5.60
WEST PROPERTY LINES	Illuminance	Fc	0.11	0.5	0.0	N.A.
Front Drive	Illuminance	Fc	2.81	8.0	0.5	5.62













THANK YOU!





7250 - EAST ELEVATION



7250 - WEST ELEVATION



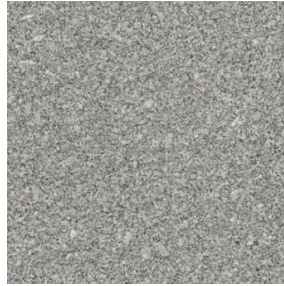
LEVEL 1 MATERIALS



HYBRID WINDOW
WALL - SSG SYSTEM



ARCHITECTURAL
PRECAST - COLOR 1



STONE BASE



METAL PANEL - COLOR 1



METAL PANEL - COLOR 2

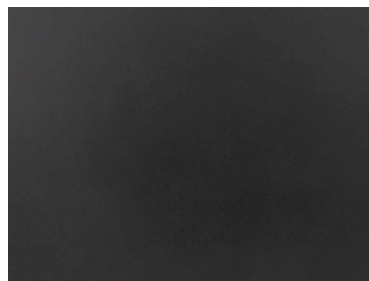
LEVELS 2-5 MATERIALS



HYBRID WINDOW
WALL - SSG SYSTEM



ARCHITECTURAL BRICK

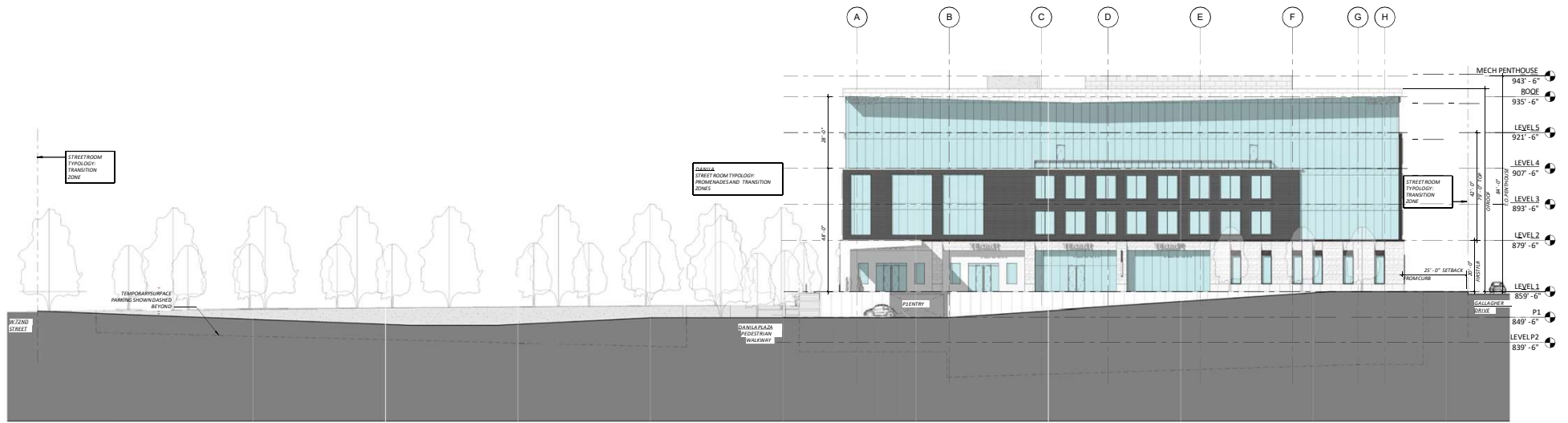


METAL PANEL - COLOR 1

ROOF MATERIALS



FLAT LOCK ZINC PANEL



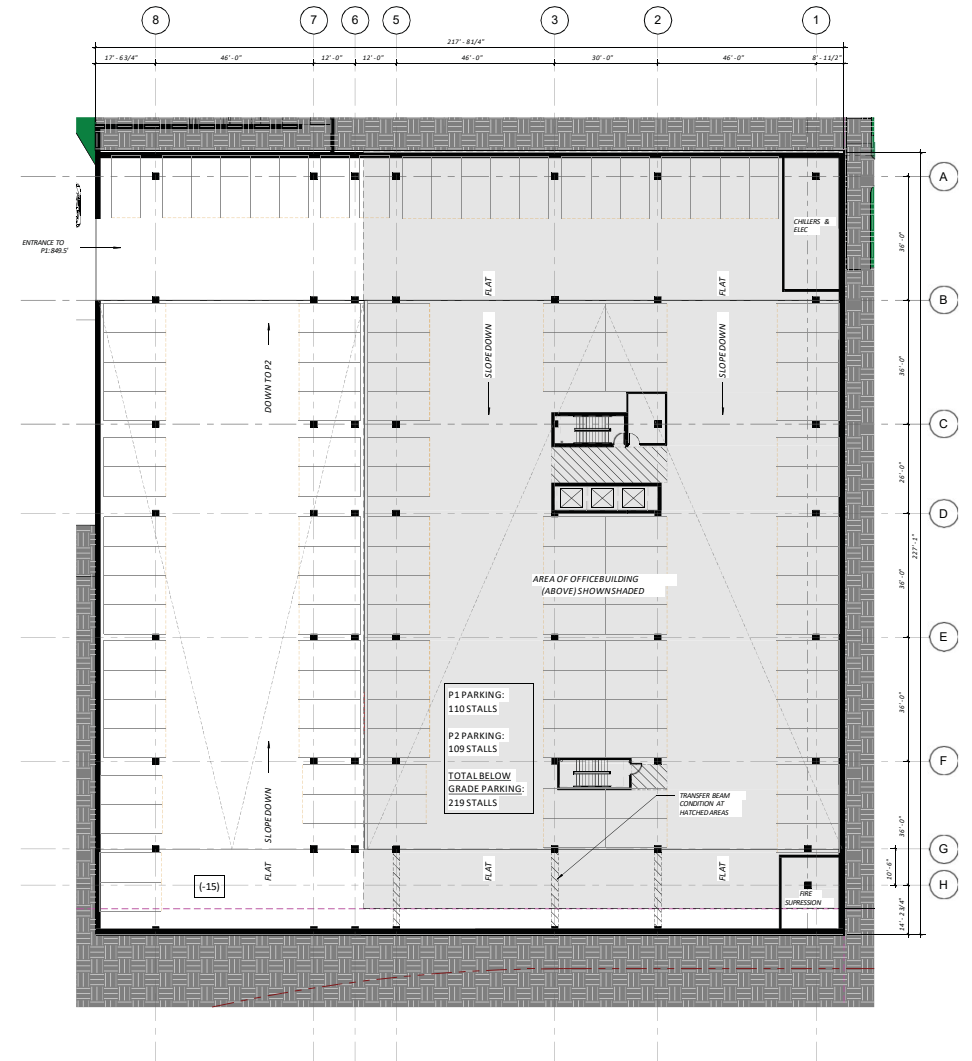
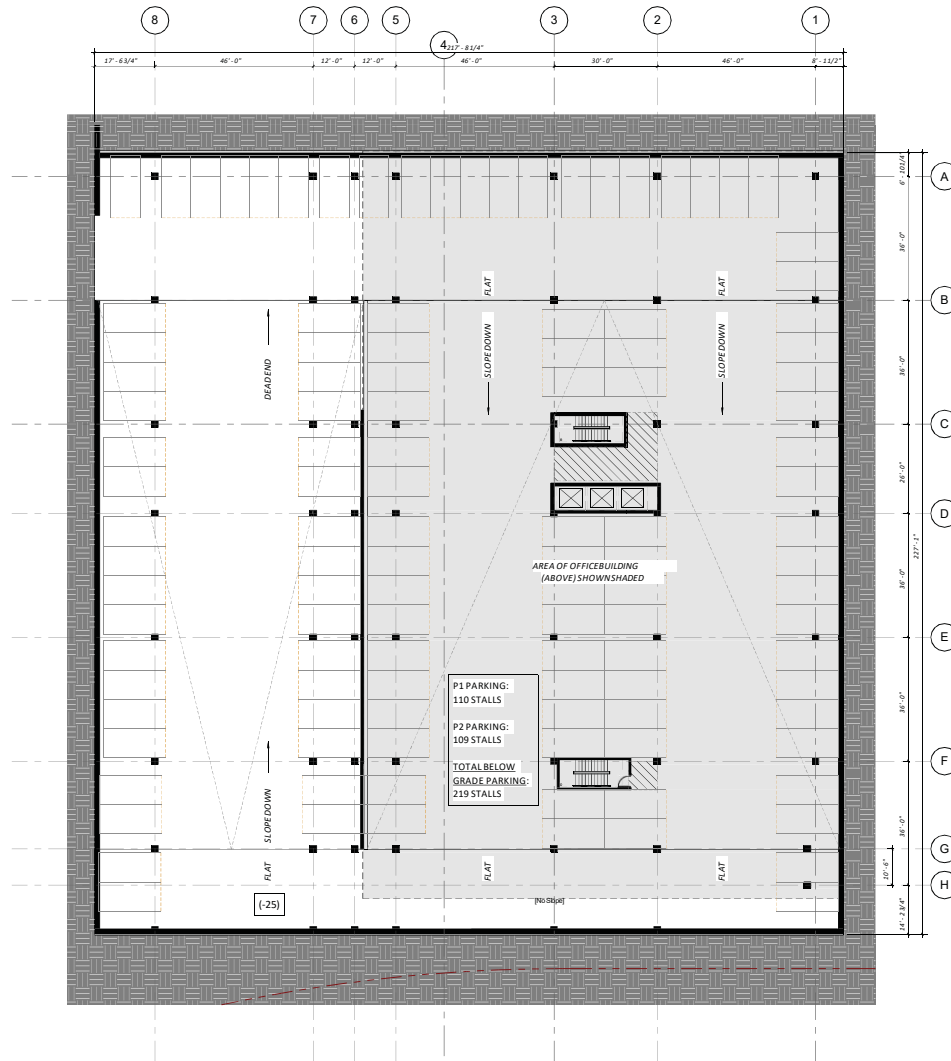
1 PHASE 1 - NORTH / SOUTH SITE SECTION
 AD 5/16" = 1'-0"

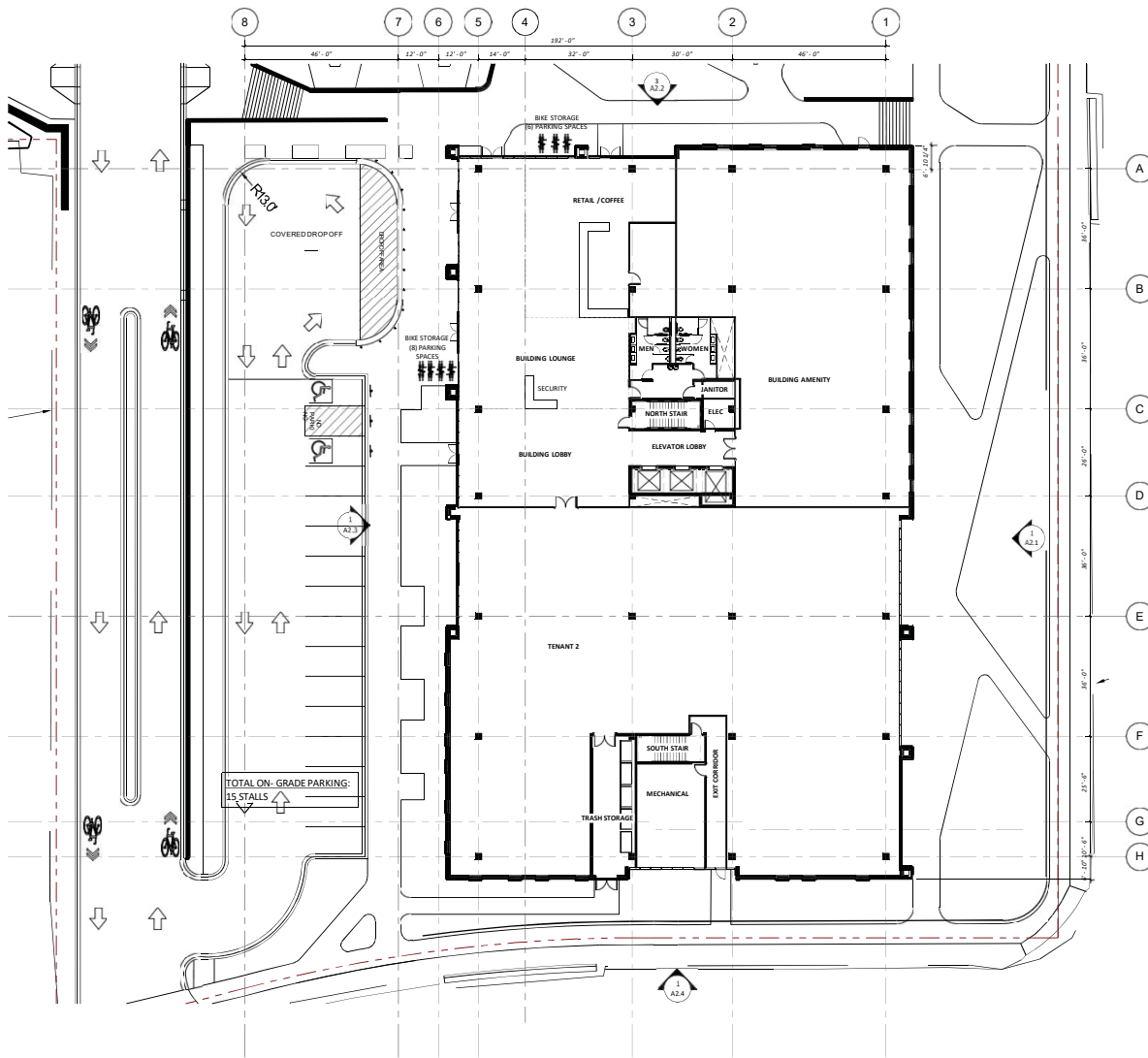


2 PHASE 1 - EAST / WEST SITE SECTION AT DANILA
 AD 5/16" = 1'-0"

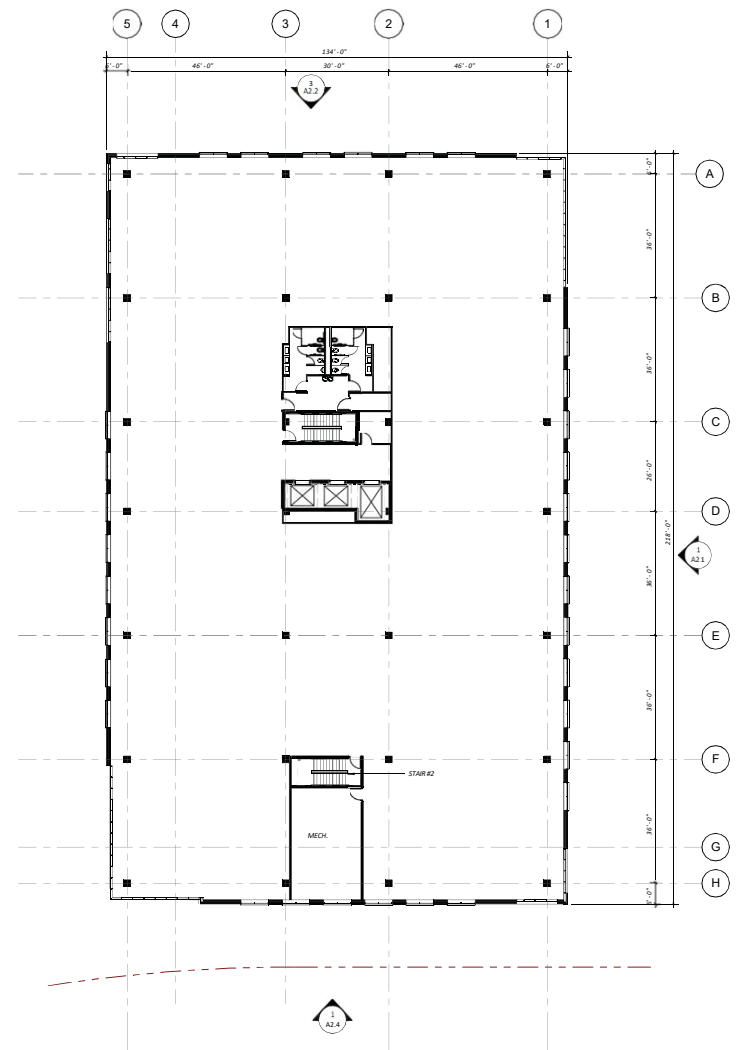


PHASE 2 - NORTH / SOUTH SITE SECTION
RDS 215-1-0'

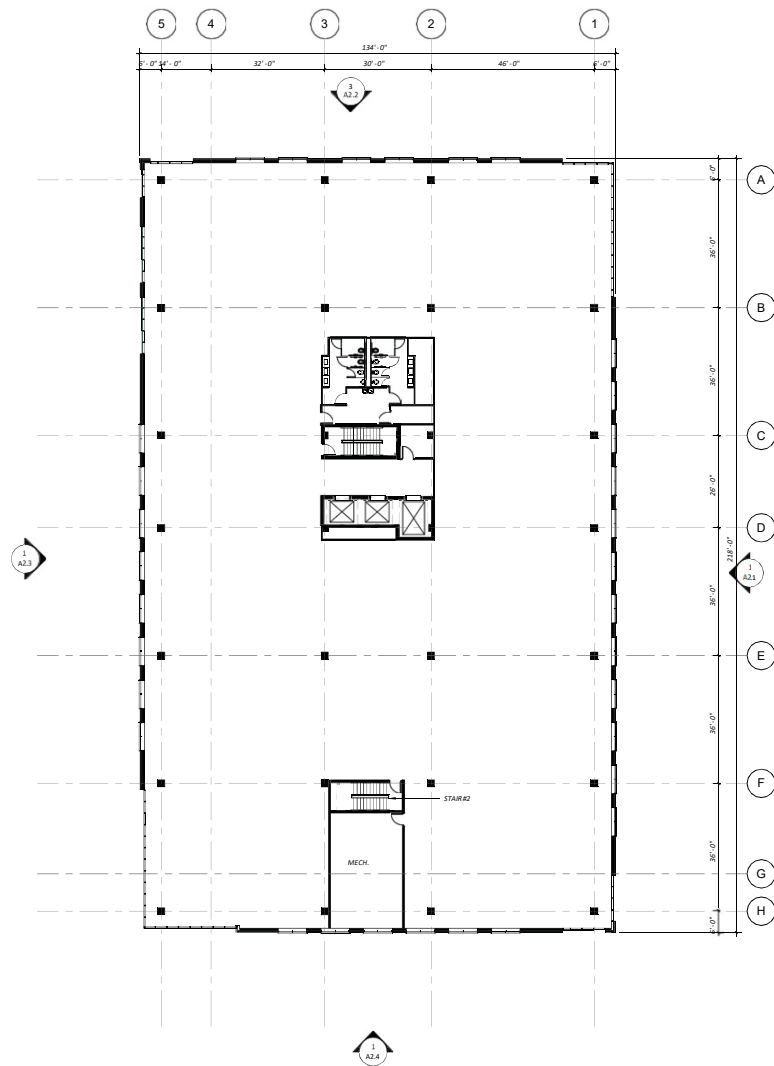




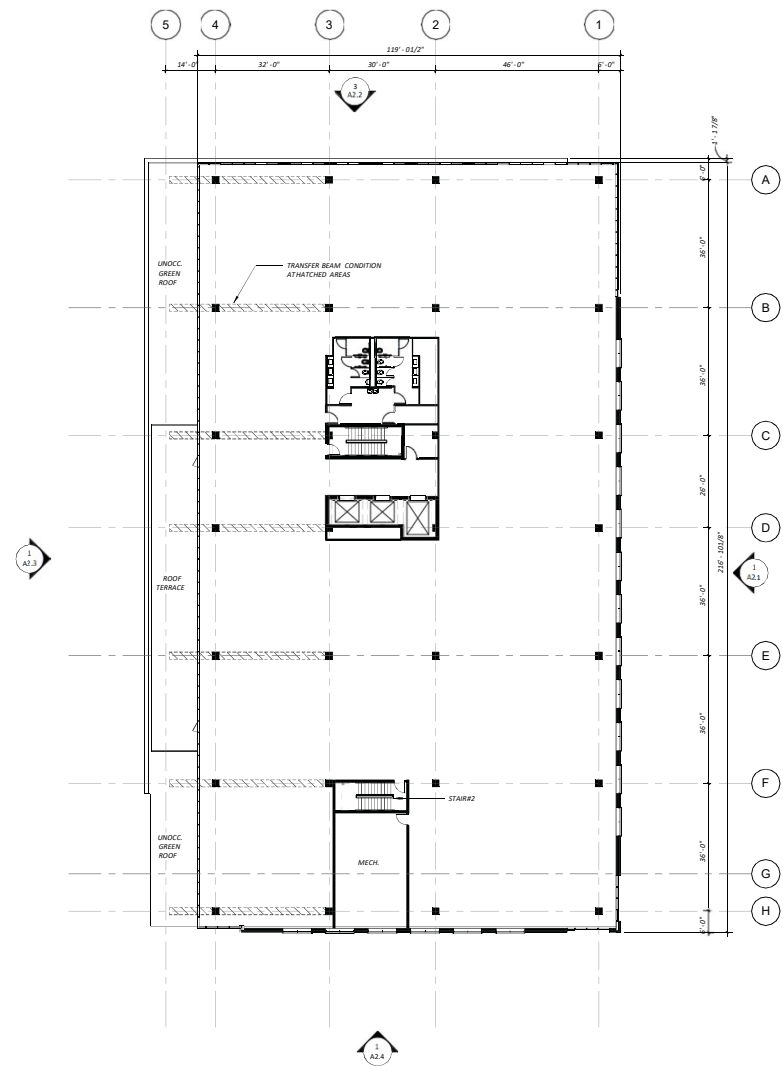
1 LEVEL 1 FLOOR PLAN & SITE PLAN
A1.2 3/16" = 1'-0"



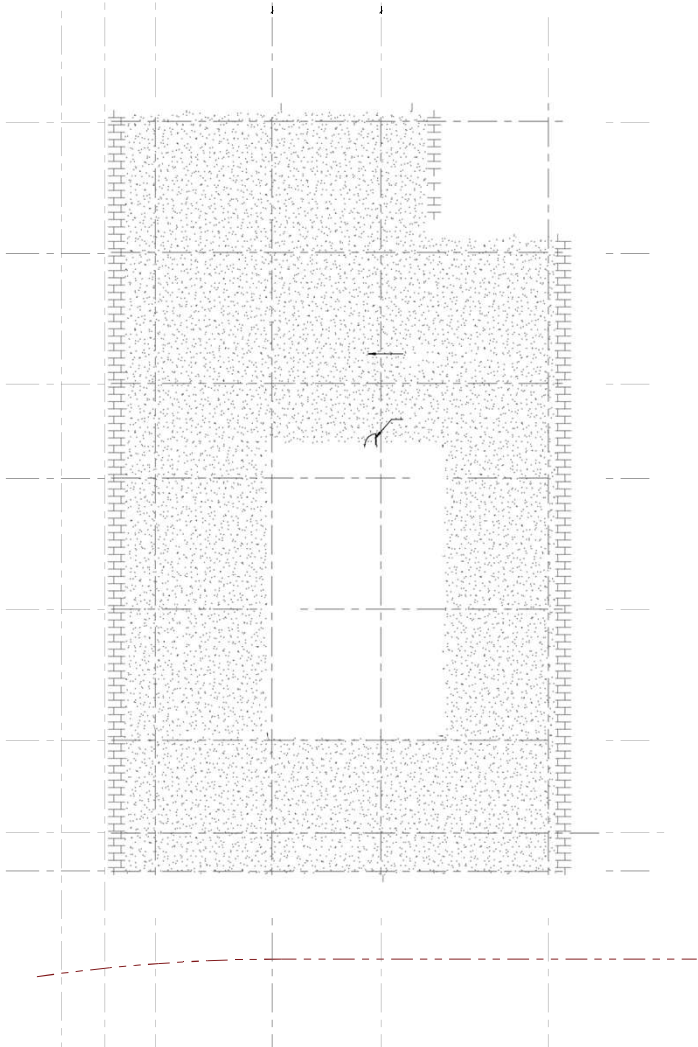
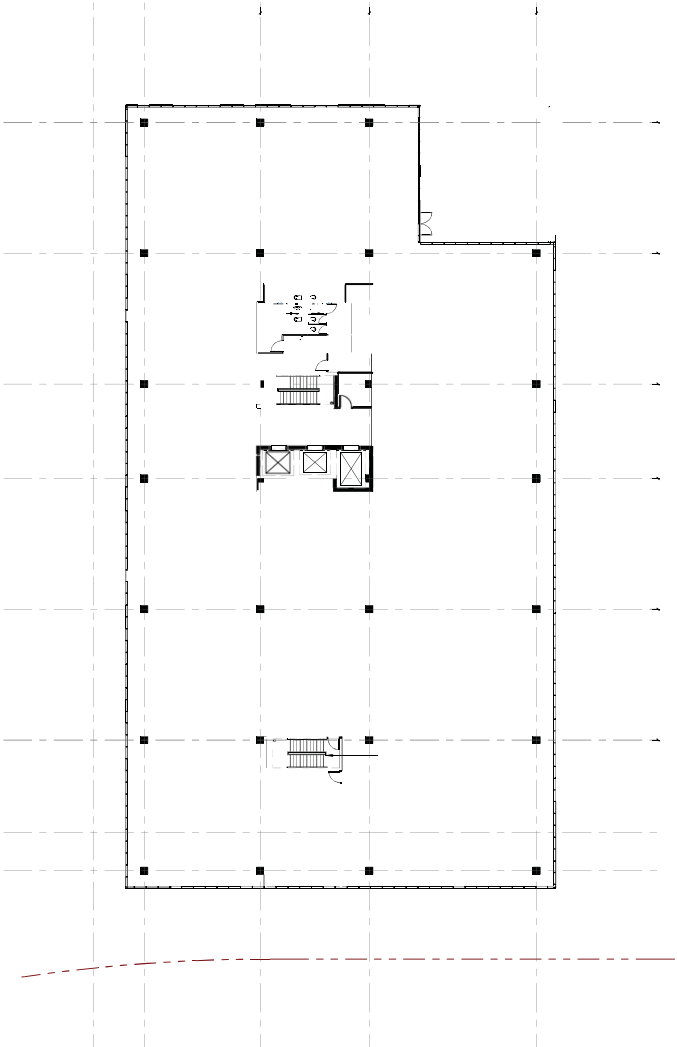
2 LEVEL 2 FLOOR PLAN
A1.2 3/16" = 1'-0"

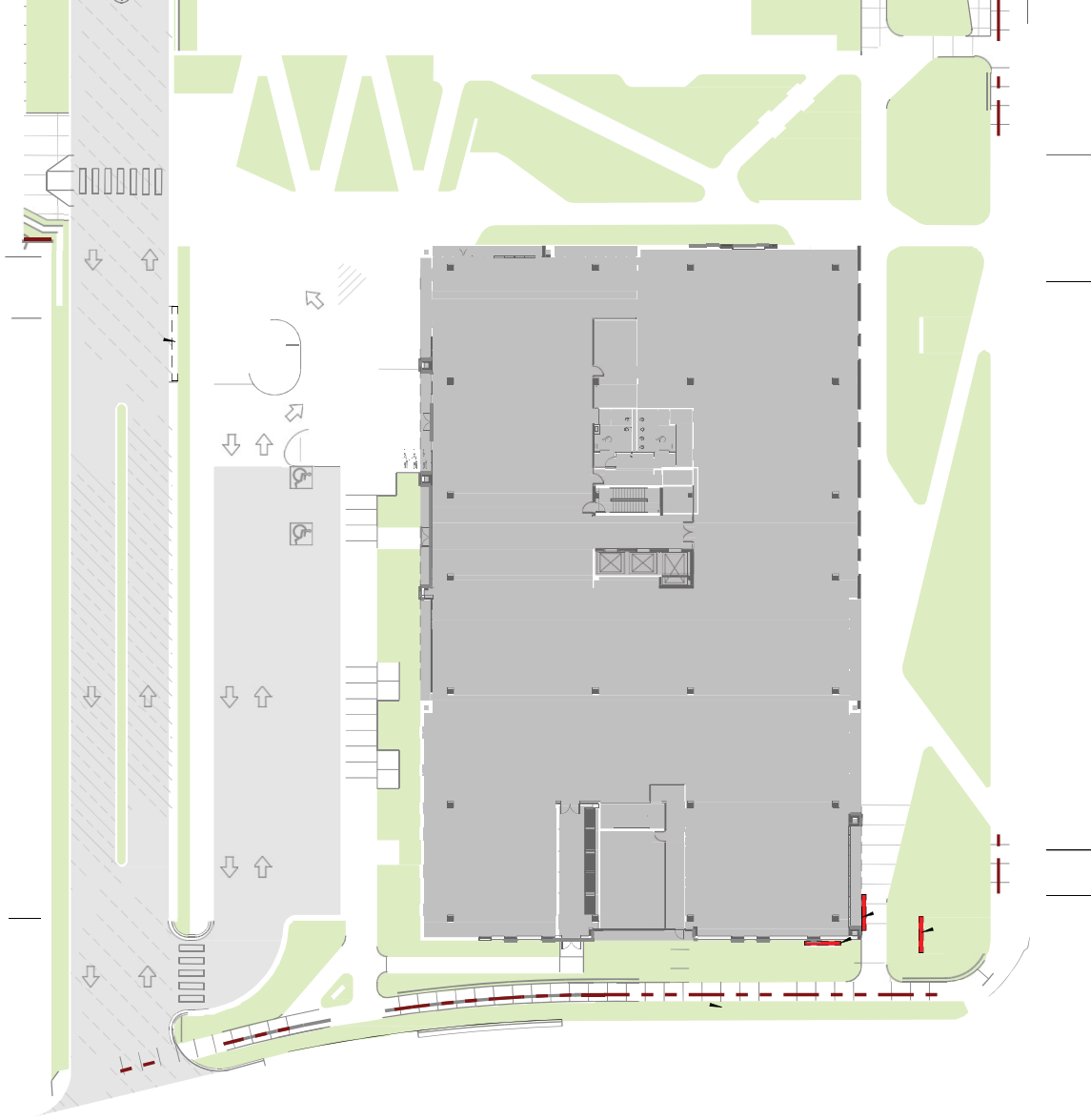


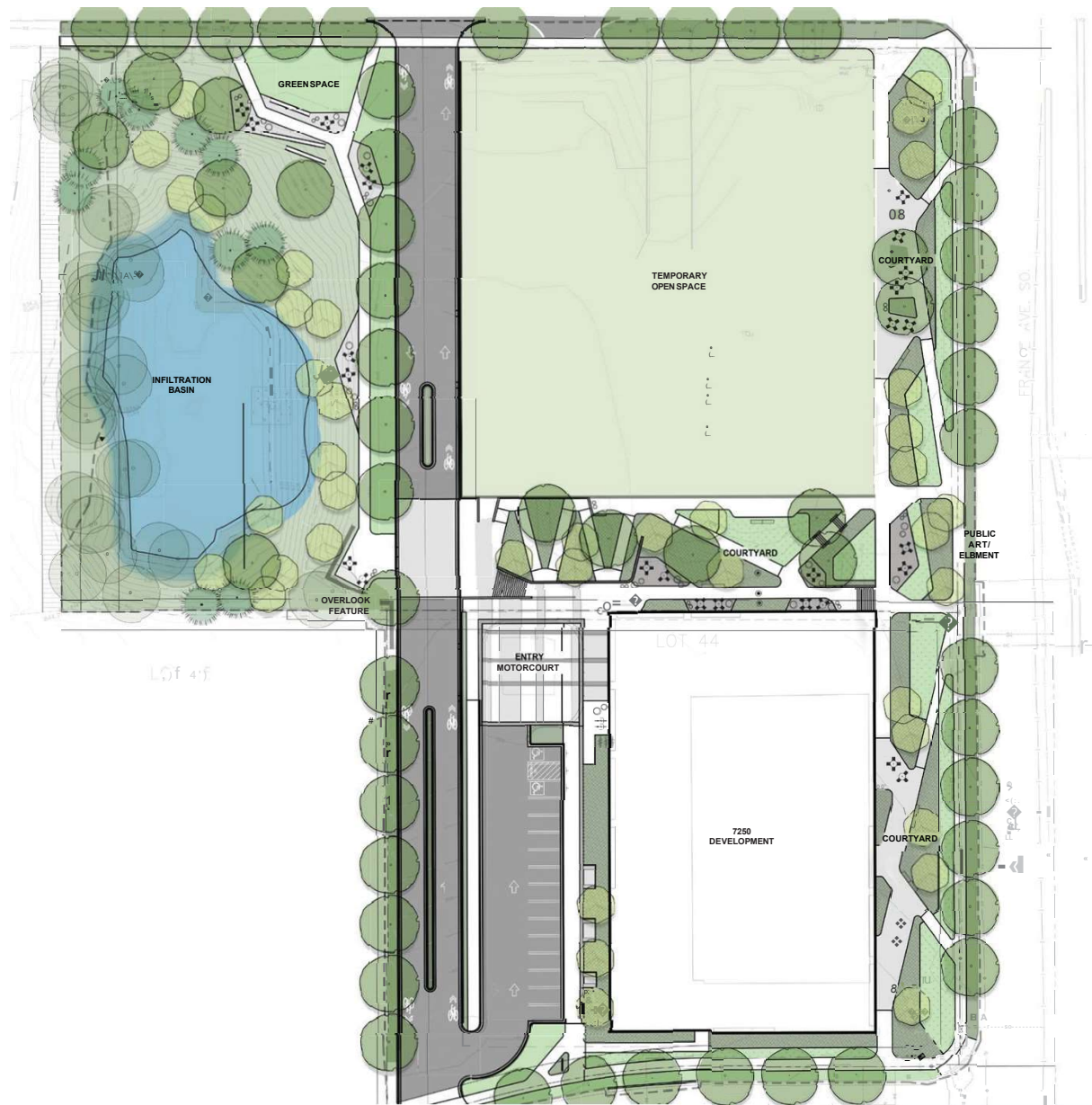
1 LEVEL 3 FLOOR PLAN
A1.3 3/16" = 1'-0"

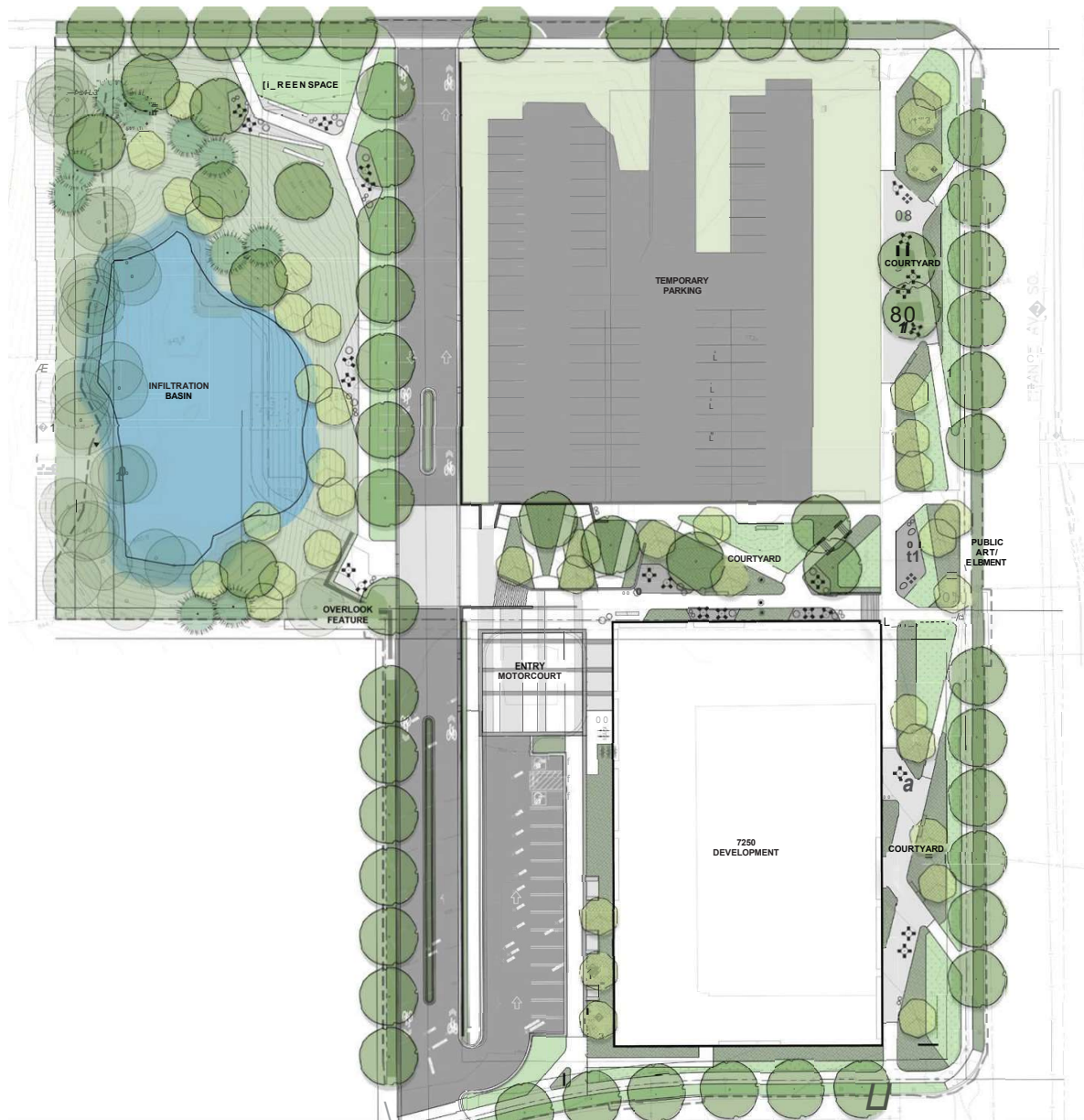


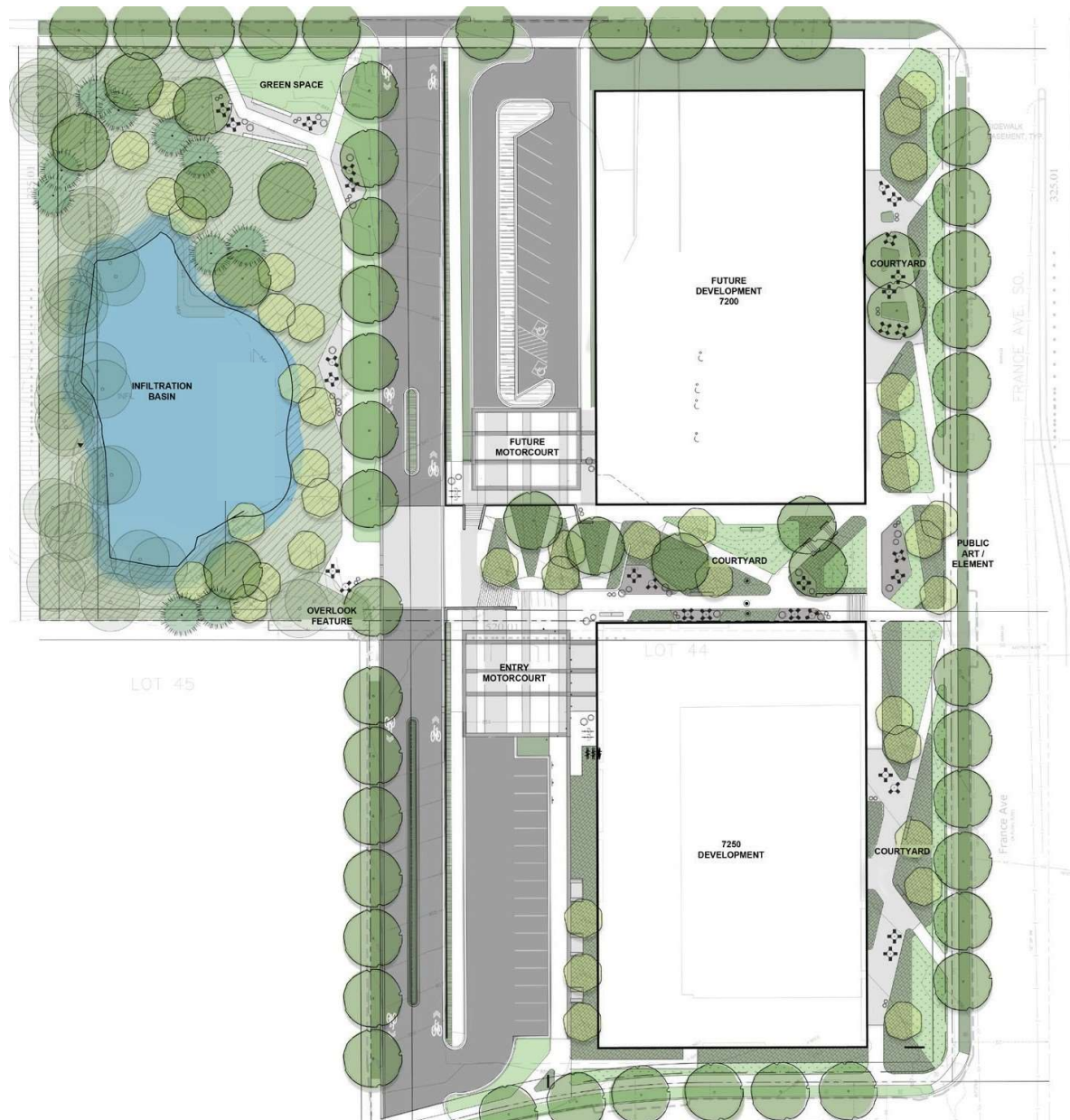
2 LEVEL 4 FLOOR PLAN
A1.3 3/16" = 1'-0"

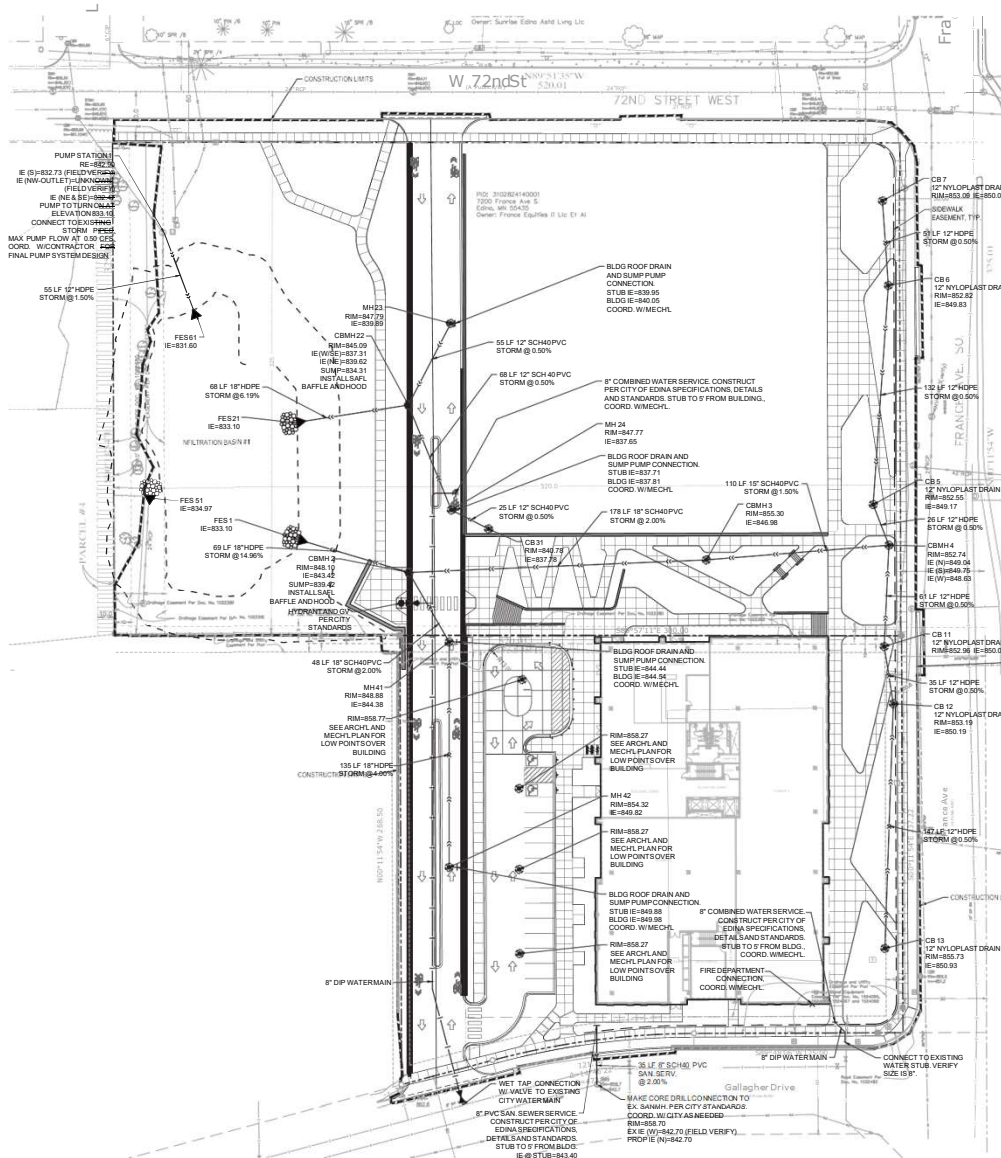


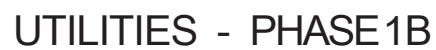


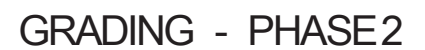
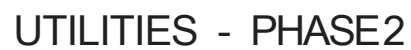














CITY OF EDINA

4801 West 50th Street

Edina, MN 55424

www.edinamn.gov

Date: September 14, 2022

Agenda Item #: VIII.A.

To: Planning Commission

Item Type:

Report and Recommendation

From: Cary Teague, Community Development Director

Item Activity:

Subject: 2023 Planning Commission Work Plan

Action

ACTION REQUESTED:

Approve the 2023 Planning Commission Work Plan.

INTRODUCTION:

See the attached proposed work plan.

ATTACHMENTS:

2023 PC Work Plan



Commission: Planning Commission

2023 Annual Work Plan Proposal

Initiative # 1	Initiative Type <input type="checkbox"/> Project <input type="checkbox"/> Ongoing / Annual <input type="checkbox"/> Event		
	Council Charge <input type="checkbox"/> 1 (Study & Report) <input type="checkbox"/> 2 (Review & Comment) <input checked="" type="checkbox"/> 3 (Review & Recommend) <input checked="" type="checkbox"/> 4 (Review & Decide)		
Initiative Title: Review Land Use Applications		Deliverable: Final Decisions on variances and recommendations to the City Council on CUP, Site Plans and Rezoning	Leads: Staff Sub-Committee or Working Group
Target Completion Date On-going			
Budget Required: (Completed by staff) No budget required.			
Staff Support Required (Completed by staff): Yes. 60-80 staff hours per week (between 3 planners and administrative assistant)			
Liaison Comments: This is the bulk of the Planning Commission responsibilities; including reviewing the packets (staff reports, plans, studies, and applicant narratives twice per month)			
City Manager Comments:			
Progress Q1:			
Progress Q2:			
Progress Q3:			
Progress Q4:			

Initiative #2	Initiative Type <input type="checkbox"/> Project <input type="checkbox"/> Ongoing / Annual <input type="checkbox"/> Event		
	Council Charge <input type="checkbox"/> 1 (Study & Report) <input type="checkbox"/> 2 (Review & Comment) <input checked="" type="checkbox"/> 3 (Review & Recommend) <input type="checkbox"/> 4 (Review & Decide)		
Initiative Title: Cahill District Area Plan – Continue from 2022		Deliverable: Recommendation to City Council on adopting the District Plan into the Comprehensive Plan	Leads Agnew & Alkire Sub-Committee or Working Group
Target Completion Date June			
Budget Required: (Completed by staff) This study is already included in the City's budget.			
Staff Support Required (Completed by staff): Yes. 10-20 staff hours per week			
Liaison Comments:			
City Manager Comments:			
Progress Q1:			
Progress Q2:			
Progress Q3:			

Progress Q4:

Initiative #3	Initiative Type <input type="checkbox"/> Project <input type="checkbox"/> Ongoing / Annual <input type="checkbox"/> Event Council Charge <input type="checkbox"/> 1 (Study & Report) <input type="checkbox"/> 2 (Review & Comment) <input checked="" type="checkbox"/> 3 (Review & Recommend) <input type="checkbox"/> 4 (Review & Decide)		
Initiative Title: Consideration of the future of parking in Edina. This would be a collaborative activity shared among E&E, Transportation and Planning Commissions (1-2 members from each commission). Work product is a recommendation to our various commissions and then ultimately to the City Council about what parking initiatives to pursue over the next 10-15 years, in what order, and what commissions/resources should be assigned to each. An example idea to be considered is Parking Benefit Districts.	Deliverable: Recommendation to the City Council	Leads: Alkire	Target Completion Date On-going
		Assist: Miranda, Strauss Sub-Committee or Working Group	
Budget Required: (Completed by staff) No additional budget required. Potential consulting fees if needed would come from the Community Development Department budget.			
Staff Support Required (Completed by staff): Yes.			
Liaison Comments:			
City Manager Comments:			
Progress Q1:			
Progress Q2:			
Progress Q3:			
Progress Q4:			

Initiative #4	Initiative Type <input type="checkbox"/> Project <input type="checkbox"/> Ongoing / Annual <input type="checkbox"/> Event Council Charge <input type="checkbox"/> 1 (Study & Report) <input type="checkbox"/> 2 (Review & Comment) <input checked="" type="checkbox"/> 3 (Review & Recommend) <input type="checkbox"/> 4 (Review & Decide)		
Initiative Title: Development Review/Sketch Plan Process follow up.	Deliverable: Recommendation to City Council on development process.	Lead: Olson	Target Completion Date March
		Assist: Bennett Sub-Committee	
Budget Required: (Completed by staff) No.			
Staff Support Required (Completed by staff): Yes.			
Liaison Comments:			
City Manager Comments:			
Progress Q1:			

Progress Q2:
Progress Q3:
Progress Q4:

Initiative #5	Initiative Type <input type="checkbox"/> Project <input type="checkbox"/> Ongoing / Annual <input type="checkbox"/> Event			
	Council Charge <input type="checkbox"/> 1 (Study & Report) <input type="checkbox"/> 2 (Review & Comment) <input checked="" type="checkbox"/> 3 (Review & Recommend) <input type="checkbox"/> 4 (Review & Decide)			
Initiative Title: Consider the next “Areas of Potential Change” for future small area planning.		Deliverable: Recommendation to the City Council. These areas would then be studied further in the next Comprehensive Plan update.	Lead: Miranda Assist: Bennett Sub-Committee or Working Group	Target Completion Date May
Budget Required: (Completed by staff) No.				
Staff Support Required (Completed by staff): Yes. 10-20 hours?				
Liaison Comments:				
City Manager Comments:				
Progress Q1:				
Progress Q2:				
Progress Q3:				
Progress Q4:				

Initiative #6	Initiative Type <input type="checkbox"/> Project <input type="checkbox"/> Ongoing / Annual <input type="checkbox"/> Event			
	Council Charge <input type="checkbox"/> 1 (Study & Report) <input type="checkbox"/> 2 (Review & Comment) <input checked="" type="checkbox"/> 3 (Review & Recommend) <input type="checkbox"/> 4 (Review & Decide)			
Initiative Title: Accessory Dwelling Unit. This would be a follow-up/continuation of the work done in 2022.		Deliverable: Recommendation to the City Council of an Ordinance Amendment regulating accessory dwelling units.	Leads: Smith, Bennett Sub-Committee or Working Group	Target Completion Date August
Budget Required: (Completed by staff) No.				
Staff Support Required (Completed by staff): 5+ hours per week?				
Liaison Comments:				
City Manager Comments:				
Progress Q1:				
Progress Q2:				

Progress Q3:
Progress Q4:

Initiative #7	Initiative Type <input type="checkbox"/> Project <input type="checkbox"/> Ongoing / Annual <input type="checkbox"/> Event Council Charge <input type="checkbox"/> 1 (Study & Report) <input type="checkbox"/> 2 (Review & Comment) <input checked="" type="checkbox"/> 3 (Review & Recommend) <input type="checkbox"/> 4 (Review & Decide)			
	Initiative Title: Consider climate action plan and investigate what can be put in to City Code. (Potentially adding conditions to conditionally permitted uses focusing on sustainability.)	Deliverable: Recommendation to the City Council regarding an ordinance amendment	Leads: Smith, Padilla Sub-Committee or Working Group	Target Completion Date December
Budget Required: (Completed by staff) No.				
Staff Support Required (Completed by staff): Yes. 40+?				
Liaison Comments:				
City Manager Comments:				
Progress Q1:				
Progress Q2:				
Progress Q3:				
Progress Q4:				

Initiative #8	Initiative Type <input type="checkbox"/> Project <input type="checkbox"/> Ongoing / Annual <input type="checkbox"/> Event Council Charge <input type="checkbox"/> 1 (Study & Report) <input type="checkbox"/> 2 (Review & Comment) <input checked="" type="checkbox"/> 3 (Review & Recommend) <input type="checkbox"/> 4 (Review & Decide)			
	Initiative Title: Zoning Ordinance Amendment regarding commercial landscaping	Deliverable: Recommendation to the City Council regarding an ordinance amendment	Leads: Alkire, Olson Sub-Committee or Working Group	Target Completion Date October
Budget Required: (Completed by staff) No.				
Staff Support Required (Completed by staff): Yes.				
Liaison Comments:				
City Manager Comments:				
Progress Q1:				
Progress Q2:				
Progress Q3:				

Progress Q4:

Parking Lot: (These items have been considered by the BC, but not proposed as part of this year's work plan. If the BC decides they would like to work on them in the current year, it would need to be approved by Council.)