Agenda

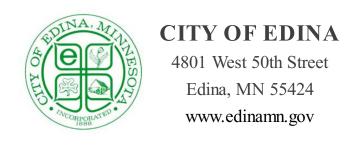
Planning Commission City Of Edina, Minnesota VIRTUAL MEETING

Members of the public can participate in the public hearings by calling 800-374-0221 with Conference ID 6037966 during the meeting. Press *1 on your telephone keypad when you are ready to testify in a public hearing. An operator will mute your line place you into a queue until it is your turn to speak. After the public hearings, the call will end.

Wednesday, July 8, 2020 7:00 PM

- I. Call To Order
- II. Roll Call
- III. Approval Of Meeting Agenda
- IV. Approval Of Meeting Minutes
 - A. Planning Commission Minutes: June 10, 2020
- V. Public Hearings
 - A. B-20-12, Sign Variances to allow two building signs, one 100 square feet and one 87 square feet at Our Lady of Grace, 5051 Eden Avenue
 - B. PUBLIC HEARING: Zoning Ordinance Amendment Consideration (Chapter 36 of the City Code) To allow taprooms, wineries, distilleries, and cocktail rooms in the POD-2, Planned Office District-2.
 - C. PUBLIC HEARING: Conditional Use Permit with Variances 4425 Valley View Road
- VI. Reports/Recommendations
- VII. Chair And Member Comments
- VIII. Staff Comments
- IX. Adjournment

The City of Edina wants all residents to be comfortable being part of the public process. If you need assistance in the way of hearing amplification, an interpreter, large-print documents or something else, please call 952-927-8861 72 hours in advance of the meeting.



Date: July 8, 2020 Agenda Item #: IV.A.

To: Planning Commission Item Type:

Minutes

Action

From: Liz Olson, Administrative Support Specialist

Item Activity:

Subject: Planning Commission Minutes: June 10, 2020

ACTION REQUESTED:

Please approve the Planning Commission Meeting Minutes from June 10, 2020.

INTRODUCTION:

ATTACHMENTS:

Planning Commission Minutes: June 10, 2020



Minutes
City Of Edina, Minnesota
Planning Commission
Virtual WebEx
June 10, 2020

I. Call To Order

Chair Olsen called the meeting to order at 7:01 PM.

II. Roll Call

Answering the roll call were: Commissioners Miranda, Berube, Thorsen, Strauss, Agnew, and Chair Nemerov. Staff Present: Cary Teague, Community Development Director, Jennifer Bennerotte, Communications Director, and Liz Olson, Administrative Support Specialist.

Absent from the roll call: Commissioners Olsen and Bennett.

III. Approval Of Meeting Agenda

Commissioner Thorsen moved to approve the June 10, 2020, agenda. Commissioner Agnew seconded the motion. Motion carried.

IV. Approval Of Meeting Minutes

A. Minutes: Planning Commission, May 27, 2020

Commissioner Thorsen moved to approve the May 27, 2020, meeting minutes. Commissioner Strauss seconded the motion. Motion carried.

V. Public Hearings

B. Parking Stall Variance and Site Plan - 6401 France for M Health

Director Teague presented the request of Parking Stall Variance and Site Plan for 6401 France for M Health. Staff recommends approval of the parking stall variance and site plan, as requested subject to the findings and conditions listed in the staff report.

Staff answered Commission questions.

Appearing for the Applicant

Ms. Faye Drake, Chief Nursing Officer and Vice President of Patient Care at M Healthcare introduced herself and addressed the Commission.

Mr. Pete Sullivan, Perkins and Will, introduced himself and made addressed the Commission.

The applicant answered the Commissions questions.

Public Hearing

There were online submittals from Better Together Edina regarding issues with pedestrian safety and noise.

Ms. Lori Grotz, 5513 Park Place addressed the Commission.

Commissioner Thorsen moved to close the public hearing. Commissioner Agnew seconded the motion. Motion carried.

Traffic and Parking Consultant, Chuck Rickert was also at the meeting and reviewed the traffic plan with the Commission.

The Commission discussed the variance and site plan.

Motion

Commissioner Thorsen moved that the Planning Commission recommend approval to the City Council of the Variance and Site Plan as outlined in the staff memo subject to the conditions and findings therein. Commissioner Strauss seconded the motion. Motion carried unanimously.

A. Preliminary Plat with Multiple Variances – 5928 Abbott Avenue for Second Story Build

Director Teague presented the request of Preliminary Plat with Multiple Variances for 5928 Abbott Avenue for Second Story Build. Staff recommends approval of the proposed two lot subdivision of 5928 Abbott Avenue South, as requested subject to the findings and conditions listed in the staff report.

Staff addressed Commission questions.

Appearing for the Applicant

Mr. and Mrs. Michielutti, owners, introduced themselves and addressed the Commission.

Public Hearing

No one wished to address the Commission.

Commissioner Thorsen moved to close the public hearing. Commissioner Miranda seconded the motion. Motion carried.

Chair Nemerov indicated the City received some comments from Better Together Edina, some being pro and some con. He reviewed the comments.

Mr. Michielutti answered the Commission's questions.

The Commission discussed the variance and site plan and concluded:

- The request appears to meet all of the requirements at this time.
- Dividing the lot is not unreasonable because it was two lots before.
- The lots will fit into the neighborhood.
- Returning the two lots to the original state.
- Concerned with naturally occurring affordable housing in the City.

Motion

Commissioner Thorsen moved that the Planning Commission recommend approval to the City Council of the proposed two lot subdivision of 5928 Abbott Avenue South as outlined in the staff memo subject to the conditions and findings therein. Commissioner Miranda seconded the motion. Motion carried unanimously.

VII. Reports/Recommendations

None.

VIII. Correspondence and Petitions

It was noted the City received some correspondence regarding affordable housing.

IX. Chair and Member Comments

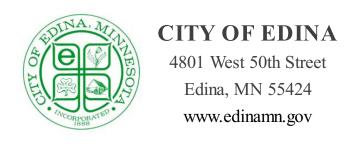
Chair Nemerov shared an article he read to the Commission. He also reviewed the recent Planning Commission Worksession that took place.

X. Staff Comments

Cary Teague indicated the June 24, 2020 Planning Commission meeting will be a Planning Commission work session only.

XI. Adjournment

Commissioner Thorsen moved to adjourn the June 10, 2020, Meeting of the Edina Planning Commission at 8:48 PM. Commissioner Agnew seconded the motion. Motion carried.



Date: July 8, 2020 Agenda Item #: V.A.

To: Planning Commission Item Type:

From: Emily Bodeker, Assistant City Planner

Item Activity:

Action

Subject: B-20-12, Sign Variances to allow two building signs,

one 100 square feet and one 87 square feet at Our

Lady of Grace, 5051 Eden Avenue

ACTION REQUESTED:

Approve the variances as requested.

INTRODUCTION:

The applicant is asking for two different sign variances. One variance for the number wall signs, and one for the size of the proposed wall signs. Both proposed signs are located on the north elevation of the building.

The first proposed sign, shown in sign location A, is a 100 square foot sign located on the north east corner of the building. The second proposed sign, shown in sign location B, is an 87 square foot sign located on the northwest corner over the entry of the building.

ATTACHMENTS:

Staff Report Applicant Submittal Aerial Map

STAFF REPORT



Date:

July 8, 2020

To:

PLANNING COMMISSION

From:

Emily Bodeker, Assistant City Planner

Subject:

B-20-12, Sign variance to allow two building signs, one 100 square feet and one 87 square feet

at Our Lady of Grace, 5051 Eden Avenue

Information / Background:

The applicant, Our Lady of Grace, is requesting sign variances for multiple wall signs at 5051 Eden Avenue. The subject property is located on the west side of HWY 100, south of Eden Avenue.

The applicant is asking for two different sign variances. One variance for the number wall signs, and one for the size of the proposed wall signs. Both proposed signs are located on the north elevation of the building.

The first proposed sign, shown in sign location A, is a 100 square foot sign located on the north east corner of the building. The second proposed sign, shown in sign location B, is an 87 square foot sign located on the northwest corner over the entry of the building.

Our Lady of Grace received a conditional use permit with variances in 2019 for the expansion of the religious use at the subject property. The request in 2019 included a sign variance for a 100 square foot sign. At that time, the applicant did not have the design of the proposed sign, therefore the Council asked that the applicant apply for a sign variance when the proposed signage was designed.

Surrounding Land Uses

Northerly: An office building; zoned POD-I, Planned Office District and a vacant parcel zoned PID,

Planned Industrial District and guided MXC, Mixed Use Center

Easterly: Highway 100

Southerly: Single Family Residences; zoned R-1, Low Density Residential and Guided Low Density

Residential

Westerly: Grandview Square; zoned MDD-4, Mixed Development District and Guided MXC, Mixed

Use Center.

Existing Site Features

The subject property, 5051 Eden Avenue, is 20.67 acres and is the current site of the existing Our Lady of Grace Church.

Planning

Guide Plan designation: PSP, Public/Semi-Public

Zoning: R-I, Single Dwelling Unit District

Compliance Table Building Identification Signs R-1 District

	Sign Standards- Building Identification Signs (Churches, Schools)	Proposed Sign #I Location A (North Elevation)	Proposed Sign #2 Location B (North Elevation)
Number of Signs-	l per building	2 new building signs proposed*	2 new building signs proposed*
Square footage -	24 square feet	100 square feet*	87 square feet*

^{*}Requires a variance

PRIMARY ISSUES & STAFF RECOMENDATION

Primary Issues

Is the proposed variance justified?

Yes, Staff believes the requested sign variances are justified. The signs are located on the north elevation towards the commercial area and HWY 100 and will not be visible from the single-family homes to the south.

Minnesota Statues and Edina Ordinances required that the following conditions must be satisfied affirmatively to grant a variance. The proposed variance will:

I) Relieve practical difficulties that prevent a reasonable use from complying with ordinance requirements.

Reasonable use does not mean that the applicant must show the land cannot be put to any reasonable use without the variance. Rather, the applicant must show that there are practical difficulties in complying with the code and that the proposed use is reasonable.

Although the location of the church and school is within residentially zoned property, the site itself allows for signage that would typically be seen in more of a commercially zoned area due to its size and location. The proposed signage will be located only on the north elevation of the building facing other commercial and office uses and partially visible from Highway 100. The size of the signs is reasonable given the size of the structure on which they would be located.

2) There are circumstances that are unique to the property, not common to every similarly zoned property, and that are not self-created?

Oftentimes churches are located completely immersed within single family neighborhoods. Our Lady of Grace acts as a transition between the neighborhoods to the south and commercial properties to the north and HWY 100 to the east. The proposed signage fits the size and scale of the building and is visible from the commercial properties to the north.

3) Will the variance alter the essential character of the neighborhood?

The location of the proposed signs will have limited impact to the surrounding residential neighborhoods. The proposed sign will fit the size and scale of the building.

Staff Recommendation

Approve the requested variances to allow a 100 square foot wall sign and an 87 square foot wall sign on the north elevation of the Our Lady of Grace at 5051 Eden Avenue based on the following findings:

- I. The criteria for the proposed sign variances are met. The proposed signage will have limited impact on surrounding residential neighborhoods and fits within the size and scale of the proposed building.
- 2. The proposed signs will help with identifying the different uses and wayfinding within the site.
- 3. The practical difficulty is the scale of the site. Smaller wall signs on the building would be more difficult to see due to the scale of the building.

4. The church acts as a transition from the residential neighborhood to the commercial node to the north. The proposed signage is facing north towards the commercial area.

Approval is subject to the following conditions:

- I. The site must be developed and maintained in conformance with the following plans:
 - Sign plans and elevations date stamped May 27, 2020.



Our Lady of Grace Master Development Plan – Variance Application Narrative

22 May 2020

Variance Request – Building Signage Size

Allowed:

24 SF per sign

Proposed:

100 SF per sign

MAY 27 2020

CITY OF EDINA

PLANNING DEPARTMENT

Variance Request - Building Signage Quantity

Allowed:

1 per building

Proposed:

1 additional building sign - (2 total)

Will the proposed variance relieve practical difficulties in complying with the zoning ordinance and that the use is reasonable?

Applicant Response: Yes, while the existing property is zoned R-1, we feels its approved use justifies review of this ordinance. The existing signage area allotment for residential homes is not sufficient for a campus of this size and location. We are proposing to use a more typical commercial building signage area to better align with the commercial projects to the north and Highway 100. Furthermore, we would like to request an additional building sign location for increased visibility and wayfinding. Signage will be located on the North facade (facing commercial) as well as the East façade (facing highway 100) to limit any visual impacts on the residences to the south. With the local context to the north and east we feel the request is reasonable. Refer to the attached exhibits for further details.

Will the proposed variance correct extraordinary circumstances applicable to this property but not applicable to other property in the vicinity or zoning district?

Applicant Response: Yes, with the unique CUP approvals already governing this use and local site context as this property transitions between residential and commercial, it is unlikely that this approval and circumstance will impact other properties in the vicinity or zoning district as they likely do not have the same or similar zoning and CUP constraints or local surrounding context. Furthermore, with continued growth, we believe that an additional building sign is necessary to help identify the added program elements across the campus.

Will the proposed variance be in harmony with the general purposes and intent of the zoning ordinance?

Applicant Response: Yes, we believe the proposed uses are in harmony with the general purpose and intent of the zoning ordinance and may be more fitting with the commercial development to the north as well as highway 100. By providing additional building signage, we can appropriately and distinctively provide a building identity to both the church and school.

Will the proposed variance alter the essential character of a neighborhood?

Applicant Response: No, the proposed design and locations should not alter the essential character of the neighborhood and will allow all signage to be appropriately scaled and positioned to fit into its context and campus size.

DESCRIPTION OF PROPERTY SURVEYED

That part of Government Lot 8 of Section 28, Township 117 North, Range 21 West of the Fifth Principal Meridian described as follows: Commencing 26 2/3 rods East of Southwest corner of Lot 8; thence North to East line of Minneapolis, Northfield and Southern Railway right of way; thence Northerly along said right of way line to a point distant 703 7/10 feet Southerly along same from Southerly line of Eden Prairie Road; thence East 193 2/10 feet; thence North to center of said road; thence Northeasterly along same to North line of Lot 8; thence East to Northeast corner of Lot 8; thence South to Southeast corner; thence West to beginning. Except that part thereof lying northeasterly and easterly of the southwesterly and westerly line of the highway taking described in Doc. No. 3799189. Also except the southerly 40 feet of said Eden Prairie Road.

And

All that part of Government Lot Eight (8), in Section Twenty-Eight (28), Township One Hundred Seventeen (117) North, Range Twenty-One (21) West, of the Fifth Principal Meridian, according to the Government Survey thereof, bounded and described as follows: Commencing at the point where the Easterly line of the right of way of the Minneapolis, Northfield and Southern Railway was located and constructed across said section is intersected by a line parallel with and forty (40) feet southerly at right angles from the center line of the Eden Prairie Road, so-called, as laid out and located through said Section, which point is hereinafter designated as Point "A", and running thence North Sixty-seven degrees, two (2) minutes East, along said parallel line and at an angle of One Hundred Sixteen (116) degrees, forty-seven (47) minutes, with said right of way line, a distance of One Hundred Twenty (120) feet, thence South two (2) degrees, thirty-three (33) minutes East 408.5 feet to a point which is the actual place of beginning of the land hereby conveyed.

Thence from said actual place of beginning South two (2) degrees, thirty-three (33) minutes East 340 feet; thence South Eighty-nine (89) degrees, Thirty-eight (38) minutes west 193.2 feet to said Easterly line of right of way; thence Northerly 340 feet along said right of way line to a point 363.7 southerly from said Point "A", measured along said right of way line; thence Northeasterly at an angle of Ninety-four (94) degrees, Forty-eight (48) minutes with said right of way line a distance of 154.3 feet to the actual place of beginning.

And

All that part of Government Lot 8, Section 28, Township 117, Range 21, West of the 5th Principal Meridian bounded and described as follows: Commencing at the point where the Easterly line of the right of way of the Minneapolis, Northfield, and Southern Railway as located and constructed across said section, is intersected by a line parallel with and 40 feet Southerly at right angles from the center line of Eden Prairie Road, so called, as laid out and located through said Section, and running thence North 67 degrees 2 minutes, East along said parallel line and at an angle of 116 degrees 47 minutes with said right of way line, a distance of 120 feet; thence South 2 degrees 33 minutes, East 408.5 feet; thence West 154.3 feet to said Easterly line of Railway right of way at a point 363.7 feet South from the place of beginning, measured along said right of way line; thence North along said Easterly line of Railway right of way to place of beginning, Hennepin County, Minnesota.

And

PLANNING DEPARTMENT

MAY 2 7 2020



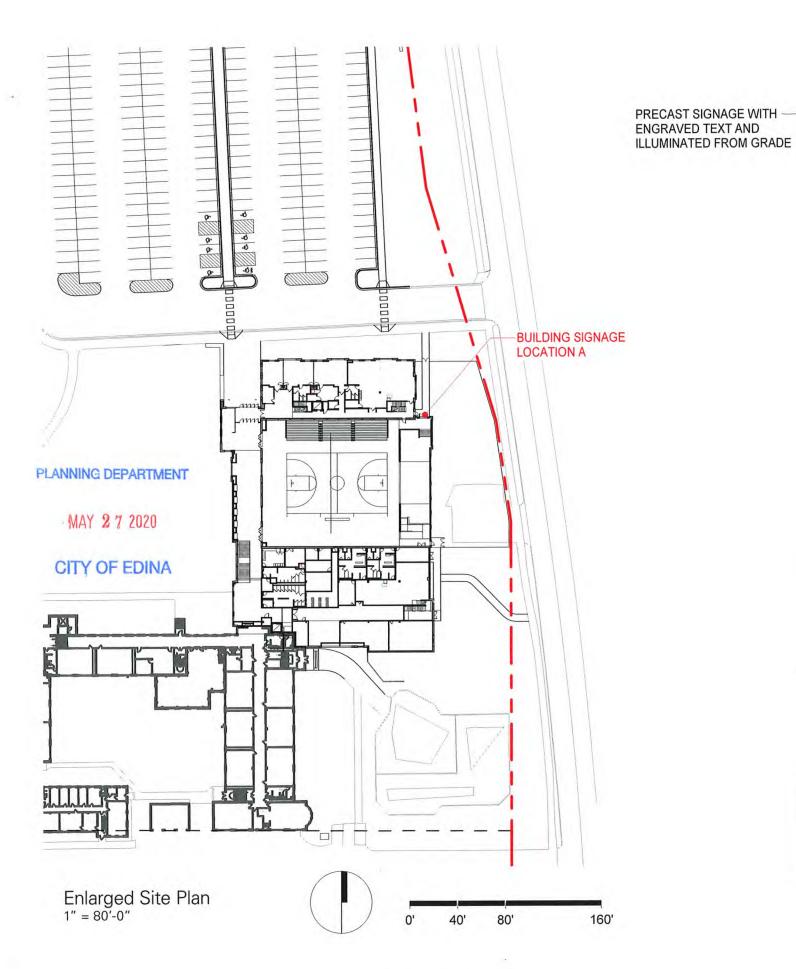
The north 150 feet of that part of Government Lot 1 of Section 33, Township 117 North, Range 21 West, lying east of the railroad right of way and west of the west line of the highway taking described in Doc. No. 3799189.

Property is located in Hennepin County, Minnesota.

PLANNING DEPARTMENT

MAY 2 7 2020

CITY OF EDINA





North Elevation

Building Signage Location A 100 sf

8' 16' 32'



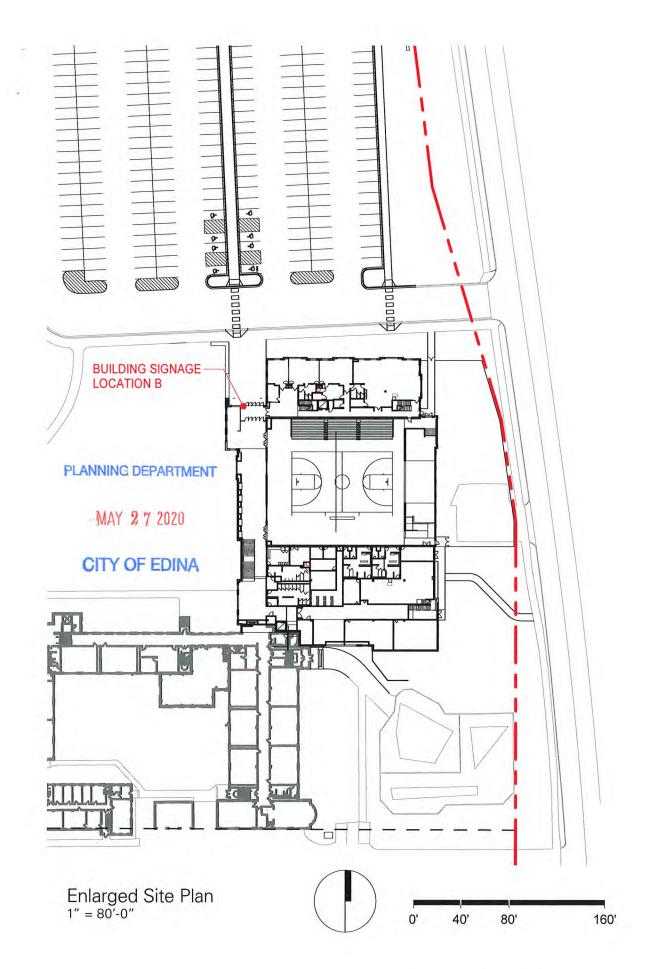
Northeast Perspective

Not to Scale



Our Lady of Grace

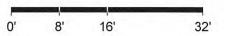
Edina, Minnesota





North Elevation 1/16" = 1'-0"

Building Signage Location B 87 sf





Entry Perspective
Not to Scale



Our Lady of Grace

Edina, Minnesota



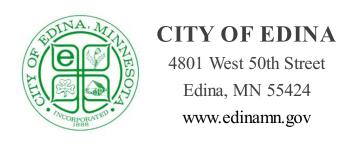
OLG



1 in = 250 ft







Date: July 8, 2020 Agenda Item #: V.B.

To: Planning Commission Item Type:

Report and Recommendation

From: Cary Teague, Community Development Director

Item Activity:

Action

Subject: PUBLIC HEARING: Zoning Ordinance Amendment

Consideration (Chapter 36 of the City Code) – To allow taprooms, wineries, distilleries, and cocktail rooms in the POD-2, Planned Office District-2.

ACTION REQUESTED:

Recommend the City Council adopt the ordinance to allow taprooms, wineries, distilleries, and cocktail rooms in the POD-2, Planned Office District-2.

INTRODUCTION:

See draft ordinance and staff report.

ATTACHMENTS:

Staff Report

Proposed Ordinance (2020)

2015-6 Ordinance

205-16 Survey of City's regulations on Taprooms & Distilleries

Applicant Narrative

Higher Resolution Zoning Map

Zoning Map to Show Where These Uses Would be Allowed

CITY OF EDINA

MEMC

City Hall • Phone 952-927-8861

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Date: July 8, 2020

To: Planning Commission

From: Cary Teague, Community Development Director

Re: Zoning Ordinance Amendment Consideration (Chapter 36 of the City Code) – To allow

taprooms, wineries, distilleries, and cocktail rooms in the POD-2, Planned Office District-2.

Request has been made by Eric Anderson, of CREFIV-CCRP Metro Boulevard Edina LLC to permit distilleries in the POD-2 Zoning District. They are considering locating a distillery within the existing office building at 7201 Metro Boulevard.

In 2015, the City of Edina adopted an ordinance to allow, breweries, taprooms, wineries, distilleries, cocktail rooms, and brewpubs in the PID, Planned Industrial District. (See the Original Ordinance Amendment attached.)

This request would expand those uses to the POD-2 District including distilleries. Properties zoned POD-2 District are all located adjacent to PID properties; primarily located in the Cahill Industrial Park and one property on France at 7550 France Avenue, and another on I-494 at 8050-8100 78th Street West. (See attached Zoning Map showing the areas where these uses would be expanded.)

By allowing these uses within the POD-2 District and not the POD-1, District the expansion would be a relatively small area and could be considered part of the PID Districts where they are already allowed.

Since 2015, the City has had one of these uses develop in Edina; and that is the Wooden Hill Brewery at 7421 Bush Lake Road. Concerns identified back in 2015 with allowing these uses were regarding noise, odor or extra police calls. The city has not experienced problems with these issues since the business has been in operation.

Within both the PID and POD-2 zoning districts, the primary uses within occur during the day. The uses suggested here operate primarily in the evening and on weekends so traffic and parking should not be an issue.

Food trucks are allowed as accessory use to taprooms, wineries, distilleries, and cocktail rooms. This was to address the concern about potentially serving alcohol without food.

Staff is supportive regarding expanding these uses to be allowed in the POD-2 District. The following is a summary of each of the "Sections" in the existing Zoning Ordinance that addresses



the different aspects of regulating these uses.

<u>Definitions.</u> Each of the uses are already defined in the Edina Zoning Ordinance. They include: Brewery, Brewpub, Distillery, Cocktail Room, Taproom, and Winery.

Distillery is defined as follows: A facility that produces distilled liquor, including all dilutions and mixtures thereof, for non-industrial uses. A distillery may include a cocktail room.

Parking Regulations. Parking regulations for each of the uses are defined. The ordinance would require one (I) space per forty (40) square feet for the "cocktail room" within the distillery. This is the same requirement for a taproom, and it has worked well for the Wooden Hill. The manufacturing parking requirement would apply to production.

Mobile Food Unit (Food Trucks). A licensed mobile food unit is allowed as an accessory use to a Taproom, Winery, Distillery or Cocktail Room, if there is no restaurant within 500 feet. This helps address the issue of serving food at these establishments. A food truck is only allowed if it is accessory to these uses. That protects existing brick and mortar restaurants from major competition from food trucks.

<u>Miscellaneous Provisions.</u> Provides the regulations for breweries, taprooms, wineries, distilleries, cocktail rooms, and brewpubs. These also are existing standards:

Sec. 36-1275. Breweries, Taprooms, Wineries, Distilleries, Cocktail Rooms and Brewpubs.

- (a) Shall comply with all regulations within Chapter 4 of the Edina City Code regarding Alcoholic Beverages.
- (b) Shall comply with all performance standards related to Sec. 36-644 (5), noise, vibration, dust, dirt, smoke, odor, glare, and liquid waste.
 - (5) Restrictions, controls, and design standards. Uses which, because of the nature of their operation, are accompanied by an excess of noise, vibration, dust, dirt, smoke, odor, noxious gases, glare, or wastes shall not be permitted. These residual features shall be considered as "excessive" when they either exceed or deviate from the limitations set forth in the following performance specifications:
 - a. Noise. Noise shall be measured on any property line of the tract on which the use is located. Noise shall be muffled so as not to become objectionable due to intermittence, beat frequency, shrillness or intensity. At the property line, the sound pressure level of noise radiated continuously from a facility shall not exceed the decibel levels given in the table set forth in this subsection. The sound pressure level shall be measured with a sound level meter and on an associated octave band analyzer, both of which shall be



manufactured according to current specifications prescribed by the American Standard Association. Measurement shall be made using the flat network of the sound level meter.

Octave Band Frequency (cycles per second)	Maximum Decibel Level	
20—75	65	
76—150	60	
151—300	55	
301—600	46	
601—1,200	40	
1,201—2,400	34	
2,401—4,800	31	
Over 4,800	28	

- b. Vibration. No use shall, at any time, cause earth vibrations perceptible beyond the limits of the tract on which the use is located.
- c. Dust and dirt. Solid or liquid particles shall not be emitted at any point in concentrations exceeding 0.3 grains per cubic foot of the conveying gas or air. For measurement of the amount of particles in gases resulting from combustion, standard corrections shall be applied to a stack temperature of 500 degrees Fahrenheit and 50 percent excess air.
- d. Smoke. Measurement shall be at the point of emission. The, then most recent Ringelmann Smoke Chart, published by the United States Bureau of Mines shall be used for the measurement of smoke and is hereby adopted and incorporated into this section by reference and made a part of this section. Smoke not darker or more opaque than No. I on the chart may be emitted. However, smoke not darker or more opaque than No. 2 on the chart may be emitted for periods not longer than four minutes in any 30-minute period. These provisions, applicable to visible grey smoke, shall also apply to visible smoke of a different color but with an equivalent apparent opacity.
- e. Odor. The use shall not, at any time, cause the discharge of toxic, noxious or odorous matter beyond the confines of the principal building in such concentrations as to be detrimental to or endanger the public health, welfare, comfort or safety.
- f. Glare. Glare, whether directed or reflected, such as from spotlights or high temperature processes, and as differentiated from general illumination, shall not be visible beyond the limits of the tract from which it originates.
- g. Liquid wastes. Liquid wastes containing any organic or toxic matter shall be discharged either into a public sanitary sewer or treated in a manner prescribed by the health department. The rate of liquid waste discharged into the city's sanitary sewerage system shall not exceed 200 gallons per lot per hour between the hours of 9:00 a.m. and 12:00 noon.



In order to ensure compliance with the performance standards set forth in this subsection, the planner may require the owner or operator of any use to make, without cost to the city, investigations and tests as the planner deems necessary to show adherence to the performance standards. The investigations and tests shall be done by an independent testing organization approved by the planner.

- (c) Parking shall comply with the standards in Section 36-1311.
- (d) Hours of operation shall comply with Section 4.
- (e) No outdoor storage is permitted on the site, including the mobile food unit, with the exception that waste handling (refuse and/or recycling) may occur in an enclosure that is fully screened from adjoining streets and residentially zoned property.
- (f) All exterior lighting shall be designed in such a way as to have no direction source of light visible from adjacent property and shall comply with the requirements of Section 36-1260.

Staff Recommendation

Staff recommends the Planning Commission recommend the City Council adopt the Ordinance to allow Breweries, Taprooms, Wineries, Distilleries and Cocktail Rooms in the POD-2 District. Approval is based on the following findings:

- I. The POD-2 Districts are located adjacent to the PID Zoning District where these uses are currently allowed.
- 2. Within both the PID and POD-2 zoning districts, the primary uses (office and light industrial) occur during the day. Breweries, Taprooms, Wineries, Distilleries and Cocktail Rooms operate primarily in the evening and on weekends so traffic and parking should not be an issue.
- 3. The City has not experienced any issues regarding the one taproom in town, the Wooden Hill Brewery.
- 4. The conditions and development standards placed on these uses have adequately addressed issues raised when the City originally considered these uses to be allowed in 2015-16.

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MEMO



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MEMO



ORDINANCE NO. 2020-__ AN ORDINANCE AMENDING CHAPTER 36 REGARDING DISTILLERIES IN THE POD-2, PLANNED OFFICE DISTRICT-2

The City Of Edina Ordains:

Section 1. Chapter 36, Sec. 36-575 "principal uses" is hereby amended to add the following:

Sec. 36-575. Principal uses.

The following are the principal uses permitted in the Planned Office District:

- (1) Business and professional offices.
- (2) Financial institutions, including drive-through facilities, but excluding pawn shops.
- (3) Post offices.
- (4) Clubs, lodge halls and nonprofit organizations, excluding those providing food or beverage services in the building or on the lot.
- (5) Facilities for athletic, health or weight control purposes, including, but not limited to, handball, racquetball, tennis, reducing salons and aerobic dance studios, provided that no seating or other facilities shall be allowed for spectator usage.
- (6) Medical and dental offices and clinics.
- (7) Employment agencies.
- (8) Travel bureaus.
- (9) Day care.
- (10) Public or private colleges, universities or schools.
- (11) Breweries, Taprooms, Wineries, Distilleries and Cocktail Rooms in the POD-2 District.

Section 2. Effective Date.

This Ordinance shall be effective on the date of second reading of this Ordinance for approval by the City Council.

First Reading: Second Reading: Published:	
Attest Sharon Allision, City Clerk	 James B. Hovland, Mayor

Please publish in the Edina Sun Current on Send two affidavits of publication Bill to Edina City Clerk

ORDINANCE NO. 2015-21 AN ORDINANCE AMENDING CHAPTER 36 REGARDING BREWERIES, TAPROOMS, WINERIES, COCKTAIL ROOMS, DISTILLERIES AND BREWPUBS

The City Of Edina Ordains:

Section 1. Chapter 36, Sec. 36-10 Definitions is hereby amended to add the following definitions:

Sec. 36-10. – Definitions.

Brewery. A facility that produces for sale malt liquor, and containing not less than one-half of one percent alcohol by volume. A brewery may include a taproom.

Brewpub. A Brewery that operates a restaurant, as defined in Chapter 4 of the City, on the same premises as the Brewery, whose malt liquor production per calendar year shall be limited by Minnesota State Statute.

Cocktail Room. A microdistillery licensed by the State of Minnesota that provided on-sale of distilled liquor produced by the distiller for the consumption on the premises of or adjacent to one distillery location owned by the distiller.

Distillery. A facility that produces distilled liquor, including all dilutions and mixtures thereof, for non-industrial uses. A distillery may include a cocktail room.

Taproom. An area for the on-sale consumption of malt liquor produced by the brewer for consumption on the premises of a brewery or an abutting property in common ownership of the brewer, which may include sales of malt liquor produced and packaged at the brewery for off premises consumption as allowed by Minnesota statutes, as may be amended.

Winery. A facility operated by the owner of a Minnesota farm and producing table, sparkling, or fortified wines from grapes, grape juice, other fruit bases, or honey with a majority of the ingredients grown or produced in Minnesota. A winery may include a tasting room.

Section 2. Chapter 36, Sec. 36-1311, minimum number of parking spaces required is hereby amended to add the following:

Sec. 36-1311. - Minimum number required.

- (bb) Taproom and Cocktail Room. One space per 40 square feet of gross floor area.
- (cc) Brewery, Winery and Distillery. One space per 1,000 square feet of gross floor area.
- (dd) Brewpub. One space per 1,000 gross square feet of production floor area, and spaces equal in number to one-third the maximum seating capacity, plus one space for each employee on the major shift for the restaurant.

Section 3. Chapter 36, Sec. 36-609, Principal Uses in the PCD-2 District is hereby amended to add the following: (This would also include the PCD-3 District.)

Sec. 36-608. - Principal uses in PCD-1 subdistrict.

The following are the principal uses permitted in the PCD-I subdistrict:

• Sec. 36-608. - Principal uses in PCD-1 subdistrict.

The following are the principal uses permitted in the PCD-I subdistrict:

- (I) Antique shops.
- (2) Art galleries.
- (3) Art studios.
- (4) Bakeries, provided the rooms containing the preparation and baking process shall not have a gross floor area in excess of 2,500 square feet.
- (5) Barbershops.
- (6) Beauty parlors.
- (7) Bicycle stores, including rental, repair and sales.
- (8) Book and stationery stores.
- (9) Camera and photographic supply stores.
- (10) Candy and ice cream stores.
- (11) Clothes pressing and tailoring shops.
- (12) Clothing stores, not exceeding 2,500 square feet of gross floor area.
- (13) Clubs, lodge halls and meeting rooms, offices and other facilities for nonprofit organizations, not exceeding 2,500 square feet of gross floor area.
- (14) Coin and philatelic stores.
- (15) Day care.
- (16) Drug stores.
- (17) Dry cleaning establishments and laundries.
- (18) Employment agencies.
- (19) Financial institutions, but excluding drive-through facilities and pawnshops.
- (20) Florist shops.
- (21) Food, grocery, meat, fish, bakery and delicatessen stores.
- (22) Garden supply, tool and seed stores.
- (23) Gift shops.
- (24) Handball courts, racquetball courts and exercise and reducing salons.
- (25) Hardware stores.
- (26) Hobby shops, for the sale of goods to be assembled and used off the premises.
- (27) Household furnishings, fixtures and accessory stores, not exceeding 2,500 square feet of gross floor area.
- (28) Interior decorating establishments.
- (29) Jewelry stores.
- (30) Launderettes.
- (31) Leather goods stores.
- (32) Liquor stores, municipally owned, off-sale.
- (33) Locksmith shops.
- (34) Medical and dental clinics.
- (35) Music and video sales and rental stores.
- (36) Musical instrument stores and repair shops.
- (37) Newsstands.
- (38) Offices, including both business and professional.
- (39) Optical stores.
- (40) Paint and wallpaper stores, not exceeding 2,500 square feet of gross floor area.
- (41) Personal apparel stores, not exceeding 2,500 square feet of gross floor area.
- (42) Picture framing and picture stores.
- (43) Repair stores and "fix-it" shops which provide services for the repair of home, garden, yard and personal use appliances.
- (44) Restaurants and brewpubs.
- , but excluding drive-ins and drive-through facilities, other than as allowed in section 36-1264(f).
- (45) Schools.

- (46) Secondhand stores, not exceeding 2,500 square feet of gross floor area, but excluding pawn shops.
- (47) Shoe sales or repair stores.
- (48) Sporting and camping goods stores, not exceeding 2,500 square feet of gross floor area.
- (49) Tailor shops.
- (50) Tobacco shops.
- (51) Toy shops.
- (52) Travel bureaus and transportation ticket offices.
- (53) Variety, gift, notion and soft goods stores.
- (54) Vending machines which are coin-or card-operated, but excluding amusement devices.

Section 4. Chapter 36 Division 9. Planned Industrial District is hereby amended to add the following:

Sec. 36-640. - Principal uses.

The following are principal uses permitted in the Planned Industrial District (PID) district:

- (I) All principal uses allowed in the Planned Office District.
- (2) Manufacturing, compounding, processing, packaging, treatment and assembly of products and materials.
- (3) Scientific research, investigation, testing or experimentation.
- (4) Warehousing of nonperishable products, if the products are owned by or consigned to the owner of the principal use or its lessee, and further, if the owner or lessee does not establish the principal use in the capacity of a carrier for the purpose of a freight operation.
- (5) Businesses that provide a service to the consumer on the consumer's property and not on the lot occupied by the principal use, including, but not limited to, building contractors, plumbing contractors, swimming pool construction and service companies, and exterminating offices.
- (6) Blueprinting, photostating and printing shops.
- (7) Office equipment showrooms and repair services.
- (8) Mini-storage warehouses for storage of domestic supplies, recreational vehicles and equipment and other private property, if the owner of the private property is responsible for transporting the property to and from the principal use, and further, if the owner of the principal use does not establish the use as a carrier for the purpose of a freight operation.
- (9) Animal hospitals, but excluding establishments with outside runs.
- (10) Currency exchanges, as defined in Minn. Stats. ch. 53A.
- (11) Breweries, Taprooms, Wineries, Distilleries and Cocktail Rooms.

Section 5. Chapter 36 Division 9. Planned Industrial District is hereby amended to add the following:

Sec. 36-642. - Accessory uses.

The following are the accessory uses permitted in the Planned Industrial District (PID):

- (I) Warehousing of products manufactured by the principal use.
- (2) Offices and administrative facilities.
- (3) Shipping and receiving spaces, mailing rooms and order pickup facilities.
- (4) Cafeterias, educational facilities, vending services and recreational establishments for persons employed by the business comprising the principal use.
- (5) Within buildings having office space comprising not less than 40,000 square feet of gross floor area, ten percent of the gross floor area may be occupied by retail uses allowed in the PCD-I and PCD-2 districts, if the accessory uses are accessible only from the interior of the principal building and have no signs or displays relative to it visible from the outside of the principal building.

- (6) Temporary retail sales pursuant to a permit issued in accordance with this division.
- (7) Off-street parking facilities.
- (8) Commercial kennels, as defined in section 8-5, which are accessory to animal hospitals, but excluding kennels with outside runs.
- (9) A mobile food unit licensed by the City of Edina (food truck) as an accessory use to a Taproom, Winery, Distillery or Cocktail Room. A mobile food unit (food truck) may only operate if there are no restaurants located within 500 feet of the taproom, winery, distillery or cocktail room, existing on the date the taproom, winery, distillery or cocktail room opens, or if at any time after the opening all restaurants within 500 feet close. If restaurants are subsequently opened, the mobile food unit may continue as an accessory use.

Section 6. Chapter 36 Article XII. Supplemental District Regulation is hereby amended to add the following:

Sec. 36-1275. Breweries, Taprooms, Wineries, Distilleries, Cocktail Rooms and Brewpubs.

- (a) Shall comply with all regulations within Chapter 4 of the Edina City Code regarding Alcoholic Beverages.
- (b) Shall comply with all performance standards related to Sec. 36-644 (5), noise, vibration, dust, dirt, smoke, odor, glare, and liquid waste.
- (c) Parking shall comply with the standards in Section 36-1311.
- (d) Hours of operation shall comply with Section 4-8-6.
- (e) No outdoor storage is permitted on the site, including the mobile food unit, with the exception that waste handling (refuse and/or recycling) may occur in an enclosure that is fully screened from adjoining streets and residentially zoned property.
- (f) All exterior lighting shall be designed in such a way as to have no direction source of light visible from adjacent property, and shall comply with the requirements of Section 36-1260.

Section 7. Effective Date.

First Reading: Second Reading:

This Ordinance shall be effective January 1st, 2016.

Published		
Attest		
	Debra A. Mangen, City Clerk	James B. Hovland, Mayor

Please publish in the Edina Sun Current on Send two affidavits of publication Bill to Edina City Clerk



ZONING ORDINANCE AMENDMENT APPLICATION

		CASE NUMBER	DATE		
			FEE PAID		
	of Edina Planning Departmen West Fiftieth Street * Edina, N		fax (952) 826-0389		
FEE:	\$1000.00				
APPL	LICANT:	370 2011 21/20 D 501	A A A A A A A A A A		
NIAM	E: Eric Anderson	TRO BOULEVARD EDI	1.50		
INAIVI	E: LICE ANDERSON	(Signature requi	red on back page)		
ADDI	RESS:	PHO	NE:		
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	,				
	iled Requirements: Unless wai ollowing items with this application			Į of	
_	Application fee (not refund	able). Make check payable to	"City of Edina."		
_	TO PERINIT DISTIL	oing why the City should amen LERIES WITHIN THE IG COCKTAILS, HOURS	POD ESTABLISH	HMENT WOULD BE WID BE 4-12 PM	DISTILLUNG THES - SAT

ZONING ORDINANCE AMENDMENT APPLICATION INFORMATION:

The City of Edina Planning Department encourages healthy development within the city of Edina. Although this document is meant to serve as a guide for the application process for development through the Planning Department it is by no means comprehensive. The Planning Staff recommend that you schedule a meeting to answer any questions or to discuss issues that may accompany your project. It is much easier to tackle problems early on in the process. The office number for the Planning Staff is (952) 826-0465.

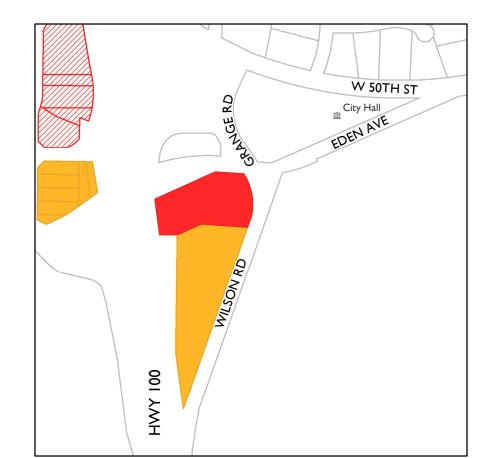
<u>Application</u>: Applications are submitted to the Planning Department. Offices are open Monday through Friday, 8 AM to 4:30 PM.

<u>Meetings and Public Hearings</u>: Applications are first considered by the Planning Commission at their regular monthly meetings. (Generally the second and fourth Wednesday of each month.) The Commission holds a public hearing and adopts a recommendation which is forwarded to the City Council for consideration.

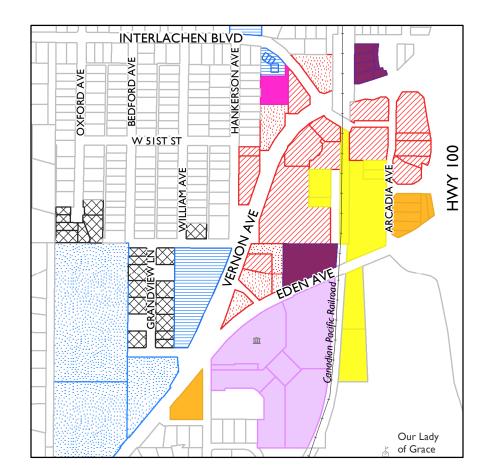
Staff Report: Staff prepares a report and recommendation and sends it along with the

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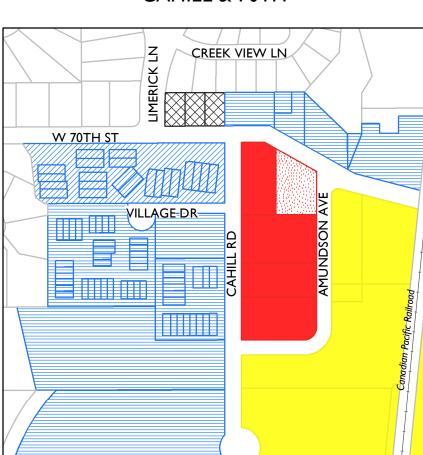
WILSON & EDEN

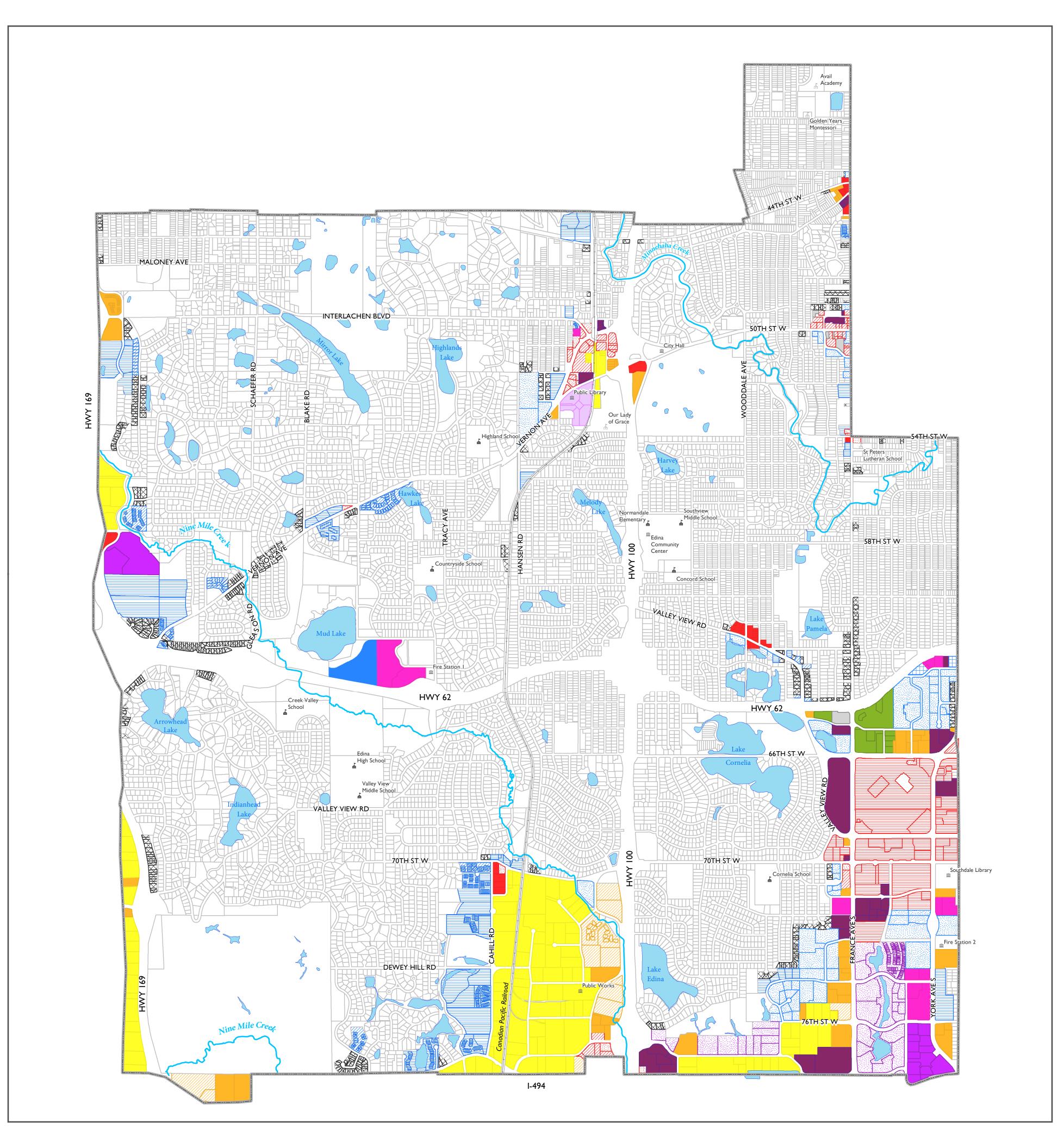


GRANDVIEW



CAHILL & 70TH

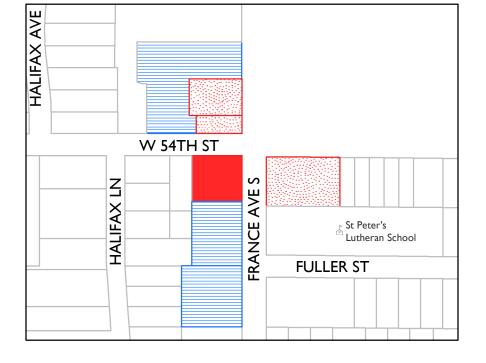




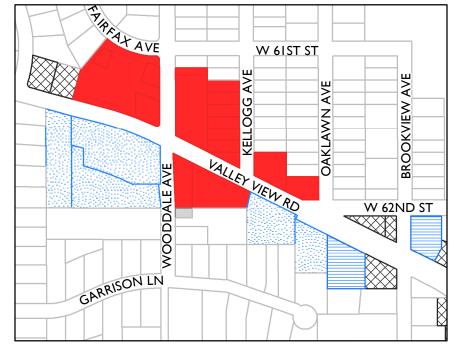
50TH & FRANCE



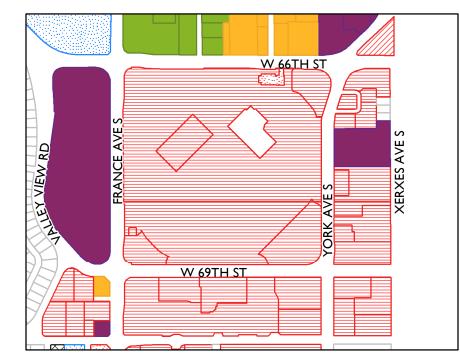
54TH & FRANCE



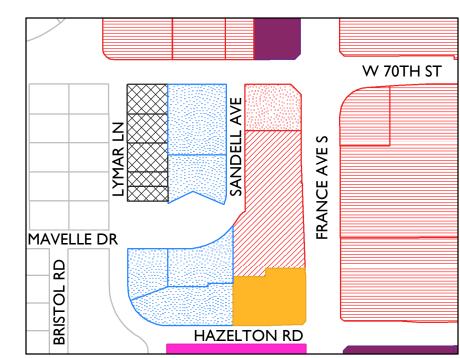
VALLEY VIEW & WOODDALE



SOUTHDALE



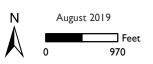
70TH & FRANCE

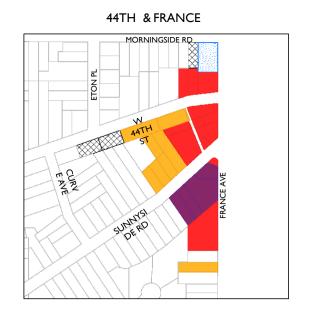


Zoning Map

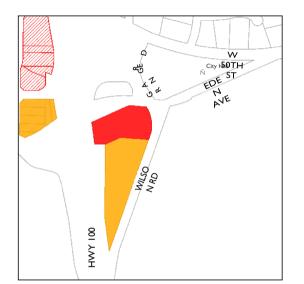
Legend

- R-I Single Dwelling Unit
- R-2 Double Dwelling Unit
- PRD-I Planned Residence
- PRD-2 Planned Residence
- PRD-3 Planned Residence
- PRD-4 Planned Residence
- PRD-5 Planned Residence
- PCD-I Planned Commercial
- PCD-2 Planned Commercial
- PCD-3 Planned Commercial
- PCD-4 Planned Commercial
- POD-I Planned Office
 - POD-2 Planned Office
- RMD Regional Medical
- PID Planned Industrial
- PUD Planned Unit Development
- APD Automotive Parking
- PSR-4 Planned Residence
- MDD-4 Mixed Development
- MDD-5 Mixed Development
- MDD-6 Mixed Development
 - Lakes
 - Creeks
- Edina Boundary
- --- Railroad
- Public Building
- Public School
- Private School

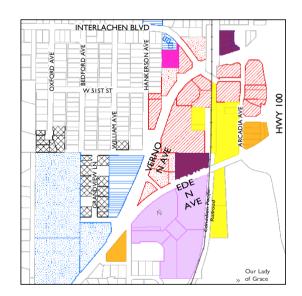




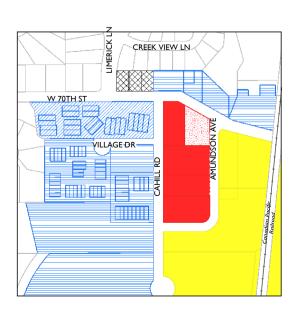
WILSON & EDEN

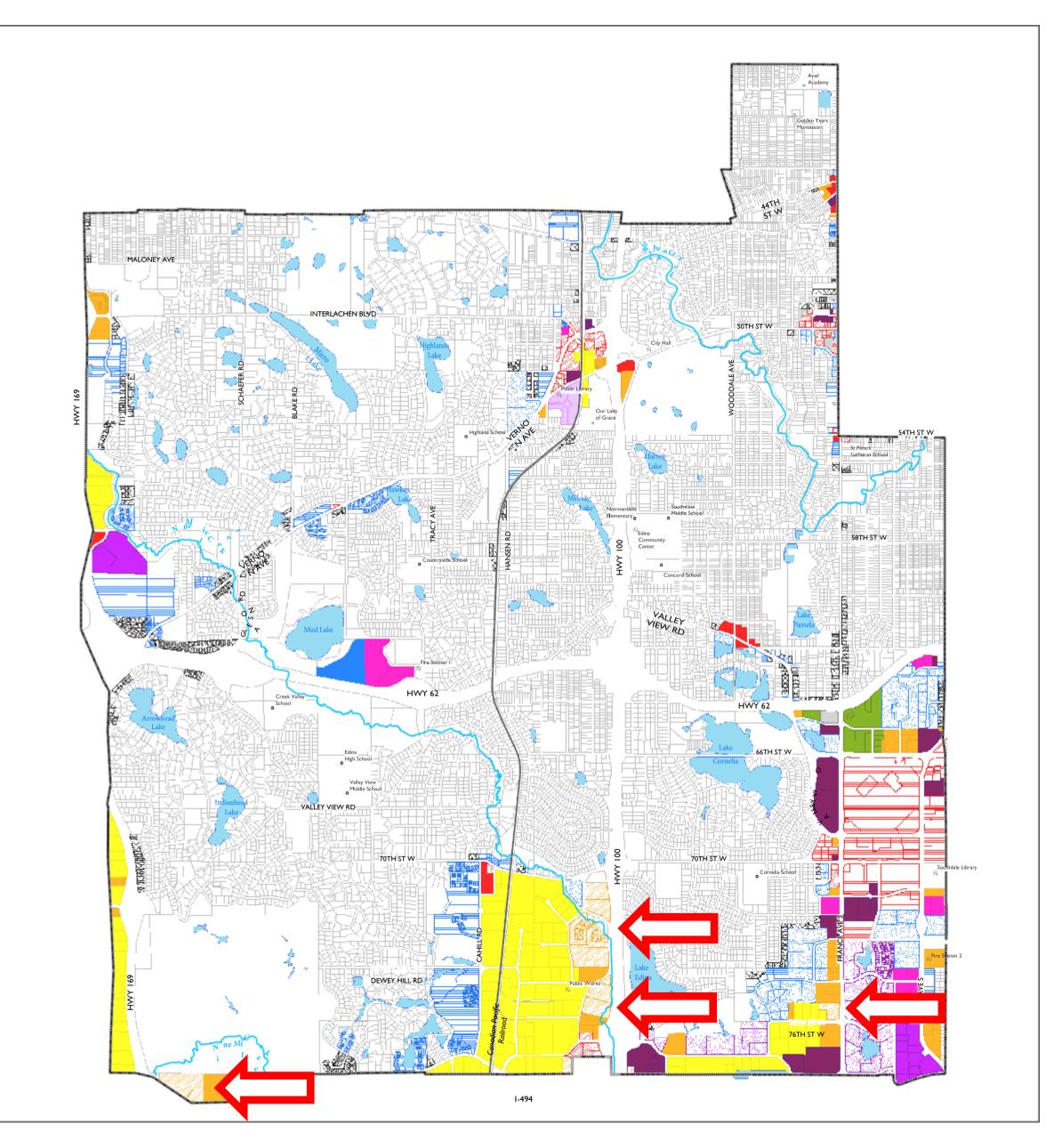


GRANDVIEW



CAHILL & 70TH

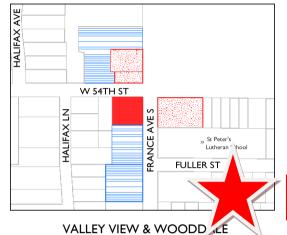


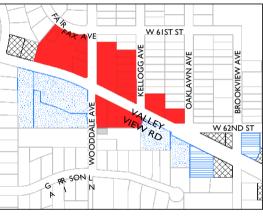






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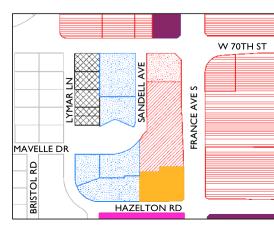




SOUTHDALE



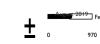
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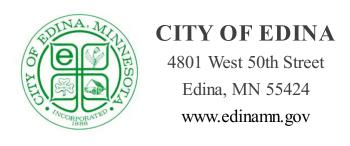


Zoning Map Legend

- R-I Single Dwelling Unit
- R-2 Double Dwelling Unit
- PRD-I Planned Residence
- PRD-2 Planned Residence
- PRD-3 Planned Residence
- PRD-4 Planned Residence
- PRD-5 Planned Residence
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- PCD-2 Planned Commercial
- PCD-3 Planned Commercial
- PCD-4 Planned Commercial

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- APD Automotive Parking
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- MDD-5 Mixed Development
- MDD-6 Mixed Development
- Lakes
 - Creeks
- ---- Edina Boundary
- ---- Railroad
- N Public Building
- Public School
- Private School





Date: July 8, 2020 Agenda Item #: V.C.

To: Planning Commission Item Type:

Report and Recommendation

From: Cary Teague, Community Development Director

Item Activity:

Subject: PUBLIC HEARING: Conditional Use Permit with

Action

Variances - 4425 Valley View Road

ACTION REQUESTED:

Recommend the City Council approve the Conditional Use Permit and Variances, subject to the findings and conditions outlined in the staff memo, to redevelop 4425 Valley View Road with multi-family housing.

INTRODUCTION:

See attached staff report.

ATTACHMENTS:

Staff Report

Building Official & Fire Marshal Review

Engineering Memo

Proposed Plans 1 of 2

Proposed Plans 2 of 2

Building Elevations

Applicant Narrative 1 of 3

Applicant Narrative 2 of 3

Applicant Narrative 3 of 3 (CUP)

Site Location

Valley View Wooddale - Small Area Plan Pages

Traffic and Parking Study

Transportation Review

Sustainability Questionnaire

STAFF REPORT



Date:

July 8, 2020

To:

Planning Commission

From:

Cary Teague, Community Development Director

Subject:

Conditional Use Permit with Variances – 4425 Valley View Road

Information / Background:

The applicant, DJR Architects is proposing to tear down the existing one-story day-care building at 4425 Valley View Road, and construct a new three-story, 21-unit market rate apartment with 32 underground parking stalls. (See attached plans and narrative.) The density proposed is consistent with the Comprehensive Plan/Small Area Plan which limits the density to 30 units per acre for this site.

The site is currently zoned PCD-1, Planned Commercial District. Multi-family residential uses are conditionally permitted. The request requires the following:

- A Conditional Use Permit to allow residential uses within a planned commercial zoning district (PCD-I).
- ➤ Building height variance from 2 stories to 3 stories. (Small Area Plan suggests 4 stories).
- Front setback variance from 36 feet to 16 feet (patio 2 feet); Side and rear setback variances from 36 to 20 feet; floor area ratio variance from 1.0 to 1.08
- Drive Aisle setback variances from 10 to 0 feet.

The property is .7 acres in size. The density proposed in the project would be 30 units per acre. (21 units total.) This site is guided in the Comprehensive Plan as NN, Neighborhood Node, which allows up to 30 units per acre in this area. (See attached Small Area Plan documents.) The proposal is generally consistent with the Comprehensive Plan/Small Area Plan in terms of height and density proposed. The applicant is proposing the buy in option

to meet the City's affordable housing policy. At 21 units, the applicant would be required to pay \$300,000 prior to issuance of a building permit.

The applicant did go through a sketch plan review for the proposed project. The plans were generally well received by both the Planning Commission and City Council. A few changes have been made:

- Revised the sidewalk along Valley View Road to a boulevard style, as recommend in the small area plan.
- Revised the drive entrance to be located on the subject property.

SUPPORTING INFORMATION

Surrounding Land Uses

Northerly: Edina Flats condos; zoned PCD-I, Planned Commercial district and guided NN,

Neighborhood Node.

Easterly: Multi-family housing; zoned PRD-4, Planned Residential District-4 and guided HDR,

High Density Residential.

Southerly: City owned property (wetland) – Further south are single-family homes; zoned R-

I, Single Dwelling Unit District; and guided Low Density Residential.

Westerly: Commercial building; zoned PCD-I, Planned Commercial District and guided NN,

Neighborhood Node

Existing Site Features

The subject properties total .7 acres in size and contains a surface parking lot and the vacated New Horizon Day Care facility. The site sits high and drops off in the back to a City of Edina owned wetland to the south.

Planning

Guide Plan designation: NN, Neighborhood Node

Zoning: PCD-I, Planned Commercial District

Parking

The proposed parking would be code compliant. One space per unit is required for units under 1,500 square feet (18); and 1.5 spaces for units over 1,500 square feet in size (3). Therefore 23 spaces are required and 32 are provided. (1.5 spaces per unit overall) All units would be enclosed and accessed behind the building.

Site Circulation

Access to all the housing units would be off Valley View Road. A new access would be created on the east end of the site. The property access for the site to the east currently encroaches on to the subject property. That owner plans to remove the encroachment and narrow their drive to be on their property. A variance is required to locate the drive aisle on the property line. Staff recommends shifting the drive-aisle on the east lot line 2-3 feet away from the lot line to provide some separation with the adjacent drive-aisle. The applicant is agreeable to the shift. The two property owners are considering construction of a shared drive for the two sites. Should the property owner to the east agree to share a driveway with the subject property, the driveway would be located on the lot line, subject to shared access and maintenance agreements between the two property owners.

Landscaping

Based on the perimeter of the site 19 over-story trees would be required. Existing trees would be removed and 23 over story trees planted, along with a full complement of under-story shrubs and bushes (153) are also proposed. To shield headlights from cars coming down the driveway to access the garage in back, staff recommends the guardrail be constructed all along the south drive to shield headlights.

Grading/Drainage/Utilities

The city engineer has reviewed the proposed plans and found them to be acceptable subject to the comments and conditions outlined in their review memo attached. Any approvals of this project would be subject to review and approval of the Minnehaha Creek Watershed Districts, as they are the City's review authority over the grading of the site.

Building/Building Material

The building materials would be a variety combination of brick, stone, lap siding, and metal trim. (See attached renderings.) The applicant will have a materials board for the Planning Commission to review at the Planning Commission meeting.

Height

The proposed height is 3 stories and 36 feet tall. The proposed height is consistent with the small area plan that calls for 4 stories on this site. The current zoning ordinance requires a maximum of two stories and 24 feet; therefore, a variance is requested.

Living Streets/Multi-Modal Consideration

Sec. 36-1274. - Sidewalks, trails, and bicycle facilities.

(a) In order to promote and provide safe and effective sidewalks and trails in the city and encourage the use of bicycles for recreation and transportation, the following improvements are required, as a condition of approval, on developments requiring the

approval of a final development plan or the issuance of a Conditional Use Permit pursuant to Article V of this Chapter:

- (I) It is the policy of the city to require the construction of sidewalks and trails wherever feasible to encourage pedestrian and bicycle connectivity throughout the city. Therefore, developments shall provide sidewalks and trails which adjoin the applicant's property:
 - a. In locations shown on the city's sidewalk and trail plan; and
 - b. In other locations where the council finds that the provision of such sidewalks and trails enhance public access to mass transit facilities or connections to other existing or planned sidewalks, trails, or public facilities.
- (2) Developments shall provide sidewalks between building entrances and sidewalks or trails which exist, or which will be constructed pursuant to this section.
- (3) Developments shall provide direct sidewalk and trail connections with adjoining properties where appropriate.
- (4) Developments must provide direct sidewalk and trail connections to transit stations or transit stops adjoining the property.
- (5) Design standards for sidewalks and trails shall be prescribed by the engineer.
- (6) Nonresidential developments having an off-street automobile parking requirement of 20 or more spaces must provide off-street bicycle parking spaces where bicycles may be parked and secured from theft by their owners. The minimum number of bicycle parking spaces required shall be five percent of the automobile parking space requirement. The design and placement of bicycle parking spaces and bicycle racks used to secure bicycles shall be subject to the approval of the city engineer. Whenever possible, bicycle parking spaces shall be located within 50 feet of a public entrance to a principal building.
- (b) The expense of the improvements set forth in subsection (a) of this section shall be borne by the applicant.

The applicant would be installing boulevard sidewalks along Valley View Road to provide a more safe and comfortable walking experience. A bike room with space for 20 bikes would be provided within the building, and additional spaces for guest bike parking near the front door.

Affordable Housing

The applicant is proposing the buy in option to meet the City's affordable housing policy. At 21 units, the applicant would be required to pay \$300,000 prior to issuance of a building permit.

Conditional Use Permit

Per Section 36-305, the City Council shall not grant a Conditional Use Permit unless it finds that the establishment, maintenance, and operation of the use:

I. Does not have an undue adverse impact on governmental facilities, utilities, services or existing or proposed improvements.

The existing utilities are adequate to serve the proposed use. A traffic study was done by Wenck Associates, which concludes that the existing roadways can support the proposed development. (See the attached traffic study.)

2. Will generate traffic within the capacity of the streets serving the property.

As mentioned, Wenck conducted a traffic study to determine the impact of the proposed development on the existing roadways. As demonstrated on page 7-1 of the study, the level of service on the adjacent roadways and intersections would remain the same. Traffic generated from a 21-unit apartment would actually be less than the previous day care use.

3. Does not have an undue adverse impact on the public health, safety, or welfare.

This site is located in the Valley View/Wooddale NN, Neighborhood Node, which is described in the Comprehensive Plan as a small to moderate-scale commercial residential or mixed-use area. The proposed residential development is consistent with the density allowed in the comprehensive plan at 30 units per acre. The height of the project is one story less that the four-story maximum suggested in the Comprehensive Plan.

4. Will not impede the normal and orderly development and improvement of other property in the vicinity.

The development fits in well on the site. It would eliminate surface parking and creates a safer pedestrian experience.

5. Conforms to the applicable restrictions and special conditions of the district in which it is located as imposed by this Section.

Except for the variances that are requested, the plans meet all city code provisions and are consistent with the Comprehensive Plan. As a conditionally permitted use within the PCD-I District, which is primarily a commercial retail zoning district; a residential use should be integrated into the area where it is located. Multi-family housing is consistent with land uses to the north and east of the site.

6. Is consistent with the Comprehensive Plan.

As mentioned previously, the proposed use and density of the proposal is consistent with the Comprehensive Plan. This site is located in the Valley View/Wooddale NN, Neighborhood Node, which is described in the Comprehensive Plan as a small to moderate-scale commercial residential or mixed-use area. The NN allows up to 30 units per acre. The Plan allows up to four stories on the site, and the proposal if for three stories.

Compliance Table

	City Standard (PCD-I)	Proposed Lot line			
Building Setbacks Front – Valley View Side – East Side – West Rear	36 feet (based on height) 36 feet (based on height) NA – PCD I 36 feet (based on height)	16 feet* (2 feet patio/post) 35 feet 7 feet 20 feet*			
Drive Aisle Building Height	10 feet 2-stories & 24 feet	0 feet* 3 stories & 36 feet*			
Floor Area Ratio (FAR) Density (small area plan)	1.0	1.08* 29.5			
Parking	I space per unit (18 units) I.5 if unit is over I,500 s.f. (3 units) = 23 spaces required	32 spaces proposed			

^{*}Variances required

Variance - Building Setbacks & Height

Per the compliance table above, variances are requested for the building setbacks and height. The PCD zoning district, however, encourages buildings to be brought up to the street to create a pedestrian friendly environment, which is what the proposed project is attempting to accomplish.

Per the Zoning Ordinance, a variance should not be granted unless it is found that the enforcement of the Ordinance would cause practical difficulties in complying with the Zoning Ordinance and that the use is reasonable. As demonstrated below, staff believes the proposal does meet the variance standards, when applying the three conditions:

Minnesota Statues and Edina Ordinances require that the following conditions must be satisfied affirmatively. The Proposed Variance will:

I) Relieve practical difficulties that prevent a reasonable use from complying with ordinance requirements.

Reasonable use does not mean that the applicant must show the land cannot be put to any reasonable use without the variance. Rather, the applicant must show that there are practical difficulties in complying with the code and that the proposed use is reasonable. "Practical difficulties" may include functional and aesthetic concerns.

Staff believes the proposed variances are reasonable. The practical difficulty is the small lot size, and odd shape which could not fit a three-story building (one story shorter than allowed in the small area plan) without the need for a variance. (See attached building pad for a three-story building.) The limited building pad also compromises the area for drive-aisle setbacks. The property to the south is a City owned wetland that makes the site appear bigger.

The proposed height is reasonable as it is one-story less than recommended in the Valley View/Wooddale small area plan and is well screened from the single-family homes to the south.

2) There are circumstances that are unique to the property, not common to every similarly zoned property, and that are not self-created?

Yes. The site is unique in the PCD-I zoning district, given small size, limited depth, and odd shape. These conditions were not created by the applicant. The City encourages buildings to be brought up to the street, rather than having large parking lots in front of the building from the adjacent streets.

3) Will the variance alter the essential character of the neighborhood?

No. The proposed buildings have been designed to fit the neighborhood and are consistent with the Valley View/Wooddale small area plan. The proposed development would add to the neighborhood and create some vibrancy at a currently predominantly vacant area.

PRIMARY ISSUES/STAFF RECOMMENDATION

Primary Issues

Are the Variances & CUP justified?

Yes, staff believes the variances are justified for the following reasons:

- 1. The practical difficulty is the small lot size, and odd shape which could not fit a three-story building (one story shorter than allowed in the small area plan) without the need for a variance. The site appears larger due to the wetland to the south located on city owned property.
- 2. The limited building pad also compromises the area for drive-aisle setbacks. Chapter 36 of the Zoning Ordinance suggests that the City Council will consider exceptions to the setback requirements if the use creates an active pedestrian and streetscape environment, which is proposed.
- 3. The proposed height is reasonable as it is one-story less than recommended in the Valley View/Wooddale small area plan. The site is well screened by existing vegetation to the single-family homes to the south.

4. As demonstrated on pages 4-5 of this report, the proposal meets the conditional use permit criteria.

5. As demonstrated on page 6 of this report, the proposal meets the variance criteria

Staff Recommendation

Recommend that the City Council approve the Conditional Use Permit with Variances at 4425 Valley View Road.

Approval is subject to the following findings:

- I. The practical difficulty is the small lot size, and odd shape which could not fit a three-story building (one story shorter than allowed in the small area plan) without the need for a variance. The limited building pad also compromises the area for drive-aisle setbacks.
- 2. The site appears larger due to the wetland to the south located on city owned property.
- 3. Chapter 36 of the Zoning Ordinance suggests that the City Council will consider exceptions to the setback requirements if the use creates an active pedestrian and streetscape environment, which is proposed.
- 4. The existing roadways would support the project. Wenck conducted a traffic impact study based on the proposed development and concluded that the existing traffic generated from the project would be supported by the existing roads.
- 5. The proposed uses are consistent with the Comprehensive Plan.
- 6. The proposal meets the Conditional Use Permit Standards of Chapter 36.
- 7. The project is consistent with the Valley View and Wooddale Small Area Plan.

Approval is subject to the following Conditions:

- 1. The Final Development Plans must be consistent with the Preliminary Development Plans dated June 22, 2020, and the materials board as presented to the Planning Commission.
- 2. The Final Landscape Plan must meet all minimum landscaping requirements per Chapter 36 of the Zoning Ordinance. A performance bond, letter-of-credit, or cash deposit must be submitted for one and one-half times the cost amount for completing the required landscaping, screening, or erosion control measures at the time of any building permit. The property owner is responsible for replacing any required landscaping that dies after the project is built.
- 3. Compliance with all the conditions outlined in the city engineer's memo dated July 1, 2020.

4. Compliance with all the conditions outlined in the building official and fire marshal memo dated June 24, 2020.

- 5. Submit a copy of the Minnehaha Creek Watershed District permit. The City may require revisions to the approved plans to meet the district's requirements.
- 6. The southbound driveway shall be shifted 2-3 feet to the west to allow a separation between the adjacent property's driveway. Should the property owner to the east agree to share a driveway with the subject property, the driveway shall be located on the lot line subject to shared access and maintenance agreements.
- 7. The guardrail along the south lot line shall be constructed to shield headlights from the single-family homes to the south.
- 8. To meet the City's affordable housing policy; at 21 units, the applicant would be required to pay \$300,000 prior to issuance of a building permit.

Deadline for a city decision: September 15, 2020

STAFF REPORT



Date: June 24, 2020

To: Cary Teague, Community Development Director

From: David Fisher, Chief Building Official & Rick Hammerschmidt, Fire Marshall

Subject: 4425 Valley View Rd – New Apartment Building

Information / Background:

New twenty-one-unit apartment building.

- This would be an R-2 building using the International Building Code.
- A NFPA 72 Fire alarm System is required.
- A minimum of a 13-R Fire Sprinkler System is required.
- A Class I Fire Standpipe System will be needed in the stairwells because of the occupied roof top level that is over 30 feet above the lowest level of fire vehicle access.
- Verify the address required.
- Recommend meeting with staff for 30, 60 and 90 percent meeting before submitted for building permit.
- Recommend escrow is provided with the demo and new building permits to assure City standards are met for code compliance.



DATE: July 1, 2020

TO: Cary Teague – Community Development Director

FROM: Zuleyka Marquez – Graduate Engineer

RE: 4425 Valley View Rd – Development Review

The Engineering Department has reviewed the subject property for pedestrian facilities, utility connections, grading, and storm water. Plans reviewed were existing conditions survey, grading plans, utility plans, and erosion control plans dated 06/05/2020.

Review Comment

Required For

Ge	neral					
I.	Deliver as-build records of public and private utility infrastructure post construction.	Certificate of Occupancy				
Su	rvey					
2.	An existing and proposed site condition survey is required.	Grading/Building Permit				
2.1	Show all existing and proposed public and private easements. Sanitary sewer easement located on the SW portion of the site not depicted on survey dated 2/7/20.	Grading/Building Permit				
Liv	ing Streets					
3.	Design sidewalks to meet ADA requirements. Sidewalk shall conform to Standard Plate 420 and be a boulevard-style sidewalk.	Grading/Building Permit				
4.	Saw cut concrete sidewalk joints on public sidewalks.	Grading/Building Permit				
Tra	affic and Street					
5.	Review fire access requirements with fire department.	Grading/Building Permit				
6.	Implement City-approved recommendations per Transportation Memo.	Grading/Building Permit				
7.	Commercial Curb Cut permit required for driveway entrance replacement. Must conform to Standard Plate 415.	Prior to Reconstructing Entrance				
8.	Valley View Rd was reconstructed in 2005 and will be milled and overlaid 2025. Road patching shall conform to Edina Standard Plates 540 and 543 prior to 2025 mill & overlay project.	Certificate of Occupancy				
9.	Bike lane pavement markings to be re-striped.	Certificate of Occupancy				
Sai	nitary and Water Utilities					
10.	Verify fire demand and hydrant locations.	Grading/Building Permit				



11.	Domestic water shall be sized by the developer's engineer.	Grading/Building Permit	
12.	Domestic sanitary shall be sized by the developer's engineer.	Grading/Building Permit	
13.	Apply for a sewer and water connection permit with Public Works.	Prior to Starting Utility Work	
13.1	Meter required for building service line and combined lines. No meter required for fire only service line.	Grading/Building Permit	
13.2	Public Works to determine acceptable installation methods.	Grading/Building Permit	
14.	Disconnected sanitary and water services to be capped at main.		
15.	A SAC determination will be required by the Metropolitan Council. The SAC determination will be used by the City to calculate sewer and water connection charges.	Grading/Building Permit	
16.	Single connection from main for fire and domestic, split after main connection.	Grading/Building Permit	
Sto	rm Water Utility		
17.	Provide geotechnical report with soil borings.	Grading/Building Permit	
18.	Provide hydraulic and hydrologic report meeting watershed and state construction site permit requirements.	Grading/Building Permit	
19.	Submit watershed district permit and copies of private maintenance agreement in favor of watershed.	Grading/Building Permit	
20.	Demonstrate no increase in peak flood elevations for the I-percent-annual-chance flood event (NOAA Atlas I4, I00-year).	Grading/Building Permit	
21.	The local I-percent-annual-chance flood elevation is 867.7'. The lowest floor elevation is required at no less than 869.7'.	Grading/Building Permit	
22.	The proposed below-grade garage elevation is 880.55', above the I-percent-annual-chance flood elevation. Note, the structure (including the parking garage) must be flood proofed to two feet above the applicable I-percent-annual-chance flood elevation in accordance with the design standards listed under the CWRMP Section 3.1.2.2, item 2 (PDF page 49 of 435).	Grading/Building Permit	
Gra	ding Erosion and Sediment Control		
23.	A SWPPP consistent with the state general construction site stormwater permit is required.	Grading/Building Permit	
Mis	cellaneous		
24.	Construction staging, traffic control, and pedestrian access plans will be required.	Grading/Building Permit	

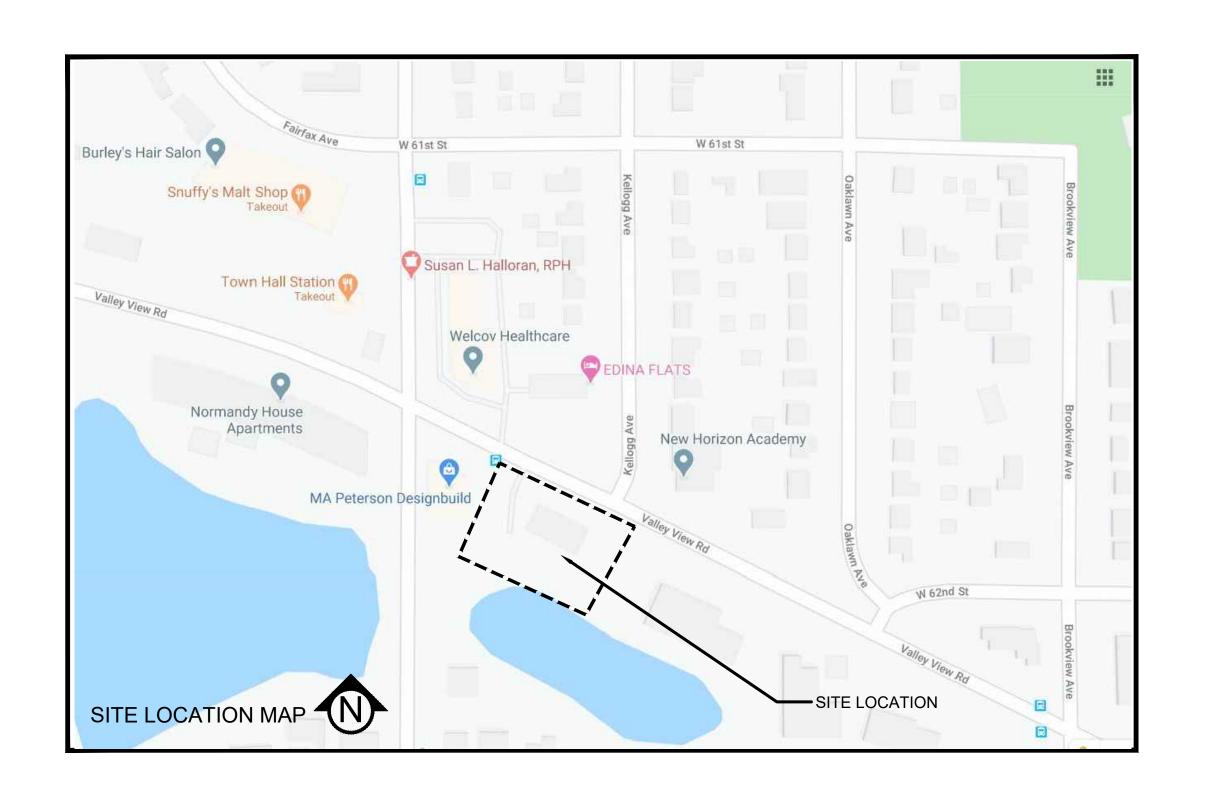


25.	Retaining walls greater than 4-feet are proposed, submit drawings, cross-section and calculations prepared and signed by a Minnesota licensed professional engineer.	Grading/Building Permit	
26.	Apply for a road/lane closure permit through Public Works as necessary.	Prior to Starting Utility Work	
27.	Protect City electrical conduit along Valley View Rd immediately adjacent to existing sidewalk during construction.	During Construction	
Otl	ner Agency Coordination		
28.	Hennepin County, MDH, MPCA and MCES permits required as needed.	Grading/Building Permit	
29.	Nine Mile Creek Watershed Districts permit is required.	Grading/Building Permit	

4425 Valley View Road

EDINA, MINNESOTA

ISSUED FOR: CITY SUBMITTAL



EXISTING ELECTRIC BOX

EXISTING GAS VALVE

ARCHITECT:

DJR 333 WASHINGTON AVENUE NORTH, SUITE 401

DEVELOPER / PROPERTY OWNER:

ABDO MARKETPLACE, LLC MINNEAPOLIS, MN MICHAEL MARGULIES 612.205.0521

MINNEAPOLIS, MN 55401

ENGINEER / LANDSCAPE ARCHITECT:

CIVIL SITE GROUP 4931 W 35TH STREET SUITE 200 ST LOUIS PARK, MN 55416 612-615-0060

SURVEYOR:

CIVIL SITE GROUP 4931 W 35TH STREET SUITE 200 ST LOUIS PARK, MN 55416 612-615-0060

MASTER LEGEND: EX. 1' CONTOUR ELEVATION INTERVAL PROPOSED MANHOLE STORM \times 1137.12 EXISTING SPOT GRADE ELEVATION PROPOSED CATCH BASIN OR CATCH BASIN MANHOLE STORM 1.0' CONTOUR ELEVATION INTERVAL ------1137----- PROPOSED GATE VALVE SPOT GRADE ELEVATION (GUTTER/FLOW LINE PROPOSED FIRE HYDRANT UNLESS OTHERWISE NOTED) PROPOSED MANHOLE SANITARY 891.00 G SPOT GRADE ELEVATION TOP OF CURB (GUTTER TOP) PROPOSED SIGN 891.00 TC SPOT GRADE ELEVATION TOP OF WALL 891.00 BS/TS SPOT GRADE ELEVATION BOTTOM OF WALL PROPOSED LIGHT DRAINAGE ARROW PROPOSED SANITARY SEWER PROPOSED STORM SEWER EMERGENCY OVERFLOW EOF=1135.52 PROPOSED WATER MAIN SILT FENCE / BIOROLL - GRADING LIMIT -----EXISTING SANITARY SEWER INLET PROTECTION EXISTING STORM SEWER **EXISTING WATER MAIN** STABILIZED CONSTRUCTION ENTRANCE EXISTING GAS MAIN EXISTING UNDERGROUND ELECTRIC SOIL BORING LOCATION EXISTING UNDERGROUND CABLE ☆ EXISTING LIGHT CURB AND GUTTER (T.O = TIP OUT) EXISTING MANHOLE EXISTING STOPBOX ☐ EXISTING CATCH BASIN **EXISTING GATE VALVE** EXISTING GAS METER

EXISTING HYDRANT



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SHEET NUMBER | SHEET TITLE

C0.0 | TITLE SHEET

V1.0 | SITE SURVEY

C2.0 SITE PLAN

C1.0 | REMOVALS PLAN

C3.0 GRADING PLAN

C4.0 UTILITY PLAN

C5.0 CIVIL DETAILS

C5.1 CIVIL DETAILS

C5.2 CIVIL DETAILS

L1.0 LANDSCAPE PLAN

SW1.2 | SWPPP - DETAILS

SW1.3 | SWPPP - NARRATIVE

L1.1 LANDSCAPE PLAN NOTES & DETAILS

SW1.0 | SWPPP - EXISTING CONDITIONS

SW1.1 SWPPP - PROPOSED CONDITIONS

DRAWN BY:kit REVIEWED BY: XX
PROJECT NUMBER: 19261

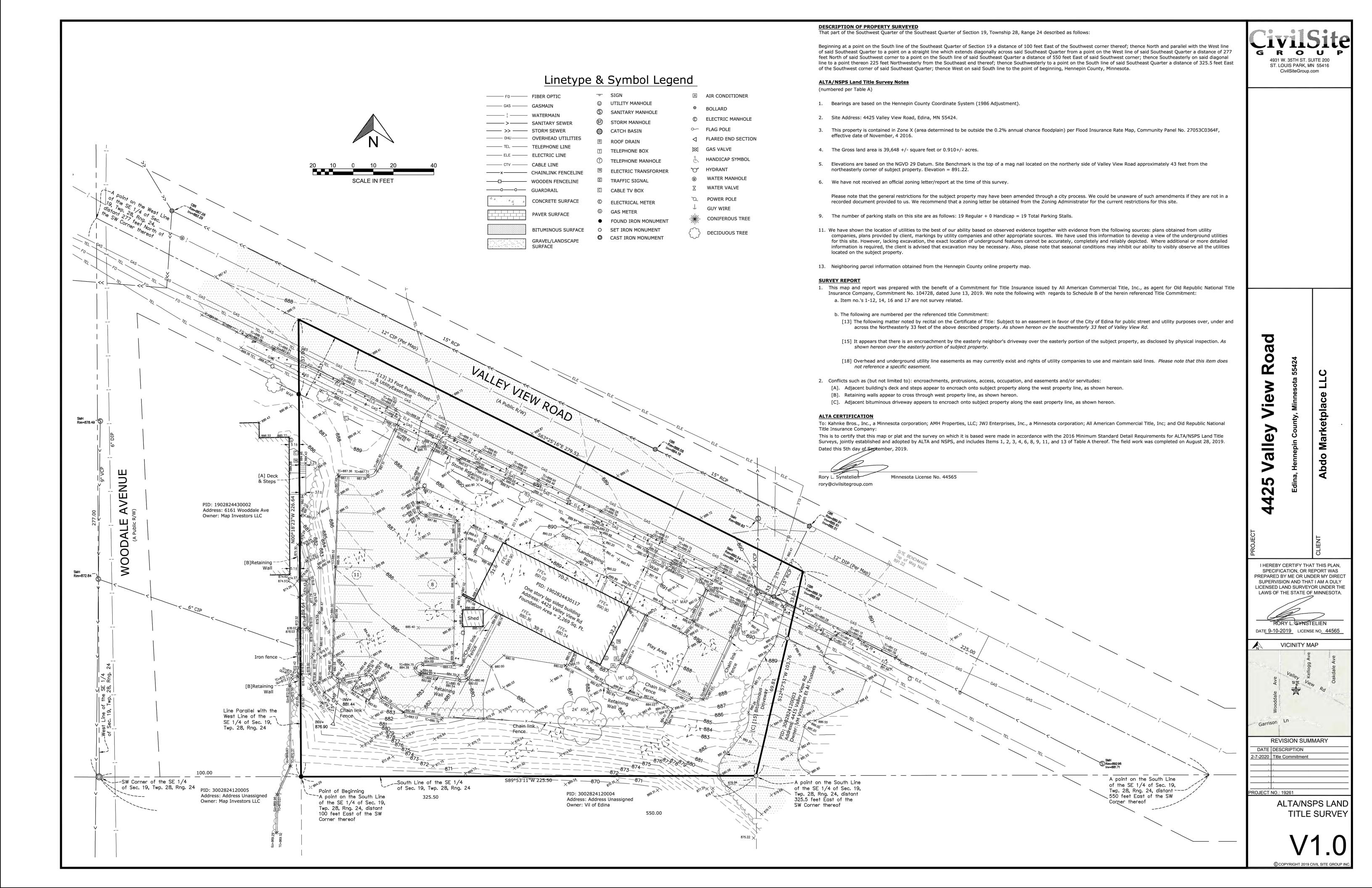
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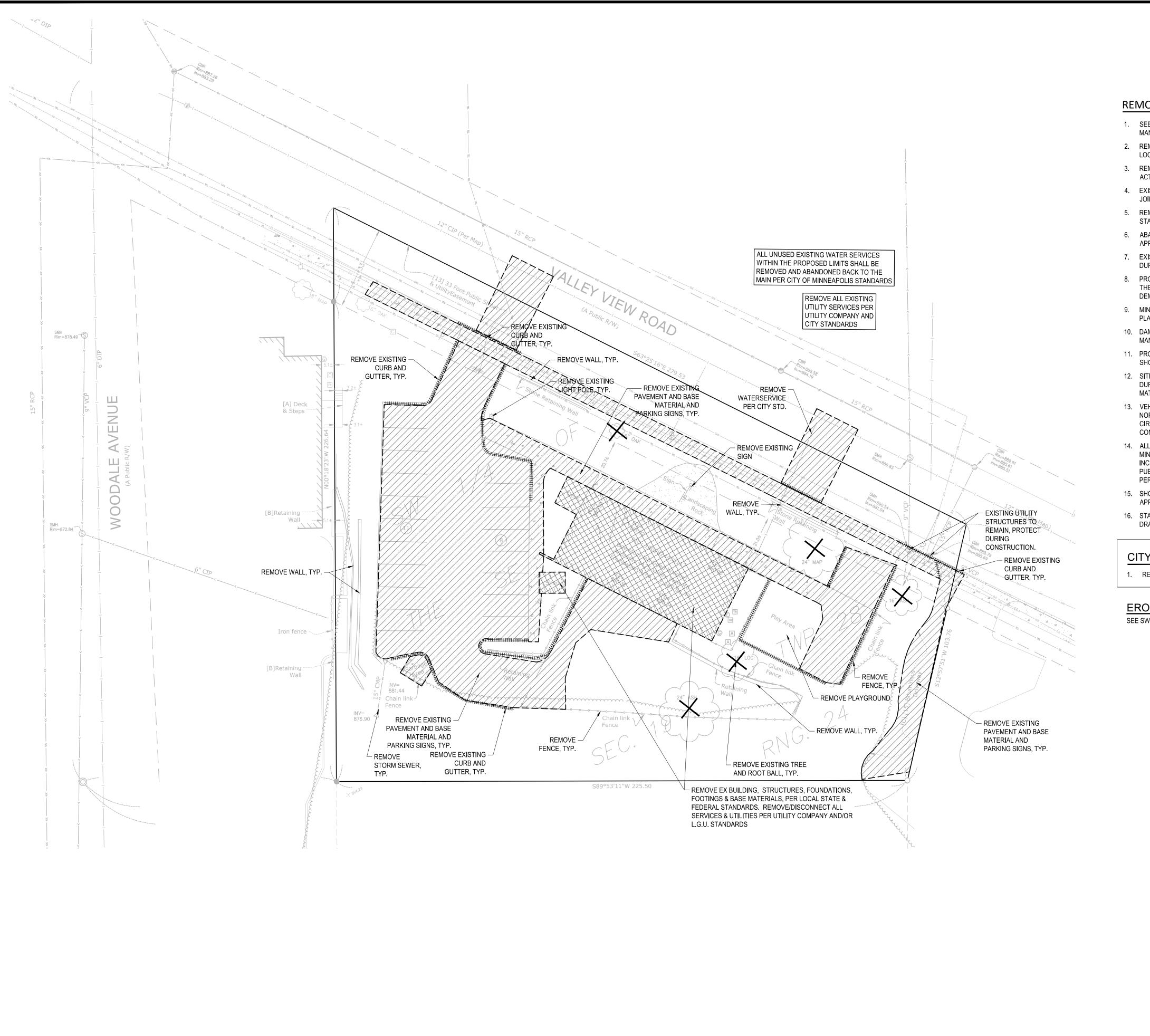
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REMOVAL NOTES:

- 1. SEE STORM WATER POLLUTION PREVENTION PLAN (SWPPP) PLAN FOR CONSTRUCTION STORM WATER MANAGEMENT PLAN.
- 2. REMOVAL OF MATERIALS NOTED ON THE DRAWINGS SHALL BE IN ACCORDANCE WITH MNDOT, STATE AND LOCAL REGULATIONS.
- 3. REMOVAL OF PRIVATE UTILITIES SHALL BE COORDINATED WITH UTILITY OWNER PRIOR TO CONSTRUCTION ACTIVITIES.
- 4. EXISTING PAVEMENTS SHALL BE SAWCUT IN LOCATIONS AS SHOWN ON THE DRAWINGS OR THE NEAREST JOINT FOR PROPOSED PAVEMENT CONNECTIONS.
- 5. REMOVED MATERIALS SHALL BE DISPOSED OF TO A LEGAL OFF-SITE LOCATION AND IN ACCORDANCE WITH STATE AND LOCAL REGULATIONS.
- 6. ABANDON, REMOVAL, CONNECTION, AND PROTECTION NOTES SHOWN ON THE DRAWINGS ARE APPROXIMATE. COORDINATE WITH PROPOSED PLANS.
- 7. EXISTING ON-SITE FEATURES NOT NOTED FOR REMOVAL SHALL BE PROTECTED THROUGHOUT THE DURATION OF THE CONTRACT.
- 8. PROPERTY LINES SHALL BE CONSIDERED GENERAL CONSTRUCTION LIMITS UNLESS OTHERWISE NOTED ON THE DRAWINGS. WORK WITHIN THE GENERAL CONSTRUCTION LIMITS SHALL INCLUDE STAGING, DEMOLITION AND CLEAN-UP OPERATIONS AS WELL AS CONSTRUCTION SHOWN ON THE DRAWINGS.
- 9. MINOR WORK OUTSIDE OF THE GENERAL CONSTRUCTION LIMITS SHALL BE ALLOWED AS SHOWN ON THE PLAN AND PER CITY REQUIREMENTS.
- 10. DAMAGE BEYOND THE PROPERTY LIMITS CAUSED BY CONSTRUCTION ACTIVITY SHALL BE REPAIRED IN A MANNER APPROVED BY THE ENGINEER/LANDSCAPE ARCHITECT OR IN ACCORDANCE WITH THE CITY.
- 11. PROPOSED WORK (BUILDING AND CIVIL) SHALL NOT DISTURB EXISTING UTILITIES UNLESS OTHERWISE SHOWN ON THE DRAWINGS AND APPROVED BY THE CITY PRIOR TO CONSTRUCTION.
- 12. SITE SECURITY MAY BE NECESSARY AND PROVIDED IN A MANNER TO PROHIBIT VANDALISM, AND THEFT, DURING AND AFTER NORMAL WORK HOURS, THROUGHOUT THE DURATION OF THE CONTRACT. SECURITY MATERIALS SHALL BE IN ACCORDANCE WITH THE CITY.
- 13. VEHICULAR ACCESS TO THE SITE SHALL BE MAINTAINED FOR DELIVERY AND INSPECTION ACCESS DURING NORMAL OPERATING HOURS. AT NO POINT THROUGHOUT THE DURATION OF THE CONTRACT SHALL CIRCULATION OF ADJACENT STREETS BE BLOCKED WITHOUT APPROVAL BY THE CITY PRIOR TO CONSTRUCTION ACTIVITIES.
- 14. ALL TRAFFIC CONTROLS SHALL BE PROVIDED AND ESTABLISHED PER THE REQUIREMENTS OF THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MMUTCD) AND THE CITY. THIS SHALL INCLUDE, BUT NOT BE LIMITED TO, SIGNAGE, BARRICADES, FLASHERS, AND FLAGGERS AS NEEDED. ALL PUBLIC STREETS SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES. NO ROAD CLOSURES SHALL BE PERMITTED WITHOUT APPROVAL BY THE CITY.
- 15. SHORING FOR BUILDING EXCAVATION MAY BE USED AT THE DISCRETION OF THE CONTRACTOR AND AS APPROVED BY THE OWNERS REPRESENTATIVE AND THE CITY PRIOR TO CONSTRUCTION ACTIVITIES.
- 16. STAGING, DEMOLITION, AND CLEAN-UP AREAS SHALL BE WITHIN THE PROPERTY LIMITS AS SHOWN ON THE DRAWINGS AND MAINTAINED IN A MANNER AS REQUIRED BY THE CITY.

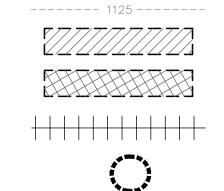
CITY OF EDINA REMOVAL NOTES:

1. RESERVED FOR CITY SPECIFIC REMOVAL NOTES.

EROSION CONTROL NOTES:

SEE SWPPP ON SHEETS SW1.0-SW1.5

REMOVALS LEGEND:



EX. 1' CONTOUR ELEVATION INTERVAL

REMOVAL OF PAVEMENT AND ALL BASE MATERIAL,
INCLUDING BIT., CONC., AND GRAVEL PVMTS.

REMOVAL OF STRUCTURE INCLUDING ALL

FOOTINGS AND FOUNDATIONS.

REMOVE CURB AND GUTTER. IF IN RIGHT-OF-WAY,
COORDINATE WITH LOCAL GOVERNING UNIT.

TREE PROTECTION

TREE REMOVAL - INCLUDING ROOTS AND STUMPS



1" = 20'-0" 10'-0" 0 20'-0" 6/12/20 Civil Engineering · Surveying · Landscape Architecture

4931 W. 35th Street, Suite 200
St. Louis Park, MN 55416

civilsitegroup.com

612-615-0060

PRELIMINARY PRELIMINARY CONSTRUCTION CONSTRUCTION

w Road

dina, Minnesota

Marl

Edina, Mi

4425 \

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PREPARED BY ME OR UNDER MY DIRECT
SUPERVISION AND THAT I AM A DULY
LICENSED PROFESSIONAL ENGINEER
UNDER THE LAWS OF THE STATE OF

Matthew R Payek

DATE XX/XX/19 LICENSE NO. 44263

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DATE DESCRIPTION

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PROJECT NUMBER: 19261

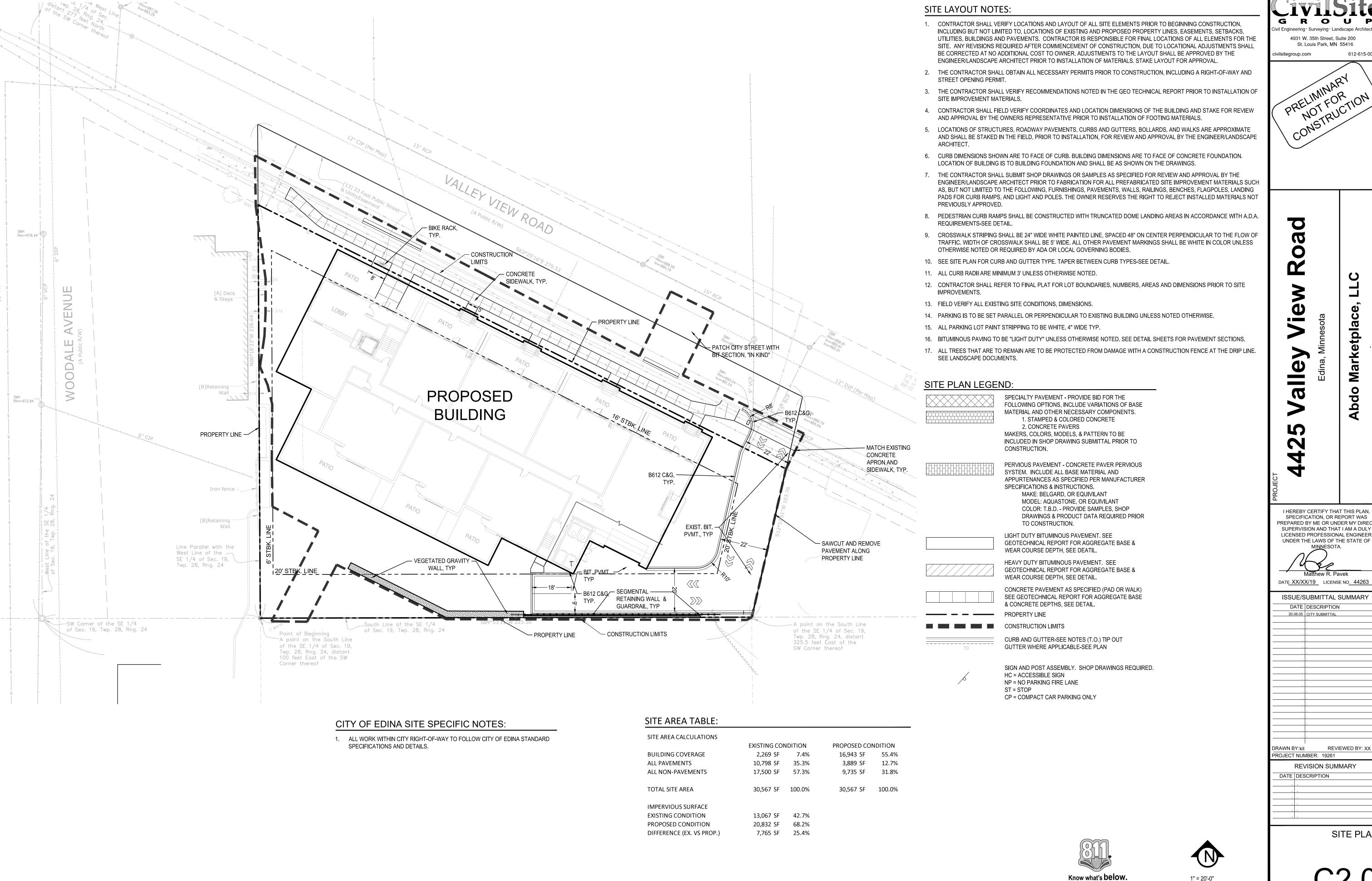
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REMOVALS PLAN

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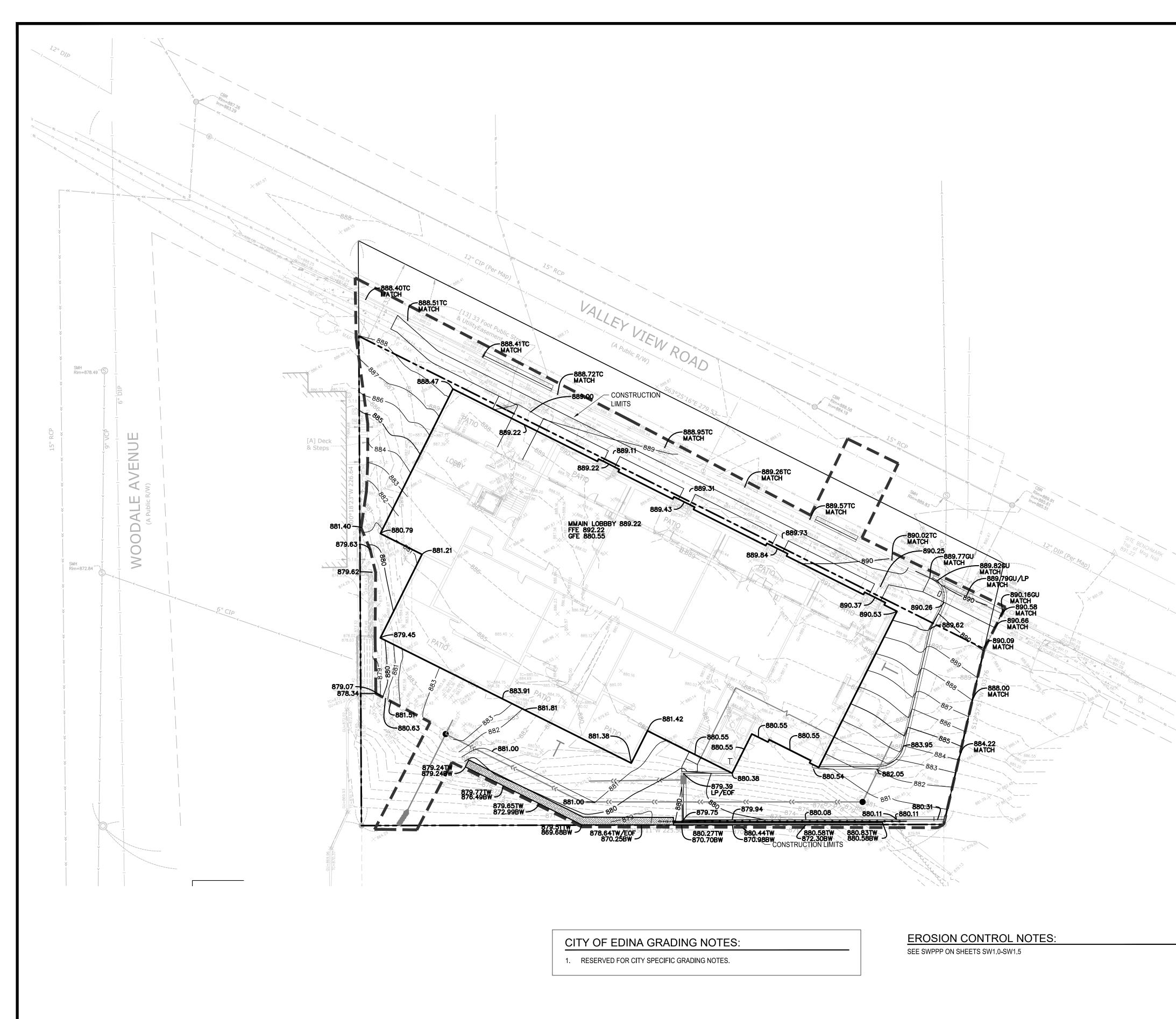
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Call before you dig.

SITE PLAN



GENERAL GRADING NOTES:

- 1. SEE SITE PLAN FOR HORIZONTAL LAYOUT & GENERAL GRADING NOTES.
- 2. THE CONTRACTOR SHALL COMPLETE THE SITE GRADING CONSTRUCTION (INCLUDING BUT NOT LIMITED TO SITE PREPARATION, SOIL CORRECTION, EXCAVATION, EMBANKMENT, ETC.) IN ACCORDANCE WITH THE REQUIREMENTS OF THE OWNER'S SOILS ENGINEER. ALL SOIL TESTING SHALL BE COMPLETED BY THE OWNER'S SOILS ENGINEER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL REQUIRED SOIL TESTS AND INSPECTIONS WITH THE SOILS ENGINEER.
- 3. GRADING AND EXCAVATION ACTIVITIES SHALL BE PERFORMED IN ACCORDANCE WITH THE NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT REQUIREMENTS & PERMIT REQUIREMENTS OF THE CITY.
- 4. PROPOSED SPOT GRADES ARE FLOW-LINE FINISHED GRADE ELEVATIONS, UNLESS OTHERWISE NOTED.
- GRADES OF WALKS SHALL BE INSTALLED WITH 5% MAX. LONGITUDINAL SLOPE AND 1% MIN. AND 2% MAX. CROSS SLOPE, UNLESS OTHERWISE NOTED.
- 6. PROPOSED SLOPES SHALL NOT EXCEED 3:1 UNLESS INDICATED OTHERWISE ON THE DRAWINGS. MAXIMUM SLOPES IN MAINTAINED AREAS IS 4:1
- 7. PROPOSED RETAINING WALLS, FREESTANDING WALLS, OR COMBINATION OF WALL TYPES GREATER THAN 4' IN HEIGHT SHALL BE DESIGNED AND ENGINEERED BY A REGISTERED RETAINING WALL ENGINEER. DESIGN DRAWINGS SHALL BE SUBMITTED FOR REVIEW AND APPROVAL PRIOR TO CONSTRUCTION.
- 8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTENANCE OF GRADE STAKES THROUGHOUT THE DURATION OF CONSTRUCTION TO ESTABLISH PROPER GRADES. THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR A FINAL FIELD CHECK OF FINISHED GRADES ACCEPTABLE TO THE ENGINEER/LANDSCAPE ARCHITECT PRIOR TO TOPSOIL AND SODDING ACTIVITIES.
- 9. IF EXCESS OR SHORTAGE OF SOIL MATERIAL EXISTS, THE CONTRACTOR SHALL TRANSPORT ALL EXCESS SOIL MATERIAL OFF THE SITE TO AN AREA SELECTED BY THE CONTRACTOR, OR IMPORT SUITABLE MATERIAL TO THE SITE.
- 10. EXCAVATE TOPSOIL FROM AREAS TO BE FURTHER EXCAVATED OR REGRADED AND STOCKPILE IN AREAS DESIGNATED ON THE SITE. THE CONTRACTOR SHALL SALVAGE ENOUGH TOPSOIL FOR RESPREADING ON THE SITE AS SPECIFIED. EXCESS TOPSOIL SHALL BE PLACED IN EMBANKMENT AREAS, OUTSIDE OF BUILDING PADS, ROADWAYS AND PARKING AREAS. THE CONTRACTOR SHALL SUBCUT CUT AREAS, WHERE TURF IS TO BE ESTABLISHED, TO A DEPTH OF 6 INCHES. RESPREAD TOPSOIL IN AREAS WHERE TURF IS TO BE ESTABLISHED TO A MINIMUM DEPTH OF 6 INCHES.
- 11. FINISHED GRADING SHALL BE COMPLETED. THE CONTRACTOR SHALL UNIFORMLY GRADE AREAS WITHIN LIMITS OF GRADING, INCLUDING ADJACENT TRANSITION AREAS. PROVIDE A SMOOTH FINISHED SURFACE WITHIN SPECIFIED TOLERANCES, WITH UNIFORM LEVELS OR SLOPES BETWEEN POINTS WHERE ELEVATIONS ARE SHOWN, OR BETWEEN SUCH POINTS AND EXISTING GRADES. AREAS THAT HAVE BEEN FINISH GRADED SHALL BE PROTECTED FROM SUBSEQUENT CONSTRUCTION OPERATIONS, TRAFFIC AND EROSION. REPAIR ALL AREAS THAT HAVE BECOME RUTTED BY TRAFFIC OR ERODED BY WATER OR HAS SETTLED BELOW THE CORRECT GRADE. ALL AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED TO EQUAL OR BETTER THAN ORIGINAL CONDITION OR TO THE REQUIREMENTS OF THE NEW WORK.
- 12. PRIOR TO PLACEMENT OF THE AGGREGATE BASE, A TEST ROLL WILL BE REQUIRED ON THE STREET AND/OR PARKING AREA SUBGRADE. THE CONTRACTOR SHALL PROVIDE A LOADED TANDEM AXLE TRUCK WITH A GROSS WEIGHT OF 25 TONS. THE TEST ROLLING SHALL BE AT THE DIRECTION OF THE SOILS ENGINEER AND SHALL BE COMPLETED IN AREAS AS DIRECTED BY THE SOILS ENGINEER. THE SOILS ENGINEER SHALL DETERMINE WHICH SECTIONS OF THE STREET OR PARKING AREA ARE UNSTABLE. CORRECTION OF THE SUBGRADE SOILS SHALL BE COMPLETED IN ACCORDANCE WITH THE REQUIREMENTS OF THE SOILS ENGINEER. NO TEST ROLL SHALL OCCUR WITHIN 10' OF ANY UNDERGROUND STORM RETENTION/DETENTION SYSTEMS.

13. TOLERANCES

- 13.1. THE BUILDING SUBGRADE FINISHED SURFACE ELEVATION SHALL NOT VARY BY MORE THAN 0.30 FOOT ABOVE, OR 0.30 FOOT BELOW, THE PRESCRIBED ELEVATION AT ANY POINT WHER MEASUREMENT IS MADE.
- 13.2. THE STREET OR PARKING AREA SUBGRADE FINISHED SURFACE ELEVATION SHALL NOT VARY BY MORE THAN 0.05 FOOT ABOVE, OR 0.10 FOOT BELOW, THE PRESCRIBED ELEVATION OF ANY POINT WHERE MEASUREMENT IS MADE.
- 13.3. AREAS WHICH ARE TO RECEIVE TOPSOIL SHALL BE GRADED TO WITHIN 0.30 FOOT ABOVE OR BELOW THE REQUIRED ELEVATION, UNLESS DIRECTED OTHERWISE BY THE ENGINEER.
- 13.4. TOPSOIL SHALL BE GRADED TO PLUS OR MINUS 1/2 INCH OF THE SPECIFIED THICKNESS.14. MAINTENANCE
- 14.1. THE CONTRACTOR SHALL PROTECT NEWLY GRADED AREAS FROM TRAFFIC AND EROSION, AND KEEP AREA FREE OF TRASH AND DEBRIS.
- 14.2. CONTRACTOR SHALL REPAIR AND REESTABLISH GRADES IN SETTLED, ERODED AND RUTTED AREAS TO SPECIFIED TOLERANCES. DURING THE CONSTRUCTION, IF REQUIRED, AND DURING THE WARRANTY PERIOD, ERODED AREAS WHERE TURF IS TO BE ESTABLISHED SHALL BE RESEEDED AND MULCHED.
- 14.3. WHERE COMPLETED COMPACTED AREAS ARE DISTURBED BY SUBSEQUENT CONSTRUCTION OPERATIONS OR ADVERSE WEATHER, CONTRACTOR SHALL SCARIFY, SURFACE, RESHAPE, AND COMPACT TO REQUIRED DENSITY PRIOR TO FURTHER CONSTRUCTION.

GRADING PLAN LEGEND:

EX. 1' CONTOUR ELEVATION INTERVAL

1137

1.0' CONTOUR ELEVATION INTERVAL

41.26

SPOT GRADE ELEVATION (GUTTER/FLOW LINE UNLESS OTHERWISE NOTED)

891.00 G

891.00 TC

SPOT GRADE ELEVATION GUTTER

891.00 BS/TS

SPOT GRADE ELEVATION TOP OF CURB

891.00 ME

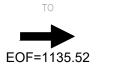
SPOT GRADE ELEVATION BOTTOM OF STAIRS/TOP OF STAIRS

891.00 ME

SPOT GRADE ELEVATION MATCH EXISTING

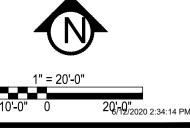
GRADE BREAK - HIGH POINTS

CURB AND GUTTER (T.O = TIP OUT)



EMERGENCY OVERFLOW





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PRELIMINARY
CONSTRUCTION

CONSTRUCTION

w Road

Edina, Minnesota

etpla

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I HEREBY CERTIFY THAT THIS PLAN,
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UNDER THE LAWS OF THE STATE OF

Matthew R. Pavek

DATE XX/XX/19 LICENSE NO. 44263

ISSUE/SUBMITTAL SUMMARY

DATE DESCRIPTION
20.06.05 CITY SUBMITTAL

N BY:kit REVIEWED BY

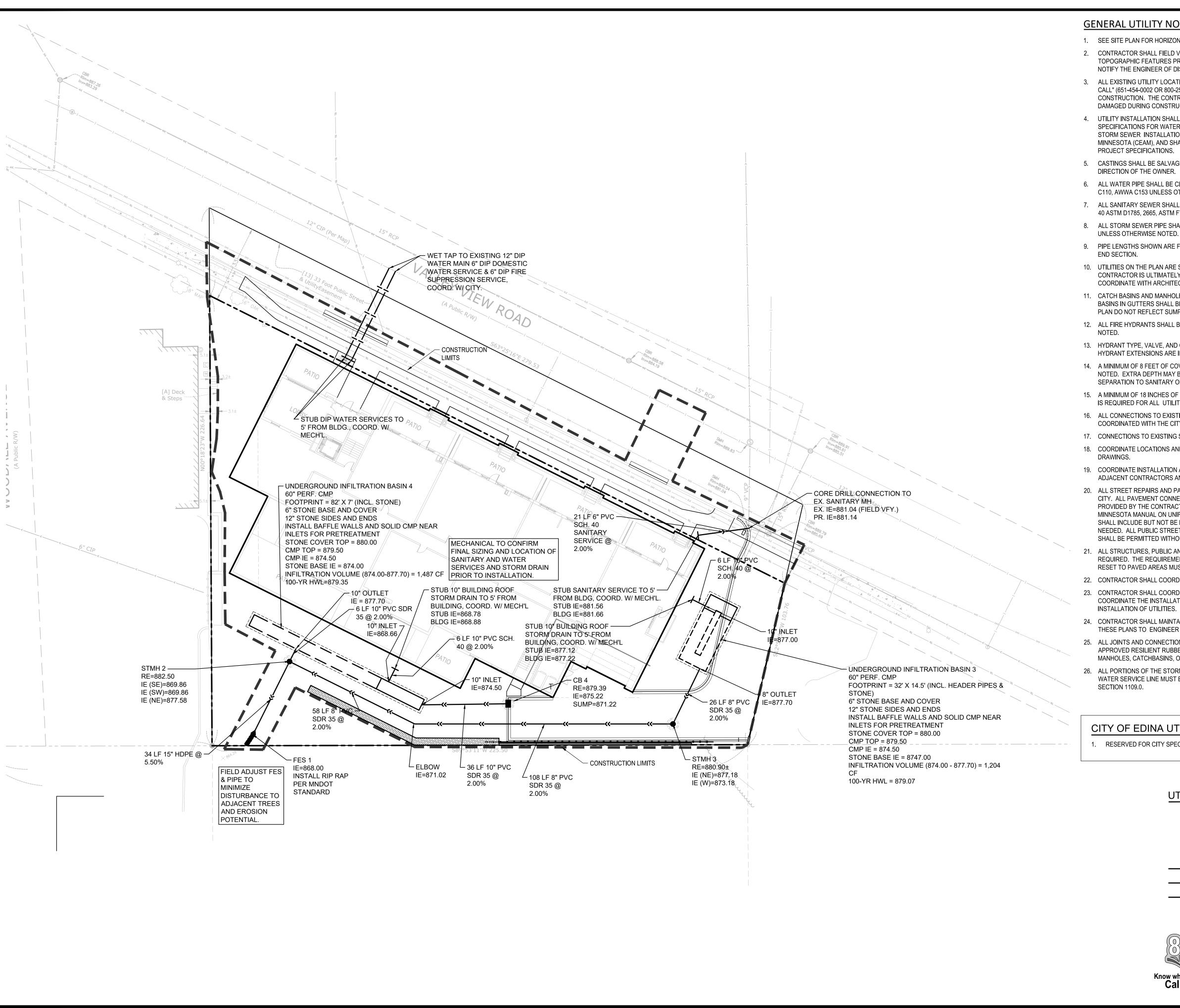
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GRADING PLAN

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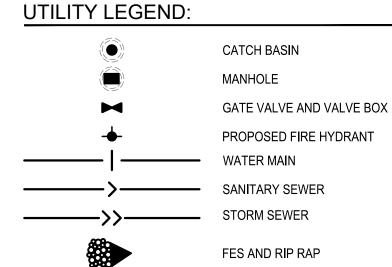


GENERAL UTILITY NOTES:

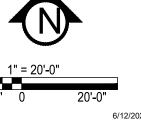
- 1. SEE SITE PLAN FOR HORIZONTAL DIMENSIONS AND LAYOUT.
- 2. CONTRACTOR SHALL FIELD VERIFY LOCATION AND ELEVATION OF EXISTING UTILITIES AND TOPOGRAPHIC FEATURES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER OF DISCREPANCIES OR VARIATIONS FROM THE PLANS.
- 3. ALL EXISTING UTILITY LOCATIONS SHOWN ARE APPROXIMATE. CONTACT "GOPHER STATE ONE CALL" (651-454-0002 OR 800-252-1166) FOR UTILITY LOCATIONS, 48 HOURS PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL REPAIR OR REPLACE ANY UTILITIES THAT ARE DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.
- 4. UTILITY INSTALLATION SHALL CONFORM TO THE CURRENT EDITION OF "STANDARD SPECIFICATIONS FOR WATER MAIN AND SERVICE LINE INSTALLATION" AND "SANITARY SEWER AND STORM SEWER INSTALLATION" AS PREPARED BY THE CITY ENGINEERS ASSOCIATION OF MINNESOTA (CEAM), AND SHALL CONFORM WITH THE REQUIREMENTS OF THE CITY AND THE PROJECT SPECIFICATIONS.
- 5. CASTINGS SHALL BE SALVAGED FROM STRUCTURE REMOVALS AND RE-USED OR PLACED AT THE
- 6. ALL WATER PIPE SHALL BE CLASS 52 DUCTILE IRON PIPE (DIP) AWWA C151, ASME B16.4, AWWA C110, AWWA C153 UNLESS OTHERWISE NOTED.
- 7. ALL SANITARY SEWER SHALL BE SDR 26 POLYVINYL CHLORIDE (PVC) ASTM D3034 & F679, OR SCH 40 ASTM D1785, 2665, ASTM F794, 1866) UNLESS OTHERWISE NOTED.
- 8. ALL STORM SEWER PIPE SHALL BE HDPE ASTM F714 & F2306 WITH ASTM D3212 SPEC FITTINGS
- 9. PIPE LENGTHS SHOWN ARE FROM CENTER TO CENTER OF STRUCTURE OR TO END OF FLARED
- 10. UTILITIES ON THE PLAN ARE SHOWN TO WITHIN 5' OF THE BUILDING FOOTPRINT. THE CONTRACTOR IS ULTIMATELY RESPONSIBLE FOR THE FINAL CONNECTION TO BUILDING LINES. COORDINATE WITH ARCHITECTURAL AND MECHANICAL PLANS.
- 11. CATCH BASINS AND MANHOLES IN PAVED AREAS SHALL BE SUMPED 0.04 FEET. ALL CATCH BASINS IN GUTTERS SHALL BE SUMPED 0.15 FEET PER DETAILS. RIM ELEVATIONS SHOWN ON THIS PLAN DO NOT REFLECT SUMPED ELEVATIONS.
- 12. ALL FIRE HYDRANTS SHALL BE LOCATED 5 FEET BEHIND BACK OF CURB UNLESS OTHERWISE
- 13. HYDRANT TYPE, VALVE, AND CONNECTION SHALL BE IN ACCORDANCE WITH CITY REQUIREMENTS HYDRANT EXTENSIONS ARE INCIDENTAL.
- 14. A MINIMUM OF 8 FEET OF COVER IS REQUIRED OVER ALL WATERMAIN, UNLESS OTHERWISE NOTED. EXTRA DEPTH MAY BE REQUIRED TO MAINTAIN A MINIMUM OF 18" VERTICAL SEPARATION TO SANITARY OR STORM SEWER LINES. EXTRA DEPTH WATERMAIN IS INCIDENTAL
- 15. A MINIMUM OF 18 INCHES OF VERTICAL SEPARATION AND 10 FEET OF HORIZONTAL SEPARATION IS REQUIRED FOR ALL UTILITIES, UNLESS OTHERWISE NOTED.
- 16. ALL CONNECTIONS TO EXISTING UTILITIES SHALL BE IN ACCORDANCE WITH CITY STANDARDS AND COORDINATED WITH THE CITY PRIOR TO CONSTRUCTION.
- 17. CONNECTIONS TO EXISTING STRUCTURES SHALL BE CORE-DRILLED.
- 18. COORDINATE LOCATIONS AND SIZES OF SERVICE CONNECTIONS WITH THE MECHANICAL
- 19. COORDINATE INSTALLATION AND SCHEDULING OF THE INSTALLATION OF UTILITIES WITH ADJACENT CONTRACTORS AND CITY STAFF.
- 20. ALL STREET REPAIRS AND PATCHING SHALL BE PERFORMED PER THE REQUIREMENTS OF THE CITY. ALL PAVEMENT CONNECTIONS SHALL BE SAWCUT. ALL TRAFFIC CONTROLS SHALL BE PROVIDED BY THE CONTRACTOR AND SHALL BE ESTABLISHED PER THE REQUIREMENTS OF THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MMUTCD) AND THE CITY. THIS SHALL INCLUDE BUT NOT BE LIMITED TO SIGNAGE, BARRICADES, FLASHERS, AND FLAGGERS AS NEEDED. ALL PUBLIC STREETS SHALL BE OPEN TO TRAFFIC AT ALL TIMES. NO ROAD CLOSURES SHALL BE PERMITTED WITHOUT APPROVAL BY THE CITY.
- ALL STRUCTURES, PUBLIC AND PRIVATE, SHALL BE ADJUSTED TO PROPOSED GRADES WHERE REQUIRED. THE REQUIREMENTS OF ALL OWNERS MUST BE COMPLIED WITH. STRUCTURES BEING RESET TO PAVED AREAS MUST MEET OWNERS REQUIREMENTS FOR TRAFFIC LOADING.
- 22. CONTRACTOR SHALL COORDINATE ALL WORK WITH PRIVATE UTILITY COMPANIES.
- 23. CONTRACTOR SHALL COORDINATE CONNECTION OF IRRIGATION SERVICE TO UTILITIES. COORDINATE THE INSTALLATION OF IRRIGATION SLEEVES NECESSARY AS TO NOT IMPACT
- 24. CONTRACTOR SHALL MAINTAIN AS-BUILT PLANS THROUGHOUT CONSTRUCTION AND SUBMIT THESE PLANS TO ENGINEER UPON COMPLETION OF WORK.
- 25. ALL JOINTS AND CONNECTIONS IN STORM SEWER SYSTEM SHALL BE GASTIGHT OR WATERTIGHT APPROVED RESILIENT RUBBER JOINTS MUST BE USED TO MAKE WATERTIGHT CONNECTIONS TO MANHOLES, CATCHBASINS, OR OTHER STRUCTURES.
- 26. ALL PORTIONS OF THE STORM SEWER SYSTEM LOCATED WITHIN 10 FEET OF THE BUILDING OR WATER SERVICE LINE MUST BE TESTED IN ACCORDANCE WITH MN RULES, CHAPTER 4714.

CITY OF EDINA UTILITY NOTES:

1. RESERVED FOR CITY SPECIFIC UTILITY NOTES.







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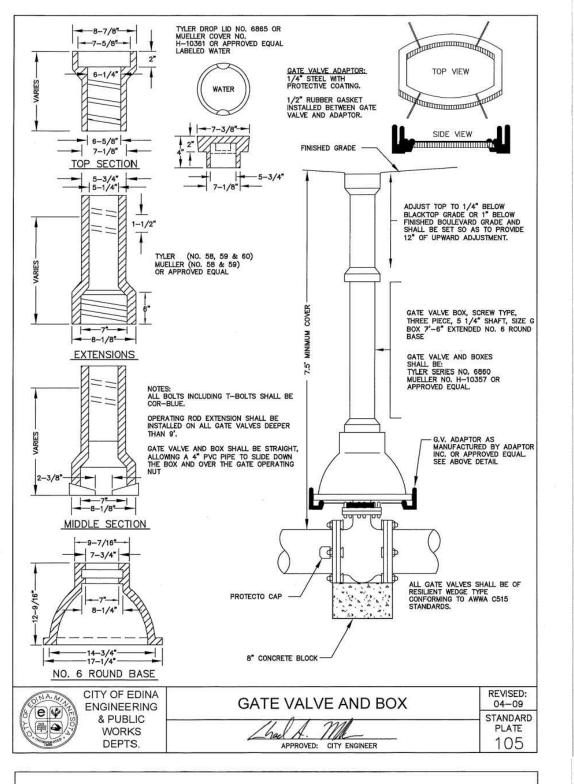
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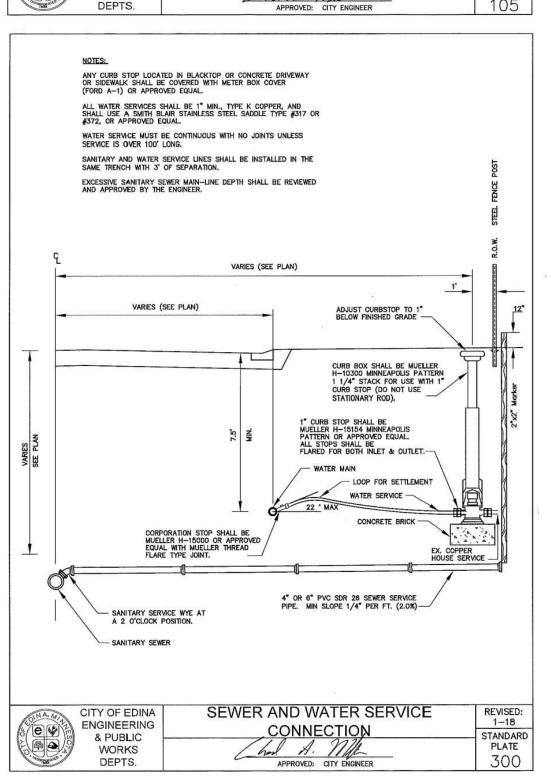
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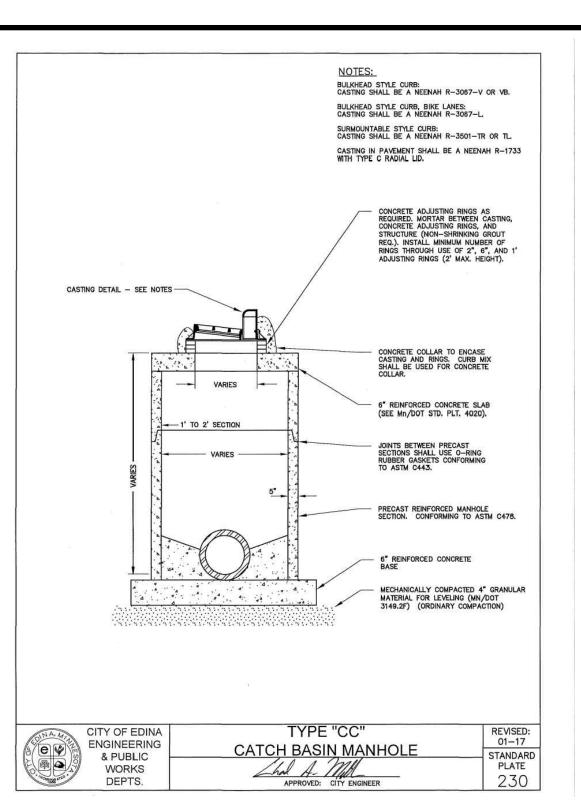
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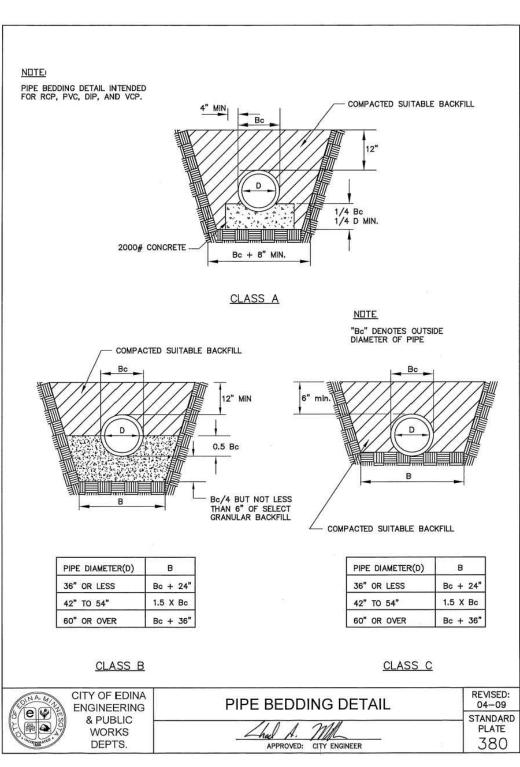
UTILITY PLAN

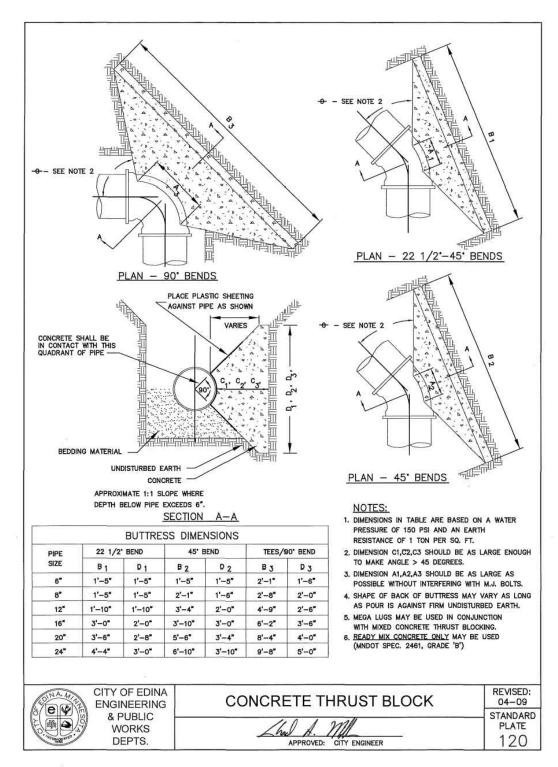
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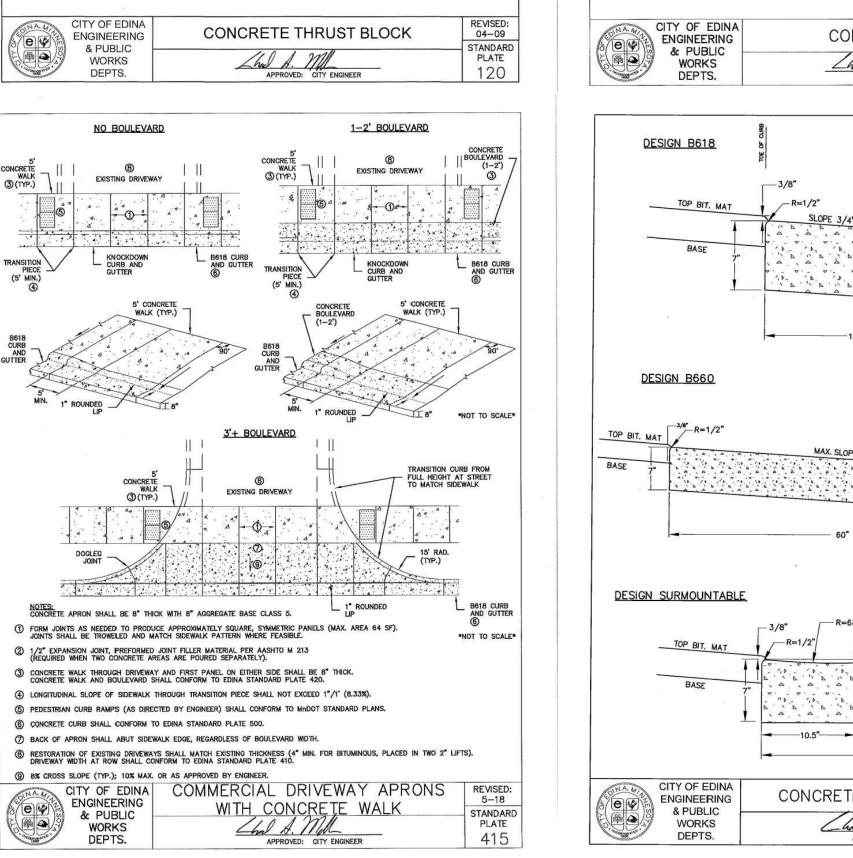


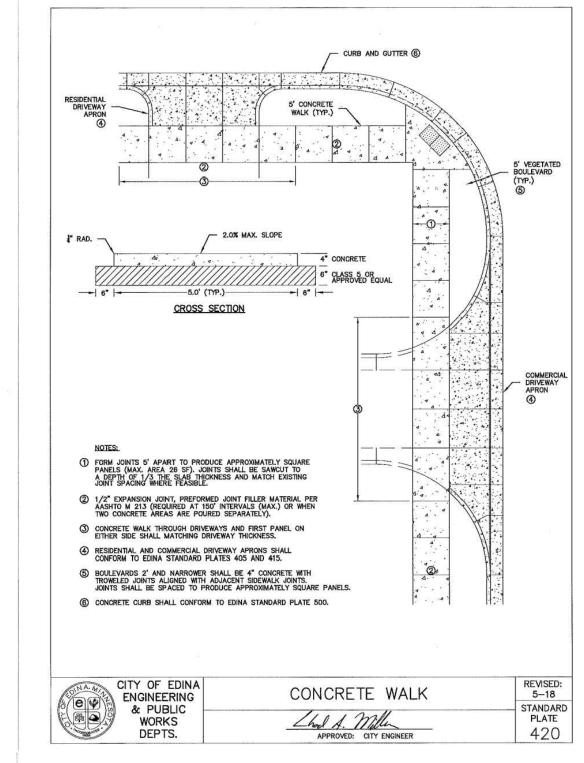


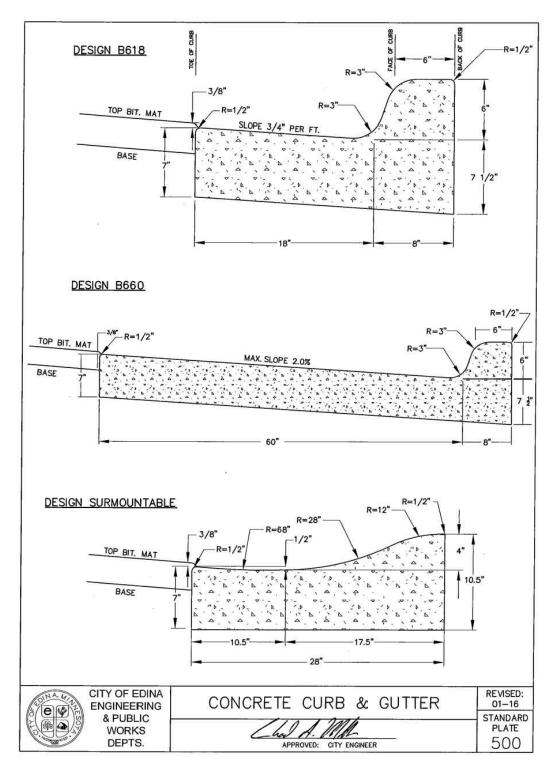


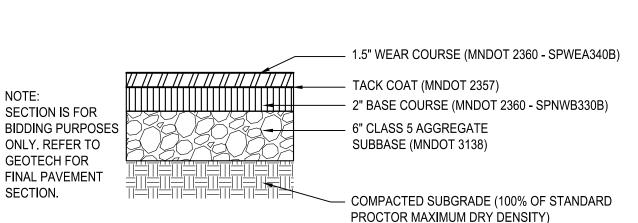












LIGHT DUTY BITUMINOUS PAVEMENT



TOOLED CONTROL JOINT

- 5" CONCRETE AS SPECIFIED

6" CLASS 5 AGGREGATE BASE

— COMPACTED SUBGRADE

TROWELLED EDGE

- LIGHT BROOM FINISH PERPENDICULAR TO

TRAFFIC WITH 3" WIDE SMOOTH

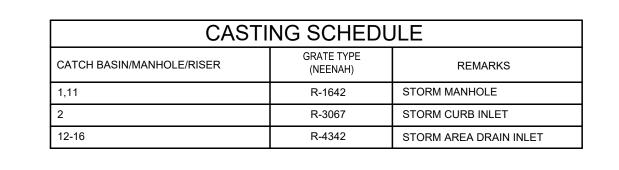
THIS OCCURS ONLY WHERE

FROM TOP OF WALK

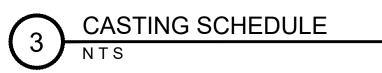
- MULCH DOWN 1"

NOTES:

MULCH MEETS EDGE OF WALK LEAVE TOP OF



CASTINGS SHALL BE NEENAH FOUNDRY CASTINGS OR APPROVED EQUIVALENT. CASTINGS SHALL BE STAMPED WITH APPROPRIATE UTILITY NAME ("STORM SEWER", "SANITARY SEWER")



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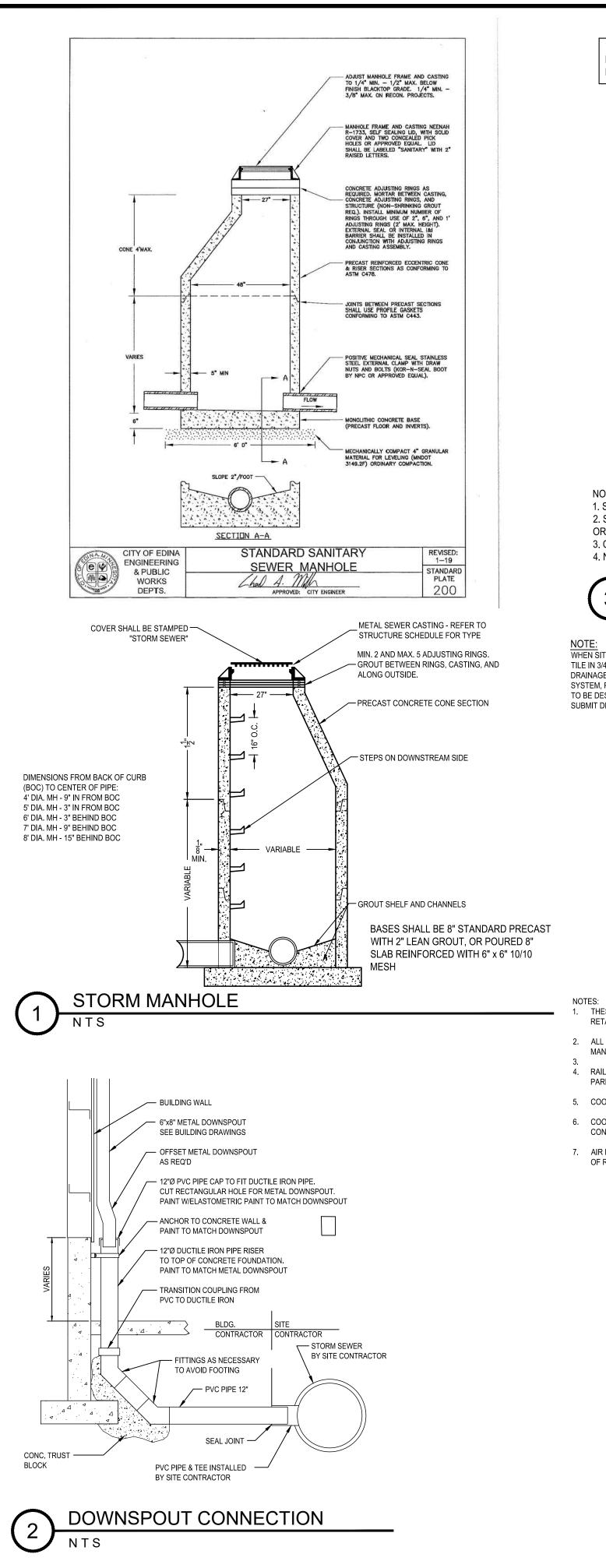
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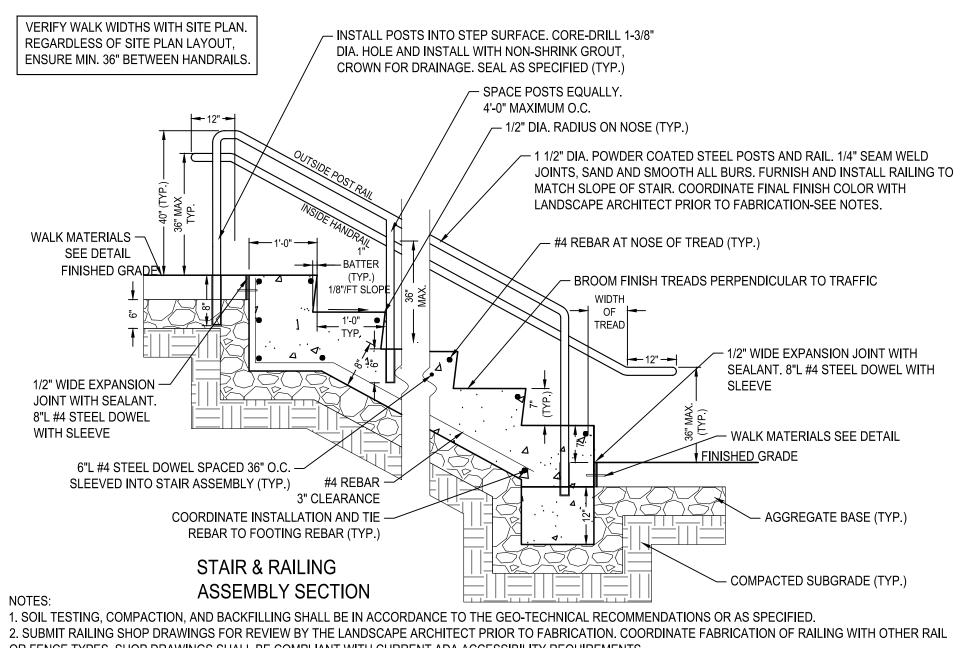
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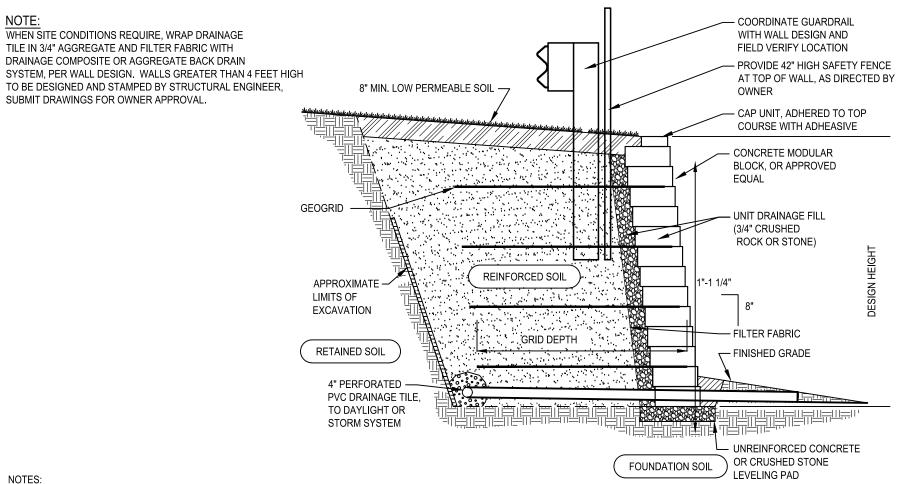




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EXTERIOR SITE STAIR & HANDRAIL (NO CHEEK/SIDE WALL)



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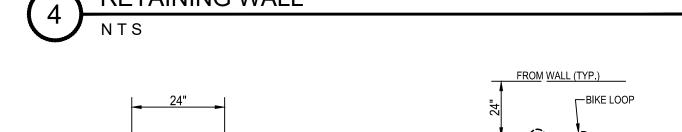
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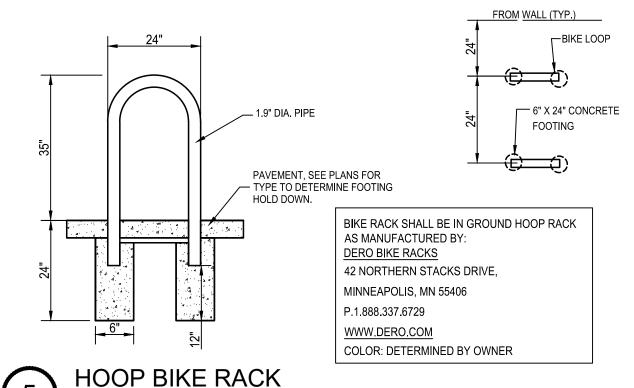
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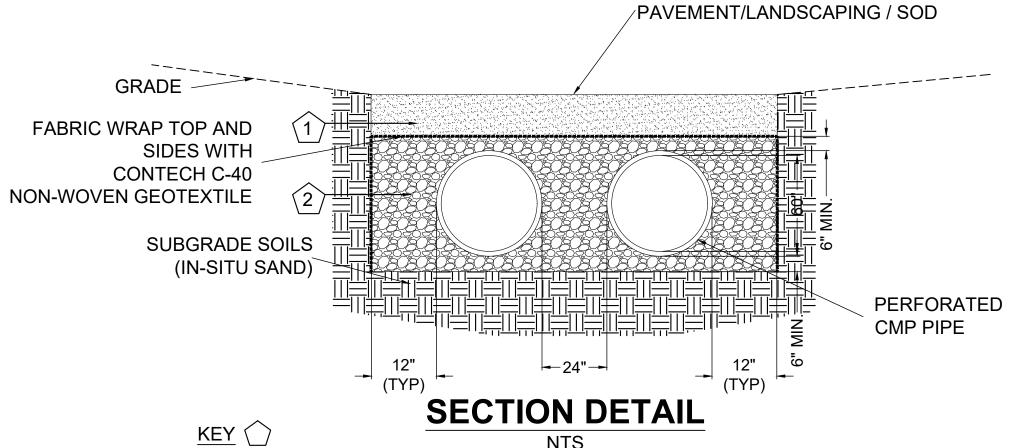




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PLANTING SOIL -SEE LANDSCAPE PLAN GEO SYNTHETIC REINFORCEMENT (TYP) NONWOVEN **GEOTEXTILE** _ STACKED PERFORATED **GEOWEB WALL** SECTIONS OUTER CELLS WITH TOPSOIL INFILL & VEGETATED - GRANULAR INFILL DRAIN TILE SLOPE TO DAYLIGHT

VEGETATED REINFORCED WALL (GEOWEB) 6'-10' HT.



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2. FREE DRAINING ANGULAR WASHED STONE 3/4" - 2" PARTICLE SIZE (NON CARBONATE). INSTALL TO MIN. 95% STANDARD DENSITY PER AASHTO T99

CONSTRUCTION SEQUENCING

- 1. INSTALL SILT FENCE AND/OR OR OTHER APPROPRIATE TEMPORARY EROSION CONTROL DEVICES TO PREVENT SEDIMENT FROM LEAVING OR ENTERING THE PRACTICE DURING CONSTRUCTION.
- 2. ALL DOWN-GRADIENT PERIMETER SEDIMENT CONTROL BMP'S MUST BE IN PLACE BEFORE ANY UP GRADIENT LAND DISTURBING ACTIVITY BEGINS.
- 3. PERFORM CONTINUOUS INSPECTIONS OF EROSION CONTROL PRACTICES.
- 4. INSTALL UTILITIES (WATER, SANITARY SEWER, ELECTRIC, PHONE, FIBER OPTIC, ETC) PRIOR TO SETTING FINAL GRADE OF BIORETENTION DEVICE.
- 5. PERFORM ALL OTHER SITE IMPROVEMENTS.
- 6. SEED AND MULCH ALL AREAS AFTER DISTURBANCE.
- CONSTRUCT RETENTION DEVICE UPON STABILIZATION OF CONTRIBUTING DRAINAGE AREA.
- 8. IMPLEMENT TEMPORARY AND PERMENATE EROSION CONTROL PRACTICES.
- UNDERGROUND INFILTRATION SYSTEM

- PLANT AND MULCH SITE.
- 10. REMOVE TEMPORARY EROSION CONTROL DEVICES AFTER THE CONTRIBUTING DRAINAGE AREA IS ADEQUATELY VEGETATED.

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- 3. ALL SUB MATERIALS BELOW THE SPECIFIED RETENTION DEPTH (ELEVATION) SHALL BE UNDISTURBED,

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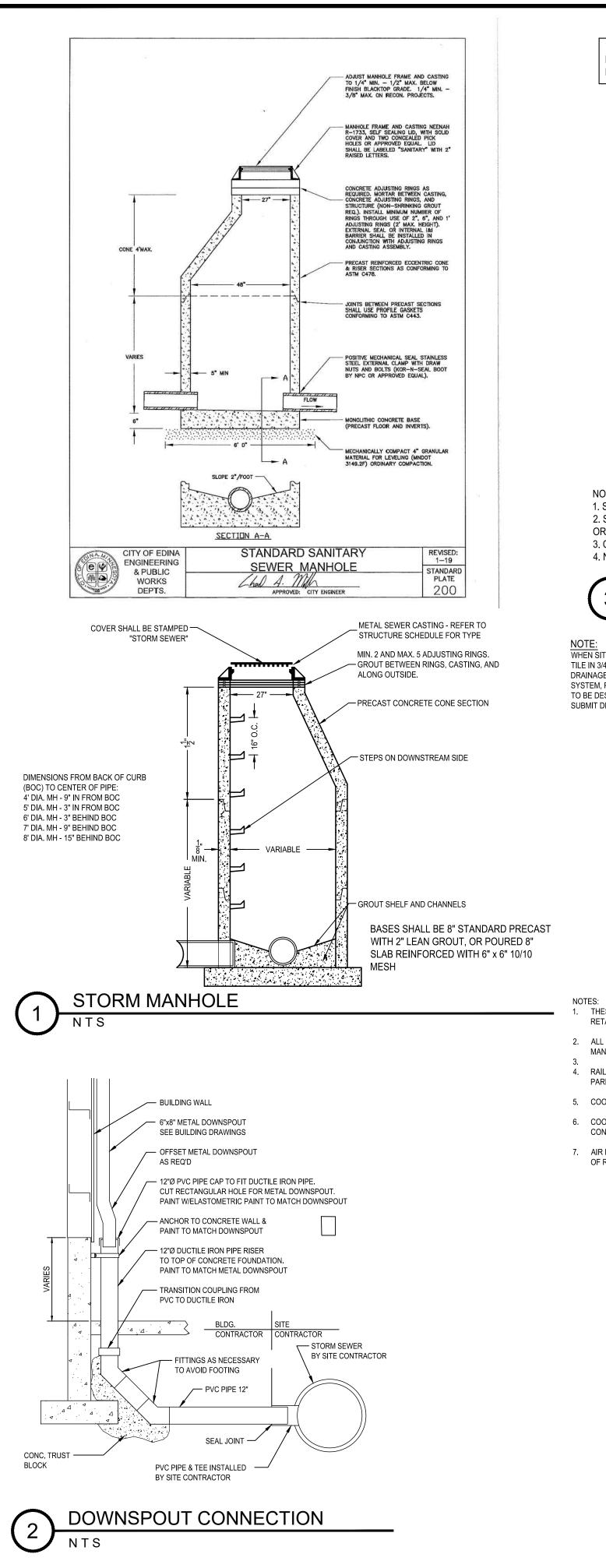
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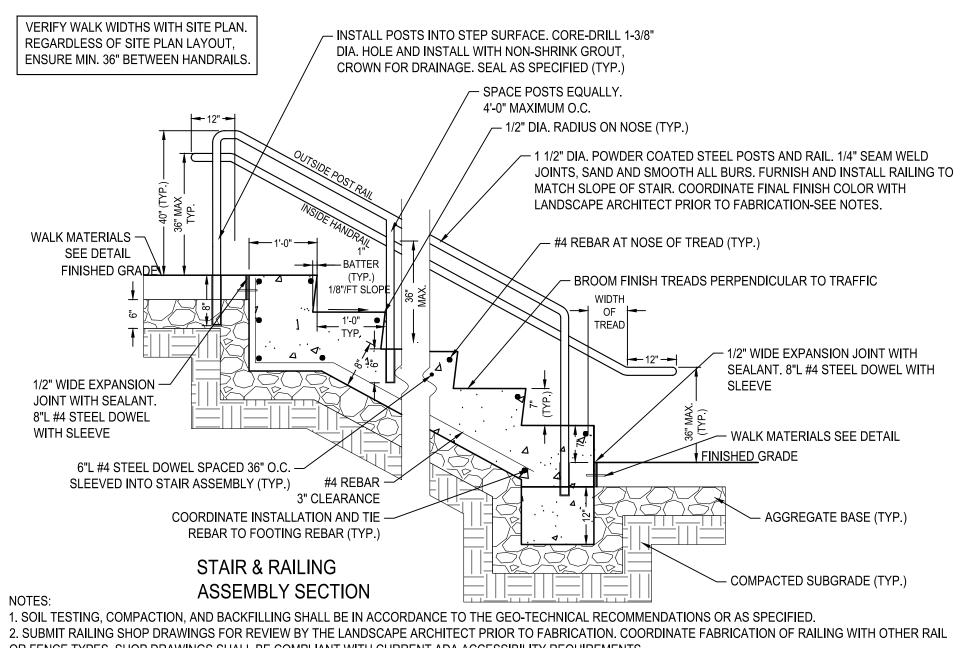
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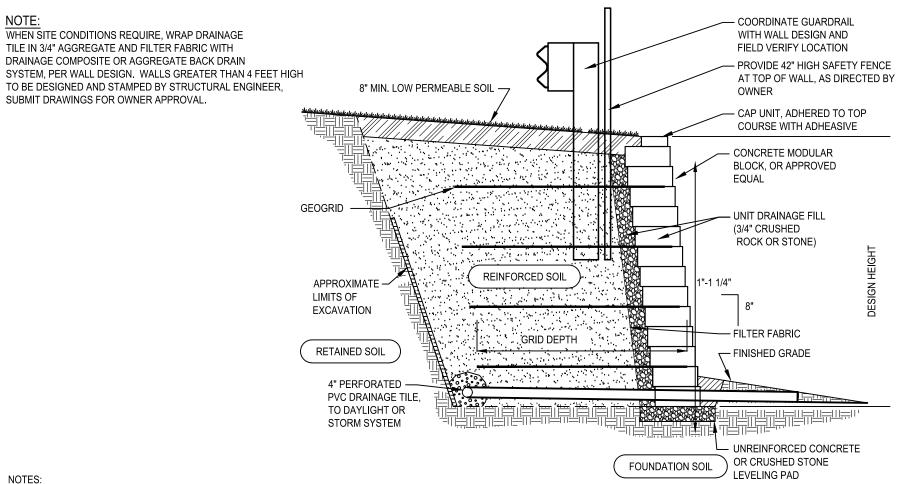




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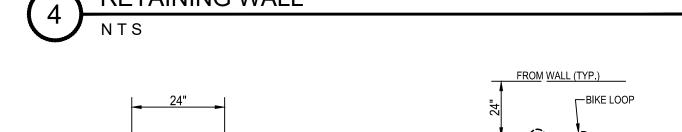
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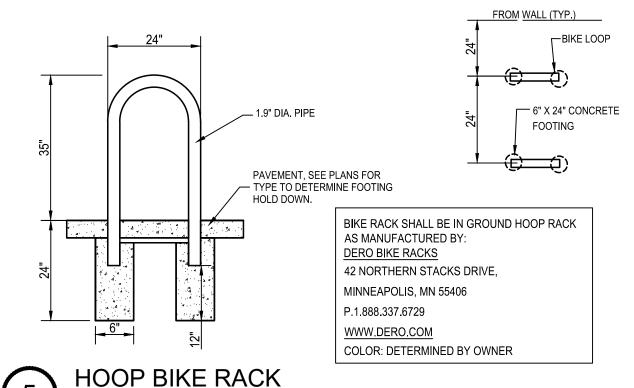
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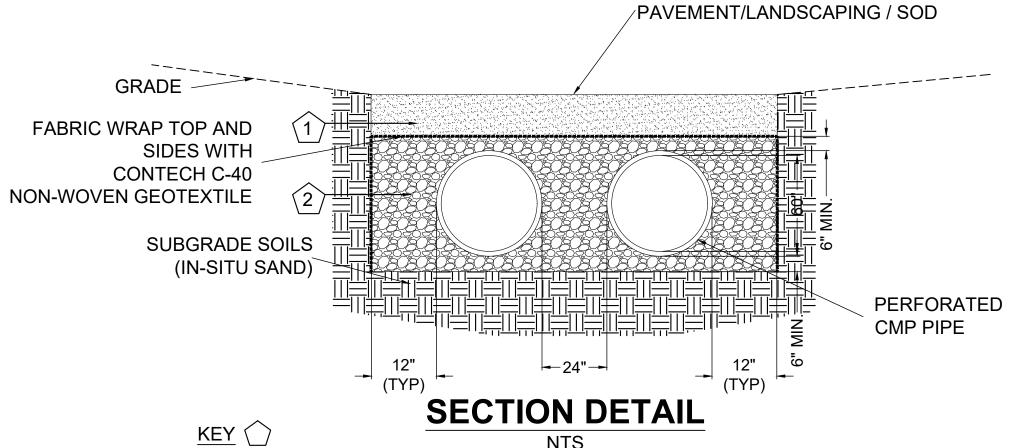




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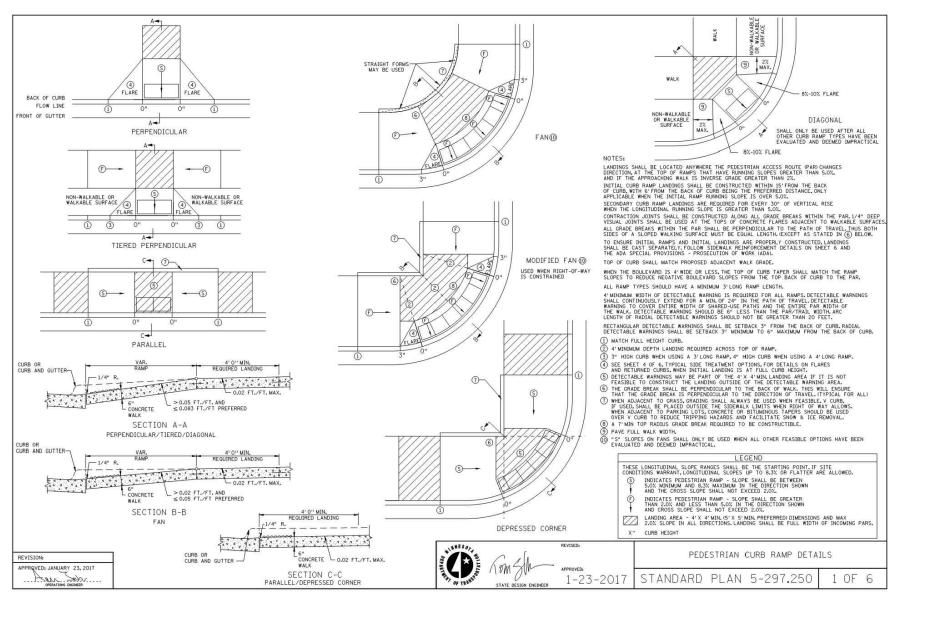
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3" MINIMUM CURB HEIGHT, 4" PREFERRED — (MEASURED AT FRONT FACE OF CURB)

SECTION A-A

TURF OR CONCRETE TOP OF SIDEWALK

DETECTABLE EDGE WITH ® CURB AND GUTTER

RADIAL DETECTABLE WARNING RECTANGULAR DETECTABLE WARNING

DETECTABLE EDGE WITHOUT CURB AND GUTTER

CURB & GUTTER

PAVED FLARES ADJACENT TO WALKABLE SURFACE

NON-WALKABLE SURFACE 116 NON-WALKABLF

1-3-1 1 1-3-1 2

PAVED FLARES
ADJACENT TO NON-WALKABLE SURFACE

NON-WALKABLE RAMP NON-WALKABLE SURFACE 156 SURFACE

1:6 GRADED GRADED FLARE

@/+3+\0/+3+\@

CURB DESIGN V CURB DESIGN V

SEE PEDESTRIAN APPROACH NOSE DETAIL RAMP DETAIL

NON-WALKABLE SURFACE SURFACE

2 0 2

ARA SOS.

RETURNED CURB (5)

TYPICAL SIDE TREATMENT OPTIONS ④ ⑩

PLACE DETECTABLE WARNINGS ENTIRE WALK/PATH WIDTH

> CROSSING SURFACE

6 6 6 6 6 6 6 6

RAILROAD CROSSING PLAN VIEW

PEDESTRIAN GATE ARM

A WALKABLE SURFACE IS DEFINED AS A PAVED SURFACE ADJACENT TO A CURB RAMP WITHOUT RAISED OBSTACLES THAT COULD MISTAKENLY BE TRAVERSED BY A USER WHO IS VISUALLY IMPAIRED.

CONCRETE FLARE LENGTHS ADJACENT TO NON-WALKABLE SURFACES SHOULD BE LESS THAN 8'LONG MEASURED ALONG THE RAMPS FROM THE BACK OF CURB.

SIDE TREATMENTS ARE APPLICABLE TO ALL RAMP TYPES AND SHOULD BE IMPLEMENTED AS NEEDED AS FIELD CONDITIONS DICTATE. THE ENGINEER SHALL DETERMINE THE RAMP SIDE TREATMENTS BASED ON MAINTENANCE OF BOTH ROADWAY AND SIDEWALK, ADJACENT PROPERTY CONSIDERATIONS, AND MITIGATING CONSTRUCTION IMPACTS

WHEN NO CONCRETE FLARES ARE PROPOSED, THE CONCRETE WALK SHALL BE FORMED AND CONSTRUCTED PERPENDICULAR TO THE EDGE OF ROADWAY. MAINTAIN 3" MAX. BETWEEN EDGE OF DOMES AND EDGE OF CONCRETE.

1 IF NO CURB AND GUTTER IS PLACED IN RURAL SECTIONS, DETECTABLE WARNINGS SHALL BE PLACED I' FROM THE EDGE OF BITUMINOUS ROADWAY AND/OR BITUMINOUS SHARED-USE PATH TO PROVIDE VISUAL CONTRAST.

(6) ALL CONSTRUCTED CURBS MUST HAVE A CONTINUOUS DETECTABLE EDGE FOR THE VISUALLY IMPAIRED. THIS DETECTABLE CORE ROUTES DETECTABLE WARRINGS WHEREVER THERE IS ZERO-HICH HICH CURB, CURB TAFERS ARE CONSIDERED A DETECTABLE EDGE WHEN ITHE TAFER STARTS WITHIN 3" OF THE EDGE OF THE DETECTABLE WARRINGS AND UNIFORMLY MISSES TO A 3-INCH MINIMAM CURB HEDHT. ANY CUBB MOT PART OF A CURB TAFER AND LESS THAN 3 INCHES IN HEIGHT IS NOT CONSIDERED A DETECTABLE EDGE AND THEREFORE IS NOT COMPLIANT WITH ACCESSIBILITY STANDARDS.

9) DRILL AND GROUT 1 - NO. 4 12" LONG REINFORCEMENT BAR (EPOXY COATED) WITH 3" MIN. COVER. REINFORCEMENT BARS ARE NOT NEEDED IF THE APPROACH NOSE IS POURED INTEGRAL WITH THE V CURB.

(O) DRILL AND GROUT 2 - NO. 4 12" LONG REINFORCEMENT BARS (EPOXY COATED) WITH 3" MIN. COVER. REINFORCEMENT BARS ARE NOT NEEDED IF THE APPROACH NOSE IS POURED INTEGRAL WITH THE CURB AND GUTTER.

1) SIDE TREATMENT EXAMPLES SHOWN ARE WHEN THE INITIAL LANDING IS APPROXIMATELY LEVEL WITH THE FULL HEIGHT CURB (LE. 6'LONG RAMP FOR 6' HIGH CURB), WHEN THE INITIAL LANDING IS MORE THAN 1" BELOW FULL HEIGHT CURB REFER TO SHEETS 1 & 2 TO MODIFY THE CURB HEIGHT TAPERS AND MAINTAIN POSITIVE BOULEVARD DRAINAGE.

(2) MEAREST FAIL FOR SKEWED RAILWAYS IN NO INSTANCE SHALL BE PLACED 12' MINIMUM TO 15' MAXIMUM FROM THE NEAREST FAIL FOR SKEWED RAILWAYS IN NO INSTANCE SHALL THE DETECTABLE WARNING BE CLOSER THAN 12' MEASURED PERPENDICULAR TO THE NEAREST RAIL.

WHAN PEDSTRIAN GATES ARE PROVIDED, DETECTABLE WARNING SURFACES SHALL.

BE PLACED ON THE SIDE OF THE GATES OPPOSITE THE RAIL, 2' FROM THE
APPROACHING SIDE OF THE GATE ARM, THIS CRITERIA GOVERNS OVER NOTE (\$\frac{1}{2}\$.

(4) CROSSING SURFACE SHALL EXTEND 2' MINIMUM PAST THE OUTSIDE EDGE OF WALK OR SHARED-USE PATH,

(5) 3' FOR MEDIANS AND SPLITTER ISLANDS, NOSE CAN BE REDUCED TO 2' ON FREE RIGHT ISLANDS,

OF DEPARTMENT OF THE PROPERTY OF THE MEDIAND OF THE PROPERTY OF THE PROPERTY OF THE PARTY OF THE PROPERTY OF THE PROPERTY OF THE PARTY OF THE PROPERTY OF THE PARTY OF THE PARTY OF THE PROPERTY OF THE PARTY OF

1-23-2017 STANDARD PLAN 5-297.250 4 OF 6

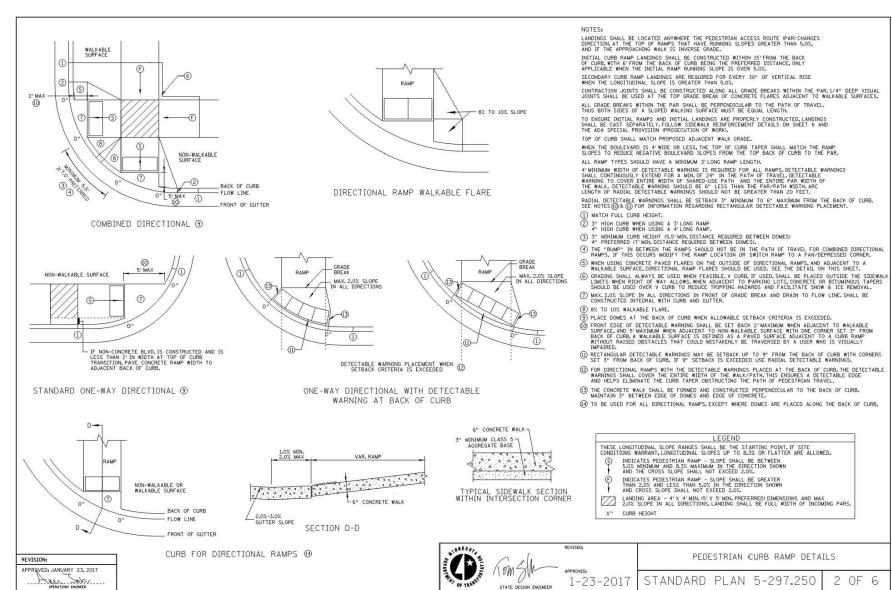
PEDESTRIAN CURB RAMP DETAILS

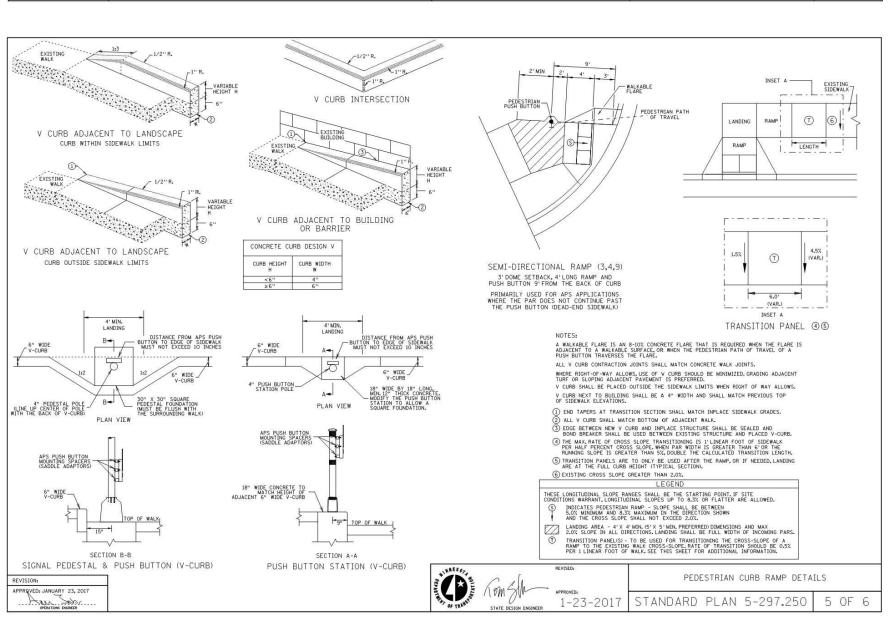
FULL CURB HEIGHT. 2'FOR 4" HIGH CURB AND 3'FOR 6" HIGH CURB.

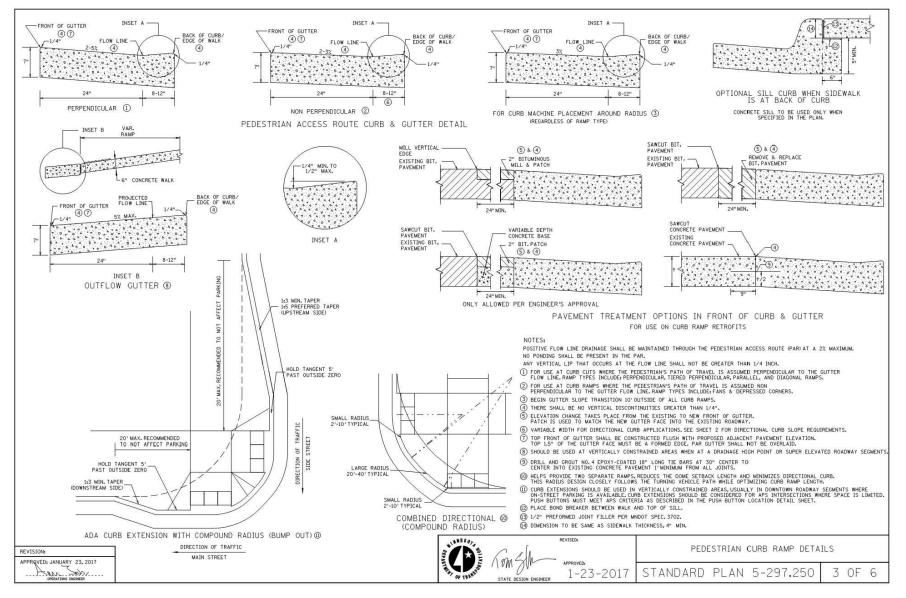
LY USED FOR MEDIANS AND ISLANDS.

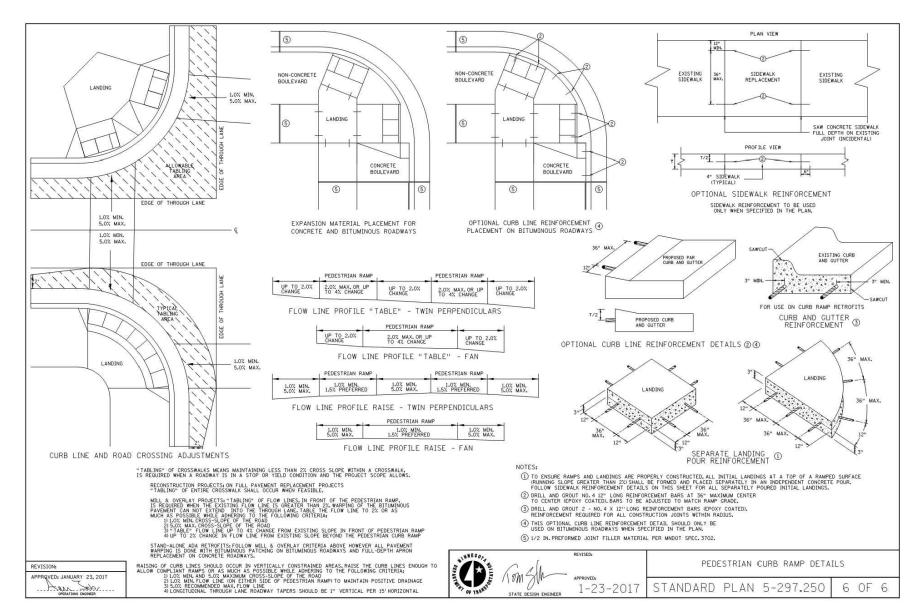
NEAREST RAIL

FACE OF CURB/PROJECTED FACE OF CURB











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UNDER THE LAWS OF THE STATE OF
MINNESOTA.

Matthew R. Pavek

DATE XX/XX/19 LICENSE NO. 44263

ISSUE/SUBMITTAL SUMMARY

DATE DESCRIPTION

20.06.05 CITY SUBMITTAL

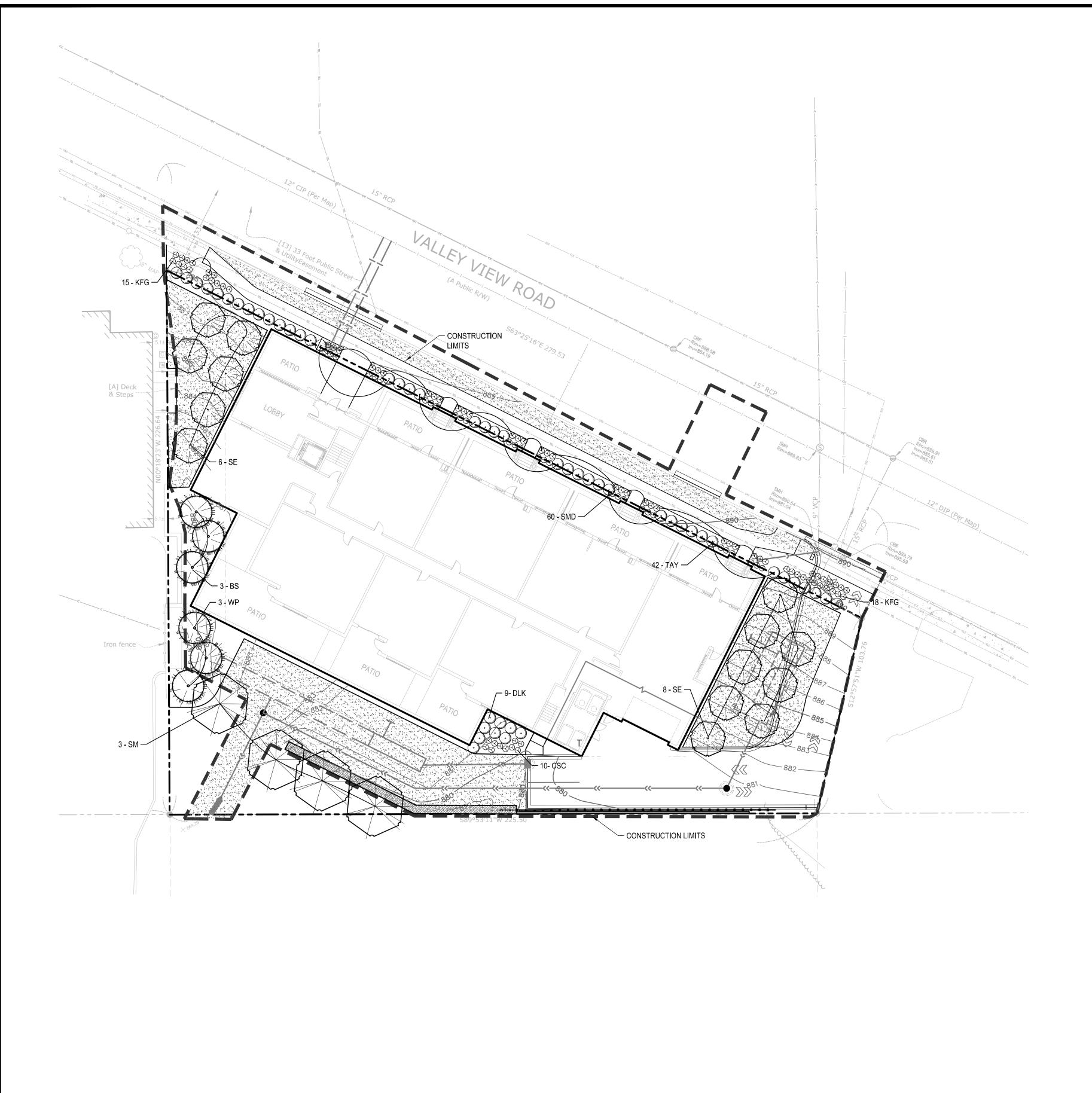
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PROJECT NUMBER: 19261
REVISION SUMMARY

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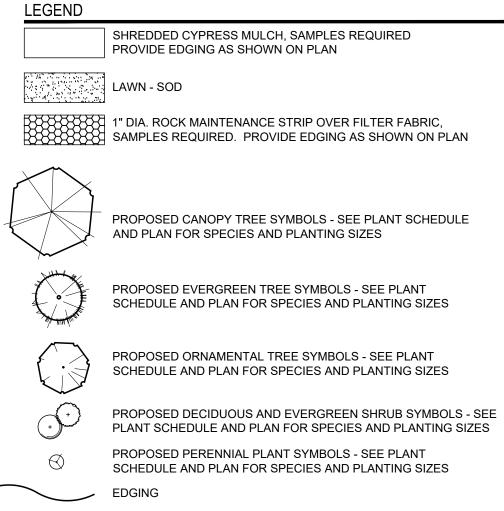
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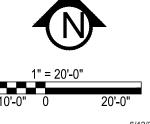
PLANT SCHEDULE - ENTIRE SITE							
SYM	QUANT. TOTAL	COMMON NAME	BOTANICAL NAME	SIZE	ROOT	COMMENTS	
		DECIDUOUS TREES					
SM	3	SUGAR MAPLE	Acer saccharum	3" CAL.	B&B	STRAIGHT LEADER. FULL FORM	
		EVERGREEN TREES					
BS	3	BLACK HILLS SPRUCE	Picea glauca 'Densata'	6' ht.	B&B	STRAIGHT LEADER. FULL FORM	
WP	3	WHITE PINE	Pinus strobus	6' ht.	B&B		
		ORNAMENTAL TREES	Malus 'Spring Snow'	1.5" CAL.	B&B	STRAIGHT LEADER. FULL FORM	
SE	14	AUTUMN BRILLIANCE SERVICEBERRY	Amelanchier x grandiflora 'Autumn Brilliance (tree form)'	1.5" CAL.	B&B		
		SHRUBS - CONIFEROUS & EVERGREEN					
TAY	42	TAUNTON'S YEW	Taxus x media 'Tauntonii'	24" HT.	CONT.		
DKL	8	DWARF KOREAN LILAC	Syringa meyeri 'Palibin'	24" HT.	CONT.		
		PERENNIALS & GRASSES					
KFG	33	KARL FOERSTER GRASS	Calamagrostis x acutiflora "Karl Foerster"	#1	CONT.		
SMD	60	STELLA SUPREME DAYLILLY	Hemerocallis 'Stella Supreme'	#1	CONT.		
CSC	10	Cheyenne Spirit Coneflower	Echinacea 'Cheyenne Spirit'	#1	CONT.		

LANDSCAPE NOTES:

- 1. WHERE SHOWN, SHRUB & PERENNIAL BEDS SHALL BE MULCHED WITH 4" DEPTH (MINIMUM AFTER INSTALLATION AND/OR TOP DRESSING OPERATIONS) OF SHREDDED CYPRESS MULCH.
- 2. ALL TREES SHALL BE MULCHED WITH SHREDDED CYPRESS MULCH TO OUTER EDGE OF SAUCER OR TO EDGE OF PLANTING BED, IF APPLICABLE. ALL MULCH SHALL BE KEPT WITHIN A MINIMUM OF 2" FROM TREE TRUNK.
- 3. IF SHOWN ON PLAN, RANDOM SIZED LIMESTONE BOULDERS COLOR AND SIZE TO COMPLIMENT NEW LANDSCAPING. OWNER TO APPROVE BOULDER SAMPLES PRIOR TO INSTALLATION.
- PLANT MATERIALS SHALL CONFORM WITH THE AMERICAN ASSOCIATION OF NURSERYMEN STANDARDS AND SHALL BE OF HARDY STOCK, FREE FROM DISEASE, DAMAGE AND DISFIGURATION. CONTRACTOR IS RESPONSIBLE FOR MAINTAINING PLUMPNESS OF PLANT MATERIAL FOR DURATION OF ACCEPTANCE PERIOD.
- UPON DISCOVERY OF A DISCREPANCY BETWEEN THE QUANTITY OF PLANTS SHOWN ON THE SCHEDULE AND THE QUANTITY SHOWN ON THE PLAN, THE PLAN SHALL GOVERN.
- 6. CONDITION OF VEGETATION SHALL BE MONITORED BY THE LANDSCAPE ARCHITECT THROUGHOUT THE DURATION OF THE CONTRACT. LANDSCAPE MATERIALS PART OF THE CONTRACT SHALL BE WARRANTED FOR ONE (1) FULL GROWING SEASONS FROM SUBSTANTIAL COMPLETION DATE.
- ALL AREAS DISTURBED BY CONSTRUCTION ACTIVITIES SHALL RECEIVE 4" LAYER TOPSOIL AND SOD AS SPECIFIED UNLESS OTHERWISE NOTED ON THE DRAWINGS.
- COORDINATE LOCATION OF VEGETATION WITH UNDERGROUND AND OVERHEAD UTILITIES, LIGHTING FIXTURES, DOORS AND WINDOWS. CONTRACTOR SHALL STAKE IN THE FIELD FINAL LOCATION OF TREES AND SHRUBS FOR REVIEW AND APPROVAL BY THE LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.
- 9. ALL PLANT MATERIALS SHALL BE WATERED AND MAINTAINED UNTIL ACCEPTANCE.
- 10. REPAIR AT NO COST TO OWNER ALL DAMAGE RESULTING FROM LANDSCAPE CONTRACTOR'S ACTIVITIES.
- 11. SWEEP AND MAINTAIN ALL PAVED SURFACES FREE OF DEBRIS GENERATED FROM LANDSCAPE CONTRACTOR'S ACTIVITIES.
- 12. REPAIR AT NO COST TO THE OWNER IRRIGATION SYSTEM DAMAGED FROM LANDSCAPE CONSTRUCTION ACTIVITIES.
- 13. PROVIDE SITE WIDE IRRIGATION SYSTEM DESIGN AND INSTALLATION. SYSTEM SHALL BE FULLY PROGRAMMABLE AND CAPABLE OF ALTERNATE DATE WATERING. THE SYSTEM SHALL PROVIDE HEAD TO HEAD OR DRIP COVERAGE AND BE CAPABLE OF DELIVERING ONE INCH OF PRECIPITATION PER WEEK. SYSTEM SHALL EXTEND INTO THE PUBLIC RIGHT-OF-WAY TO THE EDGE OF PAVEMENT/BACK OF CURB.
- 14. CONTRACTOR SHALL SECURE APPROVAL OF PROPOSED IRRIGATION SYSTEM INLCUDING PRICING FROM OWNER, PRIOR TO INSTALLATION.







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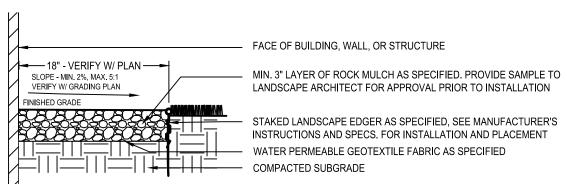
DATE XX/XX/19 LICENSE NO. 24904

ISSUE/SUBMITTAL SUMMARY DATE DESCRIPTION 20.06.05 CITY SUBMITTAL

DRAWN BY:kit REVIEWED BY: XX

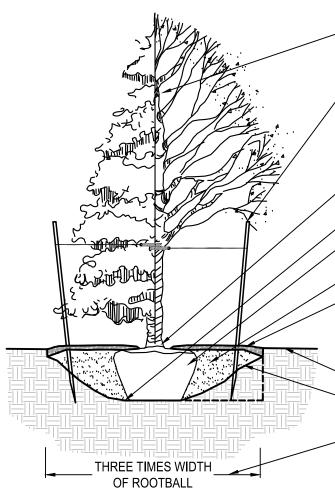
PROJECT NUMBER: 19261 **REVISION SUMMARY** DATE DESCRIPTION

LANDSCAPE PLAN



AGGREGATE MAINTANENCE STRIP

NTS



PRUNE AS FIELD DIRECTED BY THE LANDSCAPE ARCHITECT TO IMPROVE APPEARANCE (RETAIN NORMAL TREE SHAPE)

- THREE 2"X4"X8' WOODEN STAKES, STAINED BROWN WITH TWO STRANDS OF WIRE TWISTED TOGETHER. STAKES SHALL BE PLACED AT 120° TO ONE ANOTHER. WIRE SHALL BE THREADED THROUGH NYLON STRAPPING WITH GROMMETS. ALTERNATE STABILIZING METHODS MAY BE PROPOSED BY CONTRACTOR.

- TRUNK FLARE JUNCTION: PLANT TREE 1"-2" ABOVE

EXISTING GRADE

COMPACT BOTTOM OF PIT, TYP.

 CUT AND REMOVE BURLAP FROM TOP 1/3 OF ROOT BALL. IF NON-BIODEGRADABLE, REMOVE COMPLETELY
 BACKFILL AS SPECIFIED

- MULCH TO OUTER EDGE OF SAUCER OR TO EDGE OF PLANTING BED, IF APPLICABLE. ROCK OR ORGANIC MULCH, SEE GENERAL LANDSCAPE NOTES AND PLAN NOTES FOR MULCH TYPE. KEEP MULCH MIN. 2" FROM PLANT TRUNK

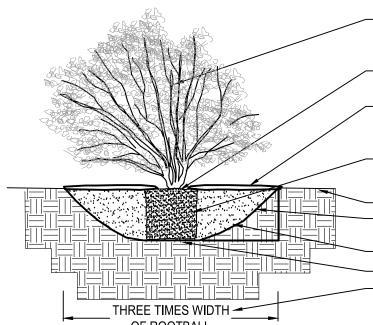
EXISTING GRADE

SLOPE SIDES OF HOLE OR VERTICAL SIDES AT EDGE OF PLANTING BED

SUBJECT: STEELING MODIFIC EXCENTIFICATION PAGES ON

RULE OF THUMB - MODIFY EXCAVATION BASED ON LOCATION OF PLANT MATERIAL AND DESIGN OF BEDS OR OVERALL PLANT PLACEMENT

DECIDUOUS & CONIFEROUS TREE PLANTING



PRUNE AS FIELD DIRECTED BY THE LANDSCAPE ARCHITECT TO IMPROVE APPEARANCE (RETAIN NORMAL SHAPE FOR SPECIES)

PLANT TOP OF ROOTBALL 1-2" ABOVE ABOVE SURROUNDING GRADE
ROCK OR ORGANIC MULCH, SEE GENERAL LAN

ROCK OR ORGANIC MULCH, SEE GENERAL LANDSCAPE NOTES AND PLAN NOTES FOR MULCH TYPE. KEEP MULCH MIN. 2" FROM PLANT TRUNK
 ROOTS AT OUTER EDGE OF ROOTBALL LOOSENED TO ENSURE PROPER BACKFILL-TO-ROOT CONTACT

EXISTING GRADE
SLOPE SIDES OF HOLE OR VERTICAL SIDES AT EDGE OF

PLANTING BED

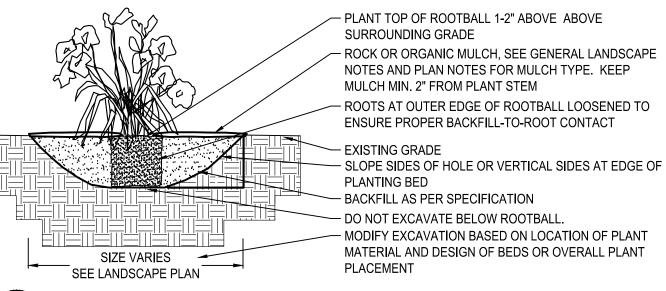
BACKFILL AS PER SPECIFICATION

BOOMER SYSTEM PROTECTION

DO NOT EXCAVATE BELOW ROOTBALL.
 RULE OF THUMB - MODIFY EXCAVATION BASED ON LOCATION OF PLANT MATERIAL AND DESIGN OF BEDS OR OVERALL PLANT PLACEMENT

DECIDUOUS & CONIFEROUS SHRUB PLANTING

NTS



PERENNIAL BED PLANTING

NTS

IRRIGATION NOTES:

1. ENTIRE SITE SHALL BE FULLY IRRIGATED. THE CONTRACTOR SHALL SUBMIT IRRIGATION SHOP DRAWINGS FOR REVIEW AND APPROVAL BY THE LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.

2. SEE MECHANICAL AND ELECTRICAL PLANS AND SPECIFICATIONS FOR IRRIGATION WATER, METER, AND POWER CONNECTIONS.

3. CONTRACTOR TO VERIFY LOCATION OF ALL UNDERGROUND/ABOVE GROUND FACILITIES PRIOR TO ANY EXCAVATION/INSTALLATION. ANY DAMAGE TO UNDERGROUND/ABOVE GROUND FACILITIES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND COSTS ASSOCIATED WITH CORRECTING DAMAGES SHALL BE BORNE ENTIRELY BY THE CONTRACTOR.

4. SERVICE EQUIPMENT AND INSTALLATION SHALL BE PER LOCAL UTILITY COMPANY STANDARDS AND SHALL BE PER NATIONAL AND LOCAL CODES. EXACT LOCATION OF SERVICE EQUIPMENT SHALL BE COORDINATED WITH THE LANDSCAPE ARCHITECT OR EQUIVALENT AT THE JOB SITE.

5. CONTRACTOR SHALL COORDINATE WITH LOCAL UTILITY COMPANY FOR THE PROPOSED ELECTRICAL SERVICE AND METERING FACILITIES.

6. IRRIGATION WATER LINE CONNECTION SIZE IS 1-½" AT BUILDING. VERIFY WITH MECHANICAL PLANS.COVAGE.

7. ALL MAIN LINES SHALL BE 18" BELOW FINISHED GRADE.

8. ALL LATERAL LINES SHALL BE 12" BELLOW FINISHED GRADE.

9. ALL EXPOSED PVC RISERS, IF ANY, SHALL BE GRAY IN COLOR.

10. CONTRACTOR SHALL LAY ALL SLEEVES AND CONDUIT AT 2'-0" BELOW THE FINISHED GRADE OF THE TOP OF PAVEMENT. EXTEND SLEEVES TO 2'-0" BEYOND PAVEMENT.

11. CONTRACTOR SHALL MARK THE LOCATION OF ALL SLEEVES AND CONDUIT WITH THE SLEEVING MATERIAL "ELLED" TO 2'-0" ABOVE FINISHED GRADE AND CAPPED.

12. FABRICATE ALL PIPE TO MANUFACTURE'S SPECIFICATIONS WITH CLEAN AND SQUARE CUT JOINTS. USE QUALITY GRADE PRIMER AND SOLVENT CEMENT FORMULATED FOR INTENDED TYPE OF CONNECTION.

13. BACKFILL ALL TRENCHES WITH SOIL FREE OF SHARP OBJECTS AND DEBRIS.

14. ALL VALVE BOXES AND COVERS SHALL BE BLACK IN COLOR.

15. GROUP VALVE BOXES TOGETHER FOR EASE WHEN SERVICE IS REQUIRED. LOCATE IN PLANT BED AREAS WHENEVER POSSIBLE.

16. IRRIGATION CONTROLLER LOCATION SHALL BE VERIFIED ON-SITE WITH OWNER'S REPRESENTATIVE.

17. CONTROL WIRES: 14 GAUGE DIRECT BURIAL, SOLID COPPER IRRIGATION WIRE. RUN UNDER MAIN LINE. USE MOISTURE-PROOF SPLICES AND SPLICE ONLY AT VALVES OR PULL BOXES. RUN SEPARATE HOT AND COMMON WIRE TO EACH VALVE AND ONE (1) SPARE WIRE AND GROUND TO FURTHEST VALVE FROM CONTROLLER. LABEL OR COLOR CODE ALL WIRES.

18. AVOID OVER SPRAY ON BUILDINGS, PAVEMENT, WALLS AND ROADWAYS BY INDIVIDUALLY ADJUSTING RADIUS OR ARC ON SPRINKLER HEADS AND FLOW CONTROL ON AUTOMATIC VALVE.

19. ADJUST PRESSURE REGULATING VALVES FOR OPTIMUM PRESSURE ON SITE.

20. USE SCREENS ON ALL HEADS.

21. A SET OF AS-BUILT DRAWINGS SHALL BE MAINTAINED ON-SITE AT ALL TIMES IN AN UPDATED CONDITION.

22. ALL PIPE 3" AND OVER SHALL HAVE THRUST BLOCKING AT EACH TURN.

23. ALL AUTOMATIC REMOTE CONTROL VALVES WILL HAVE 3" MINIMUM DEPTH OF 3/4" WASHED GRAVEL UNDERNEATH VALVE AND VALVE BOX. GRAVEL SHALL EXTENT 3" BEYOND PERIMETER OF VALVE BOX.

24. THERE SHALL BE 3" MINIMUM SPACE BETWEEN BOTTOM OF VALVE BOX COVER AND TOP OF VALVE STRUCTURE.

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Patrick J. Sarver

DATE XX/XX/19 LICENSE NO. 24904

THE LAWS OF THE STATE OF MINNESOTA

ISSUE/SUBMITTAL SUMMARY

DATE DESCRIPTION
20.06.05 CITY SUBMITTAL

DRAWN BY:kit REVIEWED BY: XX
PROJECT NUMBER: 19261

REVISION SUMMARY

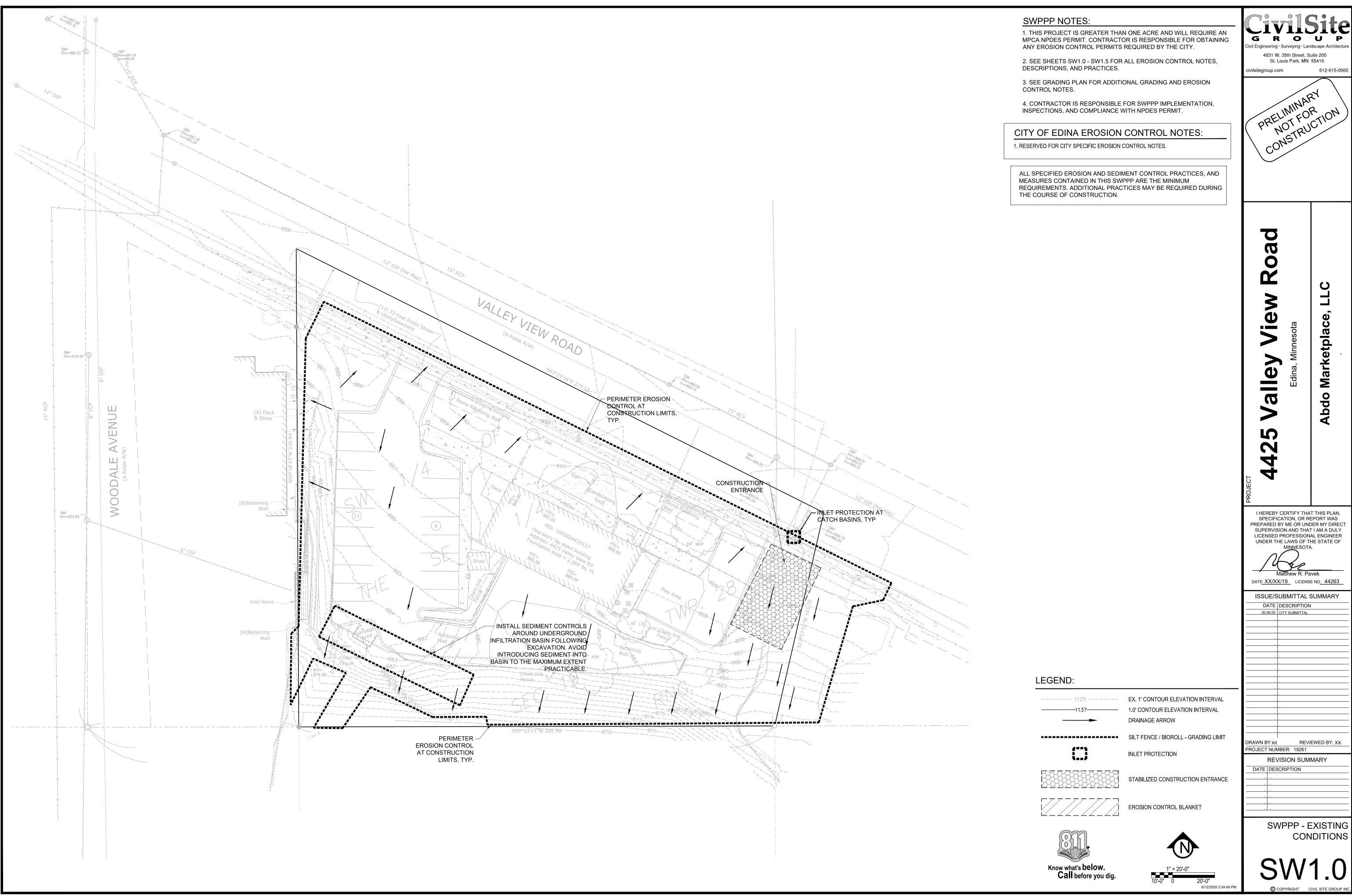
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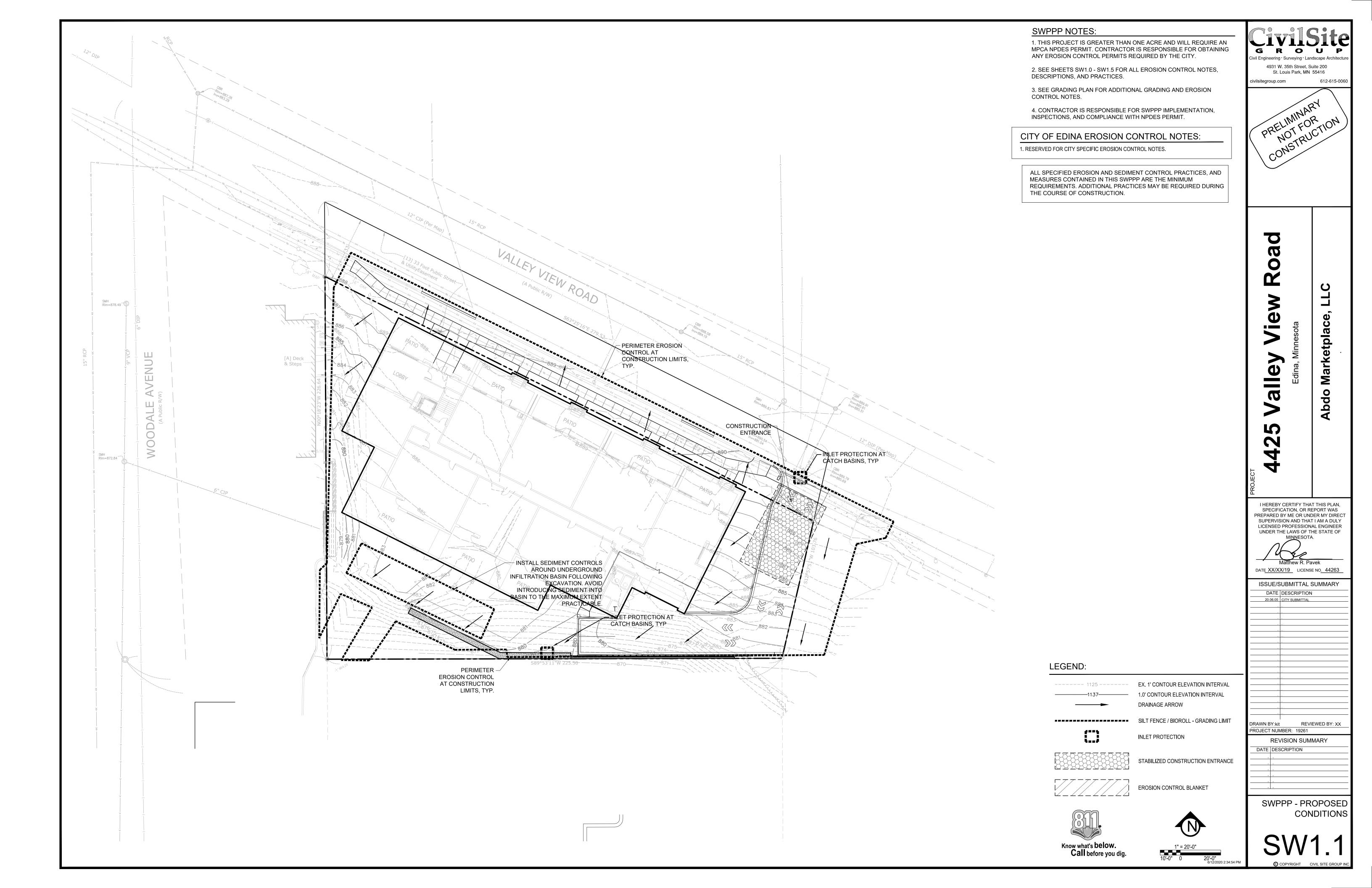
LANDSCAPE PLAN NOTES & DETAILS

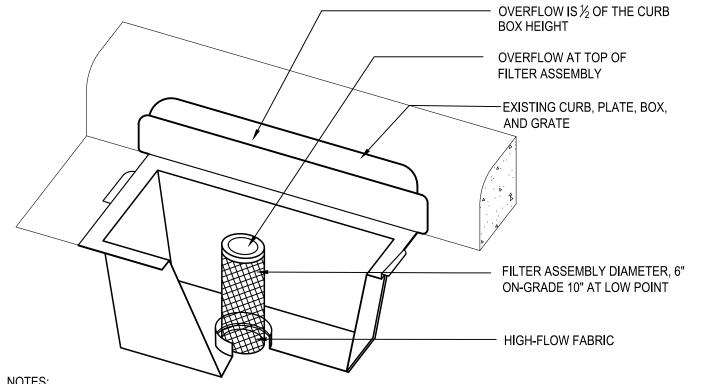
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Know what's below.

Call before you dig.



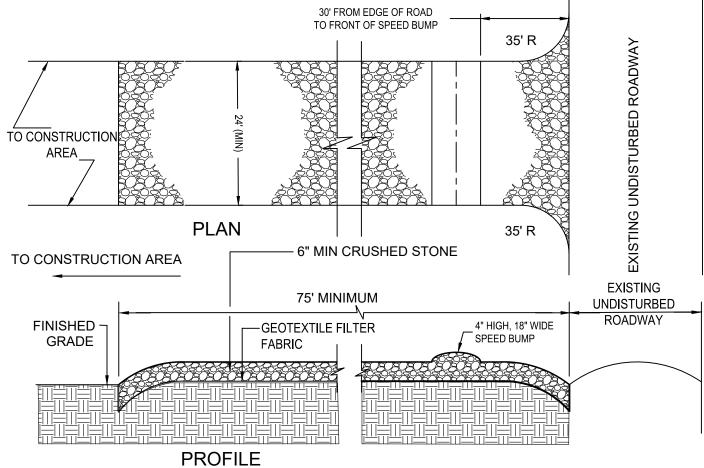




NOTES:

REPLACE INLET GRATE UPON COMPLETE INSTALLATION OF INLET PROTECTION FABRIC.
 CONTRACTOR SHALL REMOVE ALL ACCUMULATED SEDIMENT AND DEBRIS FROM THE SURFACE OF THE SYSTEM AFTER EACH STORM EVENT AND AT THE COMPLETION OF THE CONTRACT.
 REFERENCE APPLE VALLEY STANDARD PLATE ERO-4C.

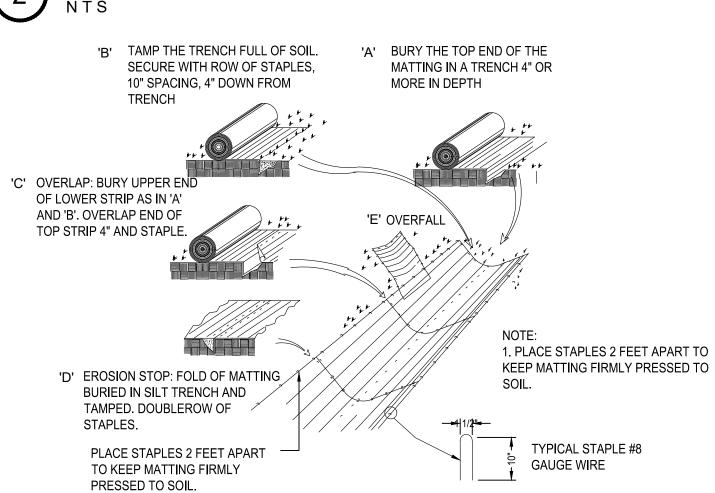
1 CURB INLET FILTER



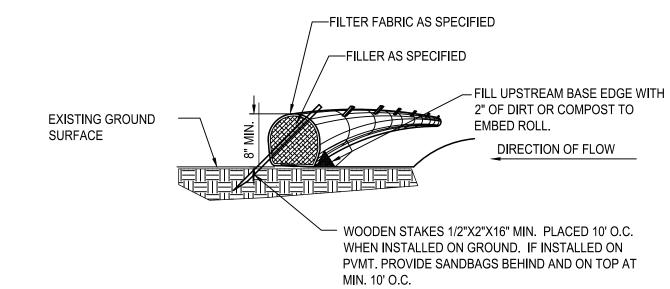
NOTES:

- 1. PROVIDE APPROPRIATE TRANSITION BETWEEN STABILIZED CONSTRUCTION ENTRANCE AND UNDISTURBED ROADWAY.
- THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO UNDISTURBED ROADWAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE OR ADDING STONE TO THE LENGTH OF THE ENTRANCE.
 REPAIR AND CLEANOUT MEASURES USED TO TRAP SEDIMENT.
- 4. ALL SEDIMENT SPILLED, DROPPED, WASHED, OR TRACKED ONTO UNDISTURBED ROADWAY SHALL BE REMOVED AS DIRECTED BY THE ENGINEER.
- 5. FINAL LOCATION AND INSTALLATION SHALL BE COORDINATED WITH THE CITY PRIOR TO CONSTRUCTION ACTIVITIES.
 6. CRUSHED STONE SHALL BE 1-1/2" DIA. CLOSE GRADED, AND IN ACCORDANCE TO MNDOT SECTION 2118.

STABILIZED CONSTRUCTION ACCESS



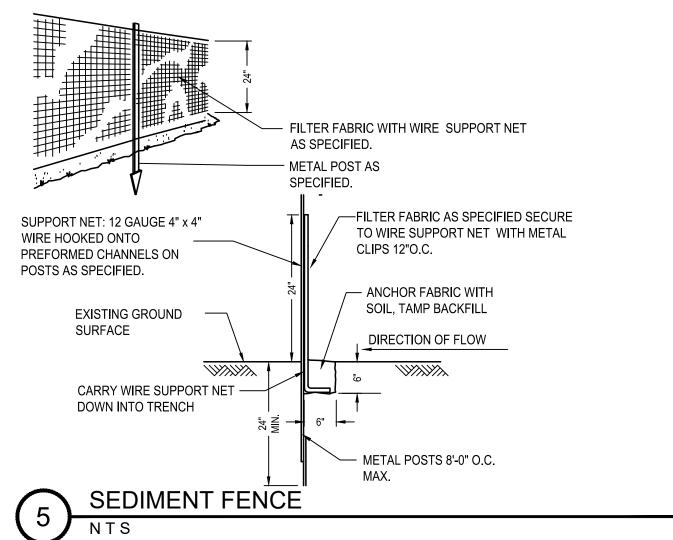




NOTE:
1. COMPOST FILTER LOGS (BIO ROLLS) SHALL BE FILTREXX EROSION CONTROL SOXX OR APPROVED EQUAL.
2. COMPOST FILLER TO BE MADE FROM A COMPOST BLEND 30%-40% GRADE 2 (SPEC 3890) AND 60%-70% PARTIALLY DECOMPOSED WOOD CHIPS, PER MNDOT SPEC 3897.

- 3. FILTER FABRIC SHALL BE GEOTEXTILE KNITTED MATERIAL WITH MAX. OPENINGS OF 3/8".
 4. IF MULTIPLE ROLLS NEEDED, OVERLAP BY MIN. 12" AT ENDS AND STAKE.
- 5. SILT SHALL BE REMOVED ONCE IT REACHES 80% OF THE HEIGHT OF THE ROLL OR AS DEEMED NECESSARY BY SITE CONTRACTOR TO MAINTAIN PROPER FUNCTION.





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Matthew R. Pav	/ek
DATE XX/XX/19 LICENSE	NO. 44263

RAWN BY:kit REVIEWED BY: XX

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PROJECT NUMBER: 19261

REVISION SUMMARY

DATE DESCRIPTION

SWPPP - DETAILS

SW1.2

THE CONTRACTOR AND ALL SUBCONTRACTORS INVOLVED WITH A CONSTRUCTION ACTIVITY THAT DISTURBS SITE SOIL OR WHO IMPLEMENT A POLLUTANT CONTROL MEASURE IDENTIFIED IN THE STORM WATER POLLUTION PREVENTION PLAN (SWPPP) MUST COMPLY WITH THE REQUIREMENTS OF THE NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM (NPDES) GENERAL PERMIT (DATED AUGUST 1, 2018 # MNR100001) AND ANY LOCAL GOVERNING AGENCY HAVING JURISDICTION CONCERNING EROSION AND SEDIMENTATION CONTROL.

STORMWATER DISCHARGE DESIGN REQUIREMENTS

THE NATURE OF THIS PROJECT WILL BE CONSISTENT WITH WHAT IS REPRESENTED IN THIS SET OF CONSTRUCTION PLANS AND SPECIFICATIONS. SEE THE SWPPP PLAN SHEETS AND SWPPP NARRATIVE (ATTACHMENT A: CONSTRUCTION SWPPP TEMPLATE) FOR ADDITIONAL SITE SPECIFIC SWPPP INFORMATION. THE PLANS SHOW LOCATIONS AND TYPES OF ALL TEMPORARY AND PERMANENT EROSION PREVENTION AND SEDIMENT CONTROL BMP'S, STANDARD DETAILS ARE ATTACHED TO THIS SWPPP

THE INTENDED SEQUENCING OF MAJOR CONSTRUCTION ACTIVITIES IS AS FOLLOWS:

- 1. INSTALL STABILIZED ROCK CONSTRUCTION ENTRANCE
- 2. INSTALLATION OF SILT FENCE AROUND SITE
- 3. INSTALL ORANGE CONSTRUCTION FENCING AROUND INFILTRATION AREAS. 4. CLEAR AND GRUB FOR TEMPORARY SEDIMENT BASIN / POND INSTALL
- 5. CONSTRUCT TEMPORARY SEDIMENT BASIN / POND (SECTION 14)
- 6. CLEAR AND GRUB REMAINDER OF SITE
- 7. STRIP AND STOCKPILE TOPSOIL 8. ROUGH GRADING OF SITE
- 9. STABILIZE DENUDED AREAS AND STOCKPILES
- 10. INSTALL SANITARY SEWER, WATER MAIN STORM SEWER AND SERVICES
- 11. INSTALL SILT FENCE / INLET PROTECTION AROUND CB'S 12. INSTALL STREET SECTION
- 13. INSTALL CURB AND GUTTER
- 14, BITUMINOUS ON STREETS 15. FINAL GRADE BOULEVARD, INSTALL SEED AND MULCH
- 16. REMOVE ACCUMULATED SEDIMENT FROM BASIN / POND
- 17. FINAL GRADE POND / INFILTRATION BASINS (DO NOT COMPACT SOILS IN INFILTRATION AREAS.) 18. WHEN ALL CONSTRUCTION ACTIVITY IS COMPLETE AND THE SITE IS STABILIZED BY EITHER SEED OR SOD/LANDSCAPING,
- REMOVE SILT FENCE AND RESEED ANY AREAS DISTURBED BY THE REMOVAL.

RECORDS RETENTION:

THE SWPPP (ORIGINAL OR COPIES) INCLUDING, ALL CHANGES TO IT, AND INSPECTIONS AND MAINTENANCE RECORDS MUST BE KEPT AT THE SITE DURING CONSTRUCTION BY THE PERMITTEE WHO HAS OPERATIONAL CONTROL OF THAT PORTION OF THE SITE. THE SWPPP CAN BE KEPT IN EITHER THE FIELD OFFICE OR IN AN ON SITE VEHICLE DURING NORMAL WORKING HOURS.

ALL OWNER(S) MUST KEEP THE SWPPP, ALONG WITH THE FOLLOWING ADDITIONAL RECORDS, ON FILE FOR THREE (3) YEARS AFTER SUBMITTAL OF THE NOT AS OUTLINED IN SECTION 4. THIS DOES NOT INCLUDE ANY RECORDS AFTER SUBMITTAL OF THE

- 2. ANY OTHER STORMWATER RELATED PERMITS REQUIRED FOR THE PROJECT;
- 3. RECORDS OF ALL INSPECTION AND MAINTENANCE CONDUCTED DURING CONSTRUCTION (SEE SECTION 11, INSPECTIONS
- 4. ALL PERMANENT OPERATION AND MAINTENANCE AGREEMENTS THAT HAVE BEEN IMPLEMENTED, INCLUDING ALL RIGHT OF
- WAY, CONTRACTS, COVENANTS AND OTHER BINDING REQUIREMENTS REGARDING PERPETUAL MAINTENANCE; AND 5. ALL REQUIRED CALCULATIONS FOR DESIGN OF THE TEMPORARY AND PERMANENT STORMWATER MANAGEMENT

SWPPP IMPLEMENTATION RESPONSIBILITIES:

- 1. THE OWNER AND CONTRACTOR ARE PERMITTEE(S) AS IDENTIFIED BY THE NPDES PERMIT.
- 2. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL ON-SITE IMPLEMENTATION OF THE SWPPP, INCLUDING THE ACTIVITIES OF ALL OF THE CONTRACTOR'S SUBCONTRACTORS.
- 3. CONTRACTOR SHALL PROVIDE A PERSON(S) KNOWLEDGEABLE AND EXPERIENCED IN THE APPLICATION OF EROSION PREVENTION AND SEDIMENT CONTROL BMPS TO OVERSEE ALL INSTALLATION AND MAINTENANCE OF BMPS AND
- 4. CONTRACTOR SHALL PROVIDE PERSON(S) MEETING THE TRAINING REQUIREMENTS OF THE NPDES PERMIT TO CONDUCT INSPECTION AND MAINTENANCE OF ALL EROSION PREVENTION AND SEDIMENT CONTROL BMPS IN ACCORDANCE WITH THE REQUIREMENTS OF THE PERMIT. ONE OF THESE INDIVIDUAL(S) MUST BE AVAILABLE FOR AN ONSITE INSPECTION WITHIN 72 HOURS UPON REQUEST BY MPCA. CONTRACTOR SHALL PROVIDE TRAINING DOCUMENTATION FOR THESE INDIVIDUAL(S) AS REQUIRED BY THE NPDES PERMIT. THIS TRAINING DOCUMENTATION SHALL BE RECORDED IN OR WITH THE SWPPP BEFORE THE START OF CONSTRUCTION OR AS SOON AS THE PERSONNEL FOR THE PROJECT HAVE BEEN DETERMINED. DOCUMENTATION SHALL INCLUDE:
- 4.1. NAMES OF THE PERSONNEL ASSOCIATED WITH THE PROJECT THAT ARE REQUIRED TO BE TRAINED PER SECTION
- 4.2. DATES OF TRAINING AND NAME OF INSTRUCTOR AND ENTITY PROVIDING TRAINING. 4.3. CONTENT OF TRAINING COURSE OR WORKSHOP INCLUDING THE NUMBER OF HOURS OF TRAINING
- 5. FOLLOWING FINAL STABILIZATION AND THE TERMINATION OF COVERAGE FOR THE NPDES PERMIT. THE OWNER IS EXPECTED TO FURNISH LONG TERM OPERATION AND MAINTENANCE (O & M) OF THE PERMANENT STORM WATER MANAGEMENT SYSTEM.

CONSTRUCTION ACTIVITY REQUIREMENTS

SWPPP AMENDMENTS (SECTION 6):

- 1. ONE OF THE INDIVIDUALS DESCRIBED IN ITEM 21.2.A OR ITEM 21.2.B OR ANOTHER QUALIFIED INDIVIDUAL MUST COMPLETE ALL SWPPP CHANGES, CHANGES INVOLVING THE USE OF A LESS STRINGENT BMP MUST INCLUDE A JUSTIFICATION DESCRIBING HOW THE REPLACEMENT BMP IS EFFECTIVE FOR THE SITE CHARACTERISTICS
- 2. PERMITTEES MUST AMEND THE SWPPP TO INCLUDE ADDITIONAL OR MODIFIED BMPS AS NECESSARY TO CORRECT PROBLEMS IDENTIFIED OR ADDRESS SITUATIONS WHENEVER THERE IS A CHANGE IN DESIGN, CONSTRUCTION, OPERATION, MAINTENANCE, WEATHER OR SEASONAL CONDITIONS HAVING A SIGNIFICANT EFFECT ON THE DISCHARGE OF POLLUTANTS TO SURFACE WATERS OR GROUNDWATER.
- 3. PERMITTEES MUST AMEND THE SWPPP TO INCLUDE ADDITIONAL OR MODIFIED BMPS AS NECESSARY TO CORRECT PROBLEMS IDENTIFIED OR ADDRESS SITUATIONS WHENEVER INSPECTIONS OR INVESTIGATIONS BY THE SITE OWNER OR OPERATOR, USEPA OR MPCA OFFICIALS INDICATE THE SWPPP IS NOT EFFECTIVE IN ELIMINATING OR SIGNIFICANTLY MINIMIZING THE DISCHARGE OF POLLUTANTS TO SURFACE WATERS OR GROUNDWATER OR THE DISCHARGES ARE CAUSING WATER QUALITY STANDARD EXCEEDANCES (E.G., NUISANCE CONDITIONS AS DEFINED IN MINN. R. 7050.0210, SUBP. 2) OR THE SWPPP IS NOT CONSISTENT WITH THE OBJECTIVES OF A USEPA APPROVED TMDL.

BMP SELECTION AND INSTALLATION (SECTION 7):

1. PERMITTEES MUST SELECT, INSTALL, AND MAINTAIN THE BMPS IDENTIFIED IN THE SWPPP AND IN THIS PERMIT IN AN APPROPRIATE AND FUNCTIONAL MANNER AND IN ACCORDANCE WITH RELEVANT MANUFACTURER SPECIFICATIONS AND ACCEPTED ENGINEERING PRACTICES.

EROSION PREVENTION (SECTION 8):

- BEFORE WORK BEGINS, PERMITTEES MUST DELINEATE THE LOCATION OF AREAS NOT TO BE DISTURBED. 2. PERMITTEES MUST MINIMIZE THE NEED FOR DISTURBANCE OF PORTIONS OF THE PROJECT WITH STEEP SLOPES, WHEN STEEP SLOPES MUST BE DISTURBED, PERMITTEES MUST USE TECHNIQUES SUCH AS PHASING AND STABILIZATION PRACTICES DESIGNED FOR STEEP SLOPES (E.G., SLOPE DRAINING AND TERRACING).
- 3. PERMITTEES MUST STABILIZE ALL EXPOSED SOIL AREAS, INCLUDING STOCKPILES. STABILIZATION MUST BE INITIATED IMMEDIATELY TO LIMIT SOIL EROSION WHEN CONSTRUCTION ACTIVITY HAS PERMANENTLY OR TEMPORARILY CEASED ON ANY PORTION OF THE SITE AND WILL NOT RESUME FOR A PERIOD EXCEEDING 14 CALENDAR DAYS. STABILIZATION MUST BE COMPLETED NO LATER THAN 14 CALENDAR DAYS AFTER THE CONSTRUCTION ACTIVITY HAS CEASED. STABILIZATION IS NOT REQUIRED ON CONSTRUCTED BASE COMPONENTS OF ROADS, PARKING LOTS AND SIMILAR SURFACES. STABILIZATION IS NOT REQUIRED ON TEMPORARY STOCKPILES WITHOUT SIGNIFICANT SILT, CLAY OR ORGANIC COMPONENTS (E.G., CLEAN AGGREGATE STOCKPILES, DEMOLITION CONCRETE STOCKPILES, SAND STOCKPILES) BUT PERMITTEES MUST PROVIDE SEDIMENT CONTROLS AT THE BASE OF THE STOCKPILE.
- 4. FOR PUBLIC WATERS THAT THE MINNESOTA DNR HAS PROMULGATED "WORK IN WATER RESTRICTIONS" DURING SPECIFIED FISH SPAWNING TIME FRAMES, PERMITTEES MUST COMPLETE STABILIZATION OF ALL EXPOSED SOIL AREAS WITHIN 200 FEET OF THE WATER'S EDGE, AND THAT DRAIN TO THESE WATERS, WITHIN 24 HOURS DURING THE RESTRICTION PERIOD.
- 5. PERMITTEES MUST STABILIZE THE NORMAL WETTED PERIMETER OF THE LAST 200 LINEAR FEET OF TEMPORARY OR PERMANENT DRAINAGE DITCHES OR SWALES THAT DRAIN WATER FROM THE SITE WITHIN 24 HOURS AFTER CONNECTING TO A SURFACE WATER OR PROPERTY EDGE, PERMITTEES MUST COMPLETE STABILIZATION OF REMAINING PORTIONS OF TEMPORARY OR PERMANENT DITCHES OR SWALES WITHIN 14 CALENDAR DAYS AFTER CONNECTING TO A SURFACE

- WATER OR PROPERTY EDGE AND CONSTRUCTION IN THAT PORTION OF THE DITCH TEMPORARILY OR PERMANENTLY
- CEASES 6. TEMPORARY OR PERMANENT DITCHES OR SWALES BEING USED AS A SEDIMENT CONTAINMENT SYSTEM DURING CONSTRUCTION (WITH PROPERLY DESIGNED ROCK-DITCH CHECKS, BIO ROLLS, SILT DIKES, ETC.) DO NOT NEED TO BE STABILIZED. PERMITTEES MUST STABILIZE THESE AREAS WITHIN 24 HOURS AFTER THEIR USE AS A SEDIMENT CONTAINMENT SYSTEM CEASES
- 7. PERMITTEES MUST NOT USE MULCH, HYDROMULCH, TACKIFIER, POLYACRYLAMIDE OR SIMILAR EROSION PREVENTION PRACTICES WITHIN ANY PORTION OF THE NORMAL WETTED PERIMETER OF A TEMPORARY OR PERMANENT DRAINAGE DITCH OR SWALE SECTION WITH A CONTINUOUS SLOPE OF GREATER THAN 2 PERCENT. 8. PERMITTEES MUST PROVIDE TEMPORARY OR PERMANENT ENERGY DISSIPATION AT ALL PIPE OUTLETS WITHIN 24 HOURS
- AFTER CONNECTION TO A SURFACE WATER OR PERMANENT STORMWATER TREATMENT SYSTEM 9. PERMITTEES MUST NOT DISTURB MORE LAND (I.E., PHASING) THAN CAN BE EFFECTIVELY INSPECTED AND MAINTAINED IN ACCORDANCE WITH SECTION 11.
- SEDIMENT CONTROL (SECTION 9):

THE DOWNGRADIENT PERIMETER

- 1. PERMITTEES MUST ESTABLISH SEDIMENT CONTROL BMPS ON ALL DOWNGRADIENT PERIMETERS OF THE SITE AND DOWNGRADIENT AREAS OF THE SITE THAT DRAIN TO ANY SURFACE WATER, INCLUDING CURB AND GUTTER SYSTEMS. PERMITTEES MUST LOCATE SEDIMENT CONTROL PRACTICES UPGRADIENT OF ANY BUFFER ZONES. PERMITTEES MUST INSTALL SEDIMENT CONTROL PRACTICES BEFORE ANY UPGRADIENT LAND-DISTURBING ACTIVITIES BEGIN AND MUST KEEP THE SEDIMENT CONTROL PRACTICES IN PLACE UNTIL THEY ESTABLISH PERMANENT COVER.
- 2. IF DOWNGRADIENT SEDIMENT CONTROLS ARE OVERLOADED, BASED ON FREQUENT FAILURE OR EXCESSIVE MAINTENANCE REQUIREMENTS, PERMITTEES MUST INSTALL ADDITIONAL UPGRADIENT SEDIMENT CONTROL PRACTICES OR REDUNDANT BMPS TO ELIMINATE THE OVERLOADING AND AMEND THE SWPPP TO IDENTIFY THESE ADDITIONAL PRACTICES AS REQUIRED IN ITEM 6.3.
- 3. TEMPORARY OR PERMANENT DRAINAGE DITCHES AND SEDIMENT BASINS DESIGNED AS PART OF A SEDIMENT CONTAINMENT SYSTEM (E.G., DITCHES WITH ROCK-CHECK DAMS) REQUIRE SEDIMENT CONTROL PRACTICES ONLY AS APPROPRIATE FOR SITE CONDITIONS.
- 4. A FLOATING SILT CURTAIN PLACED IN THE WATER IS NOT A SEDIMENT CONTROL BMP TO SATISFY ITEM 9.2 EXCEPT WHEN WORKING ON A SHORELINE OR BELOW THE WATERLINE. IMMEDIATELY AFTER THE SHORT TERM CONSTRUCTION ACTIVITY (E.G., INSTALLATION OF RIP RAP ALONG THE SHORELINE) IN THAT AREA IS COMPLETE, PERMITTEES MUST INSTALL AN UPLAND PERIMETER CONTROL PRACTICE IF EXPOSED SOILS STILL DRAIN TO A SURFACE WATER.
- 5. PERMITTEES MUST RE-INSTALL ALL SEDIMENT CONTROL PRACTICES ADJUSTED OR REMOVED TO ACCOMMODATE SHORT-TERM ACTIVITIES SUCH AS CLEARING OR GRUBBING, OR PASSAGE OF VEHICLES, IMMEDIATELY AFTER THE SHORT-TERM ACTIVITY IS COMPLETED. PERMITTEES MUST RE-INSTALL SEDIMENT CONTROL PRACTICES BEFORE THE NEXT PRECIPITATION EVENT EVEN IF THE SHORT-TERM ACTIVITY IS NOT COMPLETE.
- 6. PERMITTEES MUST PROTECT ALL STORM DRAIN INLETS USING APPROPRIATE BMPS DURING CONSTRUCTION UNTIL THEY ESTABLISH PERMANENT COVER ON ALL AREAS WITH POTENTIAL FOR DISCHARGING TO THE INLET. 7. PERMITTEES MAY REMOVE INLET PROTECTION FOR A PARTICULAR INLET IF A SPECIFIC SAFETY CONCERN (E.G. STREET FLOODING/FREEZING) IS IDENTIFIED BY THE PERMITTEES OR THE JURISDICTIONAL AUTHORITY (E.G.,
- CITY/COUNTY/TOWNSHIP/MINNESOTA DEPARTMENT OF TRANSPORTATION ENGINEER). PERMITTEES MUST DOCUMENT THE NEED FOR REMOVAL IN THE SWPPP. 8. PERMITTEES MUST PROVIDE SILT FENCE OR OTHER EFFECTIVE SEDIMENT CONTROLS AT THE BASE OF STOCKPILES ON
- 9. PERMITTEES MUST LOCATE STOCKPILES OUTSIDE OF NATURAL BUFFERS OR SURFACE WATERS, INCLUDING STORMWATER CONVEYANCES SUCH AS CURB AND GUTTER SYSTEMS UNLESS THERE IS A BYPASS IN PLACE FOR THE
- 10. PERMITTEES MUST INSTALL A VEHICLE TRACKING BMP TO MINIMIZE THE TRACK OUT OF SEDIMENT FROM THE
- CONSTRUCTION SITE OR ONTO PAVED ROADS WITHIN THE SITE. 11. PERMITTEES MUST USE STREET SWEEPING IF VEHICLE TRACKING BMPS ARE NOT ADEQUATE TO PREVENT SEDIMENT TRACKING ONTO THE STREET
- 12. PERMITTEES MUST INSTALL TEMPORARY SEDIMENT BASINS AS REQUIRED IN SECTION 14.
- 13. IN ANY AREAS OF THE SITE WHERE FINAL VEGETATIVE STABILIZATION WILL OCCUR, PERMITTEES MUST RESTRICT VEHICLE AND EQUIPMENT USE TO MINIMIZE SOIL COMPACTION.
- 14. PERMITTEES MUST PRESERVE TOPSOIL ON THE SITE, UNLESS INFEASIBLE.
- 15. PERMITTEES MUST DIRECT DISCHARGES FROM BMPS TO VEGETATED AREAS UNLESS INFEASIBLE 16. PERMITTEES MUST PRESERVE A 50 FOOT NATURAL BUFFER OR, IF A BUFFER IS INFEASIBLE ON THE SITE, PROVIDE REDUNDANT (DOUBLE) PERIMETER SEDIMENT CONTROLS WHEN A SURFACE WATER IS LOCATED WITHIN 50 FEET OF THE PROJECT'S EARTH DISTURBANCES AND STORMWATER FLOWS TO THE SURFACE WATER, PERMITTEES MUST INSTALL PERIMETER SEDIMENT CONTROLS AT LEAST 5 FEET APART UNLESS LIMITED BY LACK OF AVAILABLE SPACE. NATURAL BUFFERS ARE NOT REQUIRED ADJACENT TO ROAD DITCHES, JUDICIAL DITCHES, COUNTY DITCHES, STORMWATER CONVEYANCE CHANNELS, STORM DRAIN INLETS, AND SEDIMENT BASINS. IF PRESERVING THE BUFFER IS INFEASIBLE, PERMITTEES MUST DOCUMENT THE REASONS IN THE SWPPP. SHEET PILING IS A REDUNDANT PERIMETER CONTROL IF
- INSTALLED IN A MANNER THAT RETAINS ALL STORMWATER. 17. PERMITTEES MUST USE POLYMERS, FLOCCULANTS, OR OTHER SEDIMENTATION TREATMENT CHEMICALS IN ACCORDANCE WITH ACCEPTED ENGINEERING PRACTICES, DOSING SPECIFICATIONS AND SEDIMENT REMOVAL DESIGN SPECIFICATIONS PROVIDED BY THE MANUFACTURER OR SUPPLIER. THE PERMITTEES MUST USE CONVENTIONAL EROSION AND SEDIMENT CONTROLS PRIOR TO CHEMICAL ADDITION AND MUST DIRECT TREATED STORMWATER TO A SEDIMENT CONTROL SYSTEM FOR FILTRATION OR SETTLEMENT OF THE FLOC PRIOR TO DISCHARGE.

DEWATERING AND BASIN DRAINING (SECTION 10):

- 1. PERMITTEES MUST DISCHARGE TURBID OR SEDIMENT-LADEN WATERS RELATED TO DEWATERING OR BASIN DRAINING ON THE PROJECT SITE UNLESS INFEASIBLE. PERMITTEES MAY DEWATER TO SURFACE WATERS IF THEY VISUALLY CHECK TO ENSURE ADEQUATE TREATMENT HAS BEEN OBTAINED AND NUISANCE CONDITIONS (SEE MINN. R. 7050.0210, SUBP. 2) WILL NOT RESULT FROM THE DISCHARGE. IF PERMITTEES CANNOT DISCHARGE THE WATER TO A SEDIMENTATION BASIN PRIOR TO ENTERING A SURFACE WATER, PERMITTEES MUST TREAT IT WITH APPROPRIATE BMPS SUCH THAT THE DISCHARGE DOES NOT ADVERSELY AFFECT THE SURFACE WATER OR DOWNSTREAM PROPERTIES.
- 2. IF PERMITTEES MUST DISCHARGE WATER CONTAINING OIL OR GREASE, THEY MUST USE AN OIL-WATER SEPARATOR OR
- SUITABLE FILTRATION DEVICE (E.G., CARTRIDGE FILTERS, ABSORBENTS PADS) PRIOR TO DISCHARGE. 3. PERMITTEES MUST DISCHARGE ALL WATER FROM DEWATERING OR BASIN-DRAINING ACTIVITIES IN A MANNER THAT DOES NOT CAUSE EROSION OR SCOUR IN THE IMMEDIATE VICINITY OF DISCHARGE POINTS OR INUNDATION OF WETLANDS IN THE IMMEDIATE VICINITY OF DISCHARGE POINTS THAT CAUSES SIGNIFICANT ADVERSE IMPACT TO THE WETLAND.
- 4. IF PERMITTEES USE FILTERS WITH BACKWASH WATER, THEY MUST HAUL THE BACKWASH WATER AWAY FOR DISPOSAL RETURN THE BACKWASH WATER TO THE BEGINNING OF THE TREATMENT PROCESS, OR INCORPORATE THE BACKWASH WATER INTO THE SITE IN A MANNER THAT DOES NOT CAUSE EROSION.

INSPECTIONS AND MAINTENANCE (SECTION 11):

- 1. PERMITTEES MUST ENSURE A TRAINED PERSON, AS IDENTIFIED IN ITEM 21.2.B, WILL INSPECT THE ENTIRE CONSTRUCTION SITE AT LEAST ONCE EVERY SEVEN (7) DAYS DURING ACTIVE CONSTRUCTION AND WITHIN 24 HOURS AFTER A RAINFALL EVENT GREATER THAN 1/2 INCH IN 24 HOURS.
- 2. PERMITTEES MUST INSPECT AND MAINTAIN ALL PERMANENT STORMWATER TREATMENT BMPS. 3. PERMITTEES MUST INSPECT ALL EROSION PREVENTION AND SEDIMENT CONTROL BMPS AND POLLUTION PREVENTION MANAGEMENT MEASURES TO ENSURE INTEGRITY AND EFFECTIVENESS. PERMITTEES MUST REPAIR, REPLACE OR
- SUPPLEMENT ALL NONFUNCTIONAL BMPS WITH FUNCTIONAL BMPS BY THE END OF THE NEXT BUSINESS DAY AFTER DISCOVERY UNLESS ANOTHER TIME FRAME IS SPECIFIED IN ITEM 11.5 OR 11.6, PERMITTEES MAY TAKE ADDITIONAL TIME IF FIELD CONDITIONS PREVENT ACCESS TO THE AREA. 4. DURING EACH INSPECTION, PERMITTEES MUST INSPECT SURFACE WATERS, INCLUDING DRAINAGE DITCHES AND
- CONVEYANCE SYSTEMS BUT NOT CURB AND GUTTER SYSTEMS, FOR EVIDENCE OF EROSION AND SEDIMENT DEPOSITION. PERMITTEES MUST REMOVE ALL DELTAS AND SEDIMENT DEPOSITED IN SURFACE WATERS, INCLUDING DRAINAGE WAYS, CATCH BASINS, AND OTHER DRAINAGE SYSTEMS AND RESTABILIZE THE AREAS WHERE SEDIMENT REMOVAL RESULTS IN EXPOSED SOIL. PERMITTEES MUST COMPLETE REMOVAL AND STABILIZATION WITHIN SEVEN (7) CALENDAR DAYS OF DISCOVERY UNLESS PRECLUDED BY LEGAL, REGULATORY, OR PHYSICAL ACCESS CONSTRAINTS, PERMITTEES MUST USE ALL REASONABLE EFFORTS TO OBTAIN ACCESS. IF PRECLUDED, REMOVAL AND STABILIZATION MUST TAKE PLACE WITHIN SEVEN (7) DAYS OF OBTAINING ACCESS. PERMITTEES ARE RESPONSIBLE FOR CONTACTING ALL LOCAL, REGIONAL, STATE AND FEDERAL AUTHORITIES AND RECEIVING ANY APPLICABLE PERMITS, PRIOR TO CONDUCTING ANY WORK IN SURFACE
- 5. PERMITTEES MUST INSPECT CONSTRUCTION SITE VEHICLE EXIT LOCATIONS, STREETS AND CURB AND GUTTER SYSTEMS WITHIN AND ADJACENT TO THE PROJECT FOR SEDIMENTATION FROM EROSION OR TRACKED SEDIMENT FROM VEHICLES. PERMITTEES MUST REMOVE SEDIMENT FROM ALL PAVED SURFACES WITHIN ONE (1) CALENDAR DAY OF DISCOVERY OR, IF APPLICABLE, WITHIN A SHORTER TIME TO AVOID A SAFETY HAZARD TO USERS OF PUBLIC STREETS. 6. PERMITTEES MUST REPAIR, REPLACE OR SUPPLEMENT ALL PERIMETER CONTROL DEVICES WHEN THEY BECOME
- NONFUNCTIONAL OR THE SEDIMENT REACHES 1/2 OF THE HEIGHT OF THE DEVICE. 7. PERMITTEES MUST DRAIN TEMPORARY AND PERMANENT SEDIMENTATION BASINS AND REMOVE THE SEDIMENT WHEN THE
- DEPTH OF SEDIMENT COLLECTED IN THE BASIN REACHES 1/2 THE STORAGE VOLUME. 8. PERMITTEES MUST ENSURE THAT AT LEAST ONE INDIVIDUAL PRESENT ON THE SITE (OR AVAILABLE TO THE PROJECT SITE IN THREE (3) CALENDAR DAYS) IS TRAINED IN THE JOB DUTIES DESCRIBED IN ITEM 21.2.B.
- 9. PERMITTEES MAY ADJUST THE INSPECTION SCHEDULE DESCRIBED IN ITEM 11,2 AS FOLLOWS: a. INSPECTIONS OF AREAS WITH PERMANENT COVER CAN BE REDUCED TO ONCE PER MONTH, EVEN IF CONSTRUCTION
- ACTIVITY CONTINUES ON OTHER PORTIONS OF THE SITE; OR b. WHERE SITES HAVE PERMANENT COVER ON ALL EXPOSED SOIL AND NO CONSTRUCTION ACTIVITY IS OCCURRING ANYWHERE ON THE SITE, INSPECTIONS CAN BE REDUCED TO ONCE PER MONTH AND, AFTER 12 MONTHS, MAY BE SUSPENDED COMPLETELY UNTIL CONSTRUCTION ACTIVITY RESUMES. THE MPCA MAY REQUIRE INSPECTIONS TO RESUME IF CONDITIONS WARRANT; OR

- c. WHERE CONSTRUCTION ACTIVITY HAS BEEN SUSPENDED DUE TO FROZEN GROUND CONDITIONS, INSPECTIONS MAY BE SUSPENDED. INSPECTIONS MUST RESUME WITHIN 24 HOURS OF RUNOFF OCCURRING, OR UPON RESUMING CONSTRUCTION, WHICHEVER COMES FIRST.
- 10. PERMITTEES MUST RECORD ALL INSPECTIONS AND MAINTENANCE ACTIVITIES WITHIN 24 HOURS OF BEING CONDUCTED AND THESE RECORDS MUST BE RETAINED WITH THE SWPPP. THESE RECORDS MUST INCLUDE:
- a. DATE AND TIME OF INSPECTIONS; AND b. NAME OF PERSONS CONDUCTING INSPECTIONS; AND
- c. ACCURATE FINDINGS OF INSPECTIONS, INCLUDING THE SPECIFIC LOCATION WHERE CORRECTIVE ACTIONS ARE NEEDED; AND
- d. CORRECTIVE ACTIONS TAKEN (INCLUDING DATES, TIMES, AND PARTY COMPLETING MAINTENANCE ACTIVITIES); AND e. DATE OF ALL RAINFALL EVENTS GREATER THAN 1/2 INCHES IN 24 HOURS, AND THE AMOUNT OF RAINFALL FOR EACH EVENT. PERMITTEES MUST OBTAIN RAINFALL AMOUNTS BY EITHER A PROPERLY MAINTAINED RAIN GAUGE INSTALLED ONSITE, A WEATHER STATION THAT IS WITHIN ONE (1) MILE OF YOUR LOCATION, OR A WEATHER REPORTING SYSTEM
- THAT PROVIDES SITE SPECIFIC RAINFALL DATA FROM RADAR SUMMARIES; AND f. IF PERMITTEES OBSERVE A DISCHARGE DURING THE INSPECTION, THEY MUST RECORD AND SHOULD PHOTOGRAPH AND DESCRIBE THE LOCATION OF THE DISCHARGE (I.E., COLOR, ODOR, SETTLED OR SUSPENDED SOLIDS, OIL SHEEN,
- AND OTHER OBVIOUS INDICATORS OF POLLUTANTS); AND g. ANY AMENDMENTS TO THE SWPPP PROPOSED AS A RESULT OF THE INSPECTION MUST BE DOCUMENTED AS REQUIRED IN SECTION 6 WITHIN SEVEN (7) CALENDAR DAYS.

POLLUTION PREVENTION MANAGEMENT (SECTION 12):

- 1. PERMITTEES MUST PLACE BUILDING PRODUCTS AND LANDSCAPE MATERIALS UNDER COVER (E.G., PLASTIC SHEETING OR TEMPORARY ROOFS) OR PROTECT THEM BY SIMILARLY EFFECTIVE MEANS DESIGNED TO MINIMIZE CONTACT WITH STORMWATER. PERMITTEES ARE NOT REQUIRED TO COVER OR PROTECT PRODUCTS WHICH ARE EITHER NOT A SOURCE OF CONTAMINATION TO STORMWATER OR ARE DESIGNED TO BE EXPOSED TO STORMWATER.
- 2. PERMITTEES MUST PLACE PESTICIDES. FERTILIZERS AND TREATMENT CHEMICALS UNDER COVER (E.G., PLASTIC SHEETING OR TEMPORARY ROOFS) OR PROTECT THEM BY SIMILARLY EFFECTIVE MEANS DESIGNED TO MINIMIZE CONTACT WITH STORMWATER.
- 3. PERMITTEES MUST STORE HAZARDOUS MATERIALS AND TOXIC WASTE, (INCLUDING OIL, DIESEL FUEL, GASOLINE, HYDRAULIC FLUIDS, PAINT SOLVENTS, PETROLEUM-BASED PRODUCTS, WOOD PRESERVATIVES, ADDITIVES, CURING COMPOUNDS, AND ACIDS) IN SEALED CONTAINERS TO PREVENT SPILLS, LEAKS OR OTHER DISCHARGE. STORAGE AND DISPOSAL OF HAZARDOUS WASTE MATERIALS MUST BE IN COMPLIANCE WITH MINN. R. CH. 7045 INCLUDING SECONDARY CONTAINMENT AS APPLICABLE.
- 4. PERMITTEES MUST PROPERLY STORE, COLLECT AND DISPOSE SOLID WASTE IN COMPLIANCE WITH MINN. R. CH. 7035 5. PERMITTEES MUST POSITION PORTABLE TOILETS SO THEY ARE SECURE AND WILL NOT TIP OR BE KNOCKED OVER. PERMITTEES MUST PROPERLY DISPOSE SANITARY WASTE IN ACCORDANCE WITH MINN. R. CH. 7041.
- 6. PERMITTEES MUST TAKE REASONABLE STEPS TO PREVENT THE DISCHARGE OF SPILLED OR LEAKED CHEMICALS. INCLUDING FUEL, FROM ANY AREA WHERE CHEMICALS OR FUEL WILL BE LOADED OR UNLOADED INCLUDING THE USE OF DRIP PANS OR ABSORBENTS UNLESS INFEASIBLE. PERMITTEES MUST ENSURE ADEQUATE SUPPLIES ARE AVAILABLE AT ALL TIMES TO CLEAN UP DISCHARGED MATERIALS AND THAT AN APPROPRIATE DISPOSAL METHOD IS AVAILABLE FOR RECOVERED SPILLED MATERIALS. PERMITTEES MUST REPORT AND CLEAN UP SPILLS IMMEDIATELY AS REQUIRED BY MINN. STAT. 115.061, USING DRY CLEAN UP MEASURES WHERE POSSIBLE.
- 7. PERMITTEES MUST LIMIT VEHICLE EXTERIOR WASHING AND EQUIPMENT TO A DEFINED AREA OF THE SITE. PERMITTEES MUST CONTAIN RUNOFF FROM THE WASHING AREA IN A SEDIMENT BASIN OR OTHER SIMILARLY EFFECTIVE CONTROLS AND MUST DISPOSE WASTE FROM THE WASHING ACTIVITY PROPERLY. PERMITTEES MUST PROPERLY USE AND STORE SOAPS, DETERGENTS, OR SOLVENTS.
- 8. PERMITTEES MUST PROVIDE EFFECTIVE CONTAINMENT FOR ALL LIQUID AND SOLID WASTES GENERATED BY WASHOUT OPERATIONS (E.G., CONCRETE, STUCCO, PAINT, FORM RELEASE OILS, CURING COMPOUNDS AND OTHER CONSTRUCTION MATERIALS) RELATED TO THE CONSTRUCTION ACTIVITY. PERMITTEES MUST PREVENT LIQUID AND SOLID WASHOUT WASTES FROM CONTACTING THE GROUND AND MUST DESIGN THE CONTAINMENT SO IT DOES NOT RESULT IN RUNOFF FROM THE WASHOUT OPERATIONS OR AREAS. PERMITTEES MUST PROPERLY DISPOSE LIQUID AND SOLID WASTES IN COMPLIANCE WITH MPCA RULES. PERMITTEES MUST INSTALL A SIGN INDICATING THE LOCATION OF THE WASHOUT

PERMIT TERMINATION (SECTION 4 AND SECTION 13):

- 1. PERMITTEES MUST SUBMIT A NOT WITHIN 30 DAYS AFTER ALL TERMINATION CONDITIONS LISTED IN SECTION 13 ARE COMPLETE
- 2. PERMITTEES MUST SUBMIT A NOT WITHIN 30 DAYS AFTER SELLING OR OTHERWISE LEGALLY TRANSFERRING THE ENTIRE SITE, INCLUDING PERMIT RESPONSIBILITY FOR ROADS (E.G., STREET SWEEPING) AND STORMWATER INFRASTRUCTURE FINAL CLEAN OUT, OR TRANSFERRING PORTIONS OF A SITE TO ANOTHER PARTY. THE PERMITTEES' COVERAGE UNDER THIS PERMIT TERMINATES AT MIDNIGHT ON THE SUBMISSION DATE OF THE NOT.
- 3. PERMITTEES MUST COMPLETE ALL CONSTRUCTION ACTIVITY AND MUST INSTALL PERMANENT COVER OVER ALL AREAS PRIOR TO SUBMITTING THE NOT. VEGETATIVE COVER MUST CONSIST OF A UNIFORM PERENNIAL VEGETATION WITH A DENSITY OF 70 PERCENT OF ITS EXPECTED FINAL GROWTH. VEGETATION IS NOT REQUIRED WHERE THE FUNCTION OF A SPECIFIC AREA DICTATES NO VEGETATION, SUCH AS IMPERVIOUS SURFACES OR THE BASE OF A SAND FILTER.
- 4. PERMITTEES MUST CLEAN THE PERMANENT STORMWATER TREATMENT SYSTEM OF ANY ACCUMULATED SEDIMENT AND MUST ENSURE THE SYSTEM MEETS ALL APPLICABLE REQUIREMENTS IN SECTION 15 THROUGH 19 AND IS OPERATING AS

5. PERMITTEES MUST REMOVE ALL SEDIMENT FROM CONVEYANCE SYSTEMS PRIOR TO SUBMITTING THE NOT.

- 6. PERMITTEES MUST REMOVE ALL TEMPORARY SYNTHETIC EROSION PREVENTION AND SEDIMENT CONTROL BMPS PRIOR TO SUBMITTING THE NOT. PERMITTEES MAY LEAVE BMPS DESIGNED TO DECOMPOSE ON-SITE IN PLACE 7. FOR RESIDENTIAL CONSTRUCTION ONLY, PERMIT COVERAGE TERMINATES ON INDIVIDUAL LOTS IF THE STRUCTURES ARE FINISHED AND TEMPORARY EROSION PREVENTION AND DOWNGRADIENT PERIMETER CONTROL IS COMPLETE. THE
- 8. FOR CONSTRUCTION PROJECTS ON AGRICULTURAL LAND (E.G., PIPELINES ACROSS CROPLAND), PERMITTEES MUST RETURN THE DISTURBED LAND TO ITS PRECONSTRUCTION AGRICULTURAL USE PRIOR TO SUBMITTING THE NOT.

RESIDENCE SELLS TO THE HOMEOWNER, AND THE PERMITTEE DISTRIBUTES THE MPCA'S "HOMEOWNER FACT SHEET" TO

SEED NOTES:

ALL SEED MIXES AND APPLICATION SHALL BE IN ACCORDANCE WITH THE MNDOT SEEDING MANUAL.

GENERAL RECOMMENDATIONS:

THE CONTRACTOR IS RESPONSIBLE TO SALVAGE AND PRESERVE EXISTING TOPSOIL NECESSARY FOR FINAL STABILIZATION AND TO ALSO MINIMIZE COMPACTION IN ALL LANDSCAPE AREAS. IMMEDIATELY BEFORE SEEDING THE SOIL SHALL BE TILLED TO

TEMPORARY EROSION CONTROL SEEDING, MULCHING & BLANKET.

TEMPORARY SEED SHALL BE MNDOT SEED MIX 21-112 (WINTER WHEAT COVER CROP) FOR WINTER AND 21-111 (OATS COVER CROP) FOR SPRING/SUMMER APPLICATIONS. BOTH SEED MIXES SHALL BE APPLIED AT A SEEDING RATE OF 100 LBS/ACRE.

 IMMEDIATELY AFTER SEEDING, WITHIN 24 HOURS, MNDOT TYPE 1 MULCH SHOULD BE APPLIED TO PROTECT AND ENHANCE SEED GERMINATION. MULCH SHALL BE APPLIED AT 90% COVERAGE (2 TONS PER ACRE OF STRAW MULCH)

- 3:1 (HORIZ/VERT.) OR FLATTER MUCH SHALL BE COVERED WITH MULCH
- SLOPES STEEPER THAN 3:1 OR DITCH BOTTOMS SHALL BE COVERED WITH EROSION CONTROL BLANKET.
- SEE PLAN FOR MORE DETAILED DITCH AND STEEP SLOPE EROSION CONTROL TREATMENTS.

TRAINING SECTION 21

DESIGN ENGINEER: MATTHEW R. PAVEK P.E. TRAINING COURSE: DESIGN OF SWPPP TRAINING ENTITY: UNIVERSITY OF MINNESOTA INSTRUCTOR: JOHN CHAPMAN

DATES OF TRAINING COURSE: 5/15/2011 - 5/16/2011 **TOTAL TRAINING HOURS: 12**

RE-CERTIFICATION: 2/27/2020 (8 HOURS), EXP. 5/31/2023

ABDO MARKETPLACE, LLC MINNEAPOLIS, MN MICHAEL MARGULIES 612.205.0521

OWNER INFORMATION

CONTACT:

AREAS AND QUANTITIES

SITE AREA CALCULATIONS						
	EXISTING CONDITION		PROPOSED CONDITION			
BUILDING COVERAGE	2,269	SF	7.4%	16,943	SF	55.4%
ALL PAVEMENTS	10,798	SF	35.3%	3,889	SF	12.7%
ALL NON-PAVEMENTS	17,500	SF	57.3%	9,735	SF	31.8%
TOTAL SITE AREA	30,567	SF	100.0%	30,567	SF	100.0%
IMPERVIOUS SURFACE						
EXISTING CONDITION	13,067	SF	42.7%			
PROPOSED CONDITION	20,832	SF	68.2%			
DIFFERENCE (EX. VS PROP.)	7,765	SF	25.4%			
EROSION CONTROL QUANTITIES						
DISTURBED AREA	30,567	SF	0.70			
SILT FENCE/BIO-ROLL	850	LF				
EROSION CONTROL BLANKET	0	SF				
INLET PROTECTION DEVICES	1	EΑ				

NOTE: QUANTITIES ARE FOR INFORMATIONAL PURPOSES ONLY. CONTRACTOR SHALL DETERMINE FOR THEMSELVES THE EXACT QUANTITIES FOR BIDDING AND CONSTRUCTION.

SWPPP CONTACT PERSON

CONTRACTOR:

SWPPP INSPECTOR TRAINING: ALL SWPPP INSPECTIONS MUST BE PERFORMED BY A PERSON THAT MEETS THE TRAINING REQUIREMENTS OF THE NPDES CONSTRUCTION SITE PERMIT. TRAINING CREDENTIALS SHALL BE PROVIDED BY THE CONTRACTOR AND KEPT ON SITE WITH THE SWPPP

PARTY RESPONSIBLE FOR LONG TERM OPERATION AND MAINTENANCE OF PERMANENT STORM WATER MANAGEMENT SYSTEM

PERMANENT STORMWATER MANAGEMENT IS NOT REQUIRED AS PART OF THIS PROJECT TO MEET NPDES PERMIT REQUIREMENTS. THE PROPERTY OWNER IS RESPONSIBLE FOR THE LONG TERM OPERATION AND MAINTENANCE OF THE PROPOSED STORMWATER SYSTEM.

CONTRACTOR SHALL OBTAIN A COPY OF THE FOLLOWING SWPPP ATTACHMENTS WHICH ARE A PART OF THE OVERALL SWPPP PACKAGE:

SWPPP ATTACHMENTS (ONLY APPLICABLE IF SITE IS 1 ACRE OR GREATER):

ATTACHMENT A. CONSTRUCTION SWPPP TEMPLATE - SITE SPECIFIC SWPPP DOCUMENT ATTACHMENT B. CONSTRUCTION STORMWATER INSPECTION CHECKLIST

ATTACHMENT C. MAINTENANCE PLAN FOR PERMANENT STORM WATER TREATMENT SYSTEMS

ATTACHMENT D: STORMWATER MANAGEMENT REPORT - ON FILE AT THE OFFICE OF PROJECT ENGINEER. AVAILABLE UPON REQUEST. ATTACHMENT E: GEOTECHNICAL EVALUATION REPORT - ON FILE AT THE OFFICE OF PROJECT ENGINEER. AVAILABLE UPON REQUEST.

SUPPLEMENTARY SITE SPECIFIC EROSION CONTROL NOTES: THESE NOTES SUPERCEDE ANY GENERAL SWPPP NOTES.

THIS PROJECT IS LESS THAN 1.0 ACRES SO AN NPDES PERMIT IS NOT REQUIRED AND NEEDS TO BE SUBMITTED TO THE MPCA. PERMANENT STORMWATER IS REQUIRED PER CITY OF EDINA AND NINE MILE CREEK WATERSHED DISTRICT RULES.

PROJECT NARRATIVE:

BE CONSTRUCTED, ALONG WITH PARKING, GRADING, UTILITY AND LANDSCAPE IMPROVEMENTS. NATIVE BUFFER NARRATIVE:

PROJECT IS A REDEVELOPMENT OF AN EXISTING DAY CARE SITE. THE DAY CARE BUILDING WILL BE DEMOLISHED. A HOUSING DEVELOPMENT WILL

PRESERVING A 50' NATURAL BUFFER AROUND WATER BODIES IS NOT REQUIRED AS PART OF THIS PROJECT BECAUSE WATER BODIES ARE NOT

INFILTRATION NARRATIVE:

LOCATED ON SITE.

INFILTRATION IS REQUIRED AT THIS SITE PER CITY AND WATERSHED RULES.

SOIL CONTAMINATION NARRATIVE:

SOILS ONSITE HAVE NOT BEEN IDENTIFIED AS CONTAMINATED. AN MPCA SOILS ASSESSMENT HAS NOT BEEN COMPLETED.

SPECIAL TMDL BMP REQUIREMENTS SITE SPECIFIC (IF REQUIRED):

THIS PROJECT IS WITHIN ONE MILE AND DISCHARGES TO LAKE CORNELIA WHICH IS LISTED ON THE MPCA'S 303(D) IMPAIRED WATERS LIST FOR THE FOLLOWING IMPAIRMENTS: NUTRIENTS. BECAUSE THESE WATERS ARE LOCATED WITHIN ONE MILE OF THE SITE, BMPS AS DEFINED IN THE NPDES PERMIT ITEMS 23.9 AND 23.10 APPLY. THESE ARE AS FOLLOWS:

DURING CONSTRUCTION:

A. STABILIZATION OF ALL EXPOSED SOIL AREAS MUST BE INITIATED IMMEDIATELY TO LIMIT SOIL EROSION BUT IN NO CASE COMPLETED LATER THAN SEVEN (7) DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY B. TEMPORARY SEDIMENT BASIN REQUIREMENTS DESCRIBED IN SECTION 14. MUST BE USED FOR COMMON DRAINAGE LOCATIONS THAT SERVE AN AREA WITH FIVE (5) OR MORE ACRES DISTURBED AT ONE TIME.

PERMANENT STABILIZATION NOTES SITE SPECIFIC:

- FOR THIS PROJECT ALL AREAS THAT ARE NOT TO BE SODDED OR LANDSCAPED SHALL RECEIVE A NATIVE PERMANENT SEED MIX. AREAS IN BUFFERS AND ADJACENT TO OR IN WET AREAS MNDOT SEED MIX 33-261 (STORMWATER SOUTH AND WEST) AT 35 LBS PER
- •• DRY AREAS MNDOT SEED MIX 35-221 (DRY PRAIRIE GENERAL) AT 40 LBS PER ACRE. MAINTENANCE SHALL BE IN ACCORDANCE TO THE MNDOT SEEDING MANUAL.

4931 W. 35th Street, Suite 200 St. Louis Park, MN 55416

vilsitegroup.com

612-615-0060

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Mark

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SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DUI Y LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF

DATE XX/XX/19 LICENSE NO. 44263

DATE DESCRIPTION 20.06.05 CITY SUBMITTAL

ISSUE/SUBMITTAL SUMMARY

DATE DESCRIPTION

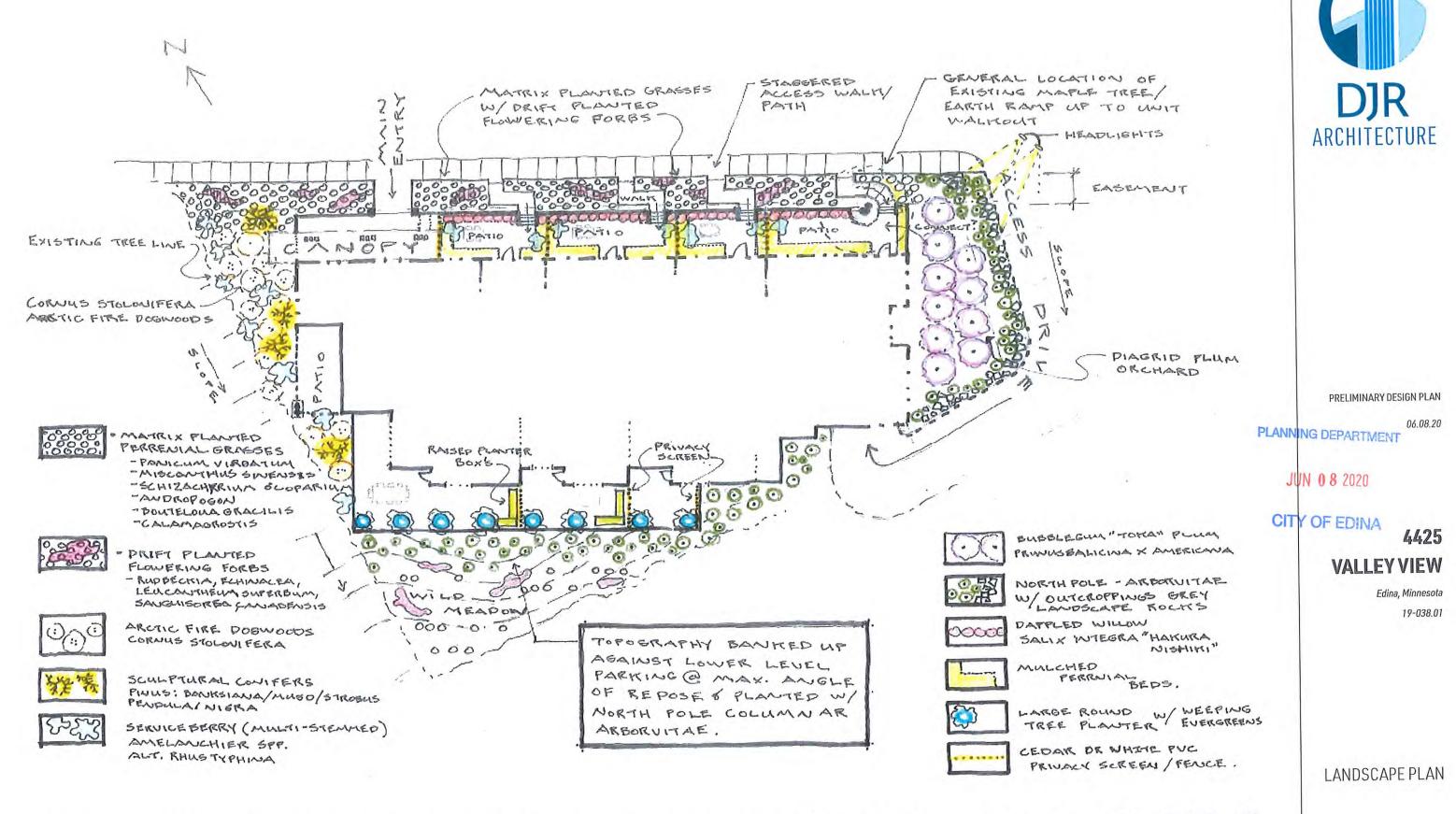
REVISION SUMMARY

REVIEWED BY: XX

SWPPP - NARRATIVE

DRAWN BY:kit

PROJECT NUMBER: 19261



4425 VALLEY VIEW ROAD - LANDSCAPE CONCEPT





06.08.20

4425 VALLEY VIEW

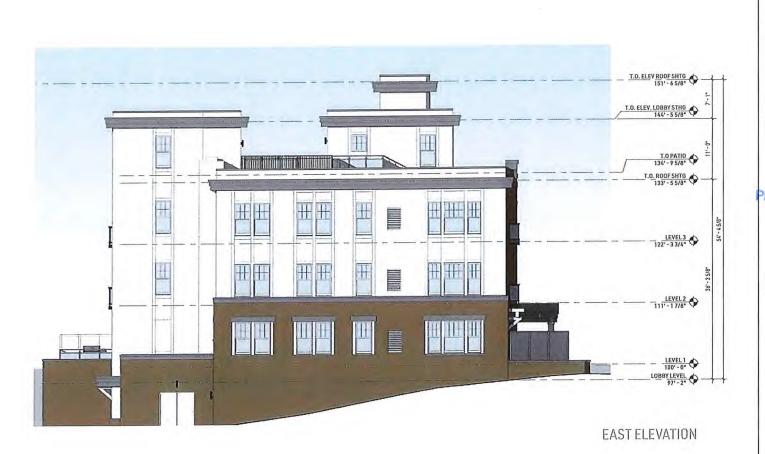
Edina, Minnesota

19-038.01 PLANNING DEPARTMENT

JUN 08 2020

CITY OF EDINA

EXTERIOR ELEVATIONS







06.08.20

4425 VALLEY VIEW

Edina, Minnesota

PLANNING DEPARTMENT

19-038.01

JUN 08 2020

CITY OF EDINA

EXTERIOR ELEVATIONS

EAST ELEVATION





06.08.20

4425 VALLEY VIEW

Edina, Minnesota 19-038.01

PLANNING DEPARTMENT

JUN 08 2020

CITY OF EDINA

RENDER

NORTHWEST RENDER





06.08.20

4425 VALLEY VIEW

Edina, Minnesota 19-038.01

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CITY OF EDINA

RENDER

NORTHEAST RENDER





06.08.20

4425 VALLEY VIEW

> Edina, Minnesota 19-038.01

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JUN 08 2020

CITY OF EDINA

RENDER

SOUTHWEST RENDER





06.08.20

4425 VALLEY VIEW

> Edina, Minnesota 19-038.01

PLANNING DEPARTMENT

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MATERIALS

Addendum to Variance Application

Applicant seeks four variances:

- 1. A variance reducing the front yard setback from 36 feet to 16 feet;
- 2. A variance reducing the rear yard setback from 36 feet to 20 feet;
- 3. A variance increasing the allowed building height from two stories and 24 feet to three stories and 36' 3 and 5/8"; and PLANNING DEPARTMENT
- 4. A variance increasing the floor area ration (FAR) from 1.0 to 1.08

JUN 08 2020

Variance 1—Front Yard Setback Variance

CITY OF EDINA

Applicant requests a variance to reduce the front setback from 36 feet to 16 feet. The 36 foot setback in this PCD-1 District is calculated using the three story height of the proposed building.

The proposed variance will relieve practical difficulties in complying with the zoning ordinance and that the use is reasonable.

The lot is a relatively narrow lot, with pond in the rear and significant trees. The narrow configuration of the lot creates a challenge for developing the site while at the same time minimally disturbing these features. The planned building contains 21 units and places all parking under the building, a high priority in the Wooddale Valley View Small Area Plan. Placing the building nearer to Valley View Road allows for the maximum opportunity to preserve existing trees and to avoid disturbing the pond and wetland area, as well as placing virtually all parking serving the building underground. Placing the building closer to Valley View Road is also an advantage to the neighborhood as closer location to the street helps integrate the building and its uses with the street and the enhance the pedestrian experience.

The proposed variance will correct extraordinary circumstances applicable to this property but not applicable to other property in the vicinity or zoning district

The conditions of this site are unique to this lot and were not created by the owner. The lot is a relatively narrow lot, with pond in the rear and significant trees. The narrow configuration of the lot creates a challenge for developing the site while at the same time minimally disturbing these features while allowing for the parking serving the building to be off-surface.

The proposed variance will be in harmony with the general purposes and intent of the zoning ordinance

Yard controls are established to provide for the orderly development and use of land and to minimize conflicts among land uses by regulating the dimension and use of yards in order to provide adequate light, air, open space and separation of uses. The proposed reduction in the

minimum front yard setback is reasonable as the proposed setback will be compatible with surrounding development and will not create conflicts with surrounding properties or uses. The placement of the building will not interfere with access to adequate light, air and open space for surrounding uses. The proposed setback serves the intent of the ordinance and the City's land use policies, specifically the City's 2040 Plan and the Wooddale Valley View Small Area Plan. In particular, the location of the building conforms with the guiding principles of the Small Area Plan which prioritizes "...ground-level frontages throughout the node are carefully designed with public-to-private relationships that encourage beauty, safety, informal interaction, walkability and a sense of place." Additionally, the Small Area Plan encourages the placement of buildings in a manner so as allow the buildings to "interact with the sidewalk and encourage pedestrian activity." Finally, the Small Area Plan prioritizes underground parking: "Residents' parking should be located under the buildings to the extent allowed by market conditions."

The proposed variance will not alter the essential character of the neighborhood

The requested variance will not alter the essential character of the neighborhood. The building setback is virtually the same as the setback of the building immediately to the west of the site at 6161 Wooddale and matches the setback of the property located 4545 Valley View Road. By appearances the proposed setback is approximately the same as the new condominium building across the street at 4416 Valley View Road. The placement of the building closer to Valley View Road maintains the essential character of the neighborhood by preserving existing trees and the pond and wetland area. Moreover, the placement of the building closer to Valley View Road conforms to the principles of the Small Area Plan, enhancing walkability, interaction with the sidewalk and encouraging pedestrian activity. The requested variance allows the essential character of the neighborhood to be maintained by facilitating the placement of all perfections.

JUN 08 2020

Variance 2—Rear Yard Setback Variance

CITY OF EDINA

Applicant requests a variance to reduce the rear setback from 36 feet to 20 feet. The 36 foot setback is calculated using the three story height of the proposed building.

The proposed variance will relieve practical difficulties in complying with the zoning ordinance and that the use is reasonable.

The lot is a relatively narrow lot, with pond in the rear and significant trees. The narrow configuration of the lot creates a challenge for developing the site while at the same time minimally disturbing these features. The planned building contains 21 units and places all parking under the building, a high priority in the Wooddale Valley View Small Area Plan Placing the building nearer to Valley View Road allows for the maximum opportunity to preserve existing trees and to avoid disturbing the pond and wetland area, as well as placing virtually all parking serving the building underground. The placement of the building allowed as the result of the variance will allow the property to be developed in accordance with the Wooddale Valley View Small Area Plan.

The proposed variance will Correct extraordinary circumstances applicable to this property but not applicable to other property in the vicinity or zoning district

The conditions of this site are unique to this lot and were not created by the owner. The lot is a relatively narrow lot, with pond in the rear and significant trees. The narrow configuration of the lot creates a challenge for developing the site while at the same time minimally disturbing these features.

The proposed variance will be in harmony with the general purposes and intent of the zoning ordinance

Yard controls are established to provide for the orderly development and use of land and to minimize conflicts among land uses by regulating the dimension and use of yards in order to provide adequate light, air, open space and separation of uses. The proposed reduction in the minimum rear yard setback is reasonable as the proposed setback will be compatible with surrounding development and will not create conflicts with surrounding properties or uses. The placement of the building does not interfere with access to adequate light, air and open space for surrounding uses. The proposed setback serves the intent of the ordinance and the City's land use policies, specifically the City's 2040 Plan and the Wooddale Valley View Small Area Plan. In particular, the location of the building conforms with the guiding principles of the Small Area Plan which prioritizes "...ground-level frontages throughout the node are carefully designed with public-to-private relationships that encourage beauty, safety, informal interaction, walkability and a sense of place." Additionally, the Small Area Plan encourages the placement of buildings in a manner so as allow the buildings to "interact with the sidewalk and encourage pedestrian activity.". The relatively minor variance to the rear yard setback is necessary to achieve the appropriate placement of the building. Finally, the Small Area Plan prioritizes underground parking: "Residents' parking should be located under the buildings to the extent allowed by market conditions.". This variance facilitate placing the residential parking underneath the building.

The proposed variance will not alter the essential character of the neighborhood

The requested variance will not alter the essential character of the neighborhood and will, in fact help assure that the character of the neighborhood is not altered. The variance allows the placement of the building in such a manner so as maximize the preservation of existing trees and the wetland pond area. Moreover, the placement of the building closer to Valley View Road will conform with the principles of the Small Area Plan, enhancing walkability, interaction with the sidewalk and encouraging pedestrian activity. The requested variances allows the essential character of the neighborhood to be maintained by facilitating the placement of all parking underground.

JUN 08 2020

Variance 3—Height Variance

CITY OF EDINA

Applicant requests a variance to increase the allowed building height in this PCD-1 District from 2 stories and 24 feet to 3 stories and 36' 3 and 5/8":.

The City's 2040 Comprehensive Plan and the Wooddale Valley View Small Area Plan guide the Property for a maximum height of four stories and 48 feet. It is anticipated the City Council will amend the current Zoning Code to adopt these new height standards.

The proposed variance will relieve practical difficulties in complying with the zoning ordinance and that the use is reasonable.

The lot is a relatively narrow lot, with pond in the rear and significant trees. The narrow configuration of the lot creates a challenge for developing the site while at the same time minimally disturbing these features. The planned building contains 21 units and places all parking under the building. The reduction of surface parking is a priority of the Wooddale Valley View Small Area Plan and the only practical way to achieve this goal is to allow the building to have three stories. The placement of the building allowed as the result of the variance will allow the property to be developed in accordance with the Wooddale Valley View Small Area Plan.

The proposed variance will correct extraordinary circumstances applicable to this property but not applicable to other property in the vicinity or zoning district

The conditions of this site are unique to this lot and were not created by the owner. The lot is a relatively narrow lot, with ponding and wetlands in the rear and significant trees. The narrow configuration of the lot creates a challenge for developing the site while at the same time minimally disturbing these features. The lot configuration does not allow for the placement of all resident parking underground without the requested height variance.

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The proposed variance will be in harmony with the general purposes and intent of the zoning ordinance

CITY OF EDINA

Height controls are established to provide for the orderly development and use of land and to minimize conflicts among land uses by regulating the building height in order to provide adequate light, air, open space and separation of uses. The proposed increase in the allowed building height is reasonable as the proposed building height will be compatible with surrounding development and will not create conflicts with surrounding properties or uses. The placement of the building will not interfere with access to adequate light, air and open space for surrounding uses. The proposed height variance is specifically called out as a desired goal for this site pursuant to the City's 2040 Comprehensive Plan and the Wooddale Valley View Small Area Plan. The Small Area Plan (p. 35) identifies this site as within the "HOD-4" district, specifically providing for buildings up to four stories in height. At three stories, the building fully adheres to the height principles set forth in the Small Area Plan

The height variance also allows the building to be located in conformity with the guiding principles of the Small Area Pan, which prioritizes "...ground-level frontages throughout the node are carefully designed with public-to-private relationships that encourage beauty, safety, informal interaction, walkability and a sense of place." Additionally, the Small Area Plan encourages the placement of buildings in a manner so as allow the buildings to "interact with the sidewalk and encourage pedestrian activity." The height variance allows this appropriate placement of the building.

The proposed variance will not alter the essential character of the neighborhood

The requested variance will not alter the essential character of the neighborhood. The variance allows the placement of the building in such a manner so as maximize the preservation

of existing trees and the wetland pond area. Moreover, the placement of the building closer to Valley View Road will conform with the principles of the Small Area Plan, enhancing walkability, interaction with the sidewalk and encouraging pedestrian activity and underground parking. The proposed building is consistent with the 1` the height of the buildings located at 4416 Valley View Road.

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Variance 4—Floor Area Ratio Variance

CITY OF EDINA

Applicant requests a variance in the required Floor Area Ratio from the 1.0 ration required in this PCD-1 District from 1.0 to 1.08. The area of the lot is 30,381 square feet and the total area of the building is no more than 33,333 square feet.

The proposed variance will relieve practical difficulties in complying with the zoning ordinance and that the use is reasonable.

As noted with respect to the other variance requests, the site lot is a relatively narrow lot, with pond in the rear and significant trees. The narrow configuration of the lot creates a challenge for developing the site while at the same time minimally disturbing these features. The planned building contains 21 units and places all parking under the building, a high priority in the Wooddale Valley View Small Area Plan. Placing the building nearer to Valley View Road allows for the maximum opportunity to preserve existing trees and not to disturb the pond and wetland area, as well as placing virtually all parking serving the building underground. Placing the building closer to the street is also an advantage to the neighborhood as closer location will help integrate the building and its uses with the street and the enhance the pedestrian experience. All of this is accomplished because of a building design that depends on a relative uniformity in the configuration of the units and how the units are stacked. The FAR variance will allow this uniformity to occur. Without the FAR variance the building could not achieve the overall configuration that provides the other benefits in terms of preservation of trees and pond areas and the underground parking.

The proposed variance will correct extraordinary circumstances applicable to this property but not applicable to other property in the vicinity or zoning district

The conditions of this site are unique to this lot and were not created by the owner. The lot is a relatively narrow lot, with pond in the rear and significant trees. The narrow configuration of the lot creates a challenge for developing the site while at the same time minimally disturbing these features while allowing for the parking serving the building to be off-surface. Without the benefit of the FAR variance, the applicant could not achieve 21 units with all of the parking located beneath the building.

The proposed variance will be in harmony with the general purposes and intent of the zoning ordinance

Floor area ratio standards are established to provide for the orderly development and use of land and to minimize conflicts among land uses by regulating the size of developments relative to the size of the lots on which the development occurs.. This is to ensure an appropriateness of size, dimension and use of yards in order to provide adequate light, air, open

space and separation of uses. The proposed increase in permitted FAR is nominal in amount and is reasonable as the resulting floor area is compatible with surrounding development and will not create conflicts with surrounding properties or uses. The floor area of the building will not interfere with access to adequate light, air and open space for surrounding uses.

The proposed variance will not alter the essential character of the neighborhood

The requested variance will not alter the essential character of the neighborhood. The amount of the FAR variance is nominal. Moreover, the minor FAR variance actually allows the building to be developed in an manner that ensures that that the essential character of the neighborhood will not be altered. The FAR variance allows for a uniformity of unit configuration and stacking. This, in turn permits placement of the building closer to Valley View Road, which maintains the essential character of the neighborhood by preserving existing trees and the pond and wetland area. As previously noted, the placement of the building closer to Valley View Road will conform with the principles of the Small Area Plan, enhancing walkability, interaction with the sidewalk and encouraging pedestrian activity. The requested variances allows the essential character of the neighborhood to be maintained by facilitating the placement of all parking underground.

PLANNING DEPARTMENT

JUN 08 2020

CITY OF EDINA

Addendum One to Application for Site Plan Approval

Explanation of Request

Applicant seeks Site Plan Approval pursuant to Section 32-122 et. seq. of the Edina Zoning Code to develop a 21 unit three-story apartment building at 4425 Valley View Road (the "Property"). The building consists of 33,333 square feet. The development will comply with City code-required parking, all of which will be located beneath the building.

The Property is zoned PCD-1. Pursuant to Section 36-612 of the Edina City Zoning Code, multifamily residential use is a conditional permitted use within PCD-1 districts pand NING DEPARTMENT applicant has applied for a Conditional Use Permit.

JUN 08 2020

Statement Supporting Site Plan Approval

CITY OF EDINA

The development is designed to conform to the goals and principles of the Wooddale Valley View Small Area Plan. It conforms to the Plan's designation of this site as "Neighborhood Node" and accomplishes the Plan's stated goal of a "graceful transition" within the neighborhood by incorporating a multifamily use. Although the Small Area Plan guides the development for four stories or 48 feet, the building is three stories and 36' 3 and 5/8". At 33,333 square feet its configuration and placement fit appropriately with the surrounding neighborhood building. The development adheres to the 30 unit per acre density standard.

The development requires four minor variances—one of which is a variance to allow the three story building as the present zoning limits height to two stories and the ordinance limiting this height is designated for amendment so the Zoning Code will confirm to the Comprehensive Plan. Two of the variances reduce the front and rea yard setbacks, but the end result of these variances will actually enhance the neighborhood. They will allow the building to be located closer to Valley View Road. This will have the result of promoting the goal of the Small Area Plan of creating a better pedestrian experience. It will also allow the maximum preservation of existing trees and the pond on the Property. These variances, together with the fourth variance increasing the FAR from 1.0 to 1.08, will also allow the building to be constructed with all of the required parking for the development placed underneath the building.

On May 13, 2020 a virtual neighborhood meeting was held after written notice was provided to all property owners within the required radius of the Property. All of the comments at the meeting were positive. Neighbor comments included an appreciation of the building design and location on the lot; approval of the 21 unit density and statements that the density, use and traffic would not have an adverse impact on the neighborhood. There were a number of comments approving the placement of the parking underneath the building and approval that the development conforms to the goals and principles of the Small Area Plan.

Owner's Development Experience

The owner has significant development experience. AMH Valley View LLC is a part of the Abdo family of companies based in Minneapolis, Minnesota. The Abdo organization has

developed over 4,000,000 square feet of commercial, retail and residential space in 11 states and 20 cities throughout the United States. The combined development value of these projects exceeds \$1.5B. Their residential development experience includes luxury condominiums in Downtown Minneapolis, a studio apartment development in downtown Minneapolis and 4 assisted living facilities in Minnesota and Wisconsin.

Additionally, other representative development projects of the owner include:

- 20 Family Dollar Stores
- 19 Dollar General Stores
- 4 Starbuck's Coffee Stores
- 5 Caribou Coffee Stores
- 15 O'Reilly Auto Stores
- 120 Walgreen's Drug Stores
- 11 Strip Mall/Shopping Centers
- 2 Foss Swim Schools
- 7 My Burger Restaurants
- 2 White Castles

PLANNING DEPARTMENT

JUN 08 2020

CITY OF EDINA

Addendum One to Conditional use Permit Application

PLANNING DEPARTMENT

JUN 08 2020

Explanation of Request

CITY OF EDINA

Applicant seeks a Conditional Use Permit to develop, construct and operate a 21 unit three-story apartment building at 4425 Valley View Road (the "Property"). The building consists of 33,333 square feet. The development will comply with City code-required parking, all of which will be located beneath the building.

The Property is zoned PCD-1. Pursuant to Section 36-612 of the Edina City Zoning Code, multifamily residential use is a conditional permitted use within PCD-1 districts.

Statement Supporting the Approval of the Conditional Use Permit

Section 36-305 of the Edina Zoning Code requires the following findings to support the approval of a request for a Conditional Use Permit. In this instance all the required findings are present.

- 1. The development does not have an adverse impact on governmental facilities, utilities or existing or proposed improvements. The 21 unit building will not increase or add to any burden on governmental facilities, utilities, services or existing or proposed improvements. No significant changes in City-installed infrastructure or services are required.
- 2. The traffic generated by the development is within the capacity of the streets servicing the Property. Recent traffic studies covering this neighborhood indicate the development should have no material adverse impact on traffic or neighborhood streets. The City has engaged at the applicant's expense a traffic study to assess the impact of the development on traffic and street capacity. The report will show that the traffic generated by the development is within the capacity of the streets serving the Property.
- 3. The development will not have an undue avers impact on public health, safety or welfare. The development will remove an aging and blighted building, replacing it with a graceful and well-designed market rate housing development. The development will service an unmet housing need for Edina citizens and will comply and advance the goals and principles of the City's Inclusionary Housing Policy.
- 4. The development will not impede the normal and orderly development and improvement of other property in the vicinity. The development will not impede the normal and orderly development and improvement of other property in the vicinity. In fact, as discussed below, because the development adheres and advances the goals and principles of the Wooddale Valley View Small Area Plan, the development enhances and encourages the development and improvement of other property in the neighborhood.
- 5. The development conforms to the applicable restrictions and specific conditions of the district in which it is located. The development conforms the requirements of a PCD-1

Site Location



1 in = 376 ft

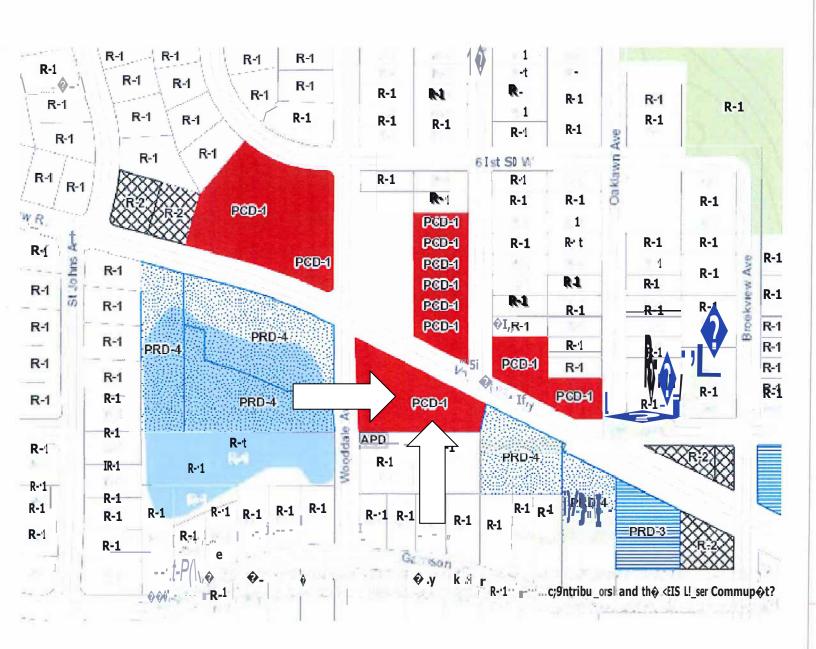


Site Location



1 in = 94 ft





4.1 Current Conditions

The 2014 pattern of land use in the Study Area is summarized by Figure 4.1 and the related table. A detailed description of land use, buildings, major tenants and property owners is presented in the *Project Archive* at EdinaMN.gov/WVV.



	Building		
	Type	Year Built	Current Land Use (at end of 2014)
1		1954	Retail and service – former gasoline service station; presently unused
2		1956	Mixed use: small offices over clothing retailer and ice cream shop
3		1957	Service business - dry cleaning
4		1957	Retail and service - strip mall (Edina Village Market (EVM))
5		1959	Retail and service - addition to strip mall (EVM)
6		1959	Service business - salon
7		1960	Residential – apartment building with underground parking
8		1960*	Small offices (ValleyWood)
9		1962	Residential – apartment building with underground parking
10		1963	Residential – apartment building with underground parking
11		1969	Residential – apartment building with underground parking
12		1969	Office – professional service business (*recently updated)
13		1970	Service business – child day care
14	no bldg		Undeveloped site – owned by the City



- = Two Story Vertical Mixed Commercial Office over Retail / Services
- = One Story Commercial
- = Multi-family Residential Duplex / Apartments / Flats

Figure 4.1

Pattern of Land Existing Properties: Building Types, Year Built and Current Land Uses in 2014

4.3 Goals and Policies

The following is a policy framework that provides guidance to the public sector for future decision making, staff review of development applications, municipal plan implementation and community based initiatives. The following takes into account the opinions and values of stakeholders, the parameters of previous planning and existing policies.

The Study Area will continue to evolve and become a mixture of housing and commercial development guided by the demands of the market, property owners' decisions, the policies and guidelines of this plan, and the *Edina Comprehensive Plan*. Though the proportion of the Study Area devoted to housing will likely be greater than it was in 2014, the exact pattern of land use over time will be determined by market forces. Current uses of land may, of course, remain as they are until owners decide to make a change.

Land Use and Community Design Goals

- 1. Activation of the Core. Concentrate activity and neighborhood-serving commercial businesses near the intersection of Wooddale Avenue and Valley View Road. Any new buildings introduced at the four corners of the Wooddale Valley View intersection should include street-level retail or gathering spaces that interact with the sidewalk and encourage pedestrian activity.
- 2. Building-to-Street Relationships. Ensure that ground-level frontages throughout the node are carefully designed with public-to-private relationships that encourage beauty, safety, informal interaction, walkability and a sense of place.
- 3. Graceful Transitions. Encourage the scale of buildings to transition from center to edge of the Neighborhood Node, with the largest buildings located near the intersection of Wooddale Avenue and Valley View Road, decreasing in scale toward the surrounding single-family neighborhood. Ensure that redevelopment near single family homes is designed sensitively.
- 4. Flexible Evolution of Land Uses. Allow existing land uses to evolve in response to the market. Current uses of land may, of course, remain as they are until owners decide to make a change: The commercial site north of Valley View Road and Oaklawn Avenue may evolve to housing. The Edina Village Market may redevelop as a mix of housing and commercial. The sites east of Kellogg Avenue immediately north of Valley View Road may evolve as either commercial or residential.
- 5. **Drive-Through Formats.** Discourage buildings with drive-through elements such as gasoline stations and those with drive-up windows. They are inconsistent with the guiding principles of this plan.

Chapter 4 - Land Use and Community Design





Four Story Limit.

Up to four stories, not to exceed 48 feet



Three Story Limit.

Up to three stories, not to exceed 36 feet



Two Story Limit.

Up to two stories, not to exceed 24 feet



Two Story Transition (36' Deep)*

Height Limit: Up to two stories, not to exceed 24 feet (vertically) within the specified 36 feet in horizontal distance from property line.

Two Story Transition (60' Deep)*
Height limit: Up to two stories, not to exceed 24 feet (vertically) within the specified 60 feet in horizontal distance from property line.

* (These 'Transition' designations were created to establish a graceful transition from Neighborhood Node properties with three story limits to immediately adjacent residential areas outside the node.)

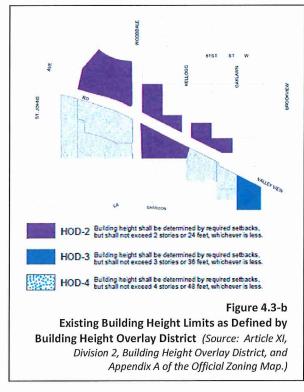


Figure 4.3 Building Height Limits Plan



E. General Guidelines for the Entire Neighborhood Node

On-Site Parking and Site Access

- Encourage Underground Parking. Residents' parking should be located under the buildings to the extent allowed by market conditions.
- Visually Buffer Surface Parking Lots. Commercial parking should be behind
 or along-side the buildings and be visually buffered by plantings so as to
 encourage an active streetscape.
- Display Consistent Signage: Signage pylons, window display signs, and onsite directional signage should be consistent in terms of colors and materials.
 Signage should clearly mark site access and direction for automobile and bicycle access from streets.
- Install Edge Treatments along Surface Parking Lots: Edge treatments along
 the borders between private surface parking lots and the adjacent sidewalks
 should include a planting strip approximately two feet wide (or more) and
 permanent planting or fencing approximately three feet high. (Following City
 standards where applicable.)
- Construct Organized Parking Lots: Surface parking lots should be constructed to adhere to City of Edina standards regarding stall width and length and aisle width. Pedestrian-oriented lighting should be installed on islands within surface parking lots. Where applicable, clearly defined pedestrian paths should be marked so that pedestrians can safely walk between the parking lot and on-site buildings.

Other Improvements

As properties redevelop, the City may require investment in on-site or public facilities that complement new projects, such as:

- Bicycle Parking. Bicycle parking facilities located close to the front doors of businesses.
- On-Site Lighting. On-site pedestrian-oriented lighting.
- Interpretive Monuments and Public Art. Entry monuments, which can serve as gateways, should be installed at strategic locations within the Study Area. The monuments could be local or regional historical markers, memorials or public art installations. Strategic locations that might serve to mark progression through or arrival in the node include, but are not limited to, the corner of Wooddale Avenue and Valley View, the intersection of 62nd Street and Valley View Road, the intersection of 62nd Street Oaklawn Avenue and Valley View Road.

Chapter 5 - Transportation and Street Design



Valley View Road, west of Wooddale Avenue, looking west.

Valley View Road

The overall right-of-way width of Valley View Road is 66 feet between property lines on the west side of Wooddale Avenue. Within the right-of-way are two travel lanes, each 17 feet wide, and one five foot-wide sidewalk on the south side of the street. There is no sidewalk on the north side of Valley View Road in this segment. Outside the right-of-way, west of Wooddale Avenue, is the Edina Village Market on the north side of Valley View Road and an apartment building on the south side of the street. As shown on the typical section, the Edina Village Market has been built on a rise that is about 12 feet higher than Valley View Road at the western property line of the Edina Village Market.

This segment of Valley View Road to the west of the intersection with Wooddale Avenue was studied because of its relationship to the Edina Village Market and, additionally, because it is a comparatively older roadway segment than Valley View Road east of the intersection.

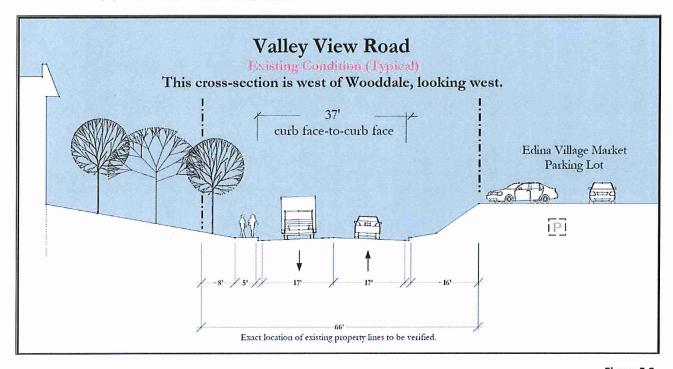
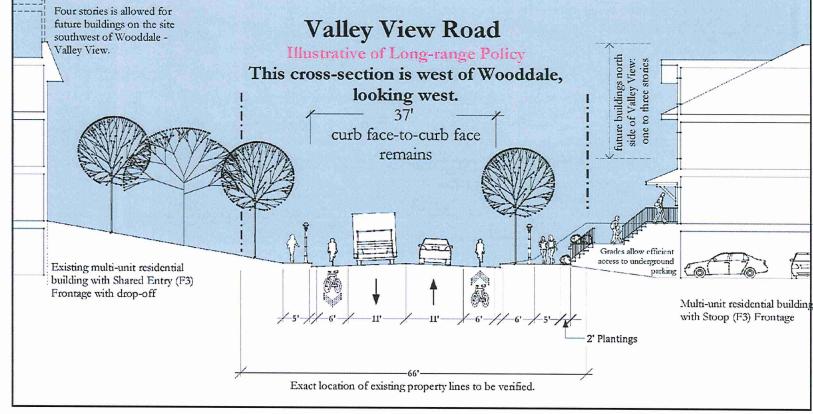


Figure 5.2 Valley View Road Existing Conditions

Figure 5.3 Valley View Road Illustrative of Long-Range Policy

Chapter 5

Transportation and Street Design



Wooddale Valley View Small Area Plan



New or Improved Sidewalks



New or Improved Tree Planted Boulevards



New Bike Lanes



New Pedestrian-Scale Lighting



New Cross-walks



Intersection Improvement(s)

Figure 5.6

Public Realm Improvements

DRAFT

Traffic and Parking Study for 4425 Valley View Road in Edina, MN

Prepared for: City of Edina



4801 W. 50th Street Edina, MN 55424

Prepared by:

WENCK Associates, Inc. 1800 Pioneer Creek Center Maple Plain, MN 55359 Phone: 7963-479-4200 Fax: 763-479-4242

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I hereby certify that this report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

_____ DATE: July 1, 2020

Edward F. Terhaar License No. 24441



1.0 Executive Summary

The purpose of this Traffic and Parking Study is to evaluate the impacts of the proposed new residential building located at 4425 Valley View Road in Edina, MN. The project site is located on the south side of Valley View Road east of Wooddale Avenue. The proposed project location is currently occupied by a vacant day care building.

This study examined weekday a.m. and p.m. peak hour traffic impacts of the proposed redevelopment at the following intersections:

- Valley View Road/Wooddale Avenue
- Valley View Road/Kellogg Avenue
- Valley View Road/62nd Street

The proposed project will involve removal of the existing building and constructing a new apartment building with 21 dwelling units. The project includes 32 underground parking stalls. As shown in the site plan, one access point is provided on Valley View Road. The project is expected to be completed in 2022.

The conclusions drawn from the information and analyses presented in this report are as follows:

- The proposed development is expected to generate 8 net trips during the weekday a.m. peak hour, 9 net trips during the weekday p.m. peak hour, and 114 net weekday daily trips.
- Traffic generated by the proposed development has minimal impact on intersection operations. No improvements are needed at the intersections analyzed to accommodate the proposed project.
- A trip generation comparison with a day care use on the site shows the number of trips generated by the proposed apartment building is lower in the a.m. and p.m. peak hours and during a typical weekday.
- The project owner is encouraged to provide bicycle spaces to promote bicycle use by residents. Long-term spaces for residents within the building and outside racks are recommended. The provision of a bicycle maintenance station will also help encourage bicycle use by residents.
- The proposed number of parking spaces can accommodate the expected peak parking demand based on Institute of Transportation Engineers (ITE) data.
- In order to promote transit to residents of the apartment building, the project owner is encouraged to provide the following information:
 - Maps that show the area bus routes and schedules.
 - o Information on starting and joining commuter programs.
 - Other information or actions that encourage use of alternative modes of transportation.



2.0 Purpose and Background

The purpose of this Traffic and Parking Study is to evaluate the impacts of the proposed new residential building located at 4425 Valley View Road in Edina, MN. The project site is located on the south side of Valley View Road east of Wooddale Avenue. The proposed project location is currently occupied by a vacant day care building. The project location is shown in **Figure 1**.

This study examined weekday a.m. and p.m. peak hour traffic impacts of the proposed redevelopment at the following intersections:

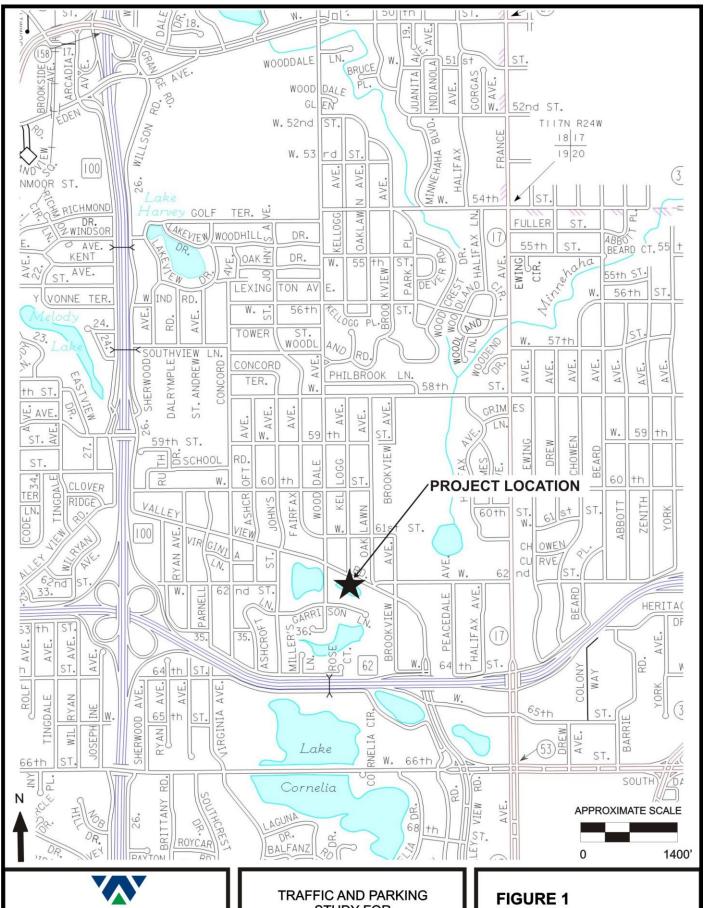
- Valley View Road/Wooddale Avenue
- Valley View Road/Kellogg Avenue
- Valley View Road/62nd Street

Proposed Development Characteristics

The proposed project will involve removal of the existing building and constructing a new apartment building with 21 dwelling units. The project includes 32 underground parking stalls. As shown in the site plan, one access point is provided on Valley View Road.

The project is expected to be completed in 2022. The current site plan is shown in **Figure 2**.





WENCK

Responsive partner. Exceptional outcomes. TRAFFIC AND PARKING STUDY FOR 4425 VALLEY VIEW ROAD IN EDINA, MN

FIGURE 1
PROJECT LOCATION

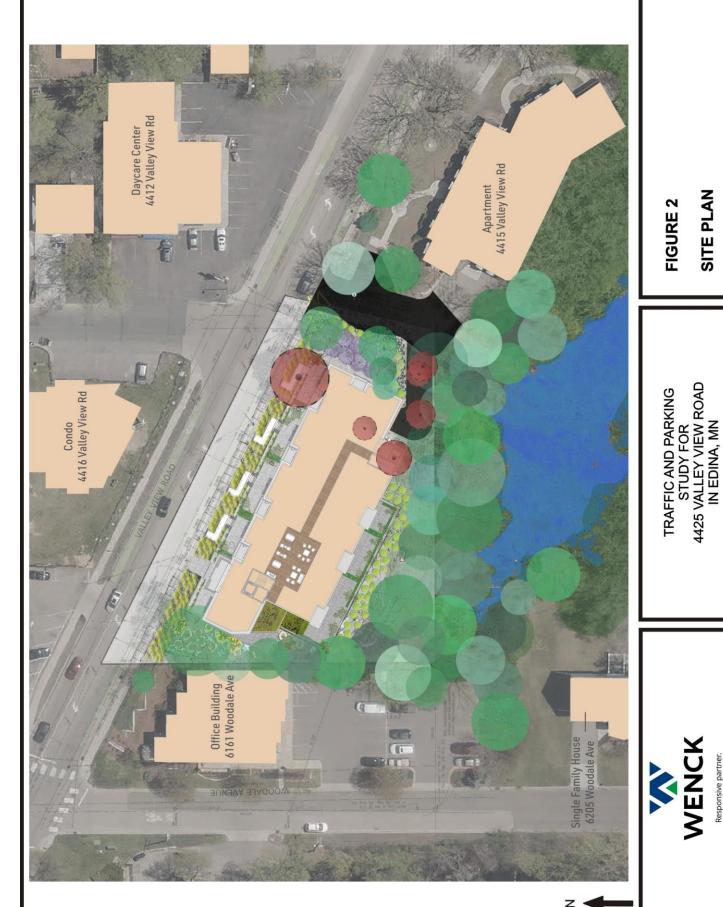


FIGURE 2

SITE PLAN



3.0 Existing Conditions

The proposed site is currently occupied by a vacant building. The site is bounded by Valley View Road on the north, an office building to the west, and residential uses to the east and south.

Near the site location, Valley View Road is a three-lane local roadway. Wooddale Avenue is a two-lane undivided north/south roadway. Kellogg Avenue and 61^{st} Street are two-lane undivided roadways. Existing conditions at the proposed project location are shown in **Figure 3** and described below.

Valley View Road/Wooddale Avenue

This four-way intersection is controlled with a traffic signal. The eastbound and westbound approaches provide one left turn lane and one through/right turn lane. The northbound and southbound approaches consist of one shared left turn/through/right turn lane. Striped crosswalks are present across all legs.

Valley View Road/Kellogg Avenue

This three-way intersection is controlled with a stop sign on the southbound Kellogg Avenue approach. The eastbound and westbound approaches provide one left turn lane and one through/right turn lane. The southbound approach consists of one shared left turn/right turn lane. A bike lane is provided on both sides of Valley View Road.

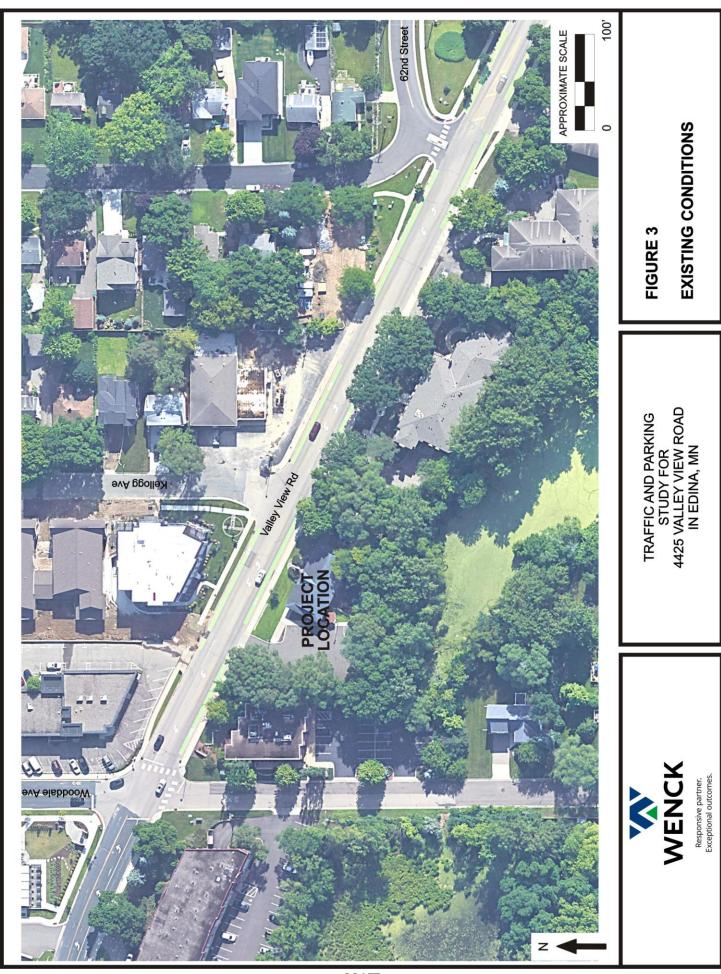
Valley View Road/62nd Street

This three-way intersection is controlled with a stop sign on the southbound 62nd Street approach. The eastbound and westbound approaches provide one left turn lane and one through/right turn lane. The southbound approach consists of one shared left turn/right turn lane. A bike lane is provided on both sides of Valley View Road.

Traffic Volume Data

Turn movement data for the intersections was collected during the weekday a.m. (7:00 - 9:00 a.m.) and p.m. (4:00 - 6:00 p.m.) peak periods in June 2020.





<u>Traffic Forecast Scenarios</u>

To adequately address the impacts of the proposed project, forecasts and analyses were completed for the year 2023. Specifically, weekday a.m. and p.m. peak hour traffic forecasts were completed for the following scenarios:

- 2020 Existing. Existing volumes were determined through traffic counts at the subject intersections. The existing volume information includes trips generated by the uses near the project site.
- 2023 No-Build. Existing volumes at the subject intersections were increased by 1.0 percent per year to determine 2023 No-Build volumes. The 1.0 percent per year growth rate was calculated based on both recent growth experienced near the site and projected growth in the area.
- 2023 Build. Trips generated by the proposed development were added to the 2023 No-Build volumes to determine 2023 Build volumes.

Trip Generation for Proposed Project

Weekday a.m. and p.m. peak hour trip generation for the proposed development were calculated based on data presented in the tenth edition of <u>Trip Generation</u>, published by the Institute of Transportation Engineers (ITE). The resultant trip generation estimates are shown in **Table 4-1**.

Table 4-1
Trip Generation for Proposed Project

Land Use	Size	Weekd	ay AM Pea	ak Hour	Weekd	ay PM Pea	ak Hour	Weekday Daily
		In	Out	Total	In	Out	Total	Total
Apartments	21 DU	2	6	8	5	4	9	114

DU=dwelling unit

As shown, the project adds 8 net trips during the a.m. peak hour, 9 net trips during the p.m. peak hour, and 114 net trips daily.

Trip Generation Comparison for Land Use Alternative

In addition to the proposed apartment building use, trip generation estimates were developed for the previous day care use for comparison purposes. The number of trips were calculated based on data presented in *Trip Generation*, Tenth Edition, published by the Institute of Transportation Engineers. The resultant trip generation estimates are shown in **Table 4-2**.



Table 4-2
Trip Generation for Alternative Land Use

Land Use	Size	Weekda	ay AM Pea	ak Hour	Weekd	ay PM Pea	ak Hour	Weekday Daily
		In	Out	Total	In	Out	Total	Total
Day care	4,480 SF	26	23	49	23	27	50	213

SF=square feet

The comparison show the number of trips generated by the proposed apartment building is lower in the a.m. and p.m. peak hours and during a typical weekday.

Trip Distribution Percentages

Trip distribution percentages for the subject development trips were established based on the nearby roadway network, existing and expected future traffic patterns, and location of the subject development in relation to major attractions and population concentrations.

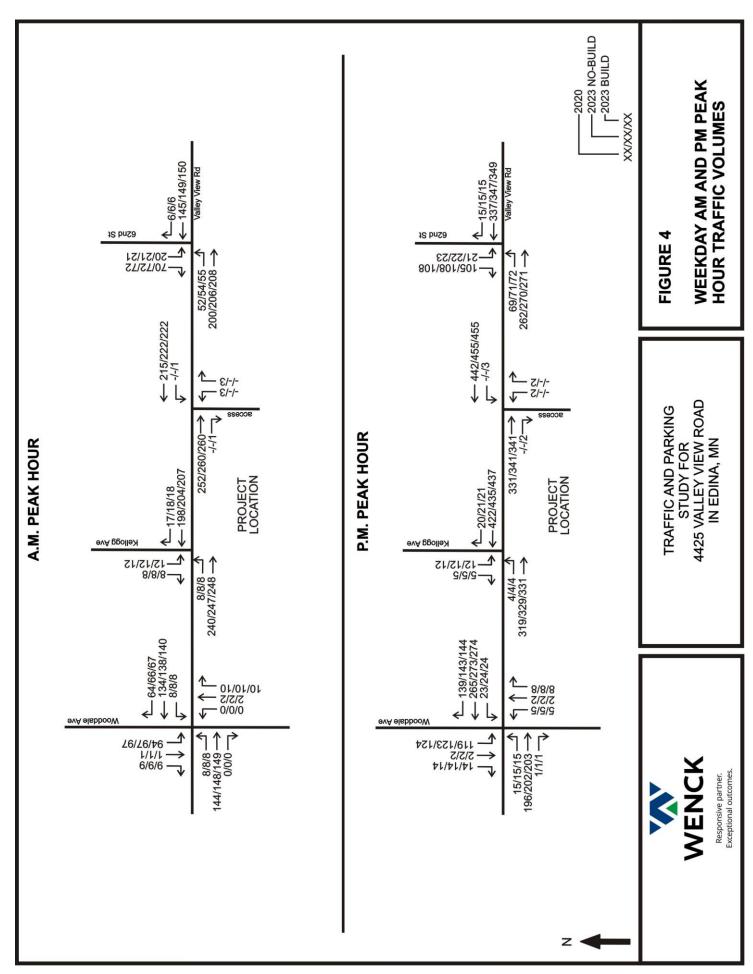
The distribution percentages for trips generated by the proposed development are as follows:

- 40 percent to/from the east on Valley View Road
- 30 percent to/from the west on Valley View Road
- 10 percent to/from the east on 62nd Street
- 15 percent to/from the north on Wooddale Avenue
- 2 percent to/from the north on Kellogg Avenue
- 3 percent to/from the north on Oaklawn Avenue

Traffic Volumes

Development trips were assigned to the surrounding roadway network using the preceding trip distribution percentages. Traffic volumes were established for all the forecasting scenarios described earlier during the weekday a.m. and p.m. peak hours. The resultant traffic volumes are presented in **Figure 4.**





Intersection Level of Service Analysis

Traffic analyses were completed for the subject intersections for all scenarios described earlier during the weekday a.m. and p.m. peak hours using Synchro software. Initial analysis was completed using existing geometrics and intersection control.

Capacity analysis results are presented in terms of level of service (LOS), which is defined in terms of traffic delay at the intersection. LOS ranges from A to F. LOS A represents the best intersection operation, with little delay for each vehicle using the intersection. LOS F represents the worst intersection operation with excessive delay. The following is a detailed description of the conditions described by each LOS designation:

- Level of service A corresponds to a free flow condition with motorists virtually unaffected by the intersection control mechanism. For a signalized or an unsignalized intersection, the average delay per vehicle would be approximately 10 seconds or less.
- Level of service B represents stable flow with a high degree of freedom, but with some influence from the intersection control device and the traffic volumes. For a signalized intersection, the average delay ranges from 10 to 20 seconds. An unsignalized intersection would have delays ranging from 10 to 15 seconds for this level.
- Level of service C depicts a restricted flow which remains stable, but with significant influence from the intersection control device and the traffic volumes. The general level of comfort and convenience changes noticeably at this level. The delay ranges from 20 to 35 seconds for a signalized intersection and from 15 to 25 seconds for an unsignalized intersection at this level.
- Level of service D corresponds to high-density flow in which speed and freedom are significantly restricted. Though traffic flow remains stable, reductions in comfort and convenience are experienced. The control delay for this level is 35 to 55 seconds for a signalized intersection and 25 to 35 seconds for an unsignalized intersection.
- Level of service E represents unstable flow of traffic at or near the capacity of the intersection with poor levels of comfort and convenience. The delay ranges from 55 to 80 seconds for a signalized intersection and from 35 to 50 seconds for an unsignalized intersection at this level.
- Level of service F represents forced flow in which the volume of traffic approaching
 the intersection exceeds the volume that can be served. Characteristics often
 experienced include long queues, stop-and-go waves, poor travel times, low comfort
 and convenience, and increased accident exposure. Delays over 80 seconds for a
 signalized intersection and over 50 seconds for an unsignalized intersection
 correspond to this level of service.



The LOS results for the study intersections are discussed below.

<u>Valley View Road/Wooddale Avenue (traffic signal controlled)</u> - During the a.m. peak hour under 2020, 2023 No-Build, and 2023 Build conditions, all movements operate at LOS A. The overall intersection operates at LOS A for all scenarios.

During the p.m. peak hour under 2020, 2023 No-Build, and 2023 Build conditions, all movements at LOS B or better. The overall intersection operates at LOS A for all scenarios.

The traffic generated by the proposed development has minimal impact on the intersection operations and does not change the level of service of any movement.

<u>Valley View Road/Kellogg Avenue (southbound stop controlled)</u> - During the a.m. peak hour under 2020, 2023 No-Build, and 2023 Build conditions, all movements operate at LOS B or better. The overall intersection operates at LOS A for all scenarios.

During the p.m. peak hour under 2020, 2023 No-Build, and 2023 Build conditions, all movements operate at LOS B or better. The overall intersection operates at LOS A for all scenarios.

The traffic generated by the proposed development has minimal impact on the intersection operations and does not change the level of service of any movement.

<u>Valley View Road/62nd Street (southbound stop controlled)</u> - During the a.m. peak hour under 2020, 2023 No-Build, and 2023 Build conditions, all movements operate at LOS B or better. The overall intersection operates at LOS A for all scenarios.

During the p.m. peak hour under 2020, 2023 No-Build, and 2023 Build conditions, all movements operate at LOS B or better. The overall intersection operates at LOS A for all scenarios.

The traffic generated by the proposed development has minimal impact on the intersection operations and does not change the level of service of any movement.

Overall Traffic Impact

Trips generated by the proposed development are expected to have minimal impact on traffic operations on the surrounding street system. No improvements are needed at the subject intersections to accommodate the proposed project.

Bicycle and Pedestrian Facilities

Under existing conditions, sidewalk is provided both sides of Valley View Road and the east side of Wooddale Avenue. Sidewalk is not provided on Kellogg Avenue, Oaklawn Avenue, 61st Street, or 62nd Street. A striped bicycle lane is provided on both sides of Valley View Road. Bicycles are allowed on all the surrounding streets.

Future plans for this area include additional sidewalk on 60th Street, which is located north of the proposed project. The bicycle facilities plan shows additional future bike lanes on both Valley View Road and Wooddale Avenue. The proposed project will benefit from the existing and proposed sidewalk and bicycle facilities in this area.



The area near the project site has substantial pedestrian and bicycle traffic. Retail uses in the northwest quadrant of the Valley View Road/Wooddale Avenue intersection are popular destinations for the surrounding neighborhoods. The project owner is encouraged to provide bicycle spaces to promote bicycle use by residents. Long-term spaces for residents within the building and outside racks are recommended. The provision of a bicycle maintenance station will also help encourage bicycle use by residents.

Transit Facilities

The subject site presently is served by the Metro Transit bus route 6 on Valley View Road and Wooddale Avenue. Bus stops exist at the corner of Valley View Road and Wooddale Avenue.

In order to promote transit to residents of the apartment building, the project owner is encouraged to provide the following information:

- Maps that show the area bus routes, light rail and bus schedules, and bicycle and pedestrian facilities.
- Information on starting and joining commuter programs.
- Other information or actions that encourage use of alternative modes of transportation.



6.0 Parking Analysis

As described earlier, the project includes 32 underground parking stalls. The proposed amount of parking was compared to industry standards to determine adequacy.

Parking data from the Institute of Transportation Engineers (ITE) was used to determine the expected parking demand for the proposed land uses. Data provided in the ITE publication *Parking Generation*, 5th Edition, indicates the various proposed uses peak at different times during the day. The ITE data was adjusted to account for the expected modal split for the site.

Based on the ITE data, the peak weekday parking demand for the overall site occurs between 6 am and 8 am. The peak parking demand during that time period is 25 spaces. The 32 spaces provided can accommodate the expected peak parking demand.



7.0 Conclusions and Recommendations

The conclusions drawn from the information and analyses presented in this report are as follows:

- The proposed development is expected to generate 8 net trips during the weekday a.m. peak hour, 9 net trips during the weekday p.m. peak hour, and 114 net weekday daily trips.
- Traffic generated by the proposed development has minimal impact on intersection operations. No improvements are needed at the intersections analyzed to accommodate the proposed project.
- A trip generation comparison with a day care use on the site shows the number of trips generated by the proposed apartment building is lower in the a.m. and p.m. peak hours and during a typical weekday.
- The project owner is encouraged to provide bicycle spaces to promote bicycle use by residents. Long-term spaces for residents within the building and outside racks are recommended. The provision of a bicycle maintenance station will also help encourage bicycle use by residents.
- The proposed number of parking spaces can accommodate the expected peak parking demand based on Institute of Transportation Engineers (ITE) data.
- In order to promote transit to residents of the apartment building, the project owner is encouraged to provide the following information:
 - o Maps that show the area bus routes and schedules.
 - o Information on starting and joining commuter programs.
 - Other information or actions that encourage use of alternative modes of transportation.



8.0 Appendix

• Level of Service Worksheets





Date: July 2, 2020

To: Cary Teague – Community Development Director

Chad Millner, PE - Director of Engineering

4425 Valley View Road, Owner and Development Team Cc:

From: Andrew Scipioni – Transportation Planner

Re: 4425 Valley View Road - Transportation Review

DIR Architecture is proposing to construct a new residential building at 4425 Valley View Road. This site formerly housed a day care facility (New Horizon Academy) that has since relocated to 4412 Valley View Road. The proposed redevelopment would replace the existing one-story building with a three-story, 21-unit apartment building. Wenck Associates, Inc. completed a traffic and parking assessment for this project on behalf of the City. This memo will discuss the conclusions of that assessment and review how the proposed development complies with approved City plans and policies related to transportation.

Trip Generation

Figure I shows the estimated trip generation for the proposed residential use compared to that of the previous use. Estimated trips were calculated using the Trip Generation Manual, 10th Edition, published by the Institute for Transportation Engineers (ITE) based on the size of the vacant day care building and the number of proposed residential units.

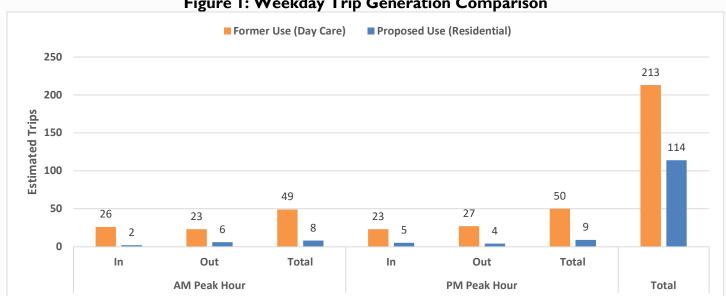


Figure 1: Weekday Trip Generation Comparison

Wenck's assessment shows a significant reduction in trip generation for the proposed use compared to the previous use. Peak hour trips would be reduced by more than 80% and total trips would be reduced by almost 50%.



In addition, three adjacent intersections were studied: Valley View Road and Wooddale Avenue, Valley View Road and Kellogg Avenue, and Valley View Road and West 62nd Street. The proposed redevelopment was found not to change the level of service (or traffic delay) for any movement at any of these intersections. Overall, minimal impact to traffic operations is anticipated and no improvements would be necessary to accommodate this project.

Parking Analysis

23 parking spaces are required for this redevelopment based on the number and size of proposed residential units (see Table I). Using ITE parking data, Wenck concluded that the peak parking demand for this project would be 25 spaces. With 32 enclosed parking spaces proposed, the project fulfills the City's minimum requirements and the estimated peak parking demand.

Table I: Required Parking Spaces

Residential Unit Size	Required Parking Spaces per Unit	Number of Units	Total Required Parking Spaces
≤ 1,500 square feet		18	18
> 1,500 square feet	1.5	3	4.5 (5)
Total		21	22.5 (23)

Compliance with Transportation Plans/Policies

Table 2 details the ways in which the proposed redevelopment supports current City transportation plans and policies.

Table 2: Compliance with Transportation Plans/Policies

Draft 2018 Compi	rehensive Plan
Goal/Policy	Explanation
 Reduce the overall dependence on and use of single-occupancy vehicles by promoting land use patterns that allow for shorter vehicle trips and the use of alternative travel options. 	The project site is adjacent to sidewalks, shared bike lanes and transit service.
 Develop and manage parking provisions to encourage joint and shared use of facilities, ride- sharing and bicycle parking. 	The project is proposed to include an outside bike rack along Valley View Road and a bike room in the underground garage with capacity for 20 bikes.
 Provide and maintain adequate access to and from, and safety on, local and regional roadway adjacent to community redevelopment and other activity that potentially impacts the City of Edina. 	The project is proposed to remove the existing driveway access off Valley View Road and utilize a shared driveway with 4412 Valley View Road.
Living Streets	Plan (2015)
Goal/Policy	Explanation
 Living Streets provide access and mobility for all transportation modes while enhancing safety and convenience for all users. 	The project provides accommodations for motorists, cyclists and pedestrians.



The City will require new developments to provide interconnected street and sidewalk networks that connect to existing or planned streets or sidewalks on the perimeter of the development.	The proposal includes replacing the existing back- of-curb sidewalk with a boulevard-style sidewalk adjacent to the property along Valley View Road.
Living Streets will improve the current and future quality of life for the public.	The proposed boulevard-style sidewalk will improve pedestrian safety and comfort adjacent to the property.
Wooddale/Valley View S	
Goal/Policy	Explanation
 Ensure safe and convenient travel for traffic passing through and within the Study Area by limiting the number of driveways providing access between the roadway system and private property. 	The project is proposed to remove the existing driveway access off Valley View Road and utilize a shared driveway with 4412 Valley View Road.
 Ensure adequate parking supplies that are located on-site in accordance with specific land uses, meet multi-modal parking needs, and are safe and secure. 	All required resident and visitor parking is located on-site in an underground garage which also includes a bike storage room.
Design public rights-of-way to facilitate and encourage safe and convenient multi-modal travel by providing sidewalks, boulevards, marked crosswalks, and pedestrian-oriented street lighting within the Study Area and connectivity for pedestrians and cyclists to surrounding neighborhoods and destinations.	The proposed boulevard-style sidewalk will improve pedestrian safety and comfort adjacent to the property. The site is located adjacent to existing pedestrian, bicycle and transit infrastructure.

Travel Demand Management Opportunities

In addition to the measures already proposed, the applicant should consider additional strategies to support alternative modes of transportation to and from the site and reduce the impact of motor vehicles in the neighborhood. Examples of other strategies include:

- Providing an on-site bicycle repair station
- Providing directional signage for location transportation amenities (e.g. bus stops, bicycle parking)
- Designating 10% of parking spaces for electric vehicles (or making 10% EV-ready)
- Providing information to tenants/employees about pedestrian and bicycle facilities, transit services, commuter programs and ride-share opportunities.
- Subsidizing transit passes for tenants/employees (Metro Transit's local Route 6 bus has a stop at Wooddale Avenue and Valley View Road. The future E Line bus rapid transit (BRT) service on France Avenue will likely have stations at West 62nd Street, approximately half a mile from the project site and accessible by existing pedestrian and bicycle facilities.)

Development Questionnaire

What this is: A brief questionnaire to help community decision makers understand how this development aligns with key focus area of sustainability that ensures both developers and the community long-term value.

Why? Through the 2014 Visioning process, environmental stewardship is one of seven strategic focus areas. Vision Edina's Environmental Stewardship states:

"There is a growing awareness of the impact that the built environment has on the natural environment, and the individual and collective responsibility we all have towards good environmental stewardship. Community residents and stakeholders believe that Edina can take an active and ambitious internal and regional leadership role in promoting more comprehensive recycling, smart building, and energy efficiency practices. These themes couple well with the parallel benefits in smarter urban planning, increased transportation options, and application of technology."

Questions	Answers
Sustainable Design & Energy	
Have you utilized Xcel Energy's Energy Design Assistance and/or Centerpoint	yes
Energy's <u>Builder and Developer programs</u> for this development?	
Will the buildings meet <u>SB2030</u> energy goals and/or will they be <u>Energy Star</u>	yes
certified? If not, please share the steps you are taking to support energy conservation.	
Will you be optimizing the roof by installing a green roof?	no
Will there be any renewable energy generation on site?	no
Will there be purchase of renewable energy credits (RECs)?	We may need
will there be parenase of renewable chergy creates (NECs):	to once
	energy
	analysis has
	been
	completed
Comments:	
Roof will be designed for possible future solar panel installation	
6	
Managing Water	-
What percent of the property is pervious surface before the redevelopment?	Pre: 57.3%
What is the percent post development?	Post: 33.5%
What new services will be pervious? (i.e. Sidewalks, driveways, overflow parking)	All landscape
	and are not
	covered by
	building,
	sidewalk or
	drive aisle.
How will the landscaping support the natural ecosystem? (i.e. Rain gardens, % native	proposing the
plants, % bee friendly pollinator plants)	capture and
	infiltration of
	the 1.1"
	rainfall from
	all of the

newly constructed impervious surfaces (building, sidewalk and pavements). We have tailored our plant pallete to be predominantly native and pollinator friendly, which makes up about It 75% of the pallete. Comments: **Managing Tree Canopy** What percent of the property is covered by tree canopy before redevelopment? Before What is the percent post development? development approximately 45%. Post development approximately 32% Will you be replanting/replacing trees at least four to five inches in diameter to We are positively impact the tree canopy (ordinance requirement is only 2.5 inches in proposing 3" diameter)? trees, as our experience is that these will perform and reach mature height faster than initially planting a larger caliper tree. Comments: **Managing Waste**

Development Questionnaire

Development Questionnaire

Will a recycling service be provided to all businesses on site?	Yes for
	residential- no
	businesses on
	site
Will an organic (i.e. food waste) recycling service be provided to all businesses on	No businesses
site?	on site
Comments:	
There will be 2 trash chutes provided. One for trash and one for recycling.	
Sustainable Transportation	
Will there be bike parking near main entrance for guests?	yes
Will there be bike parking near main entrance for guests? Do you have EV Charging Stations for owners or guests to use?	yes yes
	,
Do you have EV Charging Stations for owners or guests to use?	yes
Do you have EV Charging Stations for owners or guests to use? Will there be parking spaces provided for car-sharing vehicles to reduce the overall	yes
Do you have EV Charging Stations for owners or guests to use? Will there be parking spaces provided for car-sharing vehicles to reduce the overall number of cars?	yes
Do you have EV Charging Stations for owners or guests to use? Will there be parking spaces provided for car-sharing vehicles to reduce the overall number of cars?	yes
Do you have EV Charging Stations for owners or guests to use? Will there be parking spaces provided for car-sharing vehicles to reduce the overall number of cars?	yes