Agenda Planning Commission City Of Edina, Minnesota City Hall, Council Chambers

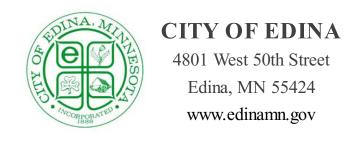
Wednesday, July 10, 2019 7:00 PM

- Call To Order
- II. Roll Call
- III. Approval Of Meeting Agenda
- IV. Approval Of Meeting Minutes
 - A. June 26, 2019 Planning Commission Minutes
- V. Public Hearings
 - A. Site Plan Review & Setback Variance 6950 France Avenue
- VI. Community Comment

During "Community Comment," the Board/Commission will invite residents to share relevant issues or concerns. Individuals must limit their comments to three minutes. The Chair may limit the number of speakers on the same issue in the interest of time and topic. Generally speaking, items that are elsewhere on tonight's agenda may not be addressed during Community Comment. Individuals should not expect the Chair or Board/Commission Members to respond to their comments tonight. Instead, the Board/Commission might refer the matter to staff for consideration at a future meeting.

- VII. Reports/Recommendations
- VIII. Correspondence And Petitions
- IX. Chair And Member Comments
- X. Staff Comments
- XI. Adjournment

The City of Edina wants all residents to be comfortable being part of the public process. If you need assistance in the way of hearing amplification, an interpreter, large-print documents or something else, please call 952-927-8861 72 hours in advance of the meeting.



Date: July 10, 2019 Agenda Item #: IV.A.

To: Planning Commission Item Type:

Minutes

Action

From: Emily Bodeker, Assistant City Palnner

Item Activity:

Subject: June 26, 2019 Planning Commission Minutes

ACTION REQUESTED:

Approve the June 26, 2019 Planning Commission minutes.

INTRODUCTION:

ATTACHMENTS:

June 26, 2019 Planning Commission Minutes



Minutes City Of Edina, Minnesota Planning Commission Edina City Hall Council Chambers June 26, 2019

I. Call To Order

Chair Olsen called the meeting to order at 7:00 PM.

II. Roll Call

Answering the roll call were: Commissioners Miranda, Berube, Mangalick, Nemerov, Bennett, Lee, Douglas, and Chair Olsen. Staff Present: Cary Teague, Community Development Director; Kris Aaker, Assistant Planner; Jackie Hoogenakker, Support; Kaylin Eidsness, Senior Communications Coordinator.

Absent from the roll call: Commissioners Thorsen, Strauss, Melton.

III. Approval Of Meeting Agenda

Commissioner Berube moved to approve the June 26, 2019, agenda. Commissioner Miranda seconded the motion. The motion carried unanimously.

IV. Approval Of Meeting Minutes

A. Minutes: Planning Commission, June 12, 2019

Commissioner Berube moved to approve the June 12, 2019, meeting minutes. Commissioner Douglas seconded the motion. Motion carried unanimously.

Director Teague introduced Commissioner Douglas as the new Planning Commissioner. Commissioner Douglas reviewed his background with the Commissioners.

V. Public Hearings

A. Variance Request B-19-10 - 4213-4215 West 50th Street

Assistant Planner Aaker presented the request of the applicant for a variance to permit a 10.3-foot setback for a garage and carport from right-of-way and variance from the minimum two-car garage requirement at 4213-4215 West 50th Street. The subject property is zoned R-2, double dwelling unit district, and is approximately 10,074 square feet in area, located south of West 50th Street consisting of a side-by-side double dwelling with a detached two-car garage accessing from West 50th Street.

Staff recommends approval of the variance requests as requested subject to the findings and conditions listed in the staff report.

Discussion/Comments/Questions

- Commissioners asked if the City had requirements or guidelines for garage access. Aaker responded she was not aware of any requirements. The garage would either need to access a street or alleyway.
- Commissioners wondered if the City would see any issue in the future with use of the right-ofway for the driveway access to the alleyway. Aaker stated it has not been a concern of the City Engineer and was one of the first questions that was asked when this proposal came in.
- Commissioners noted there was mention of eventually subdividing this parcel and wondered what
 that meant. Aaker explained there would be a party wall division with two legal descriptions and
 at some point in time the two units could be sold and owned separately. It should be noted that
 the subject lot is one lot.
- Commissioners asked for confirmation that there was no subdivision before the Commission at this time. Aaker indicated that was correct.
- Commissioners asked how a car will access the easement area on the property and which stalls
 would need to be removed from the existing parking lot. Aaker showed an aerial of the area and
 indicated two parking spots behind the garage that would need to be removed. She noted there
 was no objection when discussing this with the City Engineer and also indicated the same would
 be done for the property owner to the east.

Appearing for the Applicant

Steve Springrose, owner, introduced himself and explained he and his wife have owned the property for two years. The building was built as a duplex in the 40's and did not have a lot of improvements through the years. He explained there is nowhere to park unless they park on Jay Place. Springrose said he is attempting to put in four resident parking and four guest parking spaces on the existing property. Nothing would change on the southside of the property. Springrose stated he wanted to maintain a good relationship between the duplex and the church behind them. He mentioned that in addition to the church he has spoken to the four adjacent landowners on Jay Place and to the property owner to the east and all are in support of what is being proposed.

Springrose noted the garage as drawn on the south side has a fifteen-foot setback and in discussion with City staff it was indicated that twenty feet might be what is required there. He did not know if there was an administrative way to add a fifteen-foot setback capability to the variance, as the garage would be built to the south, even though it is not what was put into the information before the Planning Commission. Assistant Planner Aaker responded because it is an alley to the south, it is considered differently than a City street and actually a three-foot setback is required, not 15 or 20 if it were a street. Aaker stated Mr. Springrose is fine with what the drawing shows.

Discussion/Comments/Questions

- Commissioners had a concern with who would be maintaining the alleyway behind the garage.
 Springrose indicated there have not been any past concerns with the maintenance of the alleyway but if there were future concerns it would be brought forward to the City to see what could be done.
- Commissioners asked how long Springrose has lived in the home. Springrose indicated his family does
 not currently live at the residence, it is being rented out to single individuals; however, they are planning
 on moving into the house in a few years.

- Commissioners asked Springrose to expand his points regarding affordability. Springrose indicated this
 is naturally occurring low income property in the City of Edina. The rent charged is at the twentieth
 percentile of Edina incomes.
- Commissioners wondered if the renovations go through would the rents have to increase. Chair Olsen stated that is not a part of the variance and the Planning Commission needs to only focus on the variance. Springrose stated he believes if there is an increase it would not be substantial, the rent is being market driven and is affordable.
- Commissioners wondered if there was a way to consider a different paving materials choice other than hardscape. Springrose stated he has been working with City Engineering related to that, adding they are trying to stay under six hundred square feet of pavement addition. The front area paving can be removed because it will not be accessed by a vehicle which will reduce the amount of hardscape. He noted he has looked at various types of paving that would limit the footprint and runoff.

Public Hearing

None.

Commissioner Berube moved to close the public hearing. Commissioner Nemerov seconded the motion. The motion carried unanimously.

Discussion/Comments/Questions

- Commissioners discussed the concerns with single car garage and carport, the maintenance of the alleyway and the affordable housing aspect.
- Commissioners were in support of the variance.

Motion

Commissioner Berube moved that the Planning Commission recommend approval to the City Council of the variance request B-19-10 for 4213-4215 West 50th Street as outlined in the staff memo subject to the conditions and findings therein. Commissioner Lee seconded the motion. The motion carried unanimously.

B. Variance Request B-19-11 - 4439 Garrison Lane

Assistant Planner Aaker presented the request of the applicant for a 3.36-inch foot first floor height variance for a new home at 4439 Garrison Lane. The proposed first floor elevation of the house is proposed to be at 878.36, which will be 3.36 inches higher than the I-foot increase allowed by ordinance. Aaker explained the proposed survey indicates a first-floor elevation for a new homme at 878.36 with the existing first floor at 877.37, which is less than the allowable I-foot increase (complies). The survey, therefore, specifies compliance with the I-foot maximum increase rule. The survey also specifies that the basement will be a 9-foot poured walk-out. Aaker stated the property will be re-graded to accomplish a walk out. Given the survey, it appears the project can be accomplished without the need for a survey.

Staff recommends approval of the variance, as requested subject to the findings and conditions listed in the staff report. It was noted there has been some correspondence regarding this item and all have been supportive.

Discussion/Comments/Questions

- Commissioners wondered why there is a variance on an elevation if the aggregate house is still lower than the maximum. Aaker indicated the City has a rule that requires that you do not go higher than one foot above the existing entry elevation of the home that is currently there. It is a variance from the required 1-foot rule; not from building height.
- Commissioners asked if the previous house had been compliant with the today's floodplain regulations in terms of basement elevation would there still be this issue. Aaker stated what they are proposing could comply and also comply with the floodplain elevation. The current home, if it was non-conforming could be kept that way as long as there was no tear down and rebuilding. If that would be done, it would need to be elevated. Aaker noted this is a problem the City has run into. Basements are required to be elevated 2 feet above the flood ordinance; however, the zoning ordinance limits the elevation to no more than 1-foot above the existing first floor elevation. It should be noted that most of the homes were built prior to standard and are lower.
- Commissioners asked if there would be any aesthetic differences when the property is built between the trusses the builder is using and the trusses the City thinks they could use. Aaker stated visually it may not be apparent adding she has been told trusses could affect ceiling height and where duct work can be placed. She said it can be done but is not generally the standard solution. Expanding, Aaker said higher ceiling height is what is seen on new builds.

Appearing for the Applicant

Alex Swiggen, 4439 Garrison Lane, applicant and Dale Perrault, Perrault Construction, explained they are asking for 3.36 inches for the variance with the basement height due to the one-hundred-year flood plain being at 866.2 and having to have a basement floor at 8.68.2. He noted there are some other things with the property he would like to do down the road with his job and having to stay within a certain fitness level, he does a lot of things overhead and needs a certain height of ceiling for that. If the trusses were to be changed that would be slightly problem some to him. He did not think from the street there would be any way to tell that it is three inches higher than others.

Discussion/Comments/Questions

- Commissioners indicated it was not about the inches and clarified this would be going up a foot and is not staying the same. The applicant is already taking twelve inches and wants three inches more. Swiggen stated without the hundred-year flood plain they would be able to get that with the twelve inches and would not have to come for a variance.
- Commissioners thought the roof height had been at a consistent maximum height throughout the City but if the applicant is open to taking suggestions, they could look at using some hip roof forms as a way of bringing down the overall building height which would make the house look less massive. Swiggen was under the impression that roof height had nothing to do with the variance requested because the proposed house is four feet lower than what is allowed. Aaker agreed that the overall building height is not an issue of this request.

Public Hearing

None.

Commissioner Bennett moved to close the public hearing. Commissioner Miranda seconded the motion. The motion carried unanimously.

Discussion/Comments/Questions

- Commissioners did not think this variance was hurting anyone and noted the change is the ordinance.
- Commissioners agreed the applicant is stuck at a bottom limit based on the floodplain and with all
 of the support from the neighborhood the majority of Commissioners felt that the plans as
 presented would not alter the character of the neighborhood.
- Commissioners discussed the staff recommendation, and some were in support of this and some thought it was pretty clear that this is about process and could not vote in support of the request.
- Commissioners suggested that the Planning Commission "come up" with a number that could be accepted on items like this, so it is consistent throughout (ceiling height). This topic should be placed on the Commissions "to do" topics.
- Commissioners thought there should be consistency. The houses are not out of sync with the character of the neighborhood and the aggregate height is a more important number within those floors. Three and a half inches in a basement is huge and can make a person either feel they are in a cave or in a room and can make the whole house more livable. It makes sense to approve this. It was expressed that the duty of the Planning Commissions is to also consider the spirit of the law, how the neighbors feel and what it does for the community. It will also be a more valuable home because there will be a livable basement.
- Commissioners thought precedent was something very important to consider; however, in the
 past the Commission has approved a variance for someone in order to get a hot tub because of
 the physical demands of their employment and in the past variances in subdivisions were approved
 in order to enhance the resale value of the land. This was not out of character of past actions by
 the City of Edina.

A majority of the Planning Commission indicated support for the variance.

Motion

Commissioner Berube moved that the Planning Commission recommend approval to the City Council of Variance B-19-11 for 4439 Garrison Lane as outlined in the staff memo subject to the conditions and findings therein including The hardship is caused by the high water elevation and an 8 foot 9 inch basement is considered reasonable in this instance and would not negatively impact the character of the neighborhood. Commissioner Nemerov seconded the motion. The motion carried 5 ayes, I nay (Lee).

C. Subdivision with Variances - 5841 Oaklawn Avenue

Director Teague presented the request of Steve Sandberg to subdivide his property at 5841 Oaklawn Avenue into two lots. The existing home on the lot would be torn down and two new homes built on the new parcels. Both lots would gain access off Oaklawn Avenue. Teague noted the home to the south is shaped the same as the proposed south lot, due to the vacation of a portion of 59th Street. There is an existing pedestrian and utility easement located over the vacated right-of-way. Teague noted the overhead power line and stairway/sidewalk access to the park located in the easement, which would remain in place.

Staff recommends approval of the subdivision with variances, as requested subject to the findings and conditions listed in the staff report.

Discussion/Comments/Questions

- Commissioners asked Teague if Engineering looked at these as building pads because it looks like
 the topography is pretty steep. It works right now for the single house that is there right now and
 seems to stay pretty clear of that but once it is subdivided into two lots it might have to start
 cutting into the steep slope, especially on Lot I. Teague indicated Engineering did not have an
 issue with that.
- Commissioners thought there will need to be a retaining wall on Lot I on the southside in the
 future because it currently naturally slopes down. By subdividing there could potentially be some
 topography and water drainage related issues once two separate houses are built.
- Commissioners asked in regard to a technicality that was brought up in some correspondence the
 Commission received, six of the ten lots are larger than fifty percent, which is the majority unless
 the lots on the other side of the street are included then it would be thirty percent. Does the
 guideline actually pertain to one street or did it specify the neighborhood. Teague indicated there
 is no specific wording for this and is a judgement call for the Planning Commission and Council to
 look at.
- Commissioners asked for clarity on the drawing where the property lines and easement area
 would be. Teague showed the drawing and indicated within the two dotted red lines it is a utility
 easement and no building could be constructed there, which is thirty feet from line to line. He
 noted the blue outside lines were the property lines.
- Commissioners asked if Engineering was ok with a building pad being so close to the utility easement. Teague indicated Engineering was fine with that. The original proposal did extend into the easement but given the easement that is there and the potential to put in additional utility lines, protection of that pedestrian path it is very important to the neighborhood to preserve the easement area.
- Commissioners stated one of the residents expressed water run off on one of the stairs during winter and did Engineering have any concerns about that or is there enough distance from the new proposed pad to avoid that. Teague noted Engineering did not mention any concern they had on that. That is something, should this go through to draw to Engineering's attention.
- Commissioners thought it was significant for the Engineering report to state that the proposed plan does show with redevelopment of the property that the impervious surface is now increasing from 25.4 percent to 43.7 percent. It does mention that there may be retaining walls required on

- the southern side of Lot I, in which case, if over four feet will need to be designed and by the conduits those walls will be over four feet.
- Commissioners asked if there was a concern for flooding because at 5845 Kellogg there was
 issues with the drainage and neighbors were experiencing flooding because this is increasing the
 impervious coverage by a lot. Teague did not recall that. He noted a drainage area on this plat
 and thought there might need to have something like this done to the north when a specific
 building is proposed for that site.
- Commissioners stated one of the proposals the Commission recently denied was that lot had a lot of old trees and that factored into the consideration a little bit and was not aware of any trees on this lot. Are there any trees relevant? Teague did not hear about any tree issues but there were a number of large oaks on the north parcel and is not a consideration on this lot.

Appearing for the Applicant

Steve Sandberg, 5841 Oaklawn Avenue, introduced himself and his siblings and explained that they grew up at 5841 Oaklawn Avenue. The combined two lots were purchased by his father in 1949 and contractors were hired to finish off what had been started as a foundation. Sandberg noted his father lived on and took care of that property and loved Edina. It was his father's wishes to split this back into two lots.

David Kenably, Civil Site Group, Civil Engineer working on the subdivision with the Sandberg's. In regard to the topography and grading on the property, these are preliminary plans and once approved the engineering work will need to be done. Currently retaining walls are not being shown and the current house is being used as a retaining wall with landscaping along the house. The slopes are standard 3 to 1 slope through there. The drainage all flows to the City right-of-way, nothing flows to any adjacent properties. Both block one and two drains to the east but there is a low spot and swale on this property that ultimately drains to 59th and will be no increase or impact to drainage on adjacent properties.

Discussion/Comments/Questions

- Commissioners asked if the connection of the driveway to 59th is going to be eliminated, providing some more permeable cover. Kenably stated that is the current intent. Having the two driveways come off Oaklawn and the driveway in the back would be eliminated.
- Commissioners asked if the drainage to 59th is above ground drainage or is any of it below ground. Kenably indicated it is currently above ground drainage with swales but apparently the engineer at the City indicated a drain at 59th that it could potentially be connected into. Commissioners thought that was a good idea.

Public Hearing

Robert Braun (property owner to the rear) commented that he respected the wishes of the family and wants to carry them on. He indicated he has been a resident since 1993 and his grandparents owned his home since the beginning, in 1952. Some of the main issues he believes has to do with aesthetics and drainage. The change to the neighborhood has been wonderful but now it is personal because he is going to sit in his backyard, as he did eight years ago and allowed his neighbor to build, and this project will do the same. He noted there is no drainage and there is the potential for that to become an issue. He expressed concern with the loss of some old growth trees that are directly a shade path from his house

and his patio, which is something he would like to conserve. Concluding Braun stated he does not have a problem with the progress and improvement to the neighborhood but with this case, it will unfortunately change everything about his property and might squeeze him out. The flooding is real, every spring he has a stream in his backyard. There is not a lot of drainage. Retaining walls and drainage tiles are great; however, reiterated he still believes there could be issues.

Scott Armstrong, 5844 Brookview Avenue, commented he purchased his mother's property and his concern was with the fifty-foot lots and over building. He pointed out originally when this neighborhood was established the fifty-foot lots were built for ramblers and now there are six-thousand-square foot homes on these same lots. He added one concern is with the kids using the walkup to the bus stop, adding if a new six thousand square foot house were built on Lot I that could create a wind tunnel. He noted he is directly below that. Concluding Armstrong stated he was also concerned with drainage and that some beautiful oak trees could be lost as a result of this subdivision.

Commissioner Bennett moved to close the public hearing. Commissioner Berube seconded the motion. The motion carried unanimously.

Discussion/Comments/Questions

- Commissioners stated the Planning Commission has seen a number of subdivision requests and the Commission has a certain set of principles that are adhered to as to how a proposed redevelopment might alter the essential character of a neighborhood. Commissioners noted that the subject property was not similar to the requests that were denied and for that reason it was not believed the subdivision would alter the essential character of the neighborhood. (majority of lots are 50 feet) It was noted that house "aesthetics" are not reviewed by the Planning Commission. The neighbors' comments are duly noted. Drainage is a concern to the neighbors, adding it might not hurt to add another drainage element on the lot(s) especially Lot I to help with drainage and further support the project.
- Commissioners asked for clarification that this is being approved with the variances and the Commission will not see this again. Teague indicated that was correct.
- Commissioners thought about the character of the neighborhood and this is the only lot that is larger than the others. The subdivision makes sense. There is concern about the trees and drainage.
- Commissioners thought if the property were sold without subdivision the concern of an overly
 large home could become a reality; adding there is an uncertainty either way. The subdivision of
 the lot is rational because it started out as two lots and two lot vs. one lot fits in with the rest of
 the neighborhood.
- Commissioners wondered if there was any consideration of having a six-foot setback between each home to honor that cumulative twelve feet (was that not discussed). Teague stated it was looked at and if it was ten feet on the south side it seemed reasonable to allow a little closer on the north side but because of the variances, if the Commission does not think the five foot separation is appropriate, there could then be a ten foot separation between the two and the variance could be modified or not grant the variance to the north lot line then there would need to be a ten foot setback.

- Commissioners concern was the more these homes are squeezed together the higher up the structure could go which is an aesthetic concern of the neighborhood.
- Commissioners indicated it was hard to understand the variances without knowing what was going to be built. A subdivision is a different standard of approval than a variance and those two could be separated. Teague stated the Commission could and if no variances were granted, lot two is essentially unbuildable, which would make it a little more challenging. The Commission would be approving a subdivision with a lot that is unbuildable.
- Commissioners thought there were issues that by subdividing situations could be created that impact the surrounding areas. Not every lot is necessarily buildable and here the City is helping the applicant create a buildable lot as part of the subdivision.
- Commissioners were not sure about approving the variances and leaving that for a later date in the process.
- Commissioners were supportive of the overall character of the neighborhood and the lot widths to the north and south, this seems to be a reasonable fit. Variances at this time are an unknown for approval at this time.
- Commissioners thought the neighbors brought up a lot of good points about drainage and trees.
 From what the Commission has been told, it sounds like drainage will be addressed and there will be no drainage onto neighbors' properties. Assuming that is true then the drainage issue has been addressed. The City needs a stronger tree ordinance, and this cannot be voted against because of the trees at this point. In terms of altering the character of the neighborhood, splitting the lot would perfectly fit the neighborhood. There is nothing unusual about it.
- Commissioners asked for clarification on the variances. There are three variances to be
 considered, lot area variances and setback variances for Lot I. Teague indicated that was correct.
 There is no setback variance for lot two, it is the two setback variances on the south lot.
- Commissioners asked why there were not any setback variances needed for lot two. Teague indicated the square that is drawn meets all of the City's setback requirements.
- Commissioners asked why a variance is needed for Lot I, why are different setbacks applied for
 the two lots. Teague stated it is because of the lot width. The south lot is eighty feet wide and
 requires greater setbacks compared to the fifty-foot lot. That is part of what staff is showing as a
 hardship. The City is applying eighty-foot-wide setback standards to essentially a fifty-foot-wide
 lot because of the drainage and utility easement.
- Commissioners were surprised how close the 5901 building is to the stairs. The City should look
 at that; however, was not sure what could be done at this time. Teague said in hindsight the City
 should have taken a larger easement on the south lot when the right of way was vacated. Teague
 noted the subject property has some redevelopment hardships.
- Commissioners wondered if there was any value in, or possibility or considered reasonable to
 have a lower height requirement on Lot I as a condition of approval to avoid any kind of 'wind
 tunnel" effect. Teague noted this lot is eighty feet in width and lot width determines building
 height. Teague noted as a condition of approval the Commission could recommend that building
 height on Lot I be limited to a lot width of fifty feet. That could be considered as a reasonable
 condition.
- Commissioners questioned if Engineering was comfortable with the drainage and storm water management plans for the properties. Teague indicated Engineering did not express any issues in

- conversations with him. Teague suggested that another condition of approval could be added requiring tie into the existing drain tile. Commissioners expressed support for that condition adding it would not exasperate any possible problems.
- Commissioners wondered how many trees would be impacted and removed. Teague stated
 trees would be removed and clarified per the City Tree Ordinance, any tree that is removed
 within the building footprint or driveways do not have to be replaced but any tree that is removed
 outside of those areas would have to be replaced.
- Commissioners thought it made sense to grant the variance for the five feet because there is no existing house to worry about the distance.
- Commissioners were comfortable with the variance request for Lot I after understanding that the requirement it is being held to is for an eighty-foot lot and in agreement if there is something that can be done regarding potential height it might be a fair trade off. Teague agreed.

Motion

Commissioner Berube moved that the Planning Commission recommend approval to the City Council of the subdivision with variances for 5841 Oaklawn Avenue as outlined in the staff memo subject to the conditions and findings therein in addition adding the stipulations that the height on Lot I would be as though it were a fifty-foot lot versus an eighty-foot lot and analysis and studies be done to reduce damage due to drainage and connect to the drain tile and eliminating the driveway to 59th Street. Commissioner Miranda seconded the motion. The motion carried unanimously.

VI. Community Comment

None.

VII. Reports/Recommendations

A. Sketch Plan Review - 7001 & 7025 France Avenue

Director Teague presented the sketch plan to redevelop the southeast corner of 70th and France (7001 and 7025 France Avenue). The applicant would tear down the existing 66,200 square foot office and bank and build a 5,500 square foot US Bank building with drive-through; 4,000 square foot retail building with drive-through; 7,000 square foot multi-tenant retail building; 120-unit market rate apartment (181,000 square feet); and, 80-unit affordable housing apartment (75,000 square feet).

Discussion/Comments/Questions

 Commissioners asked Teague if there were any challenges presented by the sub-soil with developing underground parking on this site. Teague indicated he was not aware but is often an issue. There is a relatively high-water table issue so guessed that could not go down too far with underground parking.

Appearing for the Applicant

Kevin Meyer, Corporate Real Estate at US Bank, was in support of the sketch plan submission for 70th and France Avenue. He introduced Rich Mariano, leading the Redevelopment Group. He stated together with Ryan Companies and RSP Architects they are excited to discuss the redevelopment proposed forward thinking redevelopment for the site, inspired by the Greater Southdale District Plan. He stated US Bank has a long-valued history at this location and feel proud to be a part of the Edina community. This is a big decision for the bank to make. The proposal is being made because they recognize it is the right thing to do and no longer need this size of a facility to support their customers and as a member of the Edina community, recognize that the property is a key parcel for the Greater Southdale District Plan. Knowing this, they have been seeking the right partner for this redevelopment for several years and really believe it has been found with Ryan Companies and the proposal before the Commission. However, the development does need to meet some key criteria for US Bank to make this worthwhile. US Bank Criteria is based around their purpose and core values at the bank. The development needs to really strike a balance between the four key constituents that US Bank serves. One is the customer, two is the colleagues, three is the community and four is the shareholders. For US Bank to support the development it must balance all four of these.

Meyer stated starting with the customer, the bank proposes to build a new branch on the southwest corner of the site to reinvest in the experience for the branch customers and also by sacrificing the corner location the development proposal allows US Bank to provide uninterrupted service to support the customers throughout the redevelopment. The colleagues will benefit through the new branch that provides the right employee experience for them to better support the community in their customer base. The community of Edina will benefit from a redevelopment of the US Bank parcel to a newer high-density mixed-use development neighborhood. The Branch will also contain a community room that will be able to be used for local events for charities, financial seminars that will be open for public use. The new branch, as proposed needs to maintain high visibility and easy access with the newest technology and key customer amenities to support the business. Drive thru and parking access is needed along with signage in order to stay competitive in the Edina market. US Bank believes this proposal strikes that balance well. US Bank feels that the sketch plan as presented is a win, win, win for the City of Edina, US Bank and Ryan Companies and looks forward to partnering with the City further to bring this vision to reality.

Student Commissioner Mangalick left the meeting.

Carl Ruggs, Ryan Companies reviewed some of the key themes of the proposed plan.

Director Teague reviewed with the Commission the requirements for a sketch plan review and to focus on the design experience guidelines, the big picture items and not getting down to the details of the project.

Discussion/Comments/Questions

- Commissioners indicated their overall general thought was that this was moving in the right direction; however, they would like to see more of everything (landscaping, connectivity, etc.), except parking. Commissioners noted the location of the subject property is great; one of the best in the Southdale area, but as presented this evening it does not quite live up to its potential. Commissioner encourage more engagement in the big quadrant and the meandering path throughout the project. The affordable housing is great, the crosswalk to the Galleria and community room and public space in general is wonderful; however they would like to see more of it.
- Commissioners wondered why in an area like this, where the City invites a developer to have more than one story on France, that there are not more stories. Staggered building height would allow US Bank to be better "showcased" and create more of a presence.
- Commissioners stated there is a lot of asphalt. Consider designing the meandering path to go both ways. Reduce asphalt. This is going in the right direction but needs more.
- Commissioners thought the project was exciting. Noting there are many great concepts in this proposal; reiterating some could be taken further. Commissioners indicated that the lack of a fifty-foot setback on France was somewhat of a concern. For the retail component, if more stories were added would that could offset the cost of adding underground parking and using the surface parking for more greenspace. Commissioners stated in their opinion the east/west pedestrian walkway seemed to be lacking in greenery compared to some of the conceptual drawings shared. Commissioners stressed that the applicant consider adding more greenspace along the walkways thereby creating a project that adheres more to the concepts stipulated in the Greater Southdale Area Plan. This would also create more interest.
- Commissioners thought the concept was unique and that in some ways works and matches what is in the Greater Southdale District Plan. However, viewing the elevation, do more to carry out the "vision" and have create something great on this corner pointing out it is a very busy corner.
- A lower scale could potentially work here; however it lacks something that ties the whole thing
 together. The City is looking for that greenspace. Commissioners reiterated that there pieces of
 everything here, some are great, but doesn't tie in together well.
- Commissioners expressed the opinion that the housing was in the right spot. In essence a street wall is being created which is what is in the Greater Southdale Area District Plan. The "Plan" is not all about the height and it is nice to see the progression and the housing in the back and not looming over the intersection.
- Commissioners commented that the apartment buildings themselves appear to resemble the Byerly's apartment buildings. Do more to make these buildings stand out there needs to be something added that creates high quality.
- Commissioners noted this is a great addition and great compliment to what was already there, acknowledging the plans as presented are a vast improvement; however, may not have gone far enough.
- Commissioners wondered what type of height was being considered and was more height ever considered, and if not, why. Ruggs stated they are not really looking at doing condominiums and for an apartment building they find this is the right solution. He noted that the project work seen

behind this was approved one year to two years ago and is still not launched. Ruggs said they felt this was the highest and best use and was something that works for empty nesters because of its walkability and connectivity.

- Commissioners wondered what other kind of guardrails are on this project. They pointed out there is retail only buildings on France Avenue that is lower, adding that type of retail experience already exists in the area. Commissioners questioned if there is there potential for a different use at this time or is this what Ryan Companies is focused on. Ruggs felt with urban type sites they have previously developed that small block retail is usually located underneath the building (s). Ruggs said the development team felt on this project residents would prefer to live to the east side of the site and not on top of France Avenue, adding they believe the retail component can survive and thrive better with its own pads. Ruggs stated they reviewed many options on the highest and best use of this site.
- Commissioners thought this is a catalyst type of site and is on a very prominent corner and
 whatever is done they want to see more "wow". The Commission asked the development team
 to consider moving the street closer to the 200 block. Commissioners stated the project as
 presented is moving in the right direction with retail, residential and public realm; however, more
 thought and intensity is required to fill in the gaps that exist in the present offering.
- Commissioners indicated the project appears to be done in more of a suburban style, especially from France Avenue. This is something the City is not looking for as proposed the project does not follow the guidelines stipulated in the Greater Southdale Area Plan. Commissioners indicated that it would be a mistake having low density on this side of the street to match the low density on the other side of the street. If built as presented that could leave France Avenue feeling very much like a highway (which is what the City is trying to get away from with the Southdale Guidelines). Anything that can be done would be great. There needs to be something much more substantial on France to make it feel like it is a comfortable place to be.
- Commissioners thought the potential for more pedestrian traffic due to the addition of the E-line in the future so limiting the parking would not be a big deal. There are other ways of getting people in and around the Southdale District. It was stressed that the future needs to be looked at now. The future for the whole area is going to be much denser with more transit. The France facing side feels completely wrong.
- Commissioners thought parking could be in a ramp with retail facing on each side rather than underground if the water table is too high. The project needs to be set back father from France Avenue. The sidewalks need to be wider. Any addition of a drive thru is a huge mistake. It is important for any kind of connectivity to build a regular pattern of where the external and internal roads are. Regular spacing is really important.
- Commissioners felt the project needs to be better than what is currently there.
- Commissioners stated it is exciting to see Ryan, US Bank and RSP come forward with this project. The project as presented in the opinion of some Commissioners was not that impressive and looks like one big development with some sidewalks and drive-thru's going through it, not four separate quadrants. Commissioners indicated they would like to see something that is much more of a 4-unit grid. It was acknowledged that what was presented was an improvement over the current state, noting that five years ago, this would have been a great project but five years

from now the City would feel bad that it did not ask for something closer to the design guidelines laid out in the Greater Southdale Area Plan.

- Commissioners thought there was a difference with the development across the street because those properties are closer to residential, adding the proximity to residential created unique issues for developments on the other side of the street. The size of the lots were bigger challenges unique to that site.
- Commissioners wondered if parking could be done underground. Meyer indicated parking is underground for the residential apartment building. For retail it is harder and they thought this was a good move because it is screened. Meyer stressed that convenience parking is essential because retail tenants would not want to sign a lease if there is no available parking.
- Commissioners would like to have this plan proceed; however are looking for better design that
 adheres to the guidelines established in the Greater Southdale Area Plan. They suggested that the
 development team review the Memo from Mic Johnson along with the feedback from the
 Commission and staff.
- Commissioners indicated they want to see a fifty-foot setback from France Avenue. Do not
 match what is currently there or across the street from this. Commissioners indicated they liked
 seeing the opening onto France Avenue from the inside. Commissioners suggested more density
 with height and more public open space. Make it cohesive from within with the ability to connect
 to other areas. Reconsider parking with less open parking areas. Would like to see this
 developed as one whole site.

Chair Olsen and the Commission thanked the applicants for their presentation. Adding they look forward to seeing the development team again.

VIII. Correspondence and Petitions

None.

IX. Chair and Member Comments

Commissioner Bennett stated he sent an email to everyone about checking out the Master Plan. He thought it was a reminder to be open and accepting of bold, creative things because it is doable and could be done in Edina. He noted the Master Plan is really inspiring. Commissioner Berube agreed and brought to mind the potential for the Hennepin County Library site because that is the equivalent of four large square blocks and will be empty. That might be a really good space to do something innovative that is really planned that can be really stellar if done right.

Commissioner Miranda stated he was a huge fan of technology but was not a fan of the Toronto project at all. The main reason is privacy and that is huge. All of the platforms and business models have huge implications on politics, society and privacy. Going gung-ho on this is a mistake. He thought they needed to be really careful on how they do this even though he loved technology and what it can do.

Draft	$Minutes \square$
Approved	M inutes \boxtimes
Approved Date:	, 2019

Commissioner Bennett thought what was cool was a lot of the thinking was done outside of the box. He stated there is technology that does not harm and was a good way to see how you can present something pretty complicated and he thought Edina had a lot of great opportunity with great partners. He thought if they owned a part of the project development could be great.

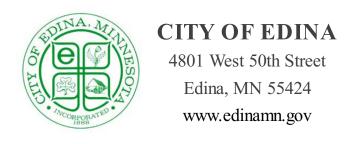
X. Staff Comments

None.

XI. Adjournment

Commissioner Douglas moved to adjourn the June 26, 2019, Meeting of the Edina Planning Commission at 10:33 PM. Commissioner Bennett seconded the motion. The motion carried unanimously.

Respectfully submitted Jackie Hoogenakker, TimeSaver



Date: July 10, 2019 **Agenda Item #**: V.A.

To: Planning Commission Item Type:

Report and Recommendation

From: Cary Teague, Community Development Director

Item Activity:

Action

Subject: Site Plan Review & Setback Variance - 6950 France

Avenue

ACTION REQUESTED:

Recommend the City Council approve the request, subject to the findings and conditions in the staff report.

INTRODUCTION:

The applicant, Luigi Bernardi, is proposing to tear down the existing 28,000 square foot office building at 6950 France Avenue, and build a new 10,000 square-foot retail building with surface parking in the rear. The request requires a Site Plan Review with a side street setback Variance from 35 feet to 16 feet from an unimproved right-of-way.

ATTACHMENTS:

Staff Report

Site Location and Southdale District Plans

Revised Engineering Memo

Traffic & Parking Study

Building Official Memo

Review from Mic Johnson, AFO

Building Rendering

Proposed Plans

Colored Site Plan/Lanscape Plan

Existing & Proposed Buildings

Building Materials

Applicant Narrative

Sustainability Questionnaire

STAFF REPORT



Date:

July 10, 2019

To:

Planning Commission

From:

Cary Teague, Community Development Director

Subject: Site Plan Review and Setback Variance – 6950 France Avenue

Information / Background:

The applicant, Luigi Bernardi, is proposing to tear down the existing 28,000 square foot office building at 6950 France Avenue, and build a new 10,000 square-foot retail building with surface parking in the rear. The request requires a Site Plan Review with a side street setback Variance from 35 feet to 16 feet from an unimproved right-of-way.

The proposed building would have three entrances to the retail space that face both France Avenue and the parking lot to the west. The building would be set 50 feet back from the paved portion of France Avenue per the Southdale District Experience Guidelines. Landscaping, stormwater features, pedestrian plaza zones with bench seating, decorative lighting and plantings are planned within the 50 foot area. (See attached plans.)

The site is relatively small at I acre. The site plan demonstrates 62 parking stalls, I I of which would be proof-of-parking. The future west promenade (suggested in the Southdale District Plan) could be provided for in the future along the west lot line. The building has been designed with large windows on all four elevations. Mic Johnson, AFO, has provided a review of the proposed project. (See attached.)

SUPPORTING INFORMATION

Surrounding Land Uses

Northerly: Bremer Bank; zoned POD, Planned Office District and guided CAC, Community

Activity Center.

The Galleria (retail shops); zoned PCD-3, Planned Commercial District-3 and Easterly:

guided CAC, Community Activity Center.

Southerly: The Vitamin Shop and Ameritrade: zoned PUD, Planned Unit Development and

guided CAC, Community Activity Center.

Westerly: Daycare; zoned PCD-3, Planned Commercial District-3 and guided CAC,

Community Activity Center.

Existing Site Features

The subject property is I acre in size, and contains a three story office building and surface parking lot.

Planning

Guide Plan designation: CAC, Community Activity Center Zoning: PCD-3, Planned Commercial District-3

Site Circulation/Access/Traffic

Primary access points to the site would remain on France Avenue. There is access also available to the site from 70th Street through existing access easements over the Kinderberry Hill Daycare site to the west.

Spack Consulting conducted a traffic study. The study concludes that the existing roadways can be supported by the project. The level of service at adjacent intersections would not be impacted. The use would generate a very slight increase in trips to the site, 230 per day with fewer am peak trips and 16 additional peak pm trips. (See attached study.)

Parking

Based on the City Code requirement, a total of 62 parking stalls would be required (see table below). The proposed plans indicate 51 parking stalls on the site, with proof of parking for an additional 11 spaces if needed. Spack also conducted a parking study, which concludes that the 51 stalls provided should be adequate for the site.

Landscaping

Based on the perimeter of the site, 21 over-story trees would be required. There are 22 existing and proposed over-story trees on the site. A full complement of understory shrubs and plants would be provided around and in front of the building. (See attached landscape plan.)

Building Design

The building materials would be European Limestone with large energy efficient glass showroom windows for the tenants on all four sides. The roof height would be 20 feet tall.

Grading/Drainage/Utilities

The city engineer has reviewed the proposed plans and found them to be acceptable subject to the comments and conditions outlined in the attached memo. Any approvals of this project would be subject to review and approval of the Nine Mile Creek Watershed District, as they are the City's review authority over the grading of the site.

Mechanical Equipment

Any rooftop and/or ground level equipment would have to be screened if visible from adjacent property lines.

Compliance Table

	City Standard (PCD-3)	Proposed Lot line Street
Building Setbacks Front – France Avenue Rear – West Side – North Side – South	35 feet 35 feet 35 feet 35 feet	40 feet 50 feet 50+ feet 16 feet* 35 feet
Building Height	4 stories & 48 feet	I story & 20 feet
Floor Area Ratio (FAR)	.75	.23
Parking	62	62 spaces (11 in proof of parking)

^{*}Variance Requested

Variance

Per the Zoning Ordinance, a variance should not be granted unless it is found that the enforcement of the ordinance would cause practical difficulties in complying with the Zoning Ordinance and that the use is reasonable. As demonstrated below, staff believes the proposal does meet the variance standards, when applying the three conditions:

Minnesota Statues and Edina Ordinances require that the following conditions must be satisfied affirmatively. The Proposed Variance will:

• Relieve practical difficulties that prevent a reasonable use from complying with ordinance requirements.

Reasonable use does not mean that the applicant must show the land cannot be put to any reasonable use without the variance. Rather, the applicant must show that there are practical difficulties in complying with the code and that the proposed use is reasonable. "Practical difficulties" may include functional and aesthetic concerns.

Practical difficulties include the small size of the site and a right-of-way that dead ends 2/3 of through the subject property. The City does not have plans to improve this right-of-way at this time; however, in the future it could be used as a pedestrian/vehicle (woonerf) to provide an east-west connection through this block per the Greater Southdale District Plan. (See attached.) If the City were to vacate this right-of-way, half of the property would be dedicated to the subject property, and the required setback would be met.

The existing structure on the site is nonconforming with side yard setbacks of 14 feet to the south lot line and 24 feet to the north; therefore the existing building is nonconforming. The cumulative side yard setbacks of the existing building are 38 feet. The cumulative setbacks for the proposed building would be 51 feet. The result would be greater separation and green space between buildings than exists today. (See attached existing and proposed building graphic.)

There would no impact to the property to the north, as the separation between the two buildings would be over 100 feet.

2) There are circumstances that are unique to the property, not common to every similarly zoned property, and that are not self-created?

Yes. The circumstances of the existing unimproved right-of-way to the north are unique to the area and the PCD-3 zoning district.

3) Will the variance alter the essential character of the neighborhood?

No. The proposed building would not alter the character of the neighborhood. The new building is actually more narrow than the existing building, which would give the appearance of wider side yard setbacks.

PRIMARY ISSUE/STAFF RECOMMENDATION

Primary Issue

Is the proposed site plan with the front yard setback variance reasonable?

Yes. Staff believes the proposed plan and variance is reasonable for the following reasons:

1. As highlighted above, the variance criteria are met.

2. Practical difficulties include the small size of the site and a right-of-way that dead ends 2/3 of through the subject property. If the City were to vacate this right-of-way, half of the property would be dedicated to the subject property, and the required setback would be met.

- 3. The City does not have plans to improve this right-of-way at this time; however, in the future it could be used as a pedestrian/vehicle (woonerf) to provide an east-west connection through this block per the Greater Southdale District Plan.
- 4. The existing structure on the site is nonconforming with side yard setbacks of 14 feet to the south lot line and 24 feet to the north; therefore the existing building is nonconforming. The cumulative side yard setbacks of the existing building are 38 feet. The cumulative setbacks for the proposed building would be 51 feet. The result would be greater separation and green space between buildings than exists today. (See attached existing and proposed building graphic.)
- 5. There would no impact to the property to the north, as the separation between the two buildings would be over 100 feet.

Staff Recommendation

Recommend that the City Council approve the site plan review and with a side street setback Variance from 35 feet to 16 feet from an unimproved right-of-way.

Approval is based on the following findings:

- 1. The proposal would meet the required standards and ordinances for a Site Plan with the exception of the setback variance.
- 2. The findings for a variance are met.
- 3. Practical difficulties include the small size of the site and a right-of-way that dead ends 2/3 of through the subject property. If the City were to vacate this right-of-way, half of the property would be dedicated to the subject property, and the required setback would be met.
- 4. The City does not have plans to improve this right-of-way at this time; however, in the future it could be used as a pedestrian/vehicle (woonerf) to provide an east-west connection through this block per the Greater Southdale District Plan.
- 5. The existing structure on the site is nonconforming with side yard setbacks of 14 feet to the south lot line and 24 feet to the north; therefore the existing building is nonconforming. The cumulative side yard setbacks of the existing building are 38 feet. The cumulative setbacks for the proposed building would be 51 feet. The result would be greater

separation and green space between buildings than exists today. (See attached existing and proposed building graphic.)

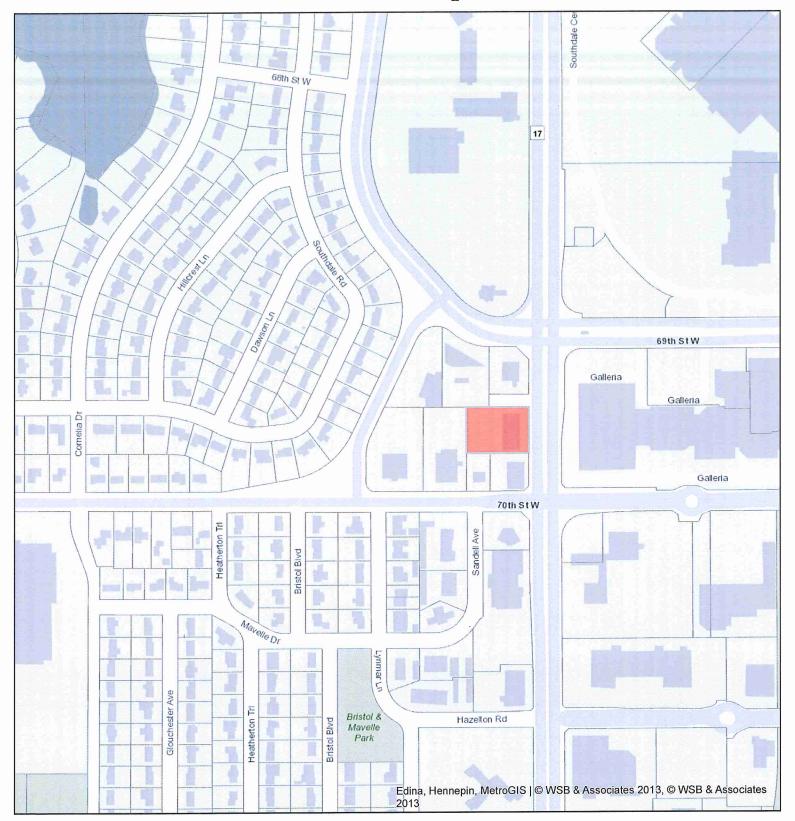
6. There would no impact to the property to the north, as the separation between the two buildings would be over 100 feet.

Approval of the site plan & variance is subject to the following conditions:

- I. Subject to staff approval, the site must be developed and maintained in substantial conformance with the following plans, unless modified by the conditions below:
 - Site plan date stamped May 31, 2018.
 - Grading plan date stamped May 31, 20187.
 - Landscaping plan date stamped May 31, 2018.
 - Building elevations date stamped May 31, 2018.
- 2. Prior the issuance of a building permit, a final landscape plan must be submitted, subject to staff approval. Landscape plan must meet all minimum Zoning Ordinance requirements. The trees on the east side of the site shall be preserved if possible during construction.
- 3. The property owner is responsible for replacing any required landscaping that dies.
- 4. Submit a copy of the Nine Mile Creek Watershed District permit. The City may require revisions to the approved plans to meet the district's requirements.
- 5. Compliance with the conditions required by the city engineer in his memo dated July 3, 2019.
- 6. Building plans are subject to review and approval of the fire marshal at the time of building permit.

Deadline for a city decision: October 1, 2019

Location Map



1 in = 376 ft





May 14, 2019 Map Powered by DataLink

Location Map



1 in = 376 ft





May 14, 2019 Map Powered by DataLink

Zoning Map



1 in = 376 ft





APD

PCD-4



May 14, 2019 Map Powered by DataLink

Greater
Southdale
District Plan



Chapter 3
District Profile

building, a one-acre indoor public park, all in a superblock with few internal streets, but with many pedestrian pathways. In the late 1980s, Centennial Lakes was developed on a 100-acre site, also with a pedestrian focus. The focal point is a 25-acre park with a 10-acre lake, integrating a mix of moderate-to-high density office, residential, entertainment, and retail uses.

These two innovative and nationally-recognized developments are prologue to the future evolution of the Greater Southdale District,



not only for their innovation in development and site planning, but perhaps more so for their innovative public-private partnerships to achieve community benefits while responding to market realities.

The Greater Southdale District Today

The existing land pattern comprises primarily superblocks with an assortment of uses – mostly multifamily residential, commercial office and retail, and office, in separate buildings. In a few areas there are newer multi-level buildings with a mix of uses, such as ground floor retail in multifamily residential buildings, reflecting the priorities of the previous comprehensive plan to create more interaction between uses. However, the predominant pattern is still auto oriented in terms of layout and scale, limiting bicycle and pedestrian circulation and activation of the street.

The Planning Process: How we got here.

In the years following adoption of the 2008 Comprehensive Plan, it became apparent that the City's planning policies did not provide adequate direction for the evolution of the Greater Southdale District. People desired some degree of certainty and were not finding it. In 2015, as a result of a redevelopment proposal at 7200 France Avenue, City Council requested a Work Group be created to develop a tool for the Planning Commission and City Council to use when evaluating Comprehensive Plan amendments and physical changes in the Greater Southdale area. The Work Group initially considered issues related to future land use and urban design for parcels along the west side of France Avenue. As the Work Group deliberated on the issues and influences within the primary study area, they also looked beyond, first to whole of the Greater Southdale area, and, to better understand the neighborhood to the west and traffic patterns, and they studied the corridor westward along 76th Street and 77th Street to determine how it might be aligned with the potential of the Greater Southdale District.

Using the citywide Vision Edina, developed in 2015, and other citywide policy documents as a foundation, the Greater Southdale Work Group created an in-depth vision for the future of the Greater Southdale District. Their process included first preparing Working Principles and Supporting Questions to shape the dialogue about how proposals for change might fit the District.



Later, with the assistance of design consultants, the

Work Group produced **Re-Visioning the Southdale District**, a graphic vision for preserving, energizing, and adding development in the area. With its comprehensive approach to placemaking, this document demonstrated the many ways land parcels and development might interact. **Re-Visioning the Southdale**

District was added to the guiding references for preparing and reviewing development proposals. Out of these explorations came **Design Experience Guidelines** which organize and explain the facets of district-wide structure and development-specific urban design considerations. They stress the importance of experience as a product of the form and scale of the public realm and adjoining buildings.

The Work Group translated the design framework into the **Greater Southdale District Plan**, addressing the broader needs of people of different life stages, income levels, and abilities, and the economic opportunities inherent in compact and more complete communities.

During the three-to-four-year planning process, the Greater Southdale Work Group comprising primarily residents, but also including business/property owner interests, most of whom are residents of Edina, met twice a month, sometimes more frequently, to research, explore, deliberate, propose, refine, and recommend directions. All their meetings and work sessions were open to the public and included a public comment period. In addition, the Work Group delivered many progress reports to the Planning Commission and City Council. There were also two Open Houses that were attended by dozens and dozens of citizens and business people.





There was general consensus regarding the need to shape and manage the growth and changes in the Greater Southdale District, including the concept of placing additional development densities and mixed land uses close to transit, as well as the emphasis on a high-quality public realm and private development. Concern was expressed by some residents regarding the specific locations and types of development along the transitions in the areas west of France Avenue. At the same time, others were concerned that the proposed **District Plan** needed to be more assertive in seeking change to adequately address potential increases in traffic and parking demand and housing choice. Housing affordability was also identified as a concern or an objective.

Vision and Aspirations

The Work Group prepared a Vision Statement as well as their Aspirations for transforming the Greater Southdale District with expanded public parks and publicly-accessible gathering opportunities, community services and facilities, cultural and entertainment venues, a well-designed mix of housing types and affordability-choices, and expanded employment and shopping opportunities.

The Vision Statement and Aspirations are not just an introduction to the **District Plan**. They are an articulation of the values that give direction to the **District Plan's** goals and policies. These declarations illustrate that the **District Plan** is not so much a blueprint, but rather a compass. The **District Plan** is a guide to evaluating options, for making choices and decisions, and for taking actions on future change in the Greater Southdale District.

The Vision Statement and Aspirations state: "This is what we want." The key organizing statements of the Work Group's Vision Statement are:

- We envision a vibrant, forward-looking and people-filled Greater Southdale District, organized around dynamic streets, engaging parks and public spaces, and well-conceived and enduring buildings.
- We welcome change on our terms.
- We envision innovation leading to extraordinary places and experiences.
- We're embarking on 50 years of well-paced steps, with each one more clearly blazing the path toward the future of the district.

The eleven Aspirations in the **District Plan** elaborate on and explain more fully the desired future experience for the District.

Renew and Repurpose: Goal and Policy Redevelopment Framework

What will the Greater Southdale District be like in 10 years, 20 years, the next several decades? The Greater Southdale District is the largest mixed-use area in Edina. The District includes a wide range of office/employment options, destination and specialty retailing, restaurants, entertainment/ hospitality facilities, major medical/health care services and facilities, a significant array of housing choices, and high- quality green spaces, most notably in the Centennial Lakes development and the Edina Promenade. The recent decade has seen a dramatic increase in development activity, mostly in the residential sector, but also in new commercial-retailing, entertainment/hospitality facilities, and medical/health care services, primarily in locations dominated by large parking lots. This infilling has been characterized by multi-level buildings with parking structures, many poorly-designed and limiting the potential for an active and pedestrian-oriented street.

As the Twin Cities continue to grow in population, Edina's Greater Southdale District is expected to continue to be a major focus for accommodating this growth. Edina's population is growing as well, with interest by young and old alike, individuals and families in being in active urban environments with a high-quality public realm, a concentration of services and amenities, and a diversity of housing types, tenures,

and affordability. Additionally, Edina's continued aging of its own population will bring increased development pressures to the district as these residents choose to leave their home but not their community. The development community is responding with new apartments for young singles and couples and with new senior and assisted living facilities near medical and other community services.







Contemporary expression of building forms, coordinated with landscape improvements, create attractive transitions from private to public realms.

The Greater Southdale District Plan proposes to use this new growth to help expand access to a great living environment, access to employment, and access to improved and expanded social and physical infrastructure. The District Plan provides a framework for decision-making by the Edina community to continue to accommodate this market demand and development interest by building on past successes. Moreover, this District Plan integrates social, economic, environmental, and urban design perspectives into that decision-making process, to enable the Edina community to make sustainable choices about the changing Greater Southdale District.

Chapter 4: Renew and Repurpose: Goals and Policy Redevelopment Framework describes core elements of the District's evolution and is organized into eight sections. While the eight sections are devoted to specific topics, it is a comprehensive and cohesive whole. In successful community building, everything is connected to everything. Together, these eight sections convey guidance for change and growth in the Greater Southdale District, to facilitate progress toward an environmentally and economically sustainable future. Together, they provide the basis for informed choices and decision-making by elected and appointed City officials and community stakeholders to facilitate the evolution of the District aligned with the vision and aspirations articulated in this District Plan.

- 4.1 Economic Vitality and Competitiveness
- 4.2 Urban Design
- 4.3 Land Use
- 4.4 Transportation and Mobility
- 4.5 Parks and Public Life
- 4.6 District Services and Facilities
- 4.7 Sustainability
- 4.8 Water Resources

Following are the Goals, or desired outcomes, for each of these District Plan sections. The District Plan also includes Policies for each of these Goals which are more specific directives to achieve the desired outcomes.

4.1 Economic Vitality and Competitiveness

Economic vitality and competitiveness do not mean only business and job growth. True prosperity, i.e. a strong, sustainable, and resilient economy, is rooted in building on existing assets and business clusters, increasing opportunities for living-wage employment, and integrating that investment energy and focus into built and natural environments. The Greater Southdale District has an enviable past as a tremendously vibrant and successful suburban mixed-used district. However, the future requires a different model of renewal and investment, one based on intensity of activity and accommodation of movement other than personal passenger vehicles.



Unique Experiences

Economic Vitality Goal #1: Offer unique experiences for living, playing, working, and learning, and memorable public places for civic and social gathering for multiple generations and diverse populations.

Influence of Trends

Economic Vitality Goal # 2: Respond to the significant forces and trends influencing the future of workforce and workplace, technology, retail, housing, and transportation.

Health Care

Economic Vitality Goal #3: Retain health care, medical facilities, and medical technology as primary activities and will, where possible, expand those facilities to best serve the community and the region.

Learning and Entertainment

Economic Vitality Goal #4: Include learning and entertainment activities that catalyze new development and accommodate interests of a more diverse district and community population.

Mobility

Economic Vitality Goal #5: Offer mobility hubs connecting to worker populations and providing more robust connections within the district, i.e. embrace improved transit as part of mobility modes as a competitive advantage.

Economic Engine

Economic Vitality Goal #6: Enhance the Greater Southdale District's significance to the Edina community as a center of jobs, retail opportunities and other services, and importantly, as a vital part of the city's tax base.

4.2 Urban Design

Community building involves balancing social, economic, and environmental needs and priorities. Great communities are designed and orchestrated so that individual private and public developments work together to create cohesive blocks, neighborhoods, districts, and memorable places. Urban design direction, based on continual improvement of the daily experience for residents, workforce, and visitors, produces higher quality buildings that inspire, as well as parks, public

The Work Group prepared the following Vision Statement as well as their Aspirations for transforming the Greater Southdale District with expanded public parks and publicly-accessible gathering opportunities, community services and facilities, cultural and entertainment venues, a well-designed mix of housing types and affordability-choices, and expanded employment and shopping opportunities.

The Vision Statement and Aspirations are not just an introduction to the **District Plan**. They are an articulation of the values that give direction to the **District Plan's** goals and policies. These declarations illustrate that the **District Plan** is not so much a blueprint, but rather a compass. The **District Plan** is a guide to evaluating options, for making choices and decisions, and for taking actions on future change in the Greater Southdale District.

With the Vision Statement and Aspirations, the Greater Southdale Work Group is saying: "This is what we want."

Greater Southdale District Vision Statement

We envision a vibrant, forward-looking and people-filled Greater Southdale District, organized around dynamic streets, engaging parks and public spaces, and well-conceived and enduring buildings. We anticipate transformation through a 50-year arc of innovation, collaboration, and new introductions, expanding on ideas that set a path of invention more than 50 years ago when the Daytons-first imagined a shopping mall as a center for the community. In all of this, we see that the best measure of success will be experiences people share in the Greater Southdale District.

We welcome change on our terms. Our vision shapes inevitable growth through a healthy mix of uses, intensities that fill public spaces with human activity, and compatibly scaled buildings, streets, and transitions. We imagine new promenades that bridge nearby neighborhoods with the life of the district, where those seams become places of lasting value. We imagine beautiful outdoor spaces for play, socializing, and respite while accommodating the functional needs of new development.

We envision innovation leading to extraordinary places and experiences. As new development is introduced, we recognize that new investment must be matched by efficient, progressive, and sometimes pioneering district infrastructure. We imagine new civic facilities throughout the district, drawing together the whole of the Edina community and becoming special focal points for the new neighborhoods of the district. And when we collaborate to create these places and experiences, benefits are directed to the community and to our partners so that, together, we all succeed.

We're embarking on 50 years of well-paced steps, with each one more clearly blazing the path toward the future of the district. We aim to achieve evolution through guidance that delivers community benefits as a natural outgrowth of great private development. We imagine a new way of considering "fit" that starts with people and the spaces they will occupy. Our vision requires imagination, forethought, iteration, not a small amount of courage, and truly constructive dialog. But it's only through those actions that we might fully realize an evolution perpetuating the vision that sprung from a farm field to become an Edina icon.

Aspirations

Aspiration #1

Imagine Greater Southdale District evolution organized around human activity, with vibrant pedestrian-focused streets, beautiful parks and public spaces, and endearing and enduring buildings where:

- A sense of invention is expected from new introductions, both public and private, that build on the district's spirit of innovation.
- Its role as regional and local center for living, shopping, working, learning, entertainment, hospitality, and medical services is enhanced.
- Other Edina neighborhoods, near and distant, benefit from investment in the district and the evolution of each parcel.
- Investment in the public realm is reflected by a commensurate investment as private parcels evolve.
- Public and civic services accommodate a growing and diverse district and community population.
- Transitions at the district's edges recognize compatible use and scale and neighboring uses are perpetuated on their terms.



A walkable public realm is based on the shape and size of the spaces between buildings, and creates the place where civic life plays out.

Aspiration #2

Make the Greater Southdale District the model of healthy urban living where:

- The district's form encourages healthy living habits, particularly through walking.
- The design of buildings and spaces, both public and private, attract the widest possible range of the district's population.
- Storm water is a valued resource by making it part of the experience of the district.

- Emissions and pollutants are mitigated through the introduction of significant tree canopy and limiting idling vehicles on streets, creating a more inviting walking experience along the district's streets.
- Sustainable solutions result in a stock of healthy buildings that compel healthy activity for their occupants.
- Public features mitigate impacts of non-local infrastructure, especially to contain the ill effects of adjacent highways.

Aspiration #3

Invent sustainable infrastructure matching the district's sense of innovation where:

- Mutually-supportive and forward-looking infrastructure is the norm, looking beyond baseline utilitarian functions of a single site to create a broadly supportive district infrastructure.
- Infrastructure aligns with the creation of public space in the district, sharing space and resources that result in compelling, attractive and high-functioning civic spaces.
- Care for and perpetuation of public infrastructure anticipates daily human activity in all seasons.



The design of this stormwater collection and treatment facility in Lincoln, Nebraska, provides beauty and landscape for public enjoyment.

Aspiration #4

Create neighborhoods of activity within the broader mixed-use patterns of the district where:

- Logical boundaries based on reasonable walking distances are established, with major streets as seams binding the activity of each side into an inviting and accessible public space.
- Focal points of public activity are found within each neighborhood.
- Key community services and facilities are present and help define the fabric of the District.
- Core services are delivered within each neighborhood or in an adjacent neighborhood.
- Neighborhoods are linked along street and park corridors highlighted by visible human activity.

Offer a spectrum of living opportunities integrated through the district where:

- Housing orients to a variety of income levels and household types.
- Ownership options constitute a significant portion of the living opportunities in the area.
- "Missing Middle" living opportunities (duplexes, triplexes, side-by-side or stacked townhouses, rowhouses with multiple units, and small buildings with four to six apartments) allow a broader range of Edina residents, workforce members and others to consider relocating to the District.
- Buildings for living strongly orient to the public spaces of each neighborhood within the District.

Aspiration #6

Expand significantly the number and extent of parks and public spaces where:

- Opportunities for the introduction of another large signature public space complement the programming and activities available at Centennial Lakes.
- An extension of the Promenade to Strachauer Park links neighbors and activity to the district.
- New promenades on the East and West edges of the District create movement corridors for pedestrians and bicyclists and serve as vital places for a transition between neighborhoods and the District.
- Parks and publicly accessible spaces are clearly visible and directly accessible from the public realm.
- Spaces for visible human activity and occupation, either public or publicly accessible, occur on every block.



This mid-block connection in Vancouver, Canada, includes an innovative footpath and bench design to create a mini-park.

Encourage district evolution based on incremental change and the creation of a great pedestrian experience where:

- A basic framework of streets and blocks encourages pedestrian activity and well-considered buildings.
- A rich variety of public or publicly accessible spaces are woven into the experience of the district.
- Sites and buildings support a pedestrian experience first, with storage of cars considered
- Development on each site links to adjacent streets and to neighboring sites to create continuous, safe, and inviting pedestrian experiences.

Aspiration #8

Build (or plan for) a street network encouraging pedestrian movement across and through the district where:

- Walkable block lengths are the baseline framework for development.
- Enhanced and more frequent street crossings facilitates pedestrian movement.
- Wide landscaped boulevards encourage pedestrian activity and create a distinct district signature.
- Community corridors within and extending well beyond the district enhance bicycle and pedestrian access while accommodating vehicle traffic on pedestrian terms.



A woonerf, or low-speed passage for cars, bikes, and pedestrians, provides a safe shared environment for movement.

Aspiration #9

Imagine transportation in the district where:

- Cars are not the focus and streets accommodate more than vehicles.
- Major streets balance access and mobility.
- Some streets serve as community corridors, linking to other community destinations with features that allow for movement in addition to cars.
- Transit is a baseline service, both within the district and to non-Edina locations.
- Transportation recognizes trends, including autonomous vehicles and a time when parking structures aren't needed for public parking

Expect the delivery of high quality, well-designed buildings and sites where:

- Spaces on sites are considered for people first, including connections between sites; then the ways structures are placed; and then places to store cars are found.
- Visible human activity is prominent and integrated at every site.
- People are brought to the streets via major building entries oriented to major streets.
- Storm water remains visible as an amenity, allowing it to become a central part of the experience of each site.





Well-designed sidewalks and entryways provide a seamless welcoming and safe environment.

Frame development guidance for evolution where:

- Development review includes the desired experience, not solely quantitative thresholds.
- Accommodation of adjacent and near parcels are considered in the evolution of a single parcel.
- Early reviews focus on ideas, patterns and relationships, not specific and engineered plans, with that part of the review process based in dialog, not presentation and reaction.
- Demonstrations of quality and especially quality from a long-term perspective are baseline considerations.
- Collaboration leads to a superior result, with the community's expectations clearly framed as part of the deliberation.
- Flexibility is not a right, but rather the natural by-product of a fair exchange for benefits, collaboration, and quality in development.







Dialogue, a sketch plan, and consideration for context is the foundation for a more transparent and collaborative development review process.

3. District Profile

Overview

To be relevant and meaningful, the vision for the Greater Southdale District must address the current realities of conditions in the district – and what is forecasted to change. The purpose of this chapter is to provide an overview of these existing conditions and forecasted trends that impact the future of the Greater Southdale District. This includes:

- Description of the study area and summary of building and land use conditions (more detailed analysis of these is included in the Land Use section)
- Summary of past planning efforts and policies in Edina with implications for the Greater Southdale District
- Demographic summary of current and forecasted population
- Economic summary of the district (more detailed analysis is included in the Economic Competitiveness section)
- Summary of trends and challenges impacting the Greater Southdale District.

Introduction

The Greater Southdale District plays a unique role in the City of Edina and Twin Cities region.

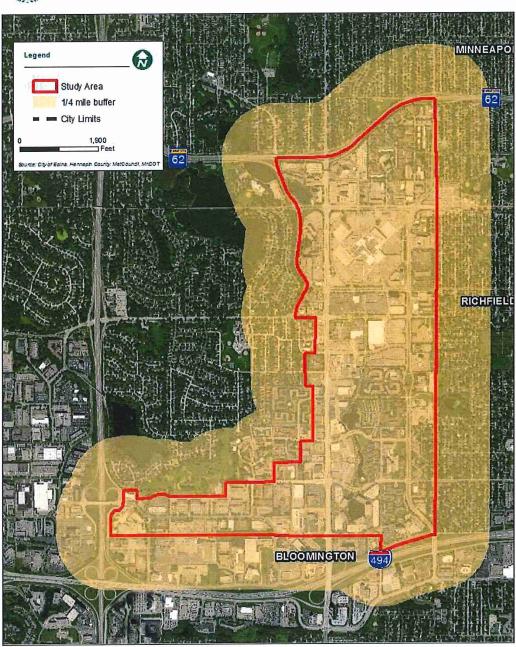
- Concentration of jobs, residences, traffic, and activity. Attracting residents, workers, customers, patients, visitors, and others from throughout the region, Greater Southdale District is a major destination.
- Role as economic engine for the city and region. The economic impact of this area is significant, particularly in terms of sustaining the tax base for the City of Edina.
- Meeting diverse housing needs of the population. Greater Southdale District has a variety of diverse housing types that meet the needs of Edina residents, and are not generally available in many areas of the city.
- Retail and services hub for the community. In addition to its role in the regional economy, Greater Southdale District meets the needs of the community for retail and services, with the capacity to evolve for changing preferences.
- Capacity for growth and change. The Greater Southdale District has been an evolving area since its inception. It has more capacity for growth and change than many other areas of the city.

Study Area

The Greater Southdale study area covers more than 850 acres of land, including the 76th Street/77th Street corridor extending westward towards Highway 100 which was added later in the Work Group study process. The Greater Southdale District itself is bordered on the west by development along France Avenue South, on the east by Xerxes Avenue South, on the north by MN Highway 62, and on the south by Interstate 494 and Minnesota Drive. This area is known throughout the region as a shopping and employment destination, with a concentration of retail and office uses. In addition, there are multiple apartment buildings, condominiums, senior living facilities and other residential and mixed-use development. **Figure 3.1** shows the study area.

Figure 3.1: Study Area for Greater Southdale District Plan





Existing Plans, Policies, and Studies

Edina Comprehensive Plan (2008)

The 2008 Comprehensive Plan serves as the city's primary policy document guiding future growth and change, until replaced by the 2018 update. The 2008 plan initiated the process to complete small area plans for targeted areas for growth and change, including part of the Greater Southdale District. The plan also established goals for affordable housing, pedestrian and bicycle networks, and park renovations. It discussed transit potential in the city, with many options impacting the Greater Southdale District, given the high concentration of housing and businesses. These goals and ideas are part of ongoing discussions as part of the 2018 Comprehensive Plan update and the Greater Southdale District Plan.

Living Streets Plan (2015)

The City's Living Streets Plan provides guidance on street design, traffic calming, bike facilities, landscaping, and lighting. It also includes best practices for community engagement during a street design process. Based on road classification, it sets standards for road cross sections, multimodal facilities, and design features. The intent is to provide for multiple modes of transportation, reduce environmental impacts, and to focus on quality of life aspects and community identity. Streets should be safer, healthier, provide more choices, and have economic and environmental benefits. This applies to virtually all Greater Southdale District streets.

Affordable Housing Policy (2015)

Edina passed an affordable housing policy in 2015, focused on maintaining a diverse population base and providing housing choices for people living and working in the city. It set in place requirements on providing affordable units as part of multi-family developments with 20 more units requiring rezoning or a comprehensive plan amendment. The requirement was set at 10% of livable area within a residential development being classified as affordable for a specified period of time, though it is possible to waive the policy if another agreed-upon benefit to affordable housing (such as a fee in lieu) is provided instead. Although goals for affordable housing apply citywide, opportunities for large scale (20+ unit) developments are primarily in change areas such as the Greater Southdale District.

The Affordable Housing Policy contains additional requirements, including specific income limits for rental housing and maximum sales prices for ownership housing. For rental housing, both incomes (adjusted for family size) and rental rates (adjusted for bedroom count and including utilities) are updated annually by the Minnesota Housing Finance Agency (MHFA) and published at mnhousing.gov. For ownership housing, affordability is tied to levels set in the MHFA's "Startup Program" (first-time buyer), with maximum sales price updated annually. The Edina City Council reviews all elements of the Affordable Housing Policy annually.

Park, Recreation, and Trails Strategic Plan (2015)

The main focus of this plan was to identify ways to make Edina's Park and Recreation amenities multigenerational, serving all residents and encouraging socialization. The plan includes specific needs for system improvements that could be incorporated in park improvements within the Greater Southdale District to better serve residents.

Vision Edina (2015)

In 2015, around the same time that the Greater Southdale Work Group was created, the Edina City Council adopted **VISION EDINA – Strategic Vision and Framework**, the outcome of a broad-based community engagement and visioning process:

Edina holds a well-earned reputation as a city of choice. It is a model of a successful, mature, and progressive urban community, that strives to lead in a modern and evolving world. We maintain our heritage and attractiveness, and afford our residents the highest quality of life, while actively embracing the future.

VISION EDINA describes key features that define Edina's future, as well as stating strategic focus area, issues, and actions. **VISION EDINA** is the key foundation element for the Work Group's explorations for the Greater Southdale area. Defining features of Edina identified through this included: Inclusive and Connected, Built-to-Scale Development, Sustainable Environment, A Community of Learning, and Future-Oriented.

Metropolitan Council System Statement (2015)

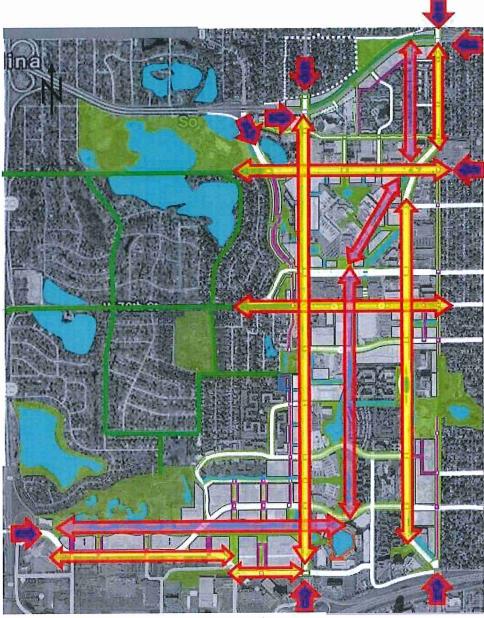
The Metropolitan Council's system statement for Edina is a guidance document for how Edina is expected to grow as part of the larger region, particularly focused on the regional system topics that are under the Metropolitan Council's purview: transportation, water resources, and regional parks and trails.

Area system statements classify communities based on the expected level of growth and change they will see prior to 2040. These numbers are goals, not mandates — and can be modified over time if growth rates differ from what is forecasted. As part of this, Edina is designated as an Urban community in the metropolitan area.

Edina's Urban designation guides new growth with an average density of at least 10 units per acre, with higher densities (10-60+ units per acre) around transit. Mixed-use development, affordable housing, and transit-supportive design are encouraged.

The Metropolitan Council projects the City of Edina to grow by about 11,800 people, 7,500 households, and 3,800 jobs by 2040, a substantial percentage of which could be accommodated in the Greater Southdale District. The need for affordable housing was also considered, and a goal of about 880 units was set for Edina – many of which could be accommodated in the Greater Southdale District as well.

Figure 4.6: Concept Illustrating Gateways and View Corridors





- France Ave. and W 78™ St.
- York Ave. and W 78th St./Edinborough Way
- France Ave. and Hwy 62
- Xerxes Ave. and Hwy 62
- Exit from Hwy 100 at W 78th St.
- · Exits from Hwy 62 at Valley View, France Ave. and Xerxes Ave.
- . W 66th St. and York Ave.

View Corridors

StreetRooms

- · France Avenue
- York Avenue to Hwy 62
- W 66th St.
 W 70th St. between the Cornelia neighborhood and Richfield

Signature Public Realm (Central Spine)

- Centennial Lakes to Fred Richards Park
- · Centennial Lakes to the Promenade

Figure 4.8: Concept Illustrating Potential Green Roofs and Green Streets



Greater Southdale District Plan – Adopted December 18, 2018 Page 58

Sketch Plans:

A revised Sketch Plan scope should give consideration to limiting submittals to:

- Site Plan demonstrating relationships to District features and adjacent and nearby parcels;
- Street level floor plan(s) addressing interactions with the street and public realm spaces;
- Plans and other illustrations depicting public or publicly-accessible spaces proposed as part of the project;
- Cross-sections demonstrating relationships of uses and forms at key locations within the site and, especially, at locations that demonstrate connections to adjacent and nearby parcels;
- Elevations demonstrating key form relationships and, especially, ways in which the proposed building interacts with the public realm;
- Conceptual approaches to accommodating water use, waste generation, energy utilization, and stormwater accommodation, particularly where new or innovative approaches might be proposed;
- Development program descriptions highlighting general intentions for use and activity proposed for the project
- Accommodation of personal passenger vehicles, shared vehicles, and building services, including patterns
 of circulation that show precedence for pedestrian movements
- Alternative configurations that might be considered to achieve greater community benefits, improved design, or enhanced function; and
- A diagram describing benefits of the proposed project for the community that might be achieved in each configuration.
- 8-C. Update the development review processes and regulations to promote a higher level of predictability and certainty for new development through clear and objective standards, while also providing discretionary review with reference to design experience guidelines as a way to facilitate flexible and innovative approaches to meet requirements.
- 8-D. Adopt Design Experience Guidelines with development standards for the design and configuration of buildings including building form, facades, heights, setbacks/build-to lines, stepbacks, frontages, landscape/open space requirements, pedestrian orientation, and connections, impacts on adjacent and nearby properties, and relationships to gateways and view corridors, as well as to roads, parks, and other infrastructure development.
- 8-E. Consider application of a community benefits review for new introductions, including contributions from new development to offset costs of providing new residential and non-residential users with new public assets, especially parks and public spaces, including consideration of whether developments should be granted concessions for providing needed new public facilities directly.'

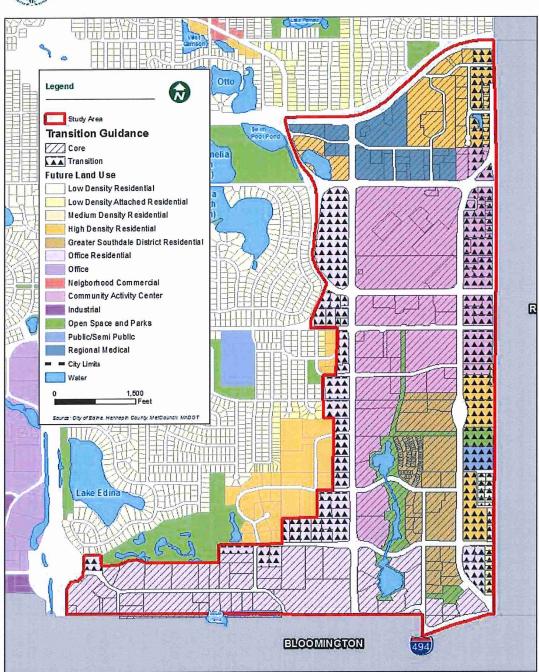
District Management

Urban Design Goal #9: Perpetuate the Greater Southdale District by close and consistent attention to the public realm and the needs of people living or working in or visiting the District.

- 9-A. Investigate the need for and feasibility of a mechanism to augment the City of Edina maintenance and enhancement of public rights-of-way and the public realm.
- 9-B. Consider methods of enhancing District hospitality and safety, advocating for aesthetic improvements, and promoting and expanding District business activity and public events.

Figure 4.17: Proposed Transition Zones



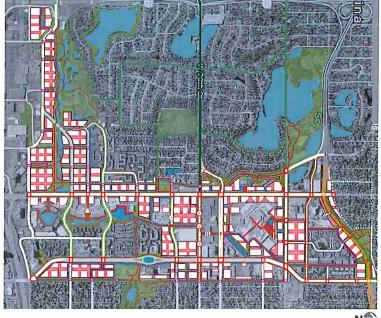


Potential Building Sites

The diagram at right illustrates how the basic principle of a 200'x 200' grid can be applied nominally on potential redevelopment sites throughout the district without consideration of property line. It is recognized that land ownership will influence the ultimate form of the grid.

Other considerations influencing block pattern:

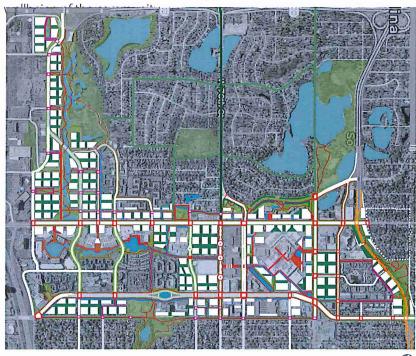
- Building scale
- Public realm connections
- Connections through blocks
- Pedestrian-oriented street intersections



The Space Between Buildings

As a result of the 200'x 200' block pattern, the space between buildings becomes an

important asset in shaping the overall sense of landscape and continuity of public realm throughout the district. Because the superblocks have primary responsibility for serving the vehicular traffic needs of the district, access to the smaller blocks for drop-off, service and parking can be planned to stay at the perimeter of a block, allowing for the spaces between the remaining block to be used for a network of green spaces that support the health and



Opportunities for the "Space Between" buildings to become places and connections to larger community pathways:

- **Parks**
- **Pocket Parks**
- Recreation
- Play Areas
- Gardens
- Stormwater Management
- Wetlands
- Waterways

STREET ROOM TYPOLOGIES

A hierarchy of streets and pathways within the district is the framework for public realm development and related building form. Each street across the district has a role in how it serves pedestrians, bicycles and vehicles in connecting sub-districts, adjacent single family neighborhoods, and the overall Greater Southdale District and creating a unified sense of place. It is the intent that street typologies define the public realm experience: the space between buildings, dimensions of building setbacks from the street, heights of facades at the building face at the street and building step backs, where the façade of the building steps back from the volume of the street room.



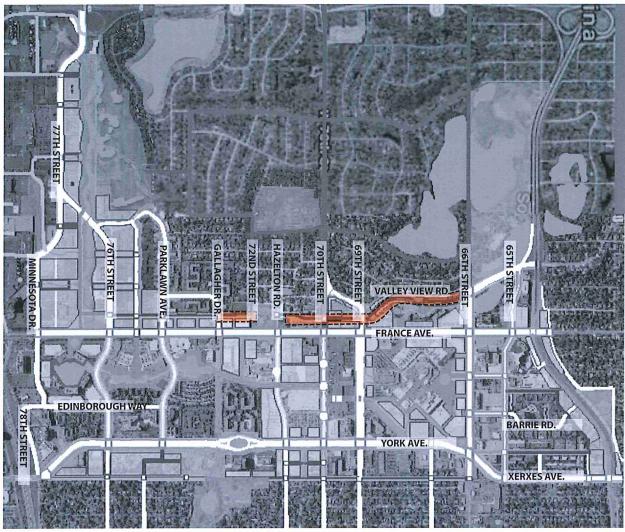
Street Room Typology 1: Promenades and Transition Zones

Promenades are new woonerf-type streets on the west side of France Avenue and on the east side of York Avenue. Within this typology, there are several different variations for new building development in these important transition zones, responding to and respecting the context of adjacent single family neighborhoods.

Street Room Typology 2: Cornelia Overlay Zone

With the understanding that there is special sensitivity related to new building development near the Cornelia neighborhood, this is a special zone governing the design of the public realm/street room on the east side of France Avenue between the north side of 69th Street and Gallagher Drive. Buildings within this zone will be expected to maintain the east side of the France Avenue street room, but will be of a lower overall scale than new

Street Room Typology 1A West Promenade / Transition to Cornelia Neighborhood

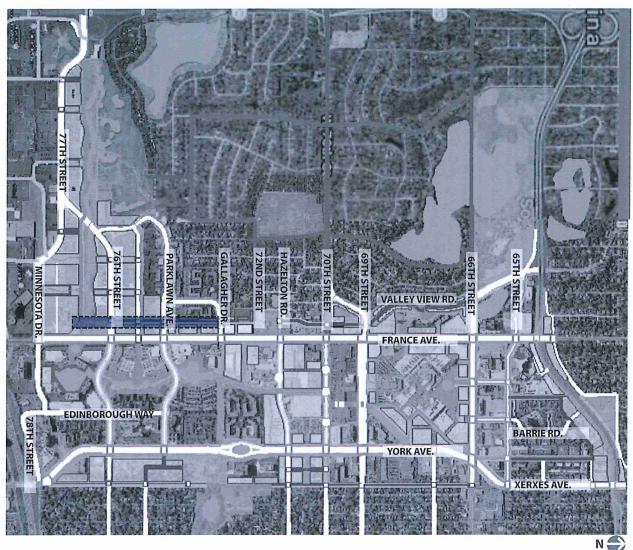




The sub-district to the west side of France between 69th and Parklawn is a unique transition zone within the Greater Southdale District. Any proposed developments within this zone should be approached with special sensitivity. This means that the street experience within that zone should be perceived as connected to the Cornelia neighborhood through landscaping and trees, and buildings that gradually transition in both height and function and use between lower intensity neighborhoods to the more commercially-focused district on the east side of France Avenue.

The West Promenade, a new north-south pedestrian, bike and vehicle street/woonerf that accommodates service access, is envisioned between France Avenue and the Cornelia neighborhood. This new shared street is intended to provide service access to new developments along France (keeping vehicular traffic out of single family neighborhoods), and providing a new framework to support the transition from townhouses and lower-scale residential buildings on the west side of the West Promenade, to slightly taller buildings on the east side of the West Promenade.

Street Room Typology 1B
West Promenade Between Parklawn and Minnesota Drive



South of the zone designated as Street Room Typology 1A (where the Cornelia transition area immediately abuts single family neighborhoods), the Street Room experience shifts to respond to its changing context. It becomes one that is more commercial in nature and in concert with the expected higher intensity in the southwest quadrant the Greater Southdale District.

As the West Promenade extends south into Typology 1B, it continues to serve pedestrian, vehicular and service access. Uniform building heights on each side of the West Promenade are intended to support the transition from existing multi-family housing to taller buildings at the south end of France Avenue near the gateway from 494.

5. Implementing and Measuring the Guidance

Ten Things to Remember

- 1. Every new development begins with the 200'x 200' block, or some variation based on context.
- 2. Every block or building in a development will need streets to connect between buildings. Not all of these streets will need to accommodate vehicles, providing the opportunity for parks, plazas or courtyards—important parts of the public realm.
- 3. Buildings will not be greater than 200 feet in length, thereby minimizing the negative impact continuous walls can have on a comfortable pedestrian experience.
- All streets are not equal. The plan outlines a hierarchy that is driven by the kind of experiences that are expected on these streets and how they facilitate an enlivened public realm.
- 5. Designated transition zones are about maintaining the quality of life in these areas without restricting growth in other parts of the district.
- 6. Promenades and East-West Streets are the bridge between single family neighborhoods, such as the Cornelia neighborhood of Edina and the west side of Richfield, to more intense parts of the district.
- 7. Street Rooms will intersect and overlap each other in many circumstances. At these intersections, lower building heights should prevail, giving the smaller scaled building precedence over larger scale buildings.
- 8. Building footprints above 60 feet in height are limited to 12,000 SF for residential uses, and 24,000 SF for commercial.
- 9. Within the first 60 vertical feet of a building, primary materials systems that are more traditional like brick, stone, glass wall systems are preferred. Above 60 feet, other materials such as metal wall systems within a larger curtainwall system, can be introduced. These baseline parameters should not be a deterrent to architectural innovation but rather are intended to serve as a measure of quality and continuity throughout the district
- 10. Transparency at the ground level facing the public realm is key to the individual experience and is a catalyst for how to activate and maintain a community-based approach to daily life and experience.



DATE: July 3, 2019

TO: Cary Teague – Planning Director
FROM: Zuleyka Marquez, Graduate Engineer

RE: 6950 France Avenue - Development Review

The Engineering Department has reviewed the subject property for street and utility concerns, grading, storm water, erosion and sediment control and for general adherence to the relevant ordinance sections. This review was performed at the request of the Planning Department; a more detailed review will be performed at the time of building permit application. Plans reviewed include the survey revised 5/30/19, existing conditions, site plan, grading plan, SWPPP, and utility plan dated 05/31/19.

Grading, Drainage, and Stormwater Mitigation

- I. The proposed plan includes full redevelopment of the property. Site drains to the west towards private property and eventually to an area of known flooding. Applicant is reducing volume of storm water heading to the west by the use of infiltration basins and an additional connection to the storm sewer line along France Ave South are proposed.
- 2. A final grade as-built survey and inspection will be required to verify compliance with the approved stormwater plan.
- 3. Provide hydraulic and hydrologic report.
- 4. Provide more detailed information for the retention system. Retention system engineer required to verify construction of the underground retention systems done per plan.
- 5. Confirm retention system is structural designed for Edina's 80,000lb fire truck load and outriggers in parking lot areas.
- 6. Records indicate a sealed well onsite. Ensure well is properly sealed after project.

Erosion and Sediment Control

7. An erosion and sediment control included on SWPPP.

Street and Curb Cut

- 8. The applicant proposes to remove and replace the existing curb for the parking lot entrances on the north and west. A curb cut permit will be required.
- 9. Maintain sidewalk access during construction. (general note)
- 10. Construction staging, traffic control, and pedestrian access plans will be required.
- 11. Review fire access requirements with fire department. Consider truck overhang when proposing plantings.
- 12. Work in France Avenue ROW will require a Hennepin County Permit.

Living Streets

- 13. Remove sidewalk along France Ave and integrate sidewalk facilities between France Ave and the building to create a much better pedestrian experience. Minimum sidewalk width 8-ft with at least an 8-ft boulevard.
- 14. Design sidewalks to meet ADA requirements.
- 15. Sawcut concrete sidewalk joints on public sidewalks.
- 16. Roadway light fixtures along France Ave shall be consistent with Canto Style fixtures.



Public Utilities

- 17. Applicant proposes to connect to the existing 6" water service along France Avenue South. A new sewer manhole is proposed near the southeast corner of the parking lot to connect to the existing sanitary sewer service. A sewer and water connection permit will be required. Apply for a sewer and water connection permits from public works for water and sanitary main.
- 18. A SAC and WAC determination will be required and Met Council and City REC fees will be calculated from the determination.

Other Items

- 19. A Nine Mile Creek Watershed District permit along with a private maintenance agreement for the infiltration basins may be required; applicant will need to verify with the district.
- 20. The ingress easement adjacent to the west property line may need to be amended per the item 12 on page 1 of 2 of the Land Title Survey.
- 21. Additional permits from Hennepin County, MDH, MPCA, and MCES may be required.



Technical Memorandum

Cary Teague, City of Edina To:

From: Max Moreland, PE **Date:** June 26, 2019

Re: Traffic Assessment – Estelle Site Redevelopment

Purpose of Report and Study Objectives

A redevelopment of the site at 6950 France Avenue in Edina, known as the Estelle Site, is proposed. This redevelopment would convert an existing 28,000 square foot office building into a 10,000 square foot retail development.

This technical memorandum presents a high-level traffic analysis of the proposed development. The primary purpose is to determine the amount of traffic to be generated by this development and how that compares to the existing traffic at the site. The assessment will also forecast the anticipated traffic impacts to two of the surrounding intersections as well as include a review of the site layout from a traffic perspective.

A site plan dated May 31, 2019 is attached for reference.

Conclusions

The changes in land uses represent the swap of an office building for a retail building. Using standard trip generation data for the retail building and comparing it to existing trip counts at the office building, the expected changes with the new development compared to the existing development are:

- An increase in total daily trips by 230 trips.
- A decrease in total a.m. peak hour trips by four trips.
- An increase in total p.m. peak hour trips by 16 trips.

Though the trip generation for the site is anticipated to increase, the site is not forecast to be a high traffic generator. No significant operational impacts are anticipated for the surrounding roadways and intersections due to trips from this proposed development.

Site Characteristics

The site is located at 6950 France Avenue. The main access point to the site is on France Avenue, but due to France Avenue being a divided road, only southbound traffic can enter the site and exiting vehicles can only head to the south. To provide access to vehicles going to/from other directions, ingress and egress easements will be given to access the site via the BMO Harris Bank with access at 3950 70th Street and the Kinderberry Hill Child Development Center with access at 3905 69th Street.

The existing site includes a 28,000 square foot office building with 80 vehicle parking stalls. The redeveloped site will include a 10,000 square foot retail building with 51 vehicle parking stalls with proof-of-parking for an additional 11 parking stalls.

Trip Generation

The traffic forecasts for the site are based on the data and methods published in the *Institute* of Transportation Engineers (ITE) Trip Generation Manual, 10th Edition. The ITE Trip Generation Manual is a compilation of traffic data for various land uses from existing developments throughout the United States.

In addition to the ITE forecasts, vehicle trip generation was collected for the existing site. Using cameras at the site access points, the vehicle trip generation for a 48-hour weekday period was collected in June of 2019. Averaging the two days, 425 vehicles entered the site and 425 vehicles exited the site over a 24-hour period. Adjusting for cut-through traffic, 37 vehicles entered, and 37 vehicles exited the site specifically for the office building in a 24-hour period.

The property owner noted the existing office building is roughly 50% occupied. Based on this information the collected counts were also doubled to reflect the full occupation scenario of the existing land-use.

Table 1 presents the ITE trip generation for both the current and future land uses for the site. It should be noted, only the raw trip generation is presented in Table 1, without reductions for pass-by or internal trips, to present a direct comparison of trip generation.

Table 1 – Total Trip Generation Comparison

Description	Daily Trips		AM Peak Hour		PM Peak Hour	
(source)	Entering	Exiting	Entering	Exiting	Entering	Exiting
50% Occupied Office						
Building	37	37	7	0	4	7
28,000 feet ²	3,	3,	,	Ü	•	,
(Traffic Counts)						
Fully Occupied Office	74	74	14	0	8	14
Building						
28,000 feet ²						
(Doubled Traffic Counts)						
Office Building						
28,000 feet ²	136	136	28	5	5	27
(ITE Trip Generation - 710)						
Retail						
10,000 feet ²	189	189	6	4	18	20
(ITE Trip Generation - 820)						
Net Change from Existing	115	115	-8	4	10	6
Full Occupation	115	115	-0	4	10	U

It can be seen in Table 1 that the measured trip generation for the office building is significantly less than the ITE estimates for the office building. This is expected as the building is only 50% occupied. This is also the case under the full occupation scenario (two times the existing counts).

Comparing the trip generation forecasts for the retail to the existing office traffic counts (50% occupation), the retail building is forecast to generate just over 300 daily trips more than the office building currently is. The a.m. peak hour volumes are forecast to be approximately the same while the p.m. peak hour volumes are forecast to be higher.

A portion of the raw trip generation shown in Table 1 for the retail site can be classified as passby trips. Pass-by trips are those vehicles already on the roads which will stop at the development site in the future. For a retail building, it is estimated that approximately onethird of site trips will be pass-by trips. With that, the new daily trip numbers for the retail site would be closer to that of the office building but still higher. Table 2 lists the new trips for the retail site, factoring out pass-by volumes.

Table 2 – New Trip Generation Forecast

Description	Daily Trips		AM Peak Hour		PM Peak Hour	
(source)	Entering	Exiting	Entering	Exiting	Entering	Exiting
Retail						
10,000 feet ²	125	125	4	2	12	13
(ITE Trip Generation - 820)						
Net Change from Existing Full Occupation	51	51	-10	2	4	-1

Intersection Impact Analysis

To see the impact of site trips on surrounding intersections, vehicles need to be distributed from the development throughout the roadway network. A trip distribution pattern for trips going to/from the proposed development was developed based on volumes of surrounding roadways. That pattern is:

- 10% of the generated traffic to/from the west on 69th Street
- ii. 25% of the generated traffic to/from the north on France Avenue
- 10% of the generated traffic to/from the east on 69th Street iii.
- 10% of the generated traffic to/from the east on 70th Street iv.
- 35% of the generated traffic to/from the south on France Avenue ٧.
- 10% of the generated traffic to/from the west on 70th Street vi.

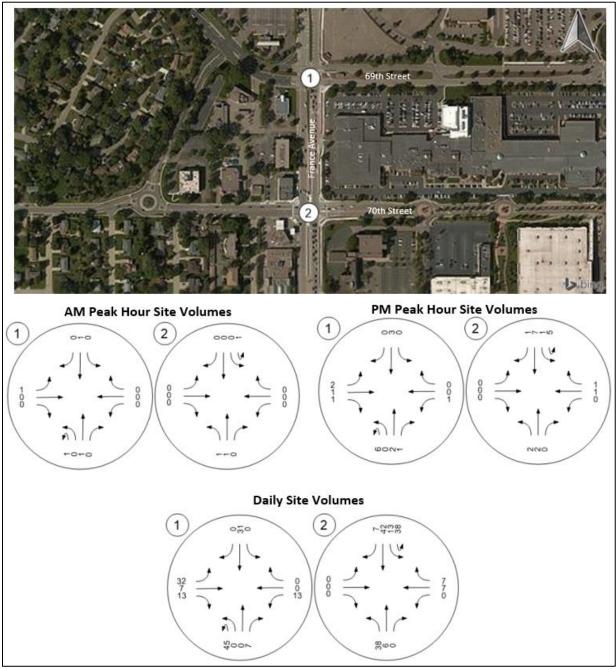
Because of the location of the site, it is anticipated that most trips to and from the site will use the France Avenue access point. Access to 69th Street or 70th Street will not be intuitive and will therefore likely only be used by regular drivers to the site, most likely employees. Additional Uturns are expected at France Avenue & 69th Street as well as at France Avenue & 70th Street due to the divided roadway. Protected left turn phasing at the signals at these two intersections does provide for those U-turn movements.

Using the trip generation and trip distribution, new trips for the retail development were routed through the following intersections:

- France Avenue & 69th Street
- France Avenue & 70th Street

Pass-by trips were also routed through the network. Pass-by trips are assumed for this analysis to all be going along France Avenue. The total site trips are shown in Figure 1.

Figure 1 - Redevelopment Trips



As seen in Figure 1, the highest hourly volume increase for any intersection movement due to the site traffic is seven vehicles making southbound through movements at France Avenue/70th Street in the PM peak hour. That equates to an increase of approximately one vehicle every eight minutes for that movement. Based on these volumes, none of the surrounding intersections are anticipated to be significantly impacted by site traffic.

Parking Review

As previously mentioned, the redeveloped site will include 51 vehicle parking stalls with proof-of-parking for an additional 11 parking stalls if required. Using the *Institute of Transportation Engineers (ITE) Parking Generation, 4th Edition,* the expected parking demand for the 10,000 square foot retail building was reviewed. As retail is heavily influenced by seasonal changes and the day of the week, multiple parking demands can be calculated for the development. Table 3, below, shows the 85th percentile and average peak period parking demands for these scenarios.

Table 3 - Total Parking Demand Comparison

Description	Average Peak Period Parking Demand		
Non-Friday Weekday (December)	38 vehicles		
Friday (December)	40 vehicles		
Weekend (December)	46 vehicles		
Non-Friday Weekday (Non-December)	26 vehicles		
Friday Weekday (Non-December)	30 vehicles		
Weekday (Non-December)	29 vehicles		

As Table 3 shows, the average peak period parking demand ranges from 26 vehicles to 46 vehicles. The 51 parking stalls provided by the proposed development supplies sufficient parking for all of the average peak period parking demands.

Based on these conditions no additional parking, over the 51 stalls shown in the proposed site plan, should be required. If parking capacity begins to be exceeded on a normal basis the addition of the 11 proof-of-parking stalls can occur. Also, with the close spacing of the adjacent developments, and the shared parking accesses, additional off-street parking stalls are located within 500 feet of the proposed developments front doors providing additional overflow parking if ever needed.

Site Plan Review

The current site plan for this development, which is attached, was reviewed from a traffic perspective.

- Car Circulation: The number of access points to the site will remain. One-way circulation
 will be present throughout the main parking area. Appropriate signage and striping will
 be needed to convey the one-way circulation.
- Truck Circulation: A trash pickup area is located on the southeast side of the parking lot. Truck turning paths should be checked to ensure collection vehicles can navigate to and from that area through the parking lot without impacting parked vehicles.
- Walking Paths: Sidewalks are proposed around the entire building connecting into the sidewalk along France Avenue giving good pedestrian access to the site.
- Bicycle Stalls: Bicycle racks are proposed at the northwest corner of the building.

- ADA: Sidewalk curbs near the three accessible parking spaces will need to be sloped to ADA requirements.
- Electric Vehicles: One electric vehicle charging station is located on site.

STAFF REPORT



Date: June 17, 2019

To: Cary Teague, Community Development Director

From: David Fisher, Chief Building Official

Subject: Site Review and Setback Variance for New Bilding at 6950 France Ave.

Information / Background:

6950 France Ave will be a new building. The building will be required to meet the State building, plumbing, mechanical and accessibility codes.

- Based on the site plan provided three accessible parking spaces will be required. There are two on the site plan now. All the accessible parking spaces require an access aisle. This could reduce the numbers of parking spaces.

2200 Zane Ave N | Minneapolis, MN 55422 www.archfieldoffice.com

City of Edina

Cary Teague, Community Development Director

4801 W. 50th Street Edina, MN 55424

From Mic Johnson, FAIA

Date July 1, 2019

Cary:

То

At your request, we reviewed the Sketch Plan submission for the proposed Shoppes at Estelle development based on our experience working with the Greater Southdale Work Group to craft a physical vision for how their guiding principles may translate to the built environment. The resulting vision for development in the district is to create an enhanced human experience along existing major and new connector streets, with overall experience shaped via landscape setbacks, building step backs, a hierarchy of street typologies, transparency at street level, minimizing the impact of the car, and managing storm water as an amenity. The outcome of our collaborations with the Work Group is described in the urban design chapter of the Greater Southdale District Plan and resulted in the Greater Southdale District Design Experience Guidelines.

The project proposed is located on a smaller parcel at 6950 France Ave S, and while it does not align with the Design Experience Guidelines in terms of overall building massing and mix of uses, we believe that the proposed project does demonstrate several positive attributes as it relates the creation of an active public realm, and exhibits general consideration for the neighborhood.

Positive attributes of the proposal include:

- Adherence to the 50' setback from France
- A "four sided" architectural response
- Screening of rooftop mechanical by the building's parapet
- Creation of a rain garden within the 50' setback to daylight storm water
- Use of natural materials and transparency at the ground floor
- Consideration of connections to a future woonerf/West Promenade as adjacent properties are redeveloped

Our specific comments on the proposed plan are as follows:

• The height of the interior of building volume meets the Design Experience Guideline goal of 20' floor-to-floor at street level, which creates greater flexibility in building use. The additional 10 feet at the parapet helps to create a more substantial presence for pedestrians along

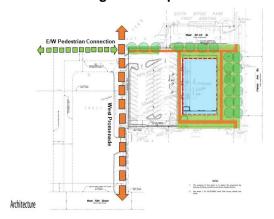
France Avenue. However, this is only half of the desired 60' podium height as described in the Experience Guidelines.

- The scale of the windows creates transparency to the interior, encouraging visual connections going both ways from exterior to interior.
- Considering the low scale of the building, is or was there consideration for vertical expansion in the future?
- Encouraging connections through blocks is an important part of the Experience Guidelines.
 Opening up and encouraging pedestrians to move through the blocks. In addition, by aligning with curb cuts across the street, in this case at the Galleria, will reinforce potential future midblock crosswalks. Setting the stage for a strong connection to the location of the West Promenade will encourage the development of this new north-south connection as well.

Site's development potential beyond the current proposal:

While we understand that the design as proposed is allowed under current zoning code, we also know the City's zoning requirements for the district will need to be revisited in the future to better align with the Greater Southdale District Plan and Design Experience Guidelines. The following diagrams represent how small sites should be considered through the lens of the Design Experience Guidelines and the Typologies that apply to this particular site (Typology 1A – West Promenade/Transition to Cornelia Neighborhood / Typology 2— Cornelia Overlay District).

1. Better alignment of public realm with Design Experience Guidelines:

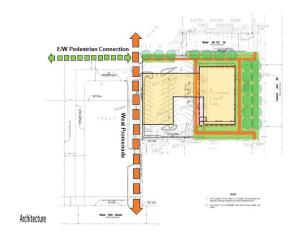


The building plan could shift to the south a bit to better accommodate the landscape strategy set forth in the Design Experience Guidelines along France, as well as eastwest connections that align with existing curb cuts across the street. The dotted red line in the diagram at left represents the building shift of the existing proposal.

The proposal as presented includes two sidewalks along France avenue – the existing 5' wide one along the curb, as well as a new, wider one closer to the building.

We suggest eliminating the one next to the street to allow a double row of trees and berming along France to enhance the pedestrian experience according to the Experience Guidelines. Additionally, in our diagram, the proposed building has been moved approximately 10 feet south (which we know is in conflict with existing zoning) to create a stronger east-west connection along the north side of the site between France Avenue and the future West Promenade. In this scheme, on grade parking for four cars would be lost to create more green space.

2. Consider the future evolution of the site:



The yellow shaded areas represent a general massing plan that could sit above the proposed retail and parking, adjacent to the Promenade. Parking could be allowed facing the promenade, but as parking requirements are reduced over time the retail parking may become partially infilled. As an example, the 20' floor-to-floor height of the first could be extended over the site of the existing surface parking to accommodate 2-story townhomes facing the new West Promenade.

Below-grade parking, which although has cost implications, ultimately creates long-term flexibility for the building owner and accommodates change in support of a changing district.



The yellow shaded areas in the section at left represent additional floor area and step backs on the west side of the property as called for in the Experience Guidelines. The light grey and blue shaded areas, and unshaded area,

represent the proposed building and on-grade parking. The dark grey area represents potential below-grade parking that supports housing above. As noted above, with a potential parking need reduction in the future, the ground level parking (with building above) could be infilled on the west side to face the new West Promenade.

Thank you for the opportunity to review. Please let me know if you have any questions. Mic



6950 France Avenue South

May 31, 2019





The Shoppes at Estelle

6950 France Avenue South Edina, MN

DEVELOPER:

PARAPET SCREENING OF ROOFTOP MECHANICAL UNITS

A0 SCALE: 1/16" = 1'-0"

ARCADIA ON FRANCE, LLC 5215 EDINA INDUSTRIAL BLVD EDINA, MN 55439

ARCHITECT:

EDWARD FARR ARCHITECTS 7710 GOLDEN TRIANGLE DRIVE EDEN PRAIRIE, MN 55344 PHONE: 952-943-9660

CONTACT: ED FARR

CIVIL ENGINEER:

LOUCKS INC. 7200 HEMLOCK LANE, STE 300 MAPLE GROVE, MN 55369 PHONE: 763-496-6753

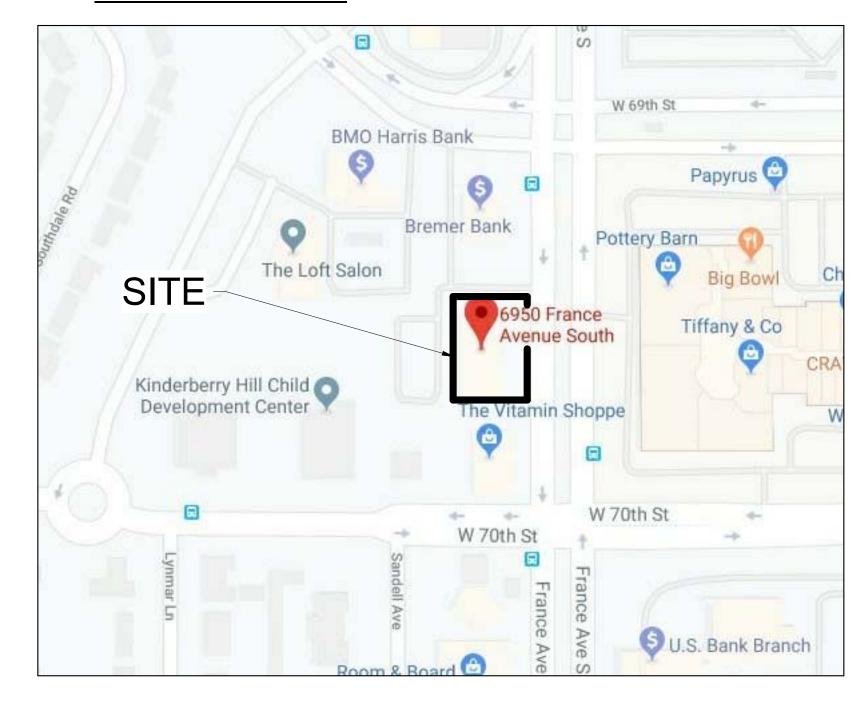
CONTACT: ZACHARY WEBBER

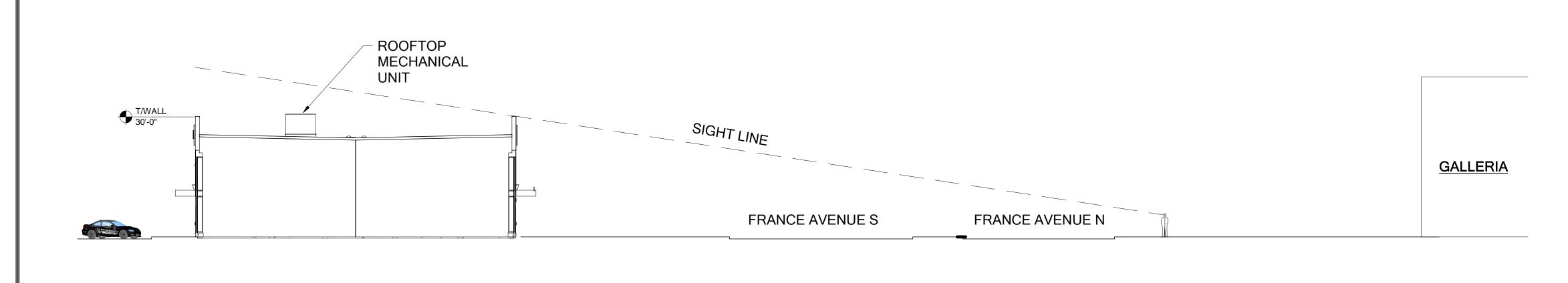
LANDSCAPING:

LOUCKS INC. 7200 HEMLOCK LANE, STE 300 MAPLE GROVE, MN 55369 PHONE: 763-496-6753

CONTACT: PAUL KANGAS

VICINITY MAP





SHEET INDEX:

COVER SHEET

SURVEY

SURVEY

EXISTING CONDITIONS

DEMOLITION PLAN

SITE PLAN

GRADING PLAN

SWPPP PLAN

SWPPP NOTES

UTILITY PLAN

DETAILS

DETAILS DETAILS

LANDSCAPE PLAN

TREE INVENTORY PLAN

FLOOR PLAN **ELEVATIONS**

LIGHTING PLAN

EDWARD FARR **ARCHITECTS INC** 7710 Golden Triangle Drive Eden Prairie, Minnesota 55344

under the laws of the State of



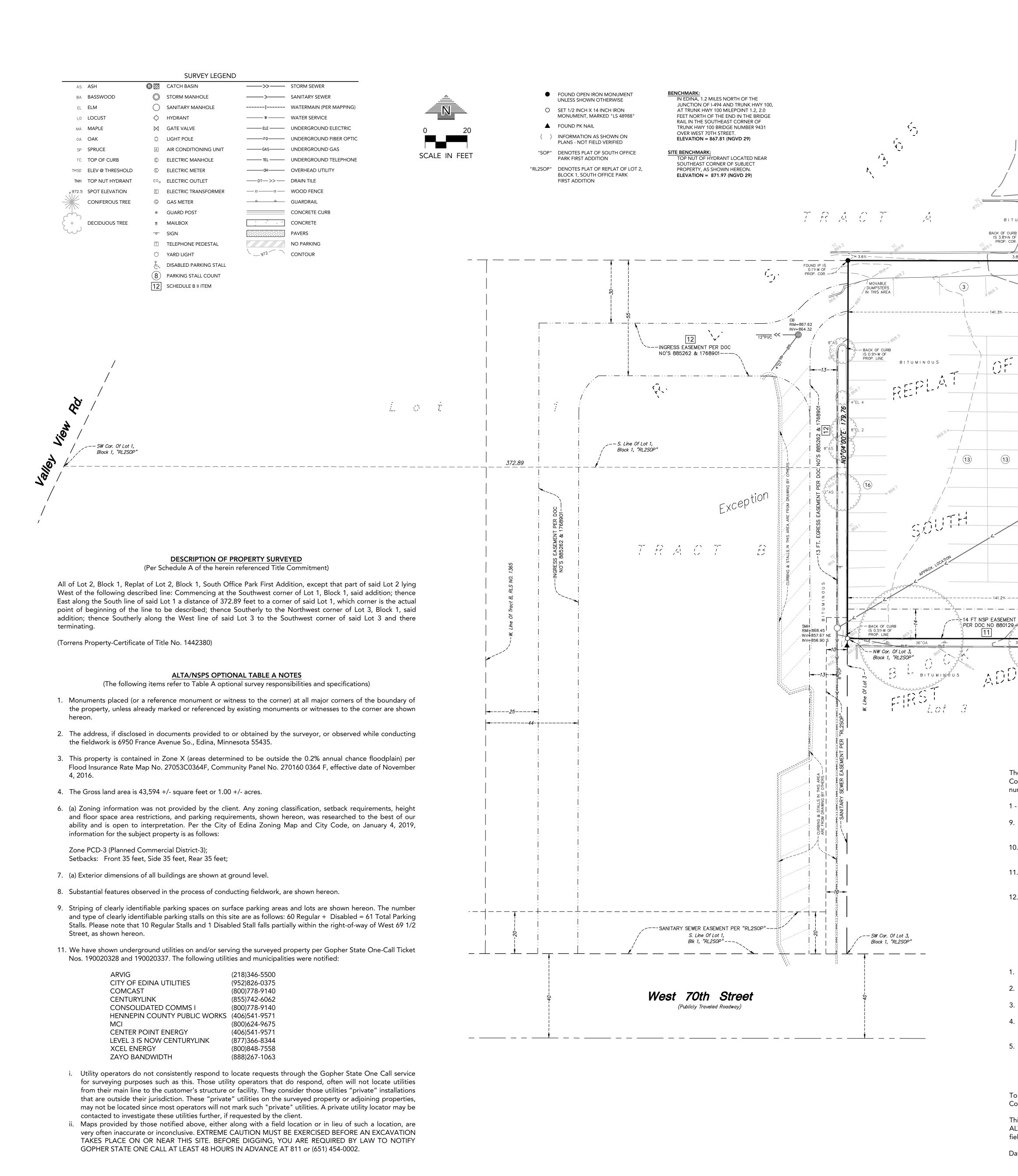
6950 France Ave. South Edina, Minnesota

05/31/2019

Sheet Title **COVER SHEET**

> <u>Sheet Number</u> **A0**

18.025



6950 FRANCE AVENUE SOUTH

> AG SPANO 14800 LANDMARK BLVI

EDINA, MN 5

PLANNING
CIVIL ENGINEERING
LAND SURVEYING
LANDSCAPE ARCHITECTURE

ENVIRONMENTAL
7200 Hemlock Lane, Suite 300
Maple Grove, MN 55369
763.424.5505
www.loucksinc.com

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SUBMITTAL/REVISIONS

1/24/19 SURVEY ISSUED 5/30/19 CLIENT COMMENTS

PROFESSIONAL SIGNATURE

I hereby certify that this survey, plan or report was prepared by me or under my direct supervision and that I am a duly Licensed Land Surveyor under the laws of the State of Minnesota.

Max L. Stanislowski - PLS

License No.

48988

Date

QUALITY CONTROL

License No. 48988
Date 1/24/19

QUALITY CONTROL

Loucks Project No. 18-697
Project Lead MLS
Drawn By SFM
Checked By MLS
Field Crew CMS

RTON HEATHERTON HEATHERTON WANTELL STANDARD STAN

GALLAGHER TO LYNMAR DELLAGORENTRY PL.

WAY

WAY

COVENTRY PL.

ALTA/NSPS LAND TITLE SURVEY 1 of 2

(Per Schedule B, Part II of the herein referenced Title Commitment)

TITLE COMMITMENT EXCEPTIONS

The property depicted on this survey and the easements of record shown hereon are the same as the property and the easements described in the Commitment for Title Insurance issued by First American Title Insurance Company, File No. 1002-267731-RTT, effective date December 19, 2018. The numbers below correspond to those in the title commitment.

1 - 8 and 13 do not require comment.

Lot

BITUMINOUS

SOUTH

69 1/2 St.

"SOP" & "RL2SOP", DOC NO'S

801954 & 843823

6950 France Avenue South 2 - Story Brick Building

вітψміноиs

FOUND IP IS 0.4½N & 0.1½W OF PROP. COR. -

- BACK OF CURB

(Publicly Traveled Roadway)

12"RCP STMH RIM=871.13

INV=866.94

- 9. Subject to utility easement as shown on the recorded plat of South Office Park First Addition recorded March 12, 1965 as Document No. 801954. Shown hereon along the north property line and near the southwest corner.
- 10. Subject to utility easement as shown on the recorded plat of Replat of Lot 2, Block 1, South Office Park First Addition recorded April 25, 1966 as Document No. 843823. Shown hereon along the north property line.
- 11. Terms, conditions, easements and incidental rights in favor of Northern States Power Company as contained in Underground Easement dated July 7, 1967, recorded July 12, 1967 as Document No. 880129. **Shown hereon along the south property line.**
- 12. Terms, conditions, easements as contained in Easement Agreement dated August 8, 1967, recorded August 25, 1967 as Document No. 885262; as affected by Agreement Amending Easement Agreement dated September 11, 1986, recorded October 24, 1986 as Document No. 1768901. Merger documentation of Dayton Development Company recorded as Document No. T05434582 remains memorialized on the Certificate of Title. Shown hereon west of the subject property. Please note, the ingress easement does not close as described. A document amending the easement may need to be recorded.

SURVEY REPORT

1. The Surveyor was not provided utility easement documents for the subject property except for those shown on the Survey.

BITUMINOUS

- 2. Snow and ice conditions during winter months may obscure otherwise visible evidence of on site improvements and/or utilities.
- 3. The bearings for this survey are based on the plat of REPLAT OF LOT 2, BLOCK 1, SOUTH OFFICE PARK FIRST ADDITION.
- 4. Additional driveway and parking stalls shown hereon, along the west side of property to W. 70th Street, are from a survey by Egan, Field & Nowak, Inc., dated January 27, 2017.
- 5. See sheet 2 for site plan overlay.

CERTIFICATION

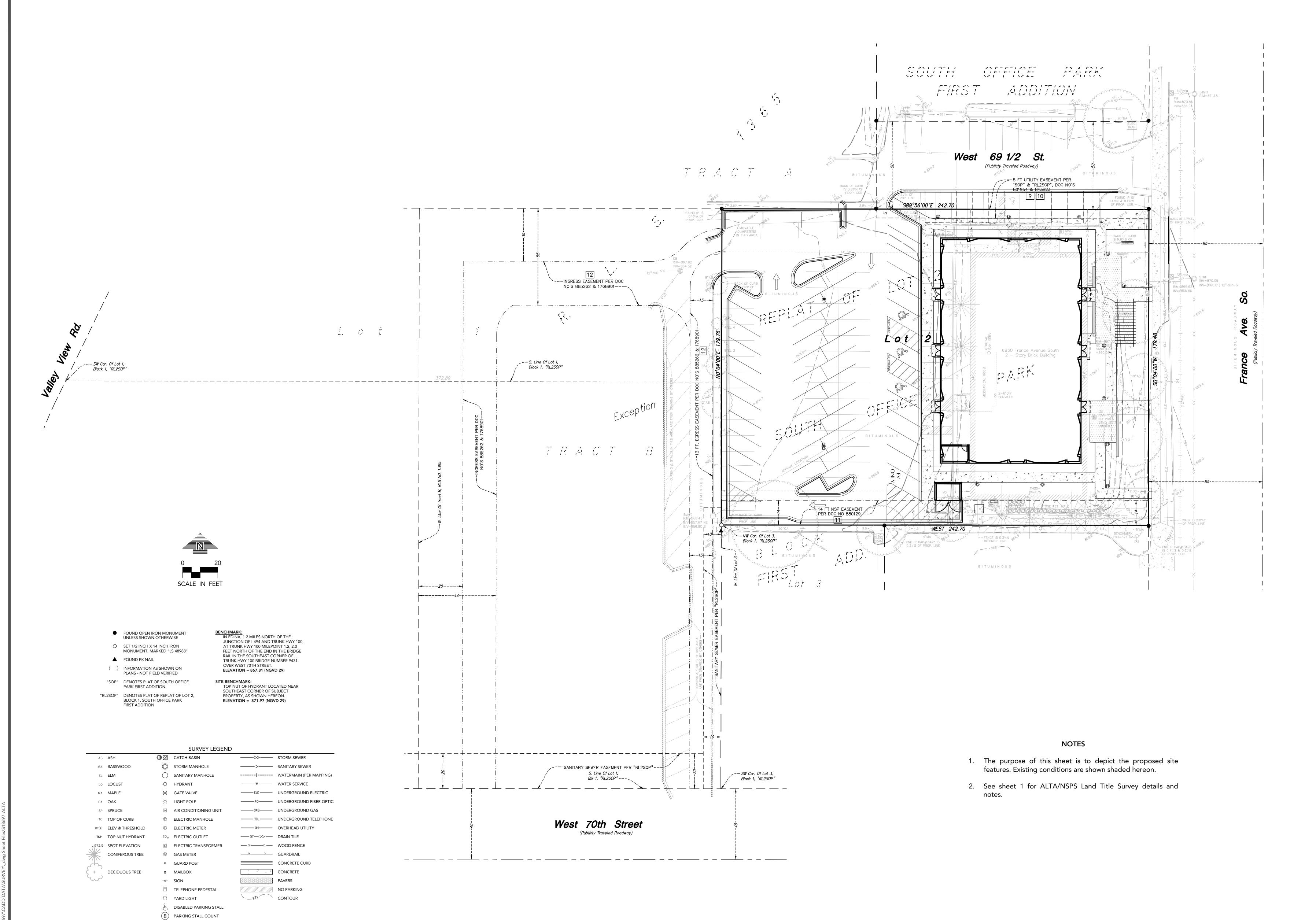
To The Spanos Corporation, a California corporation; Arcadia on France, LLC, a Minnesota limited liability company; and First American Title Insurance Company:

SURVEYOR No. 48988

This is to certify that this map or plat and the survey on which it is based were made in accordance with the 2016 Minimum Standard Detail Requirements for ALTA/NSPS Land Title Surveys, jointly established and adopted by ALTA and NSPS, and includes Items 1 - 4, 6(a), 7(a), 8, 9 and 11 of Table A thereof. The field work was completed on January 17, 2019 and on May 28, 2019.

Date of Plat or Map: May 30, 2019

Max L. Stanislowski, PLS Minnesota License No. 48988 mstanislowski@loucksinc.com



12 SCHEDULE B II ITEM

6950 FRANCE **AVENUE** SOUTH

EDINA, MN 55

AG SPAN

LOUCKS

CIVIL ENGINEERING LAND SURVEYING LANDSCAPE ARCHITECTURE ENVIRONMENTAL 7200 Hemlock Lane, Suite 300 Maple Grove, MN 55369 763.424.5505 www.loucksinc.com

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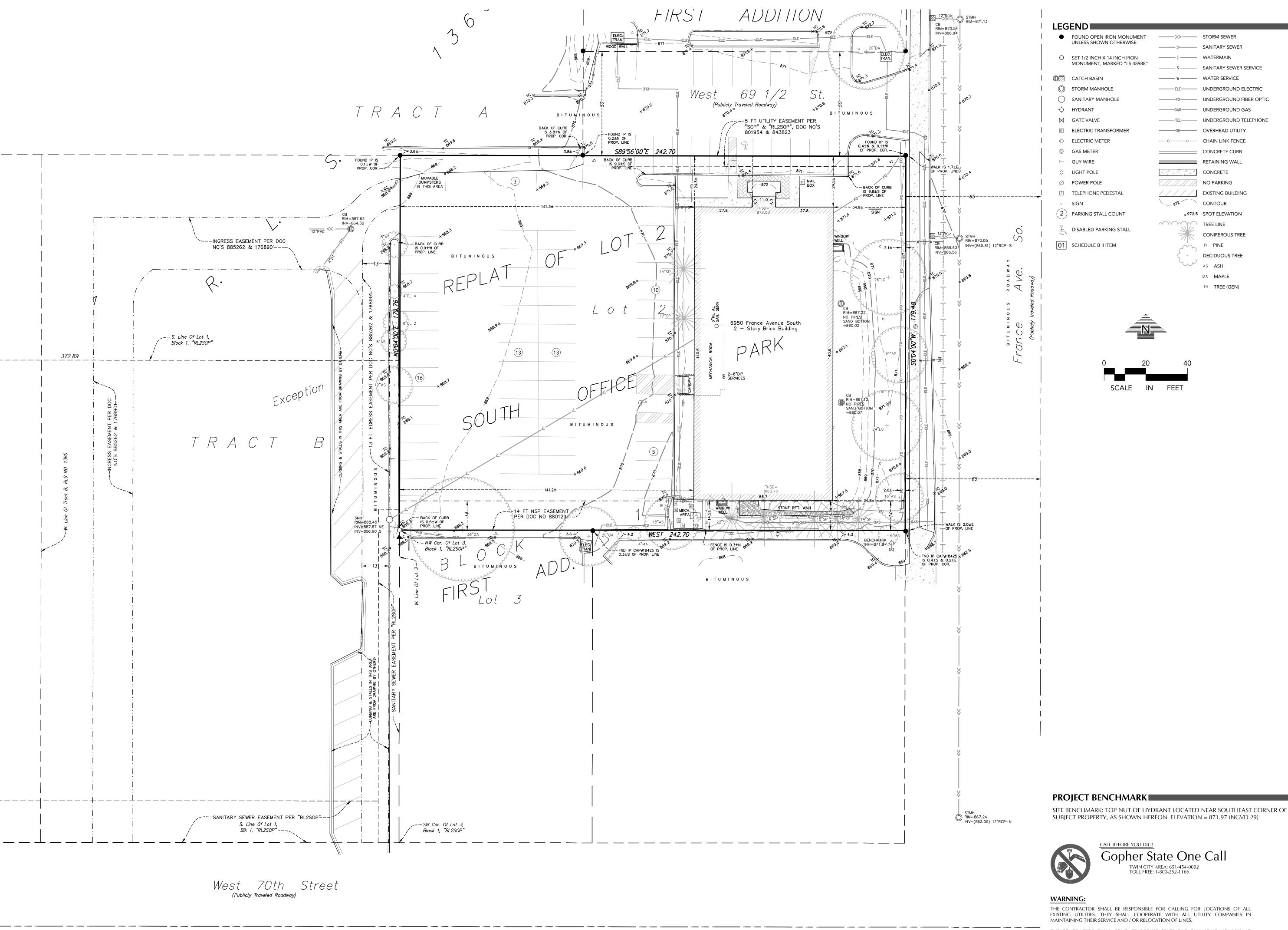
SURVEY ISSUED CLIENT COMMENTS

PROFESSIONAL SIGNATURE I hereby certify that this survey, plan or report was prepared by me or under my direct supervision and that I am a duly Licensed Land Surveyor under the laws of

License No. 1/24/19 QUALITY CONTROL Loucks Project No. 18-697 MLS Project Lead SFM Drawn By MLS Checked By

Field Crew

ALTA/NSPS LAND TITLE SURVEY



 FOUND OPEN IRON MONUMENT **UNLESS SHOWN OTHERWISE**

O SET 1/2 INCH X 14 INCH IRON MONUMENT, MARKED "LS 48988"

STORM MANHOLE SANITARY MANHOLE
 HYDRANT

E ELECTRIC TRANSFORMER E ELECTRIC METER

© GAS METER \leftarrow GUY WIRE ∅ POWER POLE

> → SIGN 2 PARKING STALL COUNT

01 SCHEDULE B II ITEM

TREE LINE **CONIFEROUS TREE** PI PINE DECIDUOUS TREE AS **ASH** MA MAPLE

---->> STORM SEWER

----- | WATERMAIN

----- W ----- WATER SERVICE

——— >——— SANITARY SEWER

——— S ——— SANITARY SEWER SERVICE

------ UNDERGROUND ELECTRIC

———GAS——— UNDERGROUND GAS

-----OH------ OVERHEAD UTILITY

— ○— ○ CHAIN LINK FENCE

CONCRETE CURB

RETAINING WALL CONCRETE

______ EXISTING BUILDING

__912 CONTOUR

————F0——— UNDERGROUND FIBER OPTIC

------ UNDERGROUND TELEPHONE

NO PARKING

× 972.5 SPOT ELEVATION

TR TREE (GEN)

SCALE IN FEET

THE SHOPPES AT ESTELLE



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Eden Prairie, Minnesota 55344 www.edfarrarch.com

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Loucks Project No. PAK

Project Lead ZHW Drawn By ZHW Checked By 05/31/19 Review Date SHEET INDEX

EXISTING CONDITIONS C1-2 DEMOLITION PLAN C2-1 SITE PLAN C3-1 GRADING PLAN C3-2 SWPPP PLAN C3-3 SWPPP NOTES UTILITY PLAN DETAILS **DETAILS** LANDSCAPE PLAN

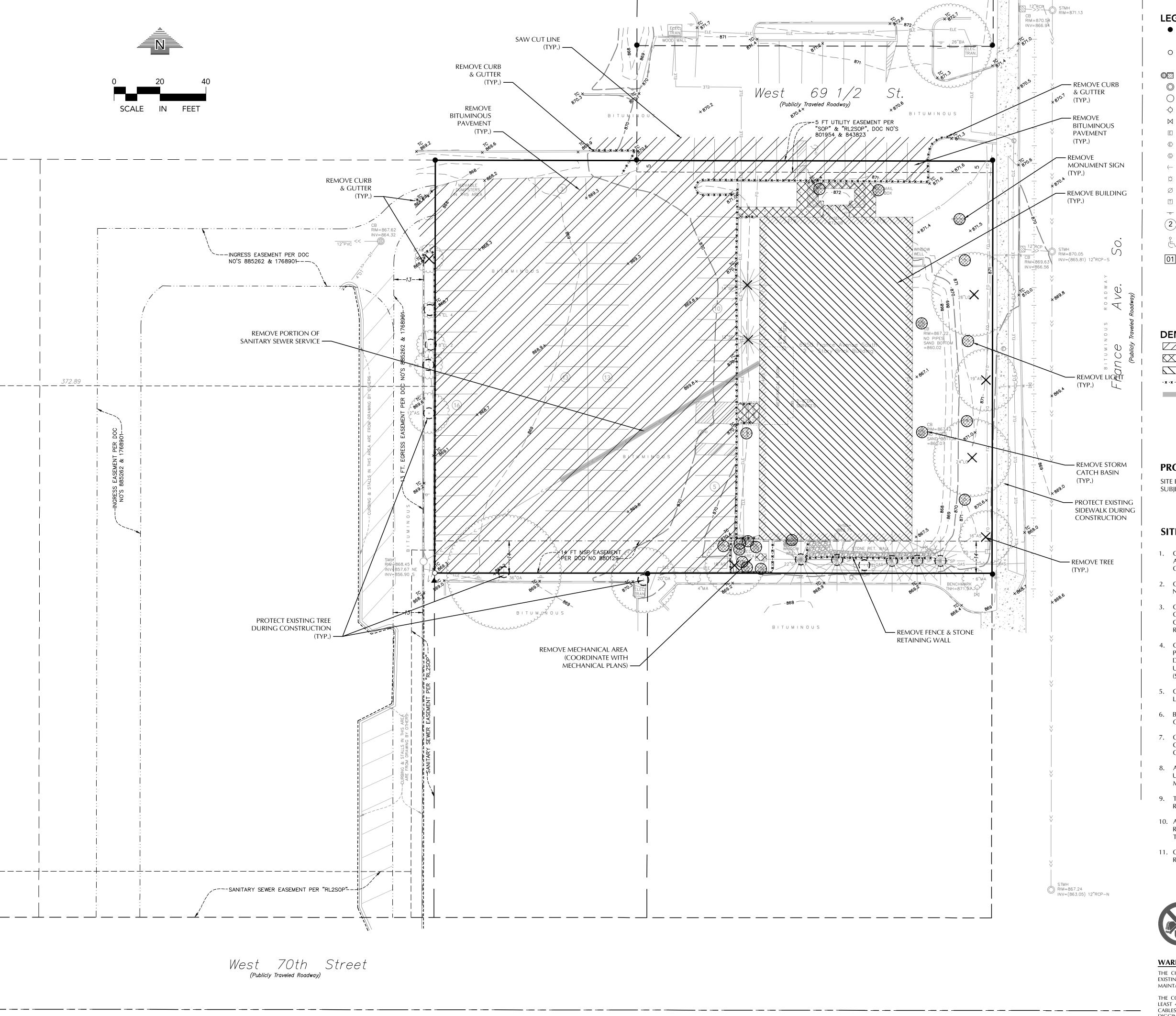
TREE INVENTORY PLAN

Gopher State One Cal TWIN CITY AREA: 651-454-0002 TOLL FREE: 1-800-252-1166

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CALLING FOR LOCATIONS OF ALL EXISTING UTILITIES. THEY SHALL COOPERATE WITH ALL UTILITY COMPANIES IN MAINTAINING THEIR SERVICE AND / OR RELOCATION OF LINES.

THE CONTRACTOR SHALL CONTACT GOPHER STATE ONE CALL AT 651-454-0002 AT LEAST 48 HOURS IN ADVANCE FOR THE LOCATIONS OF ALL UNDERGROUND WIRES, CABLES, CONDUITS, PIPES, MANHOLES, VALVES OR OTHER BURIED STRUCTURES BEFORE DIGGING. THE CONTRACTOR SHALL REPAIR OR REPLACE THE ABOVE WHEN DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.

EXISTING CONDITIONS



LEGEND FOUND OPEN IRON MONUMENT ---->> STORM SEWER UNLESS SHOWN OTHERWISE SANITARY SEWER O SET 1/2 INCH X 14 INCH IRON ----- | WATERMAIN MONUMENT, MARKED "LS 48988" SANITARY SEWER SERVICE — w — WATER SERVICE STORM MANHOLE ——ELE—— UNDERGROUND ELECTRIC SANITARY MANHOLE ————F0——— UNDERGROUND FIBER OPTIC
HYDRANT ———GAS——— UNDERGROUND GAS

UNDERGROUND TELEPHONE **E ELECTRIC TRANSFORMER** OVERHEAD UTILITY © ELECTRIC METER — ○ — ○ CHAIN LINK FENCE G GAS METER CONCRETE CURB RETAINING WALL CONCRETE POWER POLE NO PARKING

TELEPHONE PEDESTAL → SIGN (2) PARKING STALL COUNT

DISABLED PARKING STALL 01 SCHEDULE B II ITEM

×972.5 SPOT ELEVATION TREE LINE **CONIFEROUS TREE** PI PINE **DECIDUOUS TREE** as **ash** MA MAPLE

///// EXISTING BUILDING

TR TREE (GEN)

__ 912 CONTOUR

DEMOLITION LEGEND

REMOVE EXISTING BITUMINOUS SURFACE REMOVE EXISTING CONCRETE SURFACE REMOVE EXISTING BUILDING & ACCESSORIES

TREE PROTECTION FENCE

· * · * · * · * · * · * · REMOVE EXISTING CURB & GUTTER, RETAINING WALLS, FENCE, ETC.

REMOVE EXISTING UTILITIES

REMOVE EXISTING MANHOLES, POWER POLES, LIGHT POLES, BOLLARDS, SIGNS, ETC. REMOVE EXISTING TREES

PROJECT BENCHMARK

SITE BENCHMARK: TOP NUT OF HYDRANT LOCATED NEAR SOUTHEAST CORNER OF SUBJECT PROPERTY, AS SHOWN HEREON. ELEVATION = 871.97 (NGVD 29)

SITE DEMOLITION SPECIFICATIONS

- CONTRACTOR SHALL REMOVE AND/OR RELOCATE EXISTING PRIVATE UTILITIES AS NECESSARY. CONTRACTOR TO COORDINATE ACTIVITIES WITH UTILITY COMPANIES.
- CONTRACTOR SHALL PROTECT SURFACE AND SUBSURFACE FEATURES NOT NOTED FOR REMOVAL.
- CONTRACTOR TO CLEAR AND GRUB EXISTING VEGETATION WITHIN CONSTRUCTION LIMITS, STRIP TOP SOIL, AND STOCKPILE ON-SITE. REFER TO GRADING PLAN AND SWPPP FOR SEDIMENT AND EROSION CONTROL REQUIREMENTS.
- 4. CLEAR AND GRUB AND REMOVE ALL TREES, VEGETATION AND SITE DEBRIS PRIOR TO GRADING. ALL REMOVED MATERIAL SHALL BE HAULED FROM THE SITE DAILY. EROSION CONTROL MEASURES SHALL BE IMMEDIATELY ESTABLISHED UPON REMOVAL. SEE THE STORMWATER POLLUTION PREVENTION PLAN
- 5. CONTRACTOR SHALL REMOVE ALL SITE SURFACE FEATURES WITHIN REMOVAL LIMITS UNLESS OTHERWISE NOTED.
- BITUMINOUS PAVEMENT REMOVALS ARE TO BE MADE TO A VERTICAL SAW CUT OR TO A NEAT MILLED EDGE.
- 7. CONCRETE PAVEMENT, SIDEWALK, CURB & GUTTER AND OTHER POURED CONCRETE ITEMS ARE TO BE REMOVED TO AN EXISTING EXPANSION OR CONTRACTION JOINT. SAW CUT AS NECESSARY FOR A NEAT EDGE OF REMOVAL.
- 8. ALL REMOVAL ITEMS SHALL BECOME THE PROPERTY OF THE CONTRACTOR UNLESS SPECIFIED OTHERWISE AND SHALL BE DISPOSED OF OFF-SITE IN A MANNER MEETING ALL APPLICABLE REGULATIONS.
- 9. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REMOVAL AND REPLACEMENT OF ALL SIGNS, MAILBOXES, ETC.
- 10. ANY DAMAGE TO ITEMS NOT NOTED TO BE REMOVED SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE REPAIRED OR REPLACED TO ORIGINAL CONDITION WITH NO ADDITIONAL COMPENSATION.
- 11. CONTRACTOR SHALL COORDINATE WITH UTILITY COMPANIES PRIOR TO REMOVAL, RELOCATION OR PROTECTING EXISTING UTILITY LINES, POLE, ETC.



TWIN CITY AREA: 651-454-0002

WARNING:

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CALLING FOR LOCATIONS OF ALL EXISTING UTILITIES. THEY SHALL COOPERATE WITH ALL UTILITY COMPANIES IN MAINTAINING THEIR SERVICE AND / OR RELOCATION OF LINES.

THE CONTRACTOR SHALL CONTACT GOPHER STATE ONE CALL AT 651-454-0002 AT LEAST 48 HOURS IN ADVANCE FOR THE LOCATIONS OF ALL UNDERGROUND WIRES. CABLES, CONDUITS, PIPES, MANHOLES, VALVES OR OTHER BURIED STRUCTURES BEFORE DIGGING. THE CONTRACTOR SHALL REPAIR OR REPLACE THE ABOVE WHEN DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.





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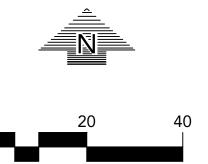
SUBMITTAL/REVISIONS

Loucks Project No.

PAK Project Lead ZHW Drawn By ZHW Checked By 05/31/19 Review Date SHEET INDEX

EXISTING CONDITIONS C1-2 DEMOLITION PLAN C2-1 SITE PLAN C3-1 GRADING PLAN C3-2 SWPPP PLAN SWPPP NOTES C3-3 C4-1 UTILITY PLAN C8-1 DETAILS C8-2 DETAILS LANDSCAPE PLAN TREE INVENTORY PLAN

DEMO PLAN



PROJECT BENCHMARK

SITE BENCHMARK: TOP NUT OF HYDRANT LOCATED NEAR SOUTHEAST CORNER OF SUBJECT PROPERTY, AS SHOWN HEREON. ELEVATION = 871.97 (NGVD 29)

SITE PLAN LEGEND





Gopher State One Call TWIN CITY AREA: 651-454-0002 TOLL FREE: 1-800-252-1166

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EGEND		
EXISTING		PROPOSED
	CATCH BASIN	
	STORM MANHOLE	•
>	FLARED END SECTION	ď
\bigcirc	Sanitary manhole	•
\(\Q	HYDRANT	•
\bowtie	GATE VALVE	H
©	POST INDICATOR VALVE	⊚ PIV
W	WATER MANHOLE / WELL	•
₽	LIGHT POLE	*
Ø	POWER POLE	•
Œ	ELECTRIC METER	
©	GAS METER	
T	TELEPHONE PEDESTAL	
- 0-	SIGN	- o-
	BENCHMARK	
Вм#	SOIL BORING	⊕ _n
2	PARKING STALL COUNT	(2)
Ŀ	ACCESSIBLE PARKING STALL	į.
——>>—	STORM SEWER	
>>DT	DRAINTILE	—>>—DT—
>	SANITARY SEWER	
FM	FORCEMAIN	FN
——————————————————————————————————————	Watermain	—— ı ——
s	Sanitary sewer service	
	WATER SERVICE	
ELE	underground electric	———ELE———
———F0———	UNDERGROUND FIBER OPTIC	——
GAS	underground gas	———GAS———
——— TEL ———	underground telephone	——теь——
———ОН———	OVERHEAD UTILITY	——ОН——
x x	FENCE	
<u> </u>	CHAIN LINK FENCE	
	CONCRETE CURB	
	RETAINING WALL	
A A A A A A	CONCRETE	N 4 N . 4 A . 4
	NO PARKING	
/////	BUILDING	
_972	CONTOUR	972
_× 972.5	SPOT ELEVATION	×972.5
1,0%	DIRECTION OF FLOW	1.0%
~~~~	TREE LINE	~~~~
	PARKING SETBACK LINE	PSBL

#### GENERAL NOTES ■

1. MINNESOTA STATE STATUTE REQUIRES NOTIFICATION PER "GOPHER STATE ONE CALL" PRIOR TO COMMENCING ANY GRADING, EXCAVATION OR UNDERGROUND WORK.

**BUILDING SETBACK LINE** 

- 2. THE CONTRACTOR SHALL FIELD VERIFY LOCATIONS AND ELEVATIONS OF EXISTING UTILITIES AND TOPOGRAPHIC FEATURES PRIOR TO COMMENCEMENT OF CONSTRUCTION ACTIVITY. THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF ANY DISCREPANCIES OR VARIATIONS FROM THE PLANS.
- 3. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO AVOID PROPERTY DAMAGE TO ADJACENT PROPERTIES DURING THE CONSTRUCTION PHASE OF THIS PROJECT. THE CONTRACTOR WILL BE HELD RESPONSIBLE FOR ANY DAMAGES TO ADJACENT PROPERTIES OCCURRING DURING THE CONSTRUCTION PHASE OF THIS
- 4. THE CONTRACTOR WILL BE RESPONSIBLE FOR PROVIDING AND MAINTAINING TRAFFIC CONTROL DEVICES SUCH AS BARRICADES, WARNING SIGNS, DIRECTIONAL SIGNS, FLAGMEN AND LIGHTS TO CONTROL THE MOVEMENT OF TRAFFIC WHERE NECESSARY. PLACEMENT OF THESE DEVICES SHALL BE APPROVED BY THE ENGINEER PRIOR TO PLACEMENT. TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE APPROPRIATE MINNESOTA DEPARTMENT OF TRANSPORTATION STANDARDS.
- 5. IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, THE CONTRACTOR WILL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS ON THE JOB SITE, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING THE PERFORMANCE OF THE WORK. THIS REQUIREMENT WILL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS.
- 6. THE DUTY OF THE ENGINEER OR THE DEVELOPER TO CONDUCT CONSTRUCTION REVIEW OF THE CONTRACTORS PERFORMANCE IS NOT INTENDED TO INCLUDE REVIEW OF THE ADEQUACY OF THE CONTRACTORS SAFETY MEASURES IN, OR NEAR THE CONSTRUCTION SITE.
- 7. BEFORE BEGINNING CONSTRUCTION THE CONTRACTOR SHALL INSTALL EROSION AND SEDIMENTATION CONTROL MEASURES IN ACCORDANCE WITH NPDES PERMIT REQUIREMENTS, BEST MANAGEMENT PRACTICES, STATE AND LOCAL REQUIREMENTS AND THE DETAILS SHOWN ON THE DETAIL SHEET(S) OF THE PROJECT PLANS.
- 8. ALL CONSTRUCTION PERMITS, APPLICATIONS AND FEES ARE THE RESPONSIBILITY OF THE CONTRACTOR.
- 9. ALL ENTRANCES AND CONNECTIONS TO CITY STREETS SHALL BE CONSTRUCTED PER THE REQUIREMENTS OF THE STATE AND LOCAL JURISDICTIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL PERMITS AND NOTIFICATIONS AS REQUIRED.
- 10. ALL STREET REPAIRS AND PATCHING SHALL BE PERFORMED PER THE REQUIREMENTS OF THE CITY. ALL TRAFFIC CONTROL SHALL BE PROVIDED BY THE CONTRACTOR AND SHALL BE ESTABLISHED PER THE REQUIREMENTS OF THE MINNESOTA MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND THE CITY. THIS SHALL INCLUDE ALL SIGNAGE, BARRICADES, FLASHERS AND FLAGGERS AS NEEDED. ALL PUBLIC STREETS SHALL BE OPEN TO TRAFFIC AT ALL TIMES.
- 11. ADJUST ALL EXISTING STRUCTURES, BOTH PUBLIC AND PRIVATE TO THE PROPOSED GRADES WHERE DISTURBED AND COMPLY WITH ALL REQUIREMENTS OF THE UTILITY OWNERS. STRUCTURES BEING RESET TO PAVED AREAS MUST MEET OWNERS REQUIREMENTS FOR TRAFFIC LOADING.
- 12.EXISTING TOPOGRAPHY PROVIDED BY LOUCKS ASSOCIATES, TOPOGRAPHIC SURVEY DATED 01/24/19.
- 13. THE CONTRACTOR MUST HAVE A CITY LICENSE.

14. A CITY RIGHT-OF-WAY PERMIT IS REQUIRED PRIOR TO WORKING WITHIN CITY ROW.





CIVIL ENGINEERING LAND SURVEYING LANDSCAPE ARCHITECTURE ENVIRONMENTAL

7200 Hemlock Lane, Suite 300 Maple Grove, MN 55369 763.424.5505 www.loucksinc.com



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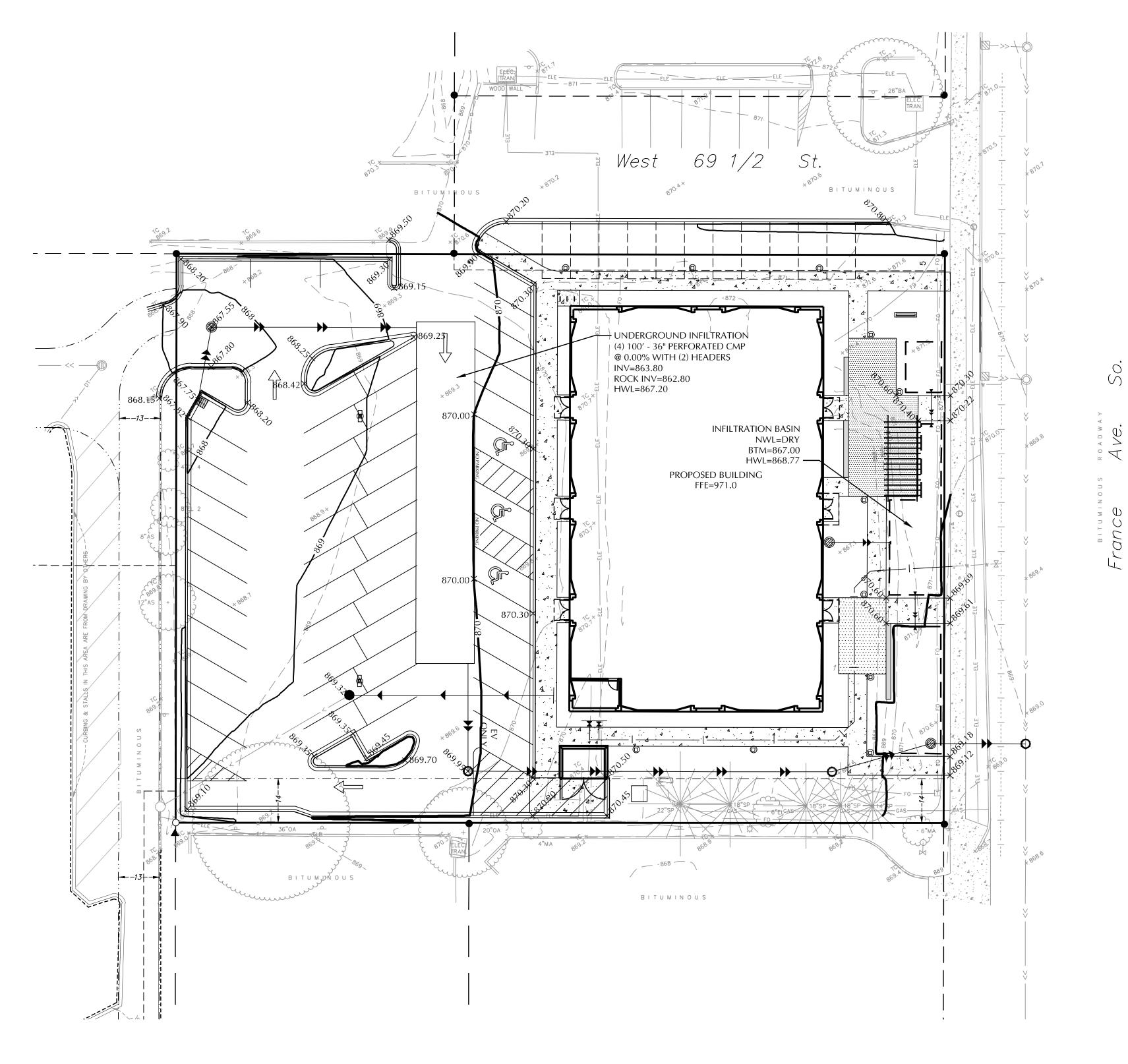
18697B Loucks Project No. PAK Project Lead ZHW Drawn By ZHW Checked By

05/31/19 Review Date

SHEET INDEX **EXISTING CONDITIONS** C1-2 DEMOLITION PLAN C2-1 SITE PLAN C3-1 GRADING PLAN C3-2 SWPPP PLAN C3-3 SWPPP NOTES UTILITY PLAN DETAILS

C4-1 C8-1 C8-2 DETAILS L1-1 LANDSCAPE PLAN TREE INVENTORY PLAN

SITE PLAN





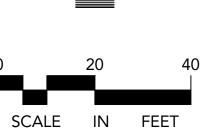


### TWIN CITY AREA: 651-454-0002 TOLL FREE: 1-800-252-1166

#### **WARNING:**

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CALLING FOR LOCATIONS OF ALL EXISTING UTILITIES. THEY SHALL COOPERATE WITH ALL UTILITY COMPANIES IN MAINTAINING THEIR SERVICE AND / OR RELOCATION OF LINES.

THE CONTRACTOR SHALL CONTACT GOPHER STATE ONE CALL AT 651-454-0002 AT LEAST 48 HOURS IN ADVANCE FOR THE LOCATIONS OF ALL UNDERGROUND WIRES, CABLES, CONDUITS, PIPES, MANHOLES, VALVES OR OTHER BURIED STRUCTURES BEFORE DIGGING. THE CONTRACTOR SHALL REPAIR OR REPLACE THE ABOVE WHEN DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.



#### PROJECT BENCHMARK

SITE BENCHMARK: TOP NUT OF HYDRANT LOCATED NEAR SOUTHEAST CORNER OF SUBJECT PROPERTY, AS SHOWN HEREON. ELEVATION = 871.97 (NGVD 29)

existing		PROPOSED
	CATCH BASIN	
	STORM MANHOLE	
>	flared end section	9
$\bigcirc$	SANITARY MANHOLE	•
<b>\( \rightarrow</b>	HYDRANT	•
$\bowtie$	GATE VALVE	H
<b>(P)</b>	POST INDICATOR VALVE	<b>⊚</b> PIV
<b>W</b>	WATER MANHOLE / WELL	•
Þ	LIGHT POLE	*
Ø	POWER POLE	ø
Œ	ELECTRIC METER	
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DAM #	BENCHMARK	
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——>> ——	STORM SEWER	
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s	SANITARY SEWER SERVICE	<b></b>
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ELE	UNDERGROUND ELECTRIC	——ELE——
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GAS	UNDERGROUND GAS	GAS
TEL	UNDERGROUND TELEPHONE	——— TEL ———
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xx	FENCE	<b>*</b> *-
	CHAIN LINK FENCE	<b></b>
	CONCRETE CURB	
	RETAINING WALL	
P	CONCRETE	A A A A A
	NO PARKING	
	BUILDING	
_972	CONTOUR	972
× 972.5	SPOT ELEVATION	×972.5
1.0%	DIRECTION OF FLOW	1.0%
~~~~	TREE LINE	~~~~
	PARKING SETBACK LINE	
	BUILDING SETBACK LINE	

GRADING, DRAINAGE & EROSION CONTROL NOTES

- 1. SPOT ELEVATIONS REPRESENT FINISHED SURFACE GRADES, GUTTER/FLOW LINE, FACE OF BUILDING, OR EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- 2. ALL DISTURBED UNPAVED AREAS ARE TO RECEIVE MINIMUM OF 4 INCHES OF PREMIUM TOP SOIL AND SEED/MULCH OR SOD. THESE AREAS SHALL BE WATERED/MAINTAINED BY THE CONTRACTOR UNTIL VEGETATION IS ESTABLISHED. VERIFY WITH LANDSCAPE PLAN.
- 3. FOR SITE RETAINING WALLS "TW" EQUALS SURFACE GRADE AT TOP FACE OF WALL (NOT TOP OF WALL), "GW" EQUALS SURFACE GRADE AT BOTTOM FACE OF WALL (NOT BOTTOM OF BURIED WALL COURSES).
- 4. REFER TO THE REPORT OF GEOTECHNICAL EXPLORATION AND REVIEW (REPORT NO. xxxxx), DATED xxxxx AS PREPARED BY xxxxx FOR AN EXISTING SUBSURFACE SITE CONDITION ANALYSIS AND CONSTRUCTION RECOMMENDATIONS.
- 5. STREETS MUST BE CLEANED AND SWEPT WHENEVER TRACKING OF SEDIMENTS OCCURS AND BEFORE SITES ARE LEFT IDLE FOR WEEKENDS AND HOLIDAYS, OR AS DIRECTED BY CITY. A REGULAR SWEEPING SCHEDULE MUST BE ESTABLISHED.
- 6. DUST MUST BE ADEQUATELY CONTROLLED.
- 7. SEE SWPPP FOR ADDITIONAL EROSION CONTROL NOTES AND REQUIREMENTS.
- 8. SEE UTILITY PLAN FOR WATER, STORM AND SANITARY SEWER INFORMATION.
- 9. SEE SITE PLAN FOR CURB AND BITUMINOUS TAPER LOCATIONS.
- 10. A STREET SWEEPER MUST BE AVAILABLE WITHIN 3 HOURS UPON NOTICE FROM THE CITY THAT THE STREETS NEED TO BE SWEPT.
- 11. THE CONTRACTOR ALONG WITH THE OWNER SHALL OBTAIN ALL NECESSARY PERMITS AND APPROVALS FROM GOVERNING AUTHORITIES, INCLUDING ANY CITY PERMITS AND THE NPDES PERMIT FROM THE MPCA.
- 12. INSTALL EROSION CONTROL AND TREE PROTECTION MEASURES BEFORE BEGINNING SITE GRADING ACTIVITIES. SOME EROSION CONTROLS SUCH AS BALE CHECKS AND TEMPORARY SILT PONDS MAY BE INSTALLED AS GRADING OCCURS IN SPECIFIC AREAS. MAINTAIN EROSION CONTROLS THROUGHOUT THE GRADING PROCESS AND REMOVE WHEN TURF HAS BEEN ESTABLISHED.
- 13. THE CONTRACTOR SHALL ADHERE TO ALL REQUIREMENTS OF THE MPCA NPDES PERMIT. THE AREA TO BE DISTURBED SHALL BE MINIMIZED AND TURF SHALL BE ESTABLISHED WITHIN THE TIME REQUIRED.
- 14. GRADES SHOWN ARE FINISHED GRADES.
- 15. FINAL GRADING TOLERANCES ARE +/-0.1 FEET TO FINISH GRADES.

THE SHOPPES AT ESTELLE **L**OUCKS LANDSCAPE ARCHITECTURE 7200 Hemlock Lane, Suite 300 EDWARD FARR **ARCHITECTS INC** 7710 Golden Triangle Drive

Eden Prairie, Minnesota 55344 www.edfarrarch.com

CIVIL ENGINEERING

LAND SURVEYING

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763.424.5505 www.loucksinc.com

Maple Grove, MN 55369

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	SUBMITTAL/REVISIONS
/31/19	CITY SUBMITTA

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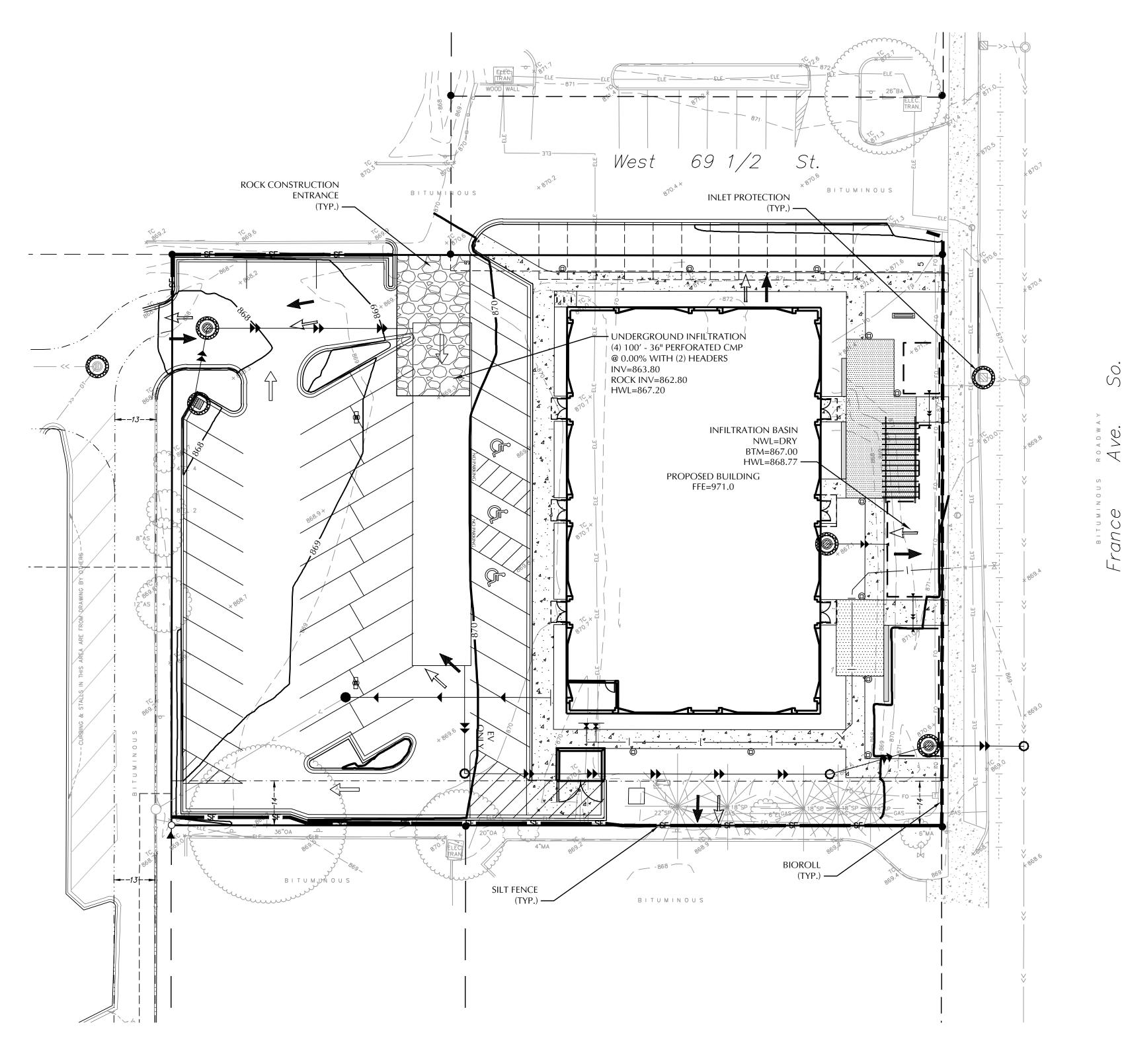
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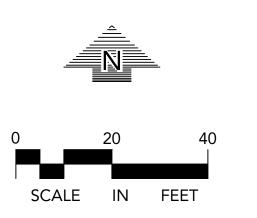
QUALITY CONTROL

	SHEET INDEX
Review Date	05/31/19
Checked By	ZHW
Drawn By	ZHW
Project Lead	PAk
Loucks Project No.	18697E

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GRADING







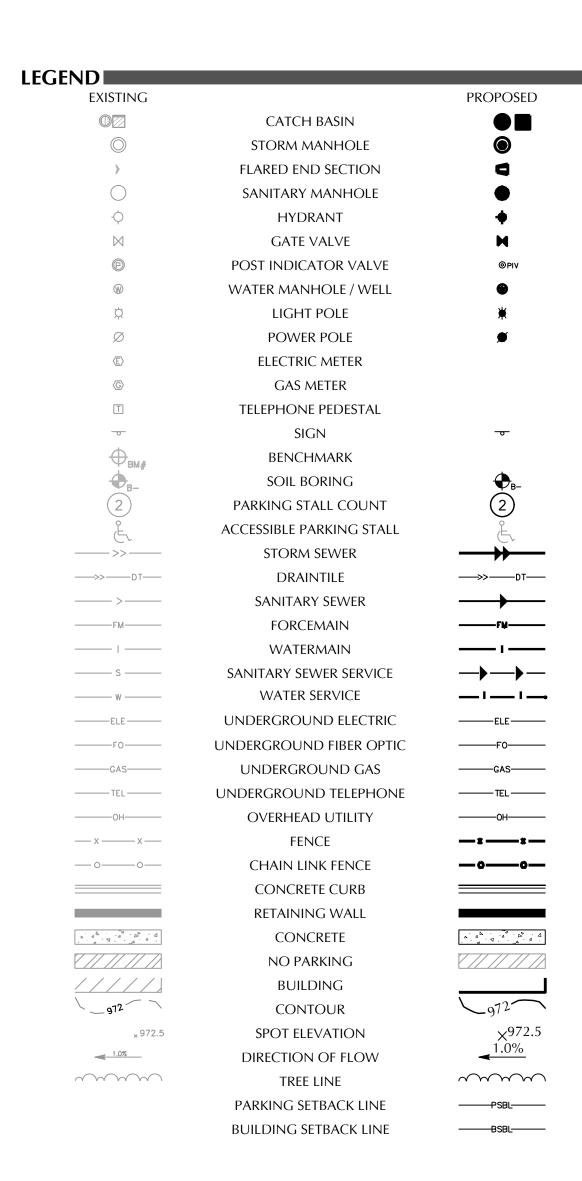
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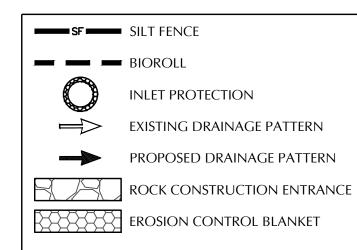
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PROJECT BENCHMARK

SITE BENCHMARK: TOP NUT OF HYDRANT LOCATED NEAR SOUTHEAST CORNER OF SUBJECT PROPERTY, AS SHOWN HEREON. ELEVATION = 871.97 (NGVD 29)



SWPPP LEGEND



THE SHOPPES
AT ESTELLE



LOUCKS

PLANNING CIVIL ENGINEERING LAND SURVEYING LANDSCAPE ARCHITECTURE ENVIRONMENTAL

7200 Hemlock Lane, Suite 300 Maple Grove, MN 55369 763.424.5505 www.loucksinc.com



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	SUBMIT	TAL/REVISION:	S
5/31/19		CITY SUBMITT	AL

PROFESSIONAL SIGNATURE

I hereby certify that this plan, specification or report wa prepared by me or under my direct super 6 bround that I am a duly Licensed Professional E Signary under the laws of the State of Minnesota.

Zathan H. Webber - PE

License place

Batta	
QU	ALITY CONTROL
Loucks Project No.	18697
Project Lead	P.A
Drawn By	ZH
Checked By	ZH
Review Date	05/31/
	SHEET INDEX

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8-1	DETAILS
8-2	DETAILS
I - 1	LANDSCAPE PLAN
2-1	TREE INVENTORY PLAN

SWPPP PLAN

- THE NATURE OF THIS PROJECT WILL CONSIST OF CONSTRUCTING A RETAIL BUILDING, SURFACE PARKING LOT, PATIO AREA, & STORM WATER TREATMENT FACILITIES.
- 2. THE INTENDED SEQUENCING OF MAJOR CONSTRUCTION ACTIVITIES ARE AS FOLLOWS:
- INSTALL VEHICLE TRACKING BMP
 INSTALL SILT FENCE AROUND SITE
- 3. INSTALL PROTECTIVE FENCE AROUND STORMWATER TREATMENT AREAS
- 4. CLEAR AND GRUB SITE
- 5. STRIP AND STOCKPILE TOPSOIL6. REMOVE PAVEMENTS AND UTILITIES
- 7. CONSTRUCT STORMWATER MANAGEMENT BASINS
- 8. INSTALL SILT FENCE AROUND BASINS 9. ROUGH GRADE SITE
- 10. IMPORT CLEAN FILL FOR REPLACEMENT AND BALANCE
- 11. INSTALL UTILITIES
- 12. INSTALL SMALL UTILITIES (GAS, TELEPHONE, ELECTRIC, CABLE, ETC.)
- 13. INSTALL BUILDING FOUNDATIONS
- 14. INSTALL CURB AND GUTTER
- 15. INSTALL PAVEMENTS AND WALKS16. FINAL GRADE SITE
- 17. REMOVE ACCUMULATED SEDIMENT FROM BASINS18. SEED AND MULCH
- 19. WHEN ALL CONSTRUCTION ACTIVITY IS COMPLETE AND THE SITE IS STABILIZED, REMOVE SILT FENCE AND RESEED ANY AREAS DISTURBED BY THE REMOVAL.
- 3. SITE DATA:
 AREA OF DISTURBANCE: 1.00± AC
- PRE-CONSTRUCTION IMPERVIOUS AREA: 0.78 AC POST-CONSTRUCTION IMPERVIOUS AREA: 0.80 AC
- GENERAL SOIL TYPE: SEE GEOTECHNICAL REPORT
- 4. THE LOCATION OF AREAS NOT TO BE DISTURBED MUST BE IDENTIFIED WITH FLAGS, STAKES, SIGNS, SILT FENCE, ETC. BEFORE CONSTRUCTION BEGINS.
- 5. ALL DISTURBED GROUND LEFT INACTIVE FOR SEVEN (7) OR MORE DAYS SHALL BE STABILIZED BY SEEDING OR SODDING (ONLY AVAILABLE PRIOR TO SEPTEMBER 15) OR BY MULCHING OR COVERING OR OTHER EQUIVALENT CONTROL MEASURE.
- 6. ON SLOPES 3:1 MAINTAIN SHEET FLOW AND MINIMIZE RILLS AND/OR GULLIES, SLOPE LENGTHS CAN NOT BE GREATER THAN 75 FEET.
- DENOTES SLOPES 3:1. ALL 3:1 SLOPES TO BE STABILIZED WITH EROSION CONTROL BLANKET
- 7. ALL STORM DRAINS AND INLETS MUST BE PROTECTED UNTIL ALL SOURCES OF POTENTIAL DISCHARGE ARE STABILIZED.
- 8. TEMPORARY SOIL STOCKPILES MUST HAVE EFFECTIVE SEDIMENT CONTROL AND CAN NOT BE PLACED IN SURFACE WATERS OR STORM WATER CONVEYANCE SYSTEMS. TEMPORARY STOCKPILES WITHOUT SIGNIFICANT AMOUNT OF SILT, CLAY, OR ORGANIC COMPOUNDS ARE EXEMPT EX: CLEAN AGGREGATE STOCK PILES, DEMOLITION CONCRETE STOCKPILES, SAND STOCKPILES.
- 9. SEDIMENT LADEN WATER MUST BE DISCHARGED TO A SEDIMENTATION BASIN WHENEVER POSSIBLE. IF NOT POSSIBLE, IT MUST BE TREATED WITH THE APPROPRIATE BMP'S.
- 10. SOLID WASTE MUST BE DISPOSED OF PROPERLY AND MUST COMPLY WITH MPCA DISPOSAL REQUIREMENTS.
- 11. EXTERNAL WASHING OF CONSTRUCTION VEHICLES MUST BE LIMITED TO A DEFINED AREA OF THE SITE, RUNOFF MUST BE PROPERLY CONTAINED.
- 12. NO ENGINE DEGREASING IS ALLOWED ON SITE.

ESTIMATED QUANTITIES

PREFABRICATED CONCRETE WASHOUT

SILT FENCE (STANDARD)

INLET PROTECTION

BIOROLL

EROSION CONTROL BLANKET

DESCRIPTION

TEMPORARY ROCK CONSTRUCTION ENTRANCE

- 13. THE OWNER WHO SIGNS THE NPDES PERMIT APPLICATION IS A PERMITTEE AND IS RESPONSIBLE FOR COMPLIANCE WITH ALL TERMS AND CONDITIONS OF THE PERMIT. THE OPERATOR (CONTRACTOR) WHO SIGNS THE NPDES PERMIT APPLICATION IS A PERMITTEE FOR ALL APPLICABLE CONSTRUCTION ACTIVITY REQUIREMENTS FOUND IN SECTIONS 3,4,6-24 OF THE NPDES PERMIT AND IS JOINTLY RESPONSIBLE WITH THE OWNER FOR COMPLIANCE WITH THOSE PORTIONS OF THE PERMIT.
- 14. TERMINATION OF COVERAGE-PERMITTEE(S) WISHING TO TERMINATE COVERAGE MUST SUBMIT A NOTICE OF TERMINATION (NOT) TO THE MPCA. ALL PERMITTEE(S) MUST SUBMIT A NOT WITHIN 30 DAYS AFTER ONE OR MORE OF THE FOLLOWING CONDITIONS HAVE BEEN MET:
- A. FINAL STABILIZATION, PER NPDES PERMIT SECTION 13 HAS BEEN ACHIEVED ON

QUANTITY

NOT ALLOWED

325

N/A

200

UNIT

EΑ

EΑ

LF

SY

EΑ

ALL PORTIONS OF THE SITE FOR WHICH THE PERMITTEE IS RESPONSIBLE

B. TRANSFER OF OWNERSHIP AS DESCRIBED IN THE PERMIT.

15. INSPECTIONS

- A. INITIAL INSPECTION FOLLOWING SILT FENCE INSTALLATION BY CITY REPRESENTATIVE IS REQUIRED.
- EXPOSED SOIL AREAS: ONCE EVERY 7 DAYS AND WITHIN 24 HOURS FOLLOWING A 0.5" OVER 24 HOUR RAIN EVENT.
- C. STABILIZED AREAS: ONCE EVERY 30 DAYS
- D. FROZEN GROUND: AS SOON AS RUNOFF OCCURS OR PRIOR TO RESUMING CONSTRUCTION.
- E. INSPECTION AND MAINTENANCE RECORDS MUST BE RETAINED FOR 3 YEARS AFTER FILING OF THE NOTICE OF TERMINATION AND MUST INCLUDE: DATE AND TIME OF ACTION, NAME OF PERSON(S) CONDUCTING WORK, FINDING OF INSPECTIONS AND RECOMMENDATIONS FOR CORRECTIVE ACTION, DATE AND AMOUNT OF RAINFALL EVENTS GREATER THAN 0.5 INCHES IN A 24 HOUR PERIOD
- 16. MINIMUM MAINTENANCE
- A. SILT FENCE TO BE REPAIRED, REPLACED, SUPPLEMENTED WHEN
- NONFUNCTIONAL, OR 1/3 FULL; WITHIN 24 HOURS
- B. SEDIMENT BASINS DRAINED AND SEDIMENT REMOVED WHEN REACHES 1/2 STORAGE VOLUME. REMOVAL MUST BE COMPLETE WITHIN 72 HOURS OF DISCOVERY.
- C. SEDIMENT REMOVED FROM SURFACE WATERS WITHIN (7)SEVEN DAYSD. CONSTRUCTION SITE EXITS INSPECTED, TRACKED SEDIMENT REMOVED WITH 24
- HOURS.
- E. PROVIDE COPIES OF EROSION INSPECTION RESULTS TO CITY ENGINEER FOR ALL EVENTS GREATER THAN ½" IN 24 HOURS
- 17. THE SWPPP, INCLUDING ALL CHANGES TO IT, AND INSPECTIONS AND MAINTENANCE RECORDS MUST BE KEPT AT THE SITE DURING CONSTRUCTION ACTIVITY BY THE PERMITTEE(S) WHO HAVE OPERATIONAL CONTROL OF THE SITE.
- 18. OWNER MUST KEEP RECORDS OF ALL PERMITS REQUIRED FOR THE PROJECT, THE SWPPP, ALL INSPECTIONS AND MAINTENANCE, PERMANENT OPERATION AND MAINTENANCE AGREEMENTS, AND REQUIRED CALCULATIONS FOR TEMPORARY AND PERMANENT STORM WATER MANAGEMENT SYSTEMS. THESE RECORDS MUST BE RETAINED FOR THREE YEARS AFTER FILING NPDES NOTICE OF TERMINATION.
- 19. SWPPP MUST BE AMENDED WHEN:
 - A. THERE IS A CHANGE IN DESIGN, OPERATION, MAINTENANCE, WEATHER OR SEASONAL CONDITIONS THAT HAS A SIGNIFICANT EFFECT ON DISCHARGE
 B. INSPECTIONS INDICATE THAT THE SWPPP IS NOT EFFECTIVE AND DISCHARGE IS
 - EXCEEDING WATER QUALITY STANDARDS.

 C. THE BMP'S IN THE SWPPP ARE NOT CONTROLLING POLLUTANTS IN DISCHARGES OR IS NOT CONSISTENT WITH THE TERMS AND CONDITIONS OF THE PERMIT.
- 20. CONCRETE WASHOUT AREA
- A. CONCRETE WASH-OUT IS NOT ALLOWED ON-SITE.
- 21. IN THE EVENT OF ENCOUNTERING A WELL OR SPRING DURING CONSTRUCTION CONTRACTOR TO CEASE CONSTRUCTION ACTIVITY AND NOTIFY ENGINEER.
- 22. PIPE OUTLETS MUST BE PROVIDED WITH TEMPORARY OR PERMANENT ENERGY DISSIPATION WITHIN 24 HOURS AFTER CONNECTION TO A SURFACE WATER.
- 23. FINAL STABILIZATION
- FINAL STABILIZATION REQUIRES THAT ALL SOIL DISTURBING ACTIVITIES HAVE BEEN COMPLETED AND THAT DISTURBED AREAS ARE STABILIZED BY A UNIFORM PERENNIAL VEGETATIVE COVER WITH 70% OF THE EXPECTED FINAL DENSITY, AND THAT ALL PERMANENT PAVEMENTS HAVE BEEN INSTALLED. ALL TEMPORARY BMP'S SHALL BE REMOVED, DITCHES STABILIZED, AND SEDIMENT SHALL BE REMOVED FROM PERMANENT CONVEYANCES AND SEDIMENTATION BASINS IN ORDER TO RETURN THE POND TO DESIGN CAPACITY.
- 24. TEMPORARY SEDIMENTATION BASINS

RECEIVING WATERS

FRANCE AVE STORM SEWER

NAME OF WATER BODY

- THE TEMPORARY SEDIMENTATION BASINS SHALL BE CONSTRUCTED AND MADE OPERATIONAL PRIOR TO DISTURBANCE OF 10 OR MORE ACRES DRAINING TO A COMMON LOCATION.
- B. TEMPORARY SEDIMENTATION BASINS ARE REQUIRED PRIOR TO RUNOFF LEAVING THE CONSTRUCTION SITE OR ENTERING SURFACE WATERS WHEN 10 OR MORE ACRES OF DISTURBED SOILS DRAIN TO A COMMON LOCATION. THE BASIN MUST PROVIDE 3,600 CUBIC FEET OF STORAGE BELOW THE OUTLET PER ACRE DRAINED. IF HYDRAULIC CALCULATIONS ARE AVAILABLE, THE TEMPORARY SEDIMENTATION BASIN MUST PROVIDE A STORAGE VOLUME EQUIVALENT TO THE 2-YEAR, 24-HOUR STORM, BUT IN NO CASE LESS THAN 1800 CUBIC FEET PER ACRE DRAINED. THE TEMPORARY SEDIMENTATION BASIN MUST BE

TYPE OF

WATER

BODY

XXXXX

SPECIAL

WATER | WATER

IMPAIRED

- CONSTRUCTED AND MADE OPERATIONAL CONCURRENT WITH THE START OF SOIL DISTURBANCE UP GRADIENT OF THE POND. THE TEMPORARY SEDIMENTATION BASIN SHALL BE DESIGNED TO PREVENT SHORT CIRCUITING. THE OUTFALL SHALL BE DESIGNED TO REMOVE FLOATABLE DEBRIS, ALLOW FOR COMPLETE DRAWDOWN OF THE POND FOR MAINTENANCE ACTIVITIES, AND HAVE ENERGY DISSIPATION. THE EMERGENCY SPILLWAY SHALL
- C. TEMPORARY SEDIMENTATION BASINS SHALL BE SITUATED OUTSIDE OF SURFACE WATERS AND ANY REQUIRED BUFFER ZONE, AND MUST BE DESIGNED TO AVOID DRAINING WETLANDS, UNLESS THE IMPACT IS IN COMPLIANCE WITH THE REQUIREMENTS OF THIS PERMIT.
- D. EXCESSIVE SEDIMENT-LADEN WATER THAT IS NOT PROPERLY FILTERED WILL NOT BE PERMITTED TO DISCHARGE FROM SITE.

25. DEWATERING AND BASIN DRAINING

- A. TURBID OR SEDIMENT-LADEN WATERS RELATED TO DEWATERING OR BASIN DRAINING SHALL BE DISCHARGED TO A TEMPORARY OR PERMANENT SEDIMENTATION BASIN ON THE PROJECT SITE UNLESS INFEASIBLE. THE TEMPORARY OR PERMANENT BASIN MAY DISCHARGE TO SURFACE WATERS IF THE BASIN WATER HAS BEEN VISUALLY CHECKED TO ENSURE ADEQUATE TREATMENT HAS BEEN OBTAINED IN THE BASIN AND THAT THE NUISANCE CONDITIONS WILL NOT RESULT FROM THE DISCHARGE. DISCHARGE POINTS SHALL BE ADEQUATELY PROTECTED FROM EROSION AND PROPER VELOCITY DISSIPATION PROVIDED.
- B. ALL WATER FROM DEWATERING OR BASIN-DRAINING ACTIVITIES MUST BE DISCHARGED IN A MANNER THAT DOES NOT CAUSE NUISANCE CONDITIONS, EROSION IN THE RECEIVING CHANNELS OR ON DOWN SLOPE PROPERTIES, OR INUNDATION IN WETLANDS CAUSING SIGNIFICANT ADVERSE IMPACTS TO THE WETLAND
- C. IF FILTERS WITH BACKWASH WATERS ARE USED, THE BACKWASH WATER SHALL BE HAULED AWAY FOR DISPOSAL, RETURNED TO THE BEGINNING OF THE TREATMENT PROCESS, OR INCORPORATED INTO SITE IN A MANNER THAT DOES NOT CAUSE EROSION. BACKWASH WATER MAY BE DISCHARGED TO SANITARY SEWER IF PERMISSION IS GRANTED BY THE SANITARY SEWER AUTHORITY.

26. POLLUTION PREVENTION

- A. BUILDING PRODUCTS THAT HAVE THE POTENTIAL TO LEACH POLLUTANTS MUST BE UNDER COVER TO PREVENT DISCHARGE OR PROTECTED BY AN EFFECTIVE MEANS DESIGNED TO MINIMIZE CONTACT WITH STORMWATER.
- B. PESTICIDES, HERBICIDES, INSECTICIDES, FERTILIZERS, TREATMENT CHEMICALS, AND LANDSCAPE MATERIALS MUST BE UNDER COVER.
- AND LANDSCAPE MATERIALS MUST BE UNDER COVER.

 C. HAZARDOUS MATERIALS AND TOXIC WASTE MUST BE PROTECTED TO PREVENT
- VANDALISM.

 D. SOLID WASTE MUST BE STORED, COLLECTED AND DISPOSED OF IN COMPLIANCE
- WITH MINN. R. CH 7035.

 E. PORTABLE TOILETS MUST BE POSITIONED SO THAT THEY ARE SECURE AND WILL
- E. PORTABLE TOILETS MUST BE POSITIONED SO THAT THEY ARE SECURE AND WILL NOT BE TIPPED OR KNOCKED OVER. SANITARY WASTE MUST BE DISPOSED OF PROPERLY IN ACCORDANCE WITH MINN. R. CH 7041.
- F. DISCHARGE OF SPILLED OR LEAKED CHEMICALS, INCLUDING FUEL, FROM ANY AREA WHERE CHEMICALS OR FUEL WILL BE LOADED OR UNLOADED SHALL BE PREVENTED USING DRIP PANS OR ABSORBENTS. SUPPLIES SHALL BE AVAILABLE AT ALL TIMES TO CLEAN UP DISCHARGED MATERIALS AND THAT AN APPROPRIATE DISPOSAL METHOD MUST BE AVAILABLE FOR RECOVERED SPILLED MATERIALS.

27. DESIGN CALCULATIONS

TEMPORARY & PERMANENT STORMWATER TREATMENT ARE DESIGNED TO MEET MPCA GENERAL & SPECIAL WATER REQUIREMENTS. CALCULATIONS ARE PART OF THE HYDROLOGY REPORT, WHICH IS TO BE CONSIDERED PART OF THE SWPPP DOCUMENTS. SEE HYDROLOGY REPORT FOR ADDITIONAL INFORMATION.

28. GENERAL STORMWATER DISCHARGE REQUIREMENTS

ALL REQUIREMENTS LISTED IN SECTIONS 5-7,14,16-19,21,24 OF THE PERMIT FOR DESIGN OF THE PERMANENT STORMWATER MANAGEMENT SYSTEM AND DISCHARGE HAVE BEEN INCLUDED IN THE PREPARATION OF THIS SWPPP. THESE INCLUDE BUT ARE NOT LIMITED TO:

- A. THE EXPECTED AMOUNT, FREQUENCY, INTENSITY, AND DURATION OF
- B. THE NATURE OF STORMWATER RUNOFF AND RUN-ON AT THE SITE.
- PEAK FLOW RATES AND STORMWATER VOLUMES TO MINIMIZE EROSION AT OUTLETS AND DOWNSTREAM CHANNEL AND STREAM BANK EROSION.

 THE RANGE OF SOIL PARTICLE SIZES EXPECTED TO BE PRESENT ON THE SITE.

29. CONSTRUCTION OF FILTRATION BASINS

TYPE OF

SPECIAL

WATER

- A. NO HEAVY TRAFFIC ON FILTRATION AREAS. CONSTRUCTION TO BE DONE WITH MINIMAL COMPACTION TO FILTRATION AREAS. IF COMPACTION IS ENCOUNTERED, BASIN SOILS FOR THE FIRST & MUST BE REMOVED & RELAID.
- B. INFILTRATION SYSTEMS MUST NOT BE EXCAVATED TO FINAL GRADE UNTIL THE CONTRIBUTING DRAINAGE AREA HAD BEEN CONSTRUCTED AND FULLY STABILIZED UNLESS RIGOROUS EROSION PREVENTION AND SEDIMENT CONTROLS ARE PROVIDED(SECTION 16.4).

CERTIFICATION

University of Minnesota Zachary H. Webber

Design of Construction SWPPP (May 31 2021)

- C. WHEN AN INFILTRATION SYSTEM IS EXCAVATED TO FINAL GRADE (OR WITHIN THREE (3) FEET OF FINAL GRADE), THE PERMITTEE(S) MUST EMPLOY RIGOROUS EROSION PREVENTION AND SEDIMENT CONTROLS (E.G., DIVERSION BERMS) TO KEEP SEDIMENT AND RUNOFF COMPLETELY AWAY FROM THE INFILTRATION AREA. THE AREA MUST BE STAKED OFF AND MARKED SO THAT HEAVY CONSTRUCTION VEHICLES OR EQUIPMENT WILL NOT COMPACT THE SOIL IN THE PROPOSED INFILTRATION AREA.
- D. TO PREVENT CLOGGING OF THE INFILTRATION OR FILTRATION SYSTEM, THE PERMITTEE(S) MUST USE A PRETREATMENT DEVICE SUCH AS A VEGETATED FILTER STRIP, SMALL SEDIMENTATION BASIN, OR WATER QUALITY INLET (E.G., GRIT CHAMBER) TO SETTLE PARTICULATES BEFORE THE STORMWATER DISCHARGES INTO THE INFILTRATION OF FILTRATION SYSTEM.

30. POST CONSTRUCTION

THE WATER QUALITY VOLUME THAT MUST BE RETAINED ON SITE BY THE PROJECT'S PERMANENT STORMWATER MANAGEMENT SYSTEM DESCRIBED IN SECTION 15 SHALL BE ONE (1) INCH OF RUNOFF FROM THE NEW IMPERVIOUS SURFACES CREATED BY THE PROJECT. SEE SECTION 15 FOR MORE INFORMATION ON INFILTRATION DESIGN, PROHIBITIONS AND APPROPRIATE SITE CONDITIONS.

31. RESPONSIBILITIES

- A. THE OWNER MUST IDENTIFY A CONTRACTOR WHO WILL OVERSEE THE SWPPP IMPLEMENTATION AND THE PERSON RESPONSIBLE FOR INSPECTION AND MAINTENANCE:
- THE OWNER MUST IDENTIFY THE A PERSON WHO WILL BE RESPONSIBLE FOR LONG TERM OPERATIONS AND MAINTENANCE OF THE PERMANENT STORMWATER MANAGEMENT SYSTEM:

32. TRAINING REQUIREMENTS

THE PERMITTES(S) SHALL ENSURE THE FOLLOWING INDIVIDUALS IDENTIFIED IN THIS PART HAVE BEEN TRAINED IN ACCORDANCE WITH THIS PERMIT'S TRAINING REQUIREMENTS.

1. WHO MUST BE TRAINED:

- A. INDIVIDUAL(S) PREPARING THE SWPPP FOR THE PROJECT
- B. INDIVIDUAL(S) OVERSEEING IMPLEMENTATION OF, REVISING, AND/OR AMENDING THE SWPPP AND INDIVIDUALS(S) PERFORMING INSPECTIONS FOR THE PROJECT. ONE OF THESE INDIVIDUAL(S) MUST BE AVAILABLE FOR AN ONSITE INSPECTION WITHIN 72 HOURS UPON REQUEST BY THE MPCA.
- C. INDIVIDUAL(S) PERFORMING OR SUPERVISING THE INSTALLATION, MAINTENANCE AND REPAIR OF BMPS. AT LEAST ONE INDIVIDUAL ON A PROJECT MUST BE TRAINED IN THESE JOB DUTIES.

TRAINING CONTENT:

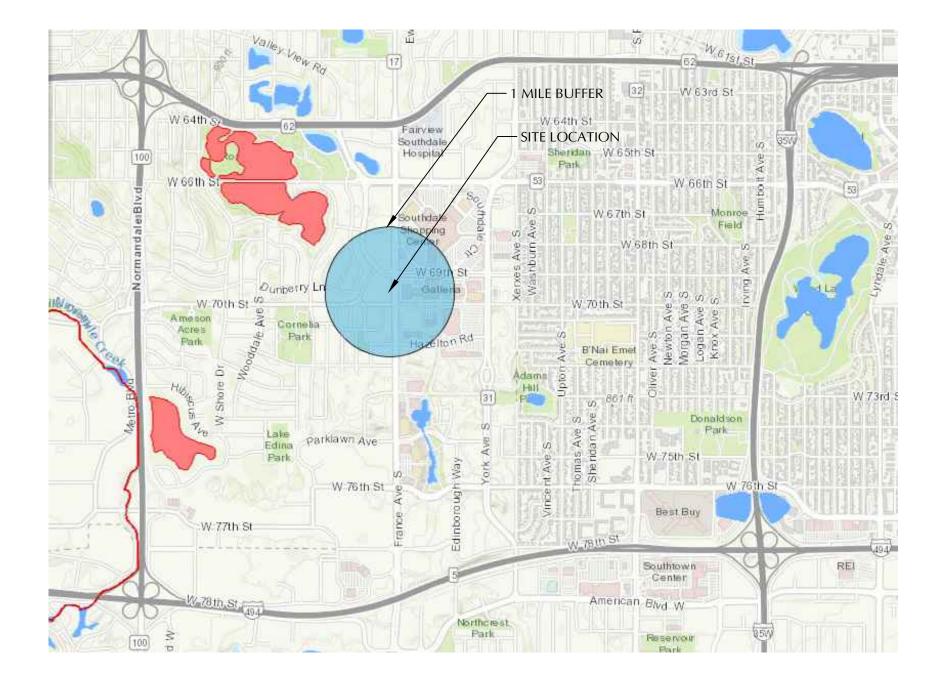
- THE CONTENT AND EXTENT OF TRAINING MUST BE COMMENSURATE WITH THE INDIVIDUAL'S JOB DUTIES AND RESPONSIBILITIES WITH REGARD TO ACTIVITIES COVERED UNDER THIS PERMIT FOR THE PROJECT. AT LEAST ONE INDIVIDUAL PRESENT ON THE PERMITTED PROJECT SITE (OR AVAILABLE TO THE PROJECT SITE IN 72 HOURS) MUST BE TRAINED IN THE JOB DUTIES DESCRIBED IN SECTION 21.2B AND SECTION 21.2C.
- 33. THE PERMITTEE(S) SHALL ENSURE THAT THE INDIVIDUALS ARE TRAINED BY LOCAL, STATE, FEDERAL AGENCIES, PROFESSIONAL OR OTHER ENTITIES WITH EXPERTISE IN EROSION PREVENTION, SEDIMENT CONTROL, PERMANENT STORMWATER MANAGEMENT AND THE MINNESOTA NPDES/SDS CONSTRUCTION STORMWATER PERMIT. AN UPDATE REFRESHER-TRAINING MUST BE ATTENDED EVERY THREE (3) YEARS STARTING THREE (3) YEARS FROM THE ISSUANCE DATE OF THIS PERMIT.

33. <u>LIST OF CONTACTS</u>

TITLE	NAME	COMPANY	PHONE NUMBER
OWNER	LUIGI BERNARDI	ARCADIA	952-831-5002
PROJECT MANAGER	PAUL KANGAS	LOUCKS	763-496-6737
ENGINEER SWPPP	ZACHARY WEBBER	LOUCKS	763-496-6753
CONTRACTOR	PENDING		
SITE MANGER			

* MPCA 24HR. HAZARDOUS SPILL HOTLINE: 651-649-5457 OF 80420798

SPECIAL & IMPAIRED WATERS MAP



THE SHOPPES
AT ESTELLE

EDINA



LOUCKS

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LANDSCAPE ARCHITECTURE
ENVIRONMENTAL

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SUBMITTAL/REVISIONS

05/31/19 CITY SUBMITTA

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PROFESSIONAL SIGNATURE

hereby certify that this plan, specification or report or prepared by me or under my direct super is by and a man a duly Licensed Professional Eligible in under aws of the State of Minnesota.

Date OUALITY CONTROL

Loucks Project No. 18697B

Project Lead PAK

Drawn By ZHW

Checked By ZHW

Review Date

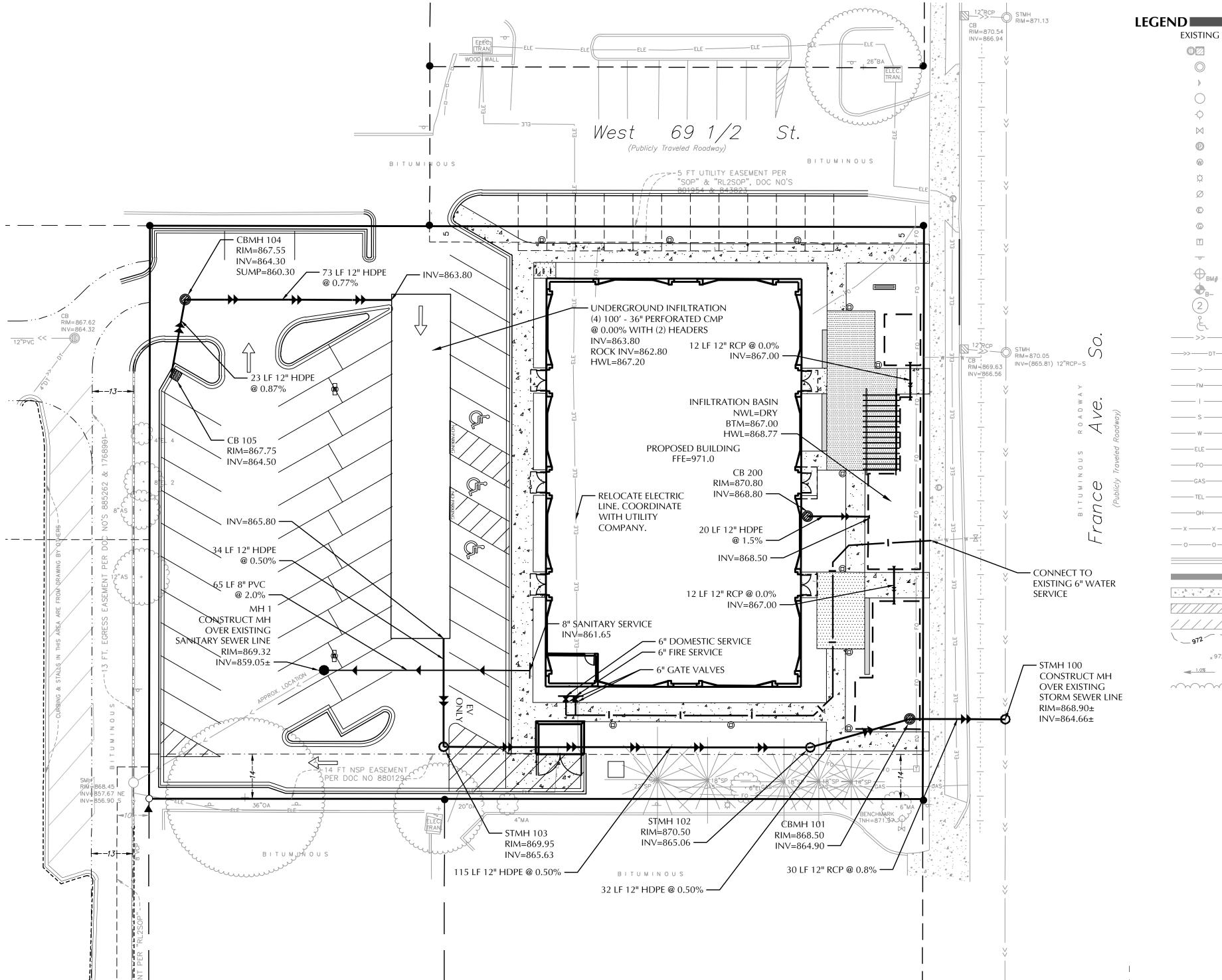
SHEET INDEX **EXISTING CONDITIONS** C1-2 DEMOLITION PLAN C2-1 SITE PLAN C3-1 GRADING PLAN C3-2 SWPPP PLAN C3-3 **SWPPP NOTES** C4-1 UTILITY PLAN C8-1 DETAILS C8-2 DETAILS LANDSCAPE PLAN

05/31/19

-1 TREE INVENTORY PLAN

SWPPP NOTES

C3-3



UTILITY NOTES

- 1. ALL SANITARY SEWER, STORM SEWER AND WATERMAIN UTILITIES SHALL BE FURNISHED AND INSTALLED PER THE REQUIREMENTS OF THE SPECIFICATIONS, THE CITY OF NEW HOPE, AND THE STANDARD UTILITIES SPECIFICATION OF THE CITY ENGINEERS ASSOCIATION OF MINNESOTA (CEAM), 2013 EDITION.
- 2. ALL UTILITY PIPE BEDDING SHALL BE COMPACTED SAND OR FINE GRANULAR MATERIAL. ALL COMPACTION SHALL BE PERFORMED PER THE REQUIREMENTS OF THE CEAM SPECIFICATION AND THE GEOTECHNICAL REPORT.
- 3. ALL CONNECTIONS TO EXISTING UTILITIES SHALL BE PERFORMED PER THE REQUIREMENTS OF THE STATE AND LOCAL JURISDICTIONS. THE CITY DEPARTMENT OF ENGINEERING AND BUILDING INSPECTIONS DEPARTMENT AND THE CONSTRUCTION ENGINEER MUST BE NOTIFIED AT LEAST 48 HOURS PRIOR TO ANY WORK WITHIN THE PUBLIC RIGHT OF WAY, OR WORK IMPACTING PUBLIC UTILITIES.
- 4. A MINIMUM OF 18 INCHES OF VERTICAL SEPARATION AND 10 FEET OF HORIZONTAL SEPARATION IS REQUIRED FOR ALL UTILITIES FROM THE WATERMAIN UNLESS OTHERWISE NOTED.
- 5. ALL NEW WATERMAIN AND SERVICES MUST HAVE A MINIMUM OF 7.5 FEET OF COVER. EXTRA DEPTH MAY BE REQUIRED TO MAINTAIN A MINIMUM 18" VERTICAL SEPARATION TO SANITARY OR STORM SEWER LINES. THE CONTRACTOR SHALL FIELD ADJUST WATERMAIN TO AVOID CONFLICTS WITH SANITARY SEWER, STORM SEWER, AND SERVICES AS REQUIRED. INSULATION OF WATER AND SANITARY SEWER LINES SHALL BE PROVIDED WHERE 7.5 FEET MINIMUM DEPTH CAN NOT BE ATTAINED.
- 6. ALL FIRE HYDRANTS SHALL BE LOCATED 5 FEET BEHIND BACK OF CURB OR EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.

7. PROPOSED PIPE MATERIALS:

WATERMAIN DIP 6" DIAMETER SANITARY SEWER PVC SDR 35 8" DIAMETER STORM SEWER RCP & HDPE 12" DIAMETER

- 8. CONTRACTOR AND MANHOLE FABRICATOR SHALL SUMP (LOWER) ALL STORM SEWER CATCH BASIN CASTINGS WITHIN PAVED AREAS 0.16 FEET OR 2-INCHES BELOW THE RIM ELEVATION DEPICTED ON THE UTILITY PLAN.
- 9. ALL STREET REPAIRS AND PATCHING SHALL BE PERFORMED PER THE REQUIREMENTS OF THE CITY. ALL TRAFFIC CONTROL SHALL BE PROVIDED BY THE CONTRACTOR AND SHALL BE ESTABLISHED PER THE REQUIREMENTS OF THE MINNESOTA MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MMUTCD) AND THE CITY. THIS SHALL INCLUDE ALL SIGNAGE, BARRICADES, FLASHERS AND FLAGGERS AS NEEDED. ALL PUBLIC STREETS SHALL BE OPEN TO TRAFFIC AT ALL TIMES. NO ROAD CLOSURES SHALL BE PERMITTED WITHOUT THE EXPRESSED AUTHORITY OF OF THE CITY.

10. THE CITY SHALL OPERATE ALL GATE VALVES.

STRUCTURE NO.	CASTING	MANHOLE SIZ
STMH 100	R-1642	48"
CBMH 101	R-4342	48"
STMH 102	R-1642	48"
STMH 103	R-1642	48"
CBMH 104	R-2552	48"
CB 105	R-3067	2'x3'
CBMH 200	STANDARD	24" NYLOPLAS

UTILITY STRUCTURE SCHEDULE

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	Ø	POWER POLE	•
	€	ELECTRIC METER	
	©	GAS METER	
	T	TELEPHONE PEDESTAL	
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ed R	———ELE———	underground electric	———ELE———
ravel	——FO——	underground fiber optic	———FO———
/ C T A V	GAS	underground gas	GAS
	——— TEL ———	underground telephone	——TEL——
OTTO (Pub	——ОН——	OVERHEAD UTILITY	———он———
) _	xx	FENCE	
_		CHAIN LINK FENCE	
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ATED		retaining wall	
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МН	1.0%	DIRECTION OF FLOW	₹ 1.0%
1G	~~~~~	TREE LINE	$\sim\sim$
R LINE		PARKING SETBACK LINE	————PSBL———
		BUILDING SETBACK LINE	——BSBL——

CATCH BASIN STORM MANHOLE

FLARED END SECTION SANITARY MANHOLE

HYDRANT GATE VALVE POST INDICATOR VALVE

WATER MANHOLE / WELL

LIGHT POLE





PROJECT BENCHMARK

SITE BENCHMARK: TOP NUT OF HYDRANT LOCATED NEAR SOUTHEAST CORNER OF SUBJECT PROPERTY, AS SHOWN HEREON. ELEVATION = 871.97 (NGVD 29)



WARNING:

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CALLING FOR LOCATIONS OF ALL EXISTING UTILITIES, THEY SHALL COOPERATE WITH ALL UTILITY COMPANIES IN MAINTAINING THEIR SERVICE AND / OR RELOCATION OF LINES.

THE CONTRACTOR SHALL CONTACT GOPHER STATE ONE CALL AT 651-454-0002 AT LEAST 48 HOURS IN ADVANCE FOR THE LOCATIONS OF ALL UNDERGROUND WIRES, CABLES, CONDUITS, PIPES, MANHOLES, VALVES OR OTHER BURIED STRUCTURES BEFORE DIGGING. THE CONTRACTOR SHALL REPAIR OR REPLACE THE ABOVE WHEN DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.



PROPOSED



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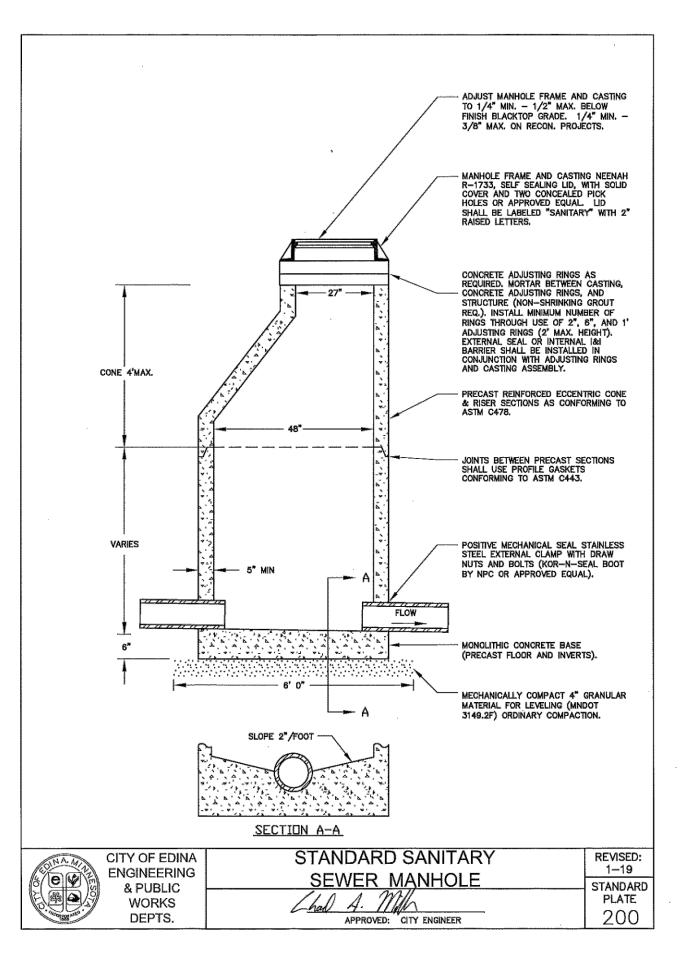
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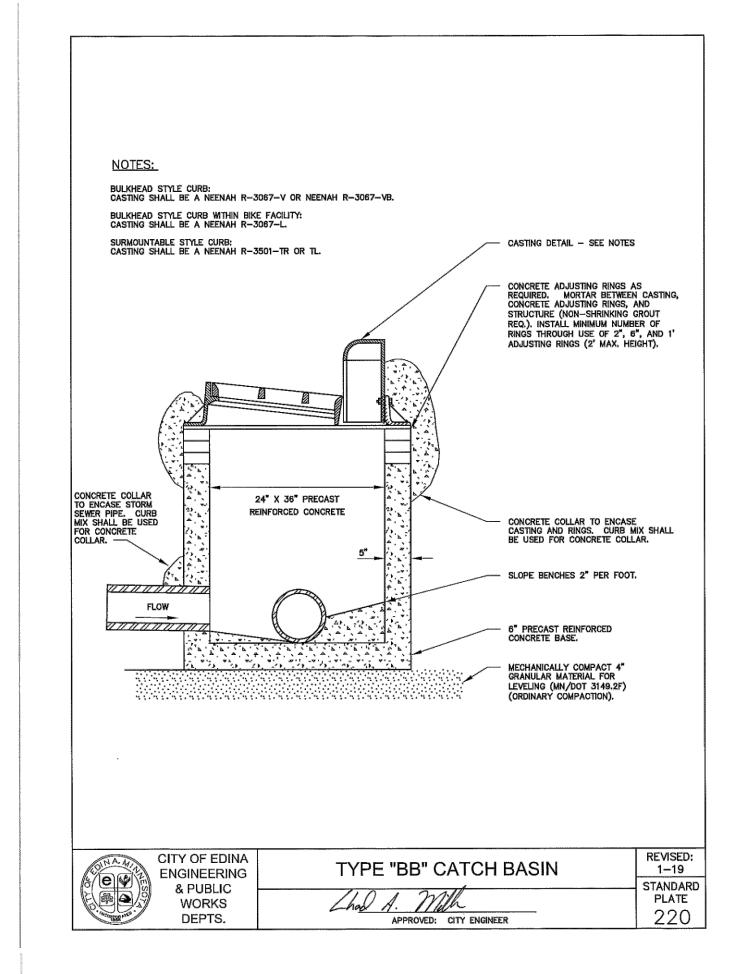
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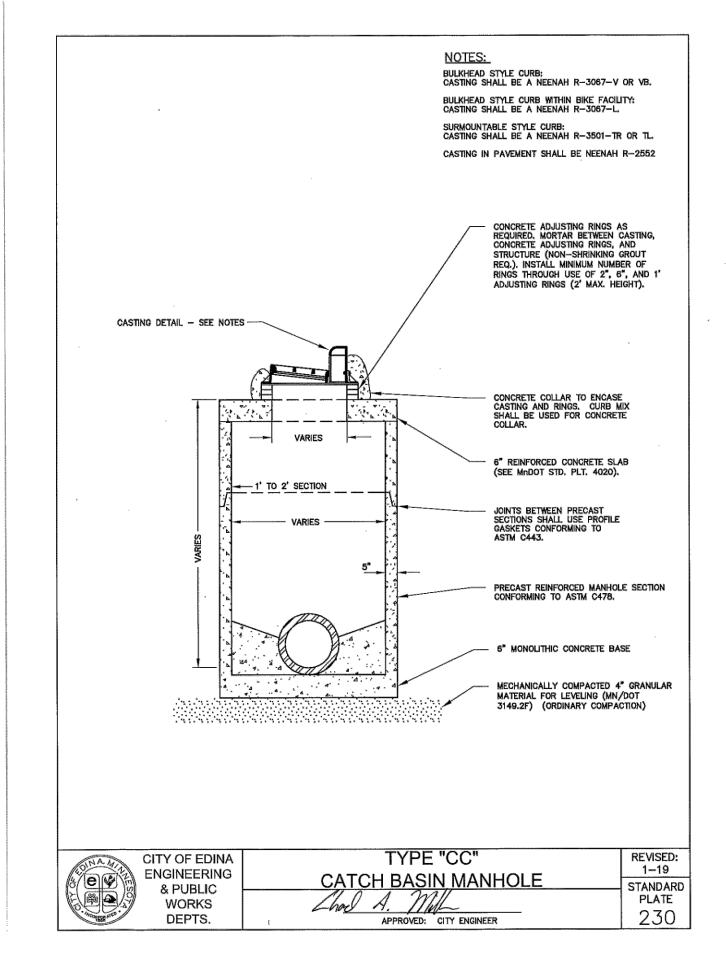
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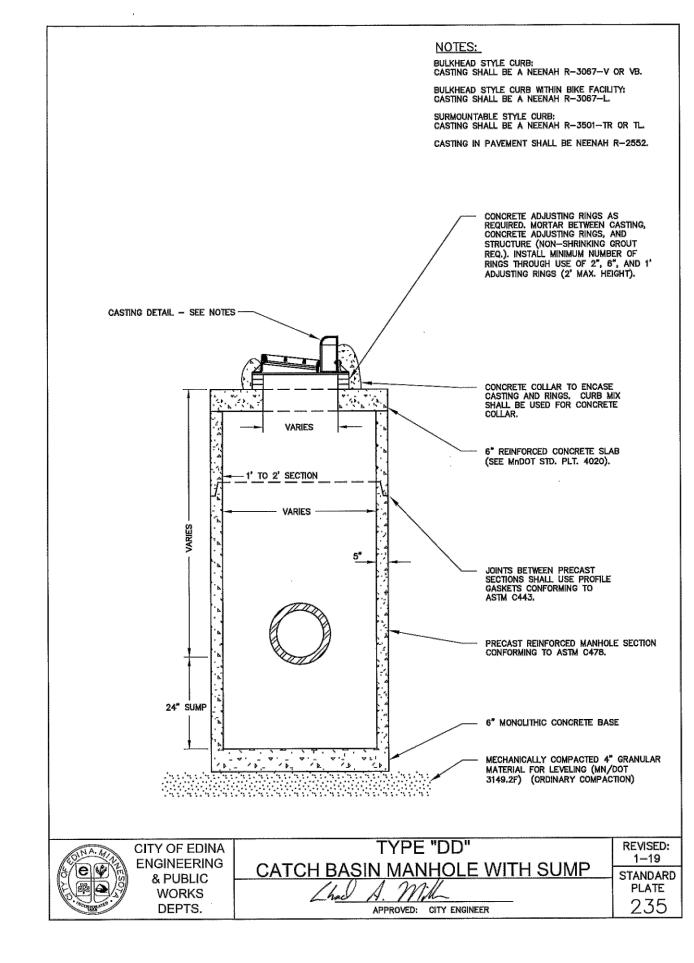
L2-1 TREE INVENTORY PLAN

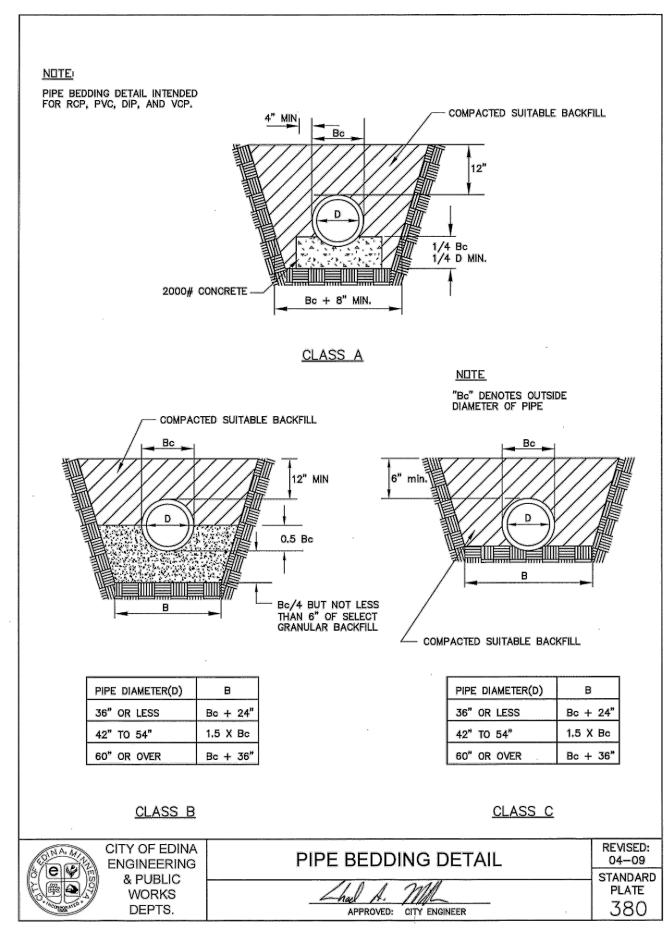
UTILITY PLAN

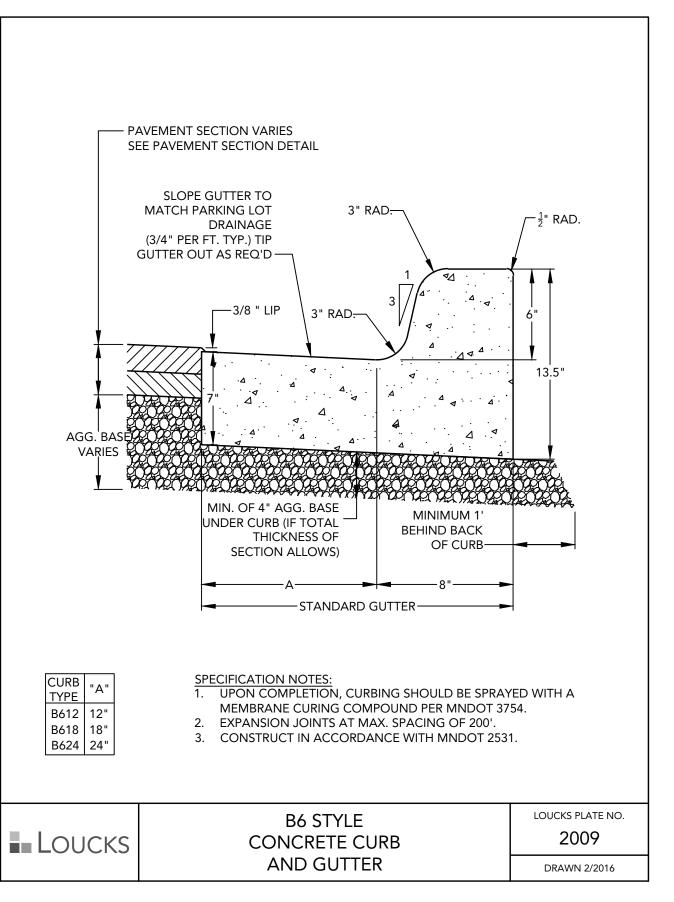


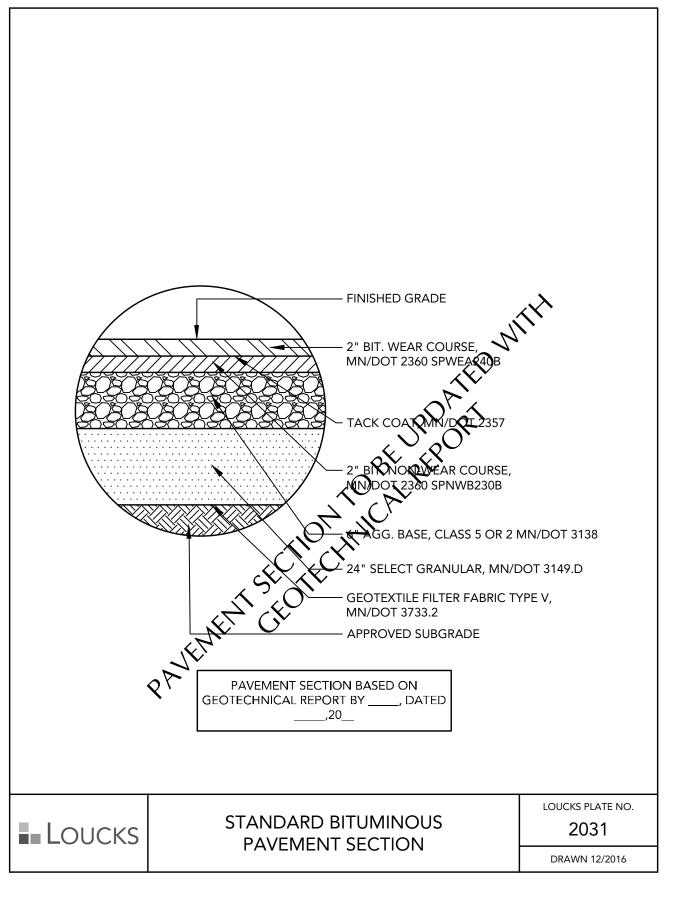


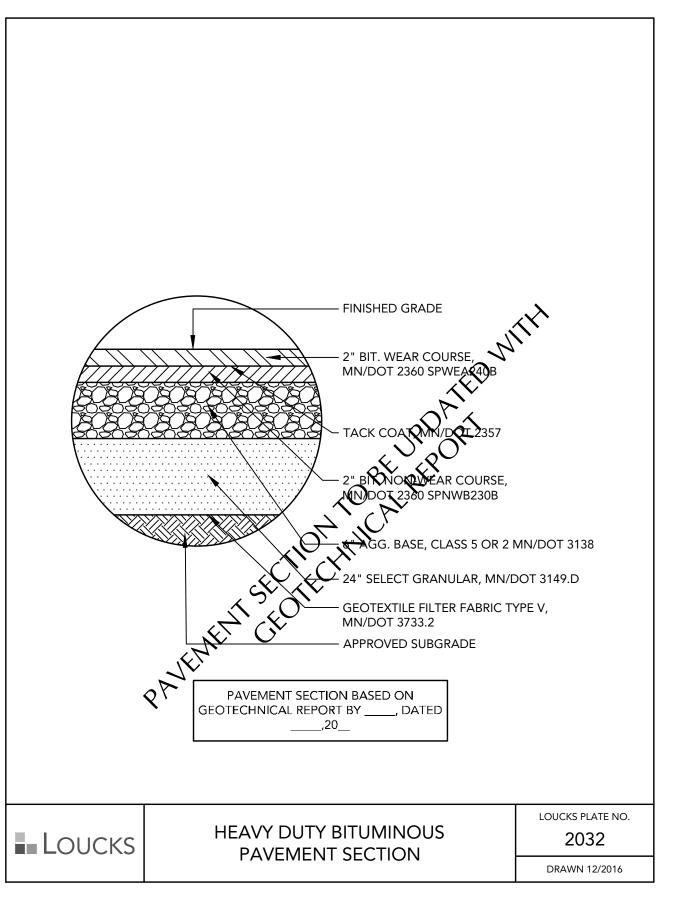
















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SUBMITTAL/REVISIONS

OS/SI/TY CITT SOBMITTAL

PROFESSIONAL SIGNATURE

I hereby certify that this plan, specification or select to prepared by me or under my direct super is bround to I am a duly Licensed Professional Eligibe in ander to laws of the State of Minnesota.

Zachar H. Webber - PE

License Tla

Data

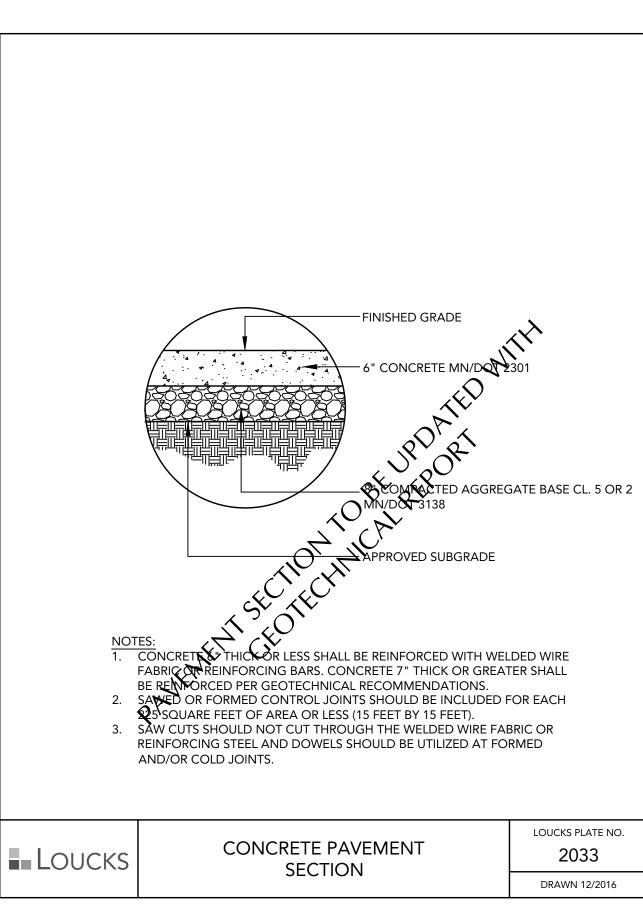
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Loucks Project No.	1869
Project Lead	P/
Drawn By	ZH
Checked By	ZH
Review Date	05/31/
	SHEET INDEX

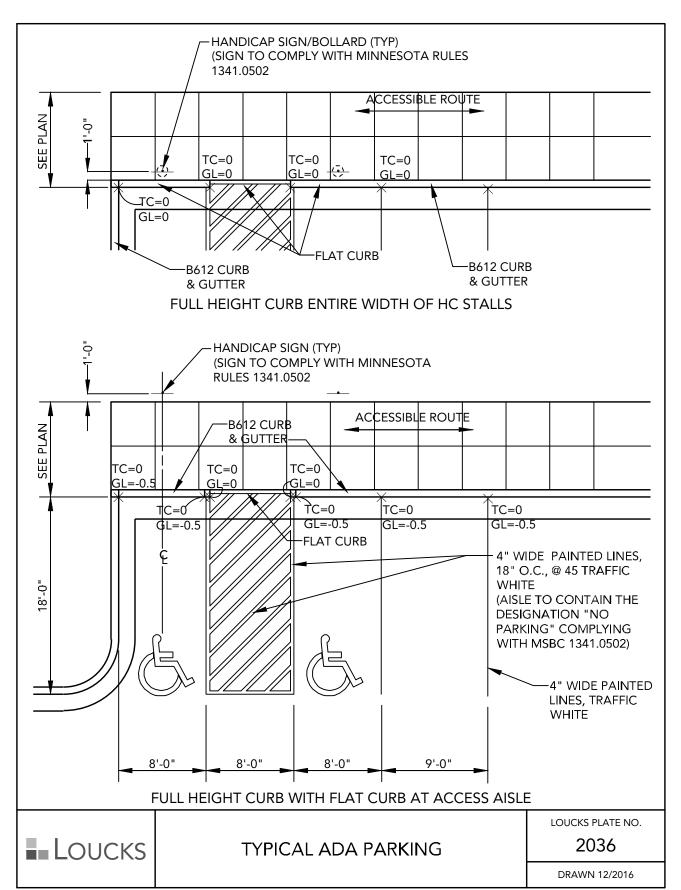
-2	DEMOLITION PLAN
-1	SITE PLAN
-1	GRADING PLAN
-2	SWPPP PLAN
-3	SWPPP NOTES
-1	UTILITY PLAN
-1.	DETAILS
-2	DETAILS

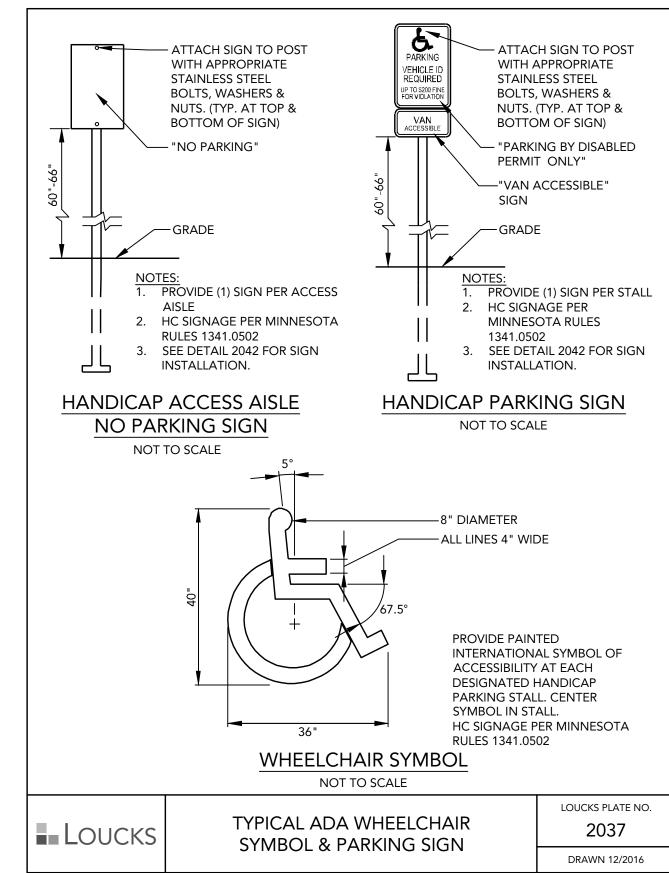
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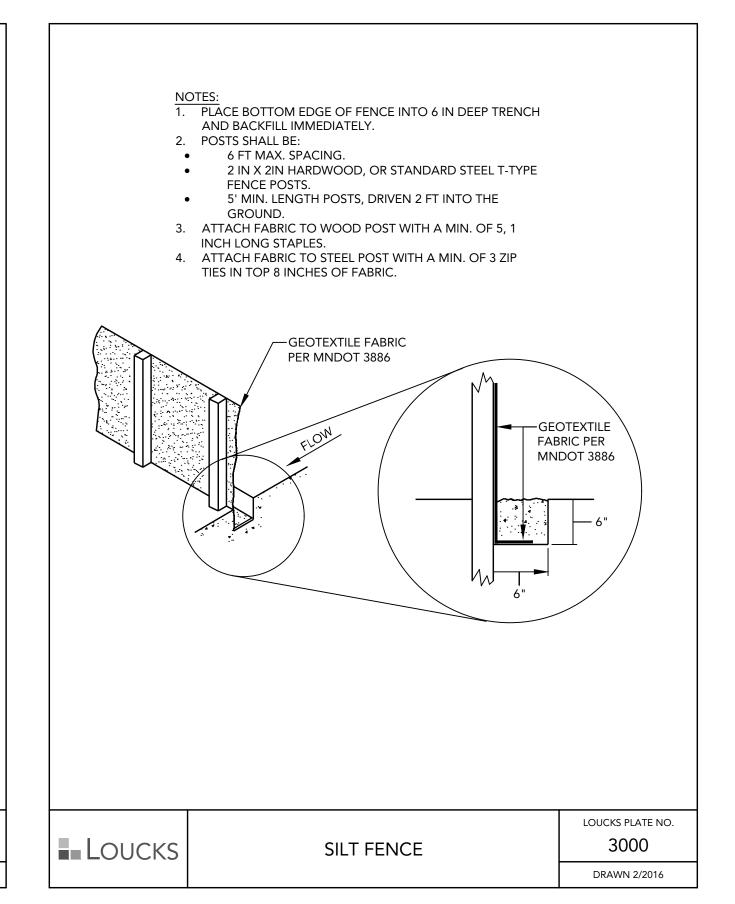
L1-1 LANDSCAPE PLAN
L2-1 TREE INVENTORY PLAN

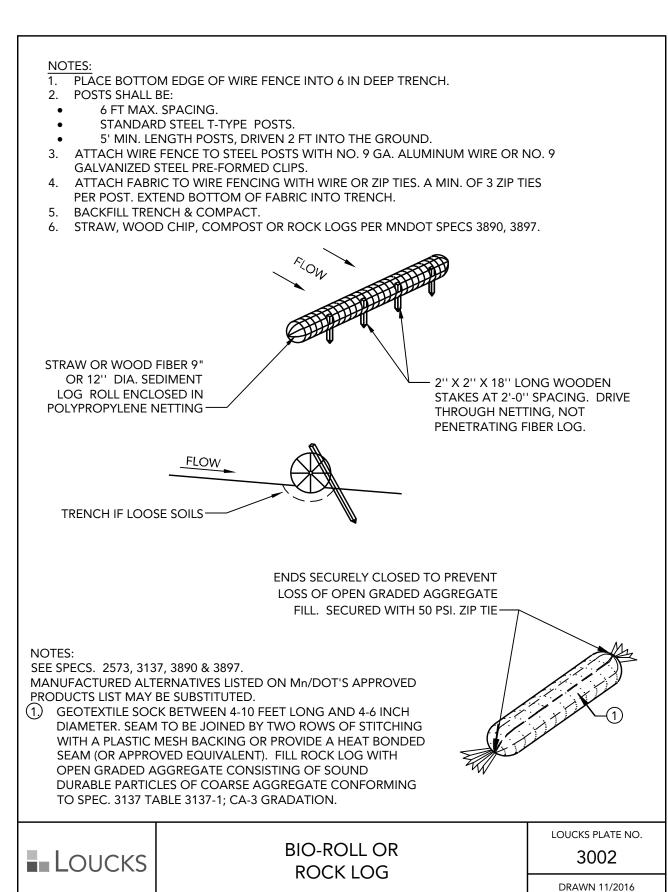
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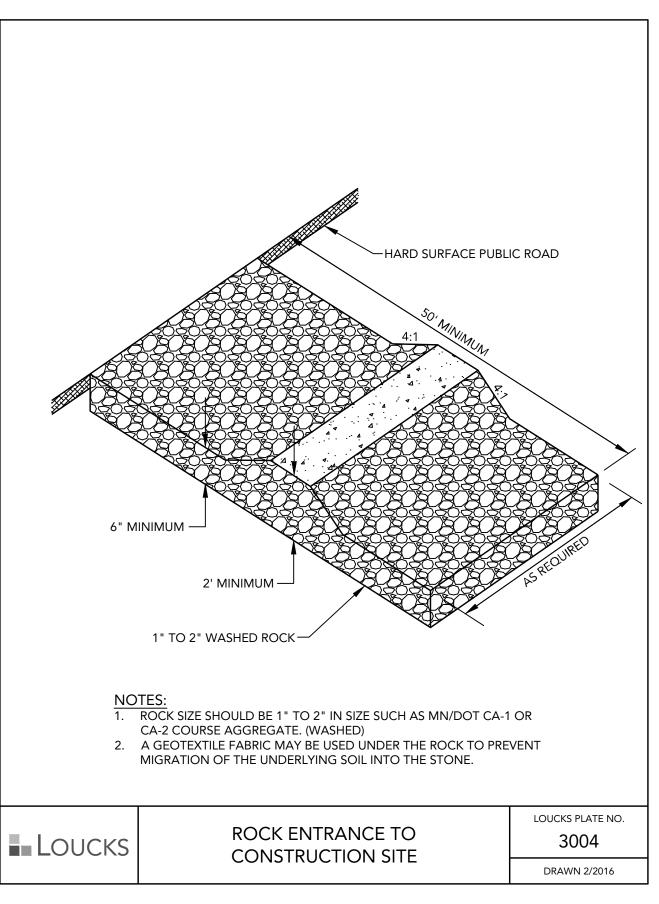


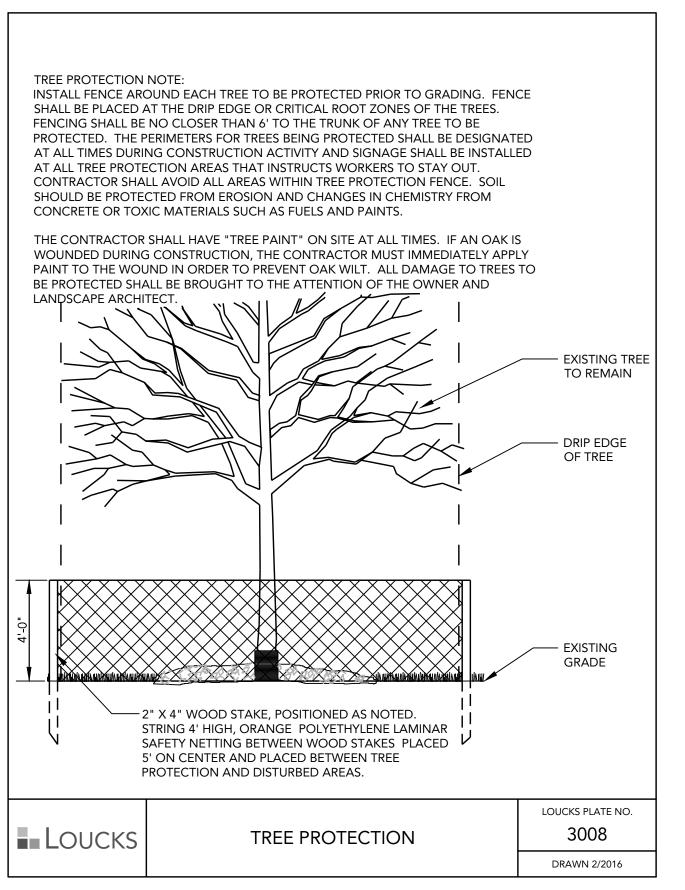


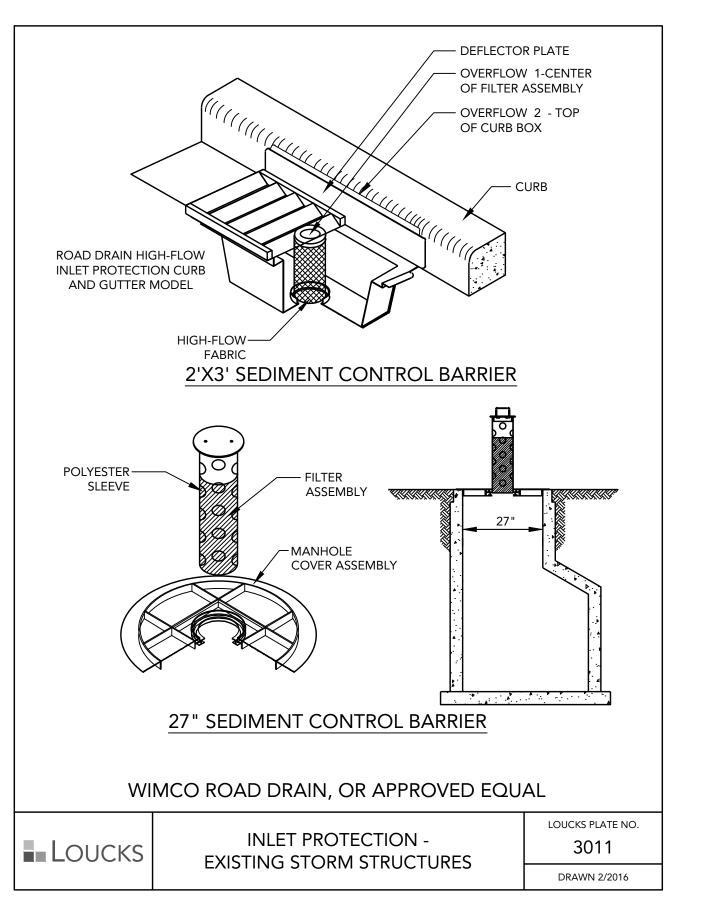
















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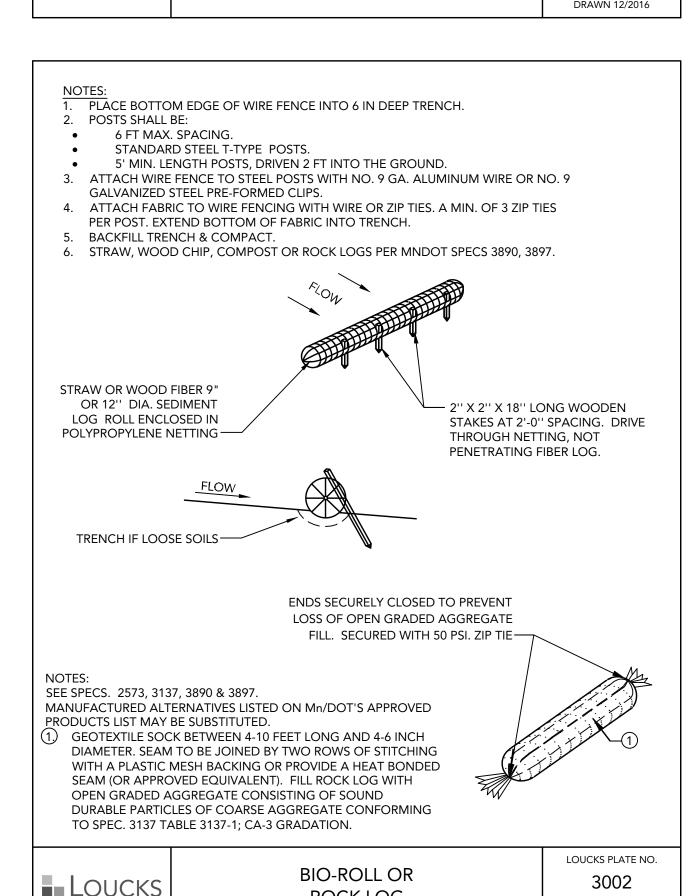
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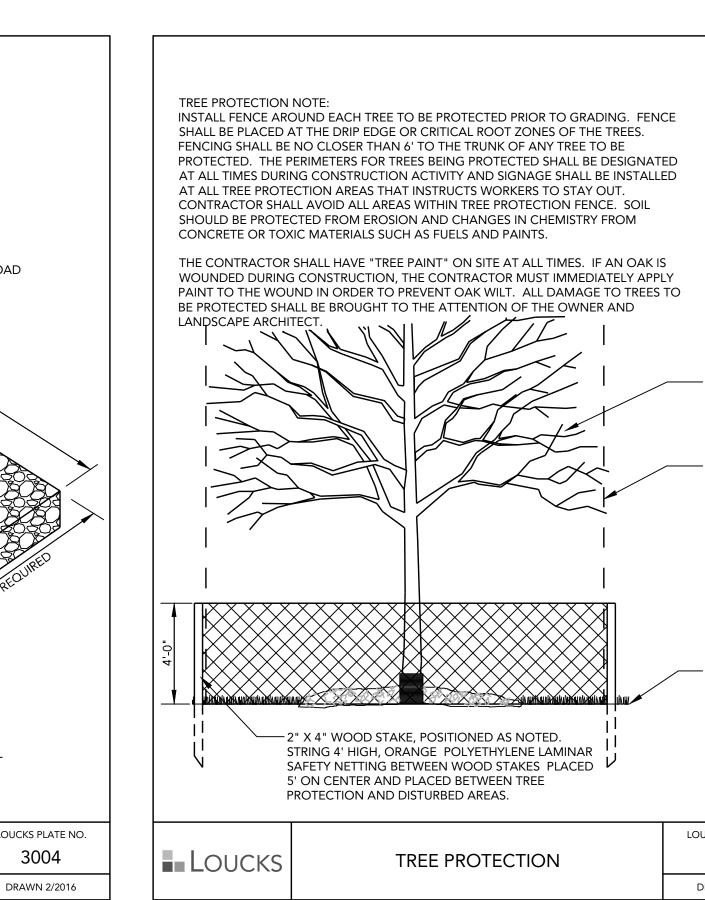
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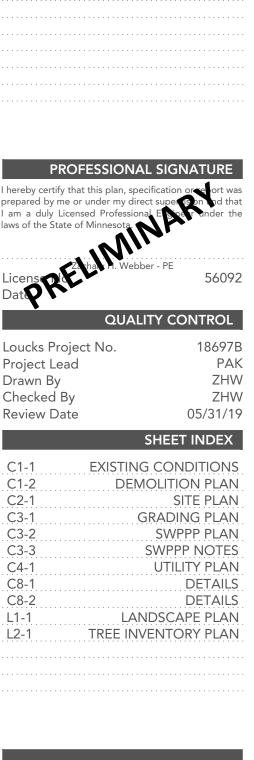
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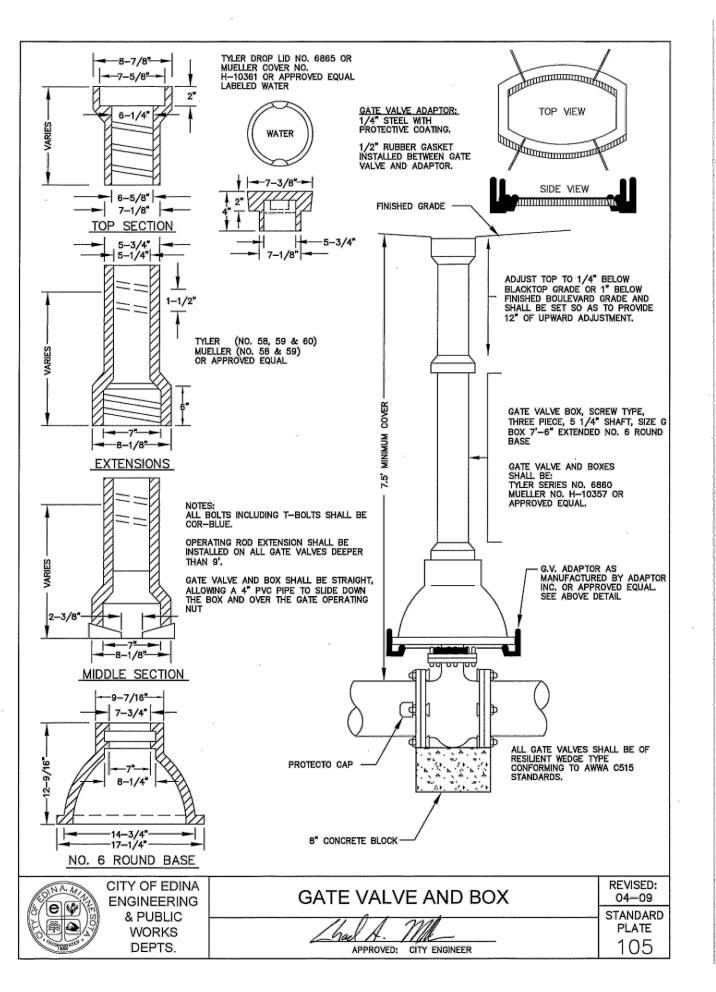
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Drawn By	ZH\
Project Lead	PA
Loucks Project No.	18697

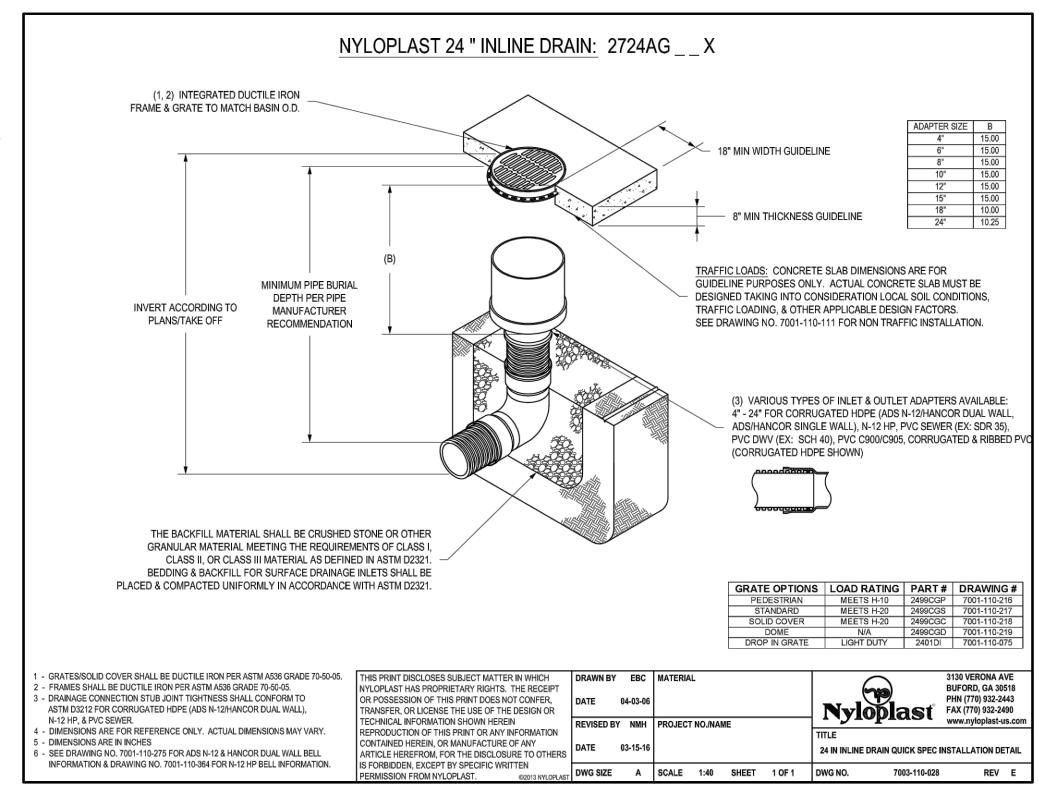
C2-1	SITE PLA
C3-1	GRADING PLA
C3-2	SWPPP PLA
C3-3	SWPPP NOT
C4-1	UTILITY PLA
C8-1	DETAI
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L1-1	LANDSCAPE PLA

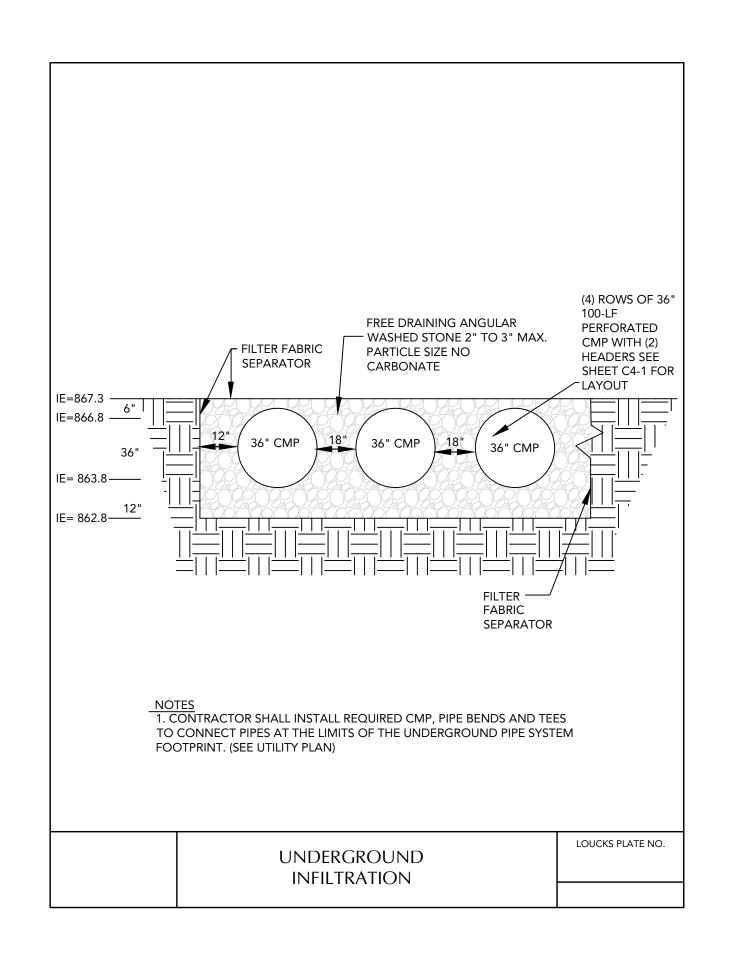
















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SUBMITTAL/REVISIONS

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Loucks Project No. Project Lead PAK ZHW Drawn By ZHW Checked By Review Date 05/31/19 SHEET INDEX

C1-1	EXISTING CONDITION
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C3-1	GRADING PLAI
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C8-2	DETAIL
L1-1	LANDSCAPE PLA
L2-1	TREE INVENTORY PLA



COORDINATE THE PHASES OF CONSTRUCTION AND PLANTING INSTALLATION WITH OTHER CONTRACTORS WORKING ON SITE.

NO PLANTING WILL BE INSTALLED UNTIL COMPLETE GRADING AND CONSTRUCTION HAS BEEN COMPLETED IN THE IMMEDIATE AREA.

WHERE SOD/SEED ABUTS PAVED SURFACES, FINISHED GRADE OF SOD/SEED SHALL BE HELD 1" BELOW SURFACE ELEVATION OF TRAIL, SLAB, CURB, ETC.

SEED ALL AREAS DISTURBED DUE TO GRADING OTHER THAN THOSE AREAS NOTED TO RECEIVE SOD. SEED SHALL BE INSTALLED AND MULCHED AS PER MNDOT SPECS.

SOD ALL DESIGNATED AREAS DISTURBED DUE TO GRADING. SOD SHALL BE LAID PARALLEL TO THE CONTOURS AND SHALL HAVE STAGGERED JOINTS. ON SLOPES STEEPER THAN 3:1 OR IN DRAINAGE SWALES, THE SOD SHALL BE STAKED TO THE GROUND.

ALL PLANT MATERIAL SHALL COMPLY WITH THE LATEST EDITION OF THE AMERICAN STANDARD FOR NURSERY STOCK, AMERICAN ASSOCIATION OF NURSERYMEN. UNLESS NOTED OTHERWISE, ALL SHRUBS SHALL HAVE AT LEAST 5 CANES AT THE SPECIFIED MINIMUM SHRUB HEIGHT OR WIDTH. ORNAMENTAL TREES SHALL HAVE NO V CROTCHES AND SHALL BEGIN BRANCHING NO LOWER THAN 3' ABOVE ROOT BALL. STREET AND BOULEVARD TREES SHALL BEGIN BRANCHING NO LOWER THAN 5' ABOVE FINISHED GRADE.

ANY CONIFEROUS TREE PREVIOUSLY PRUNED FOR CHRISTMAS TREE SALES SHALL NOT BE USED. ALL CONIFEROUS TREES SHALL BE FULL FORM, NATURAL TO THE SPECIES, WITHOUT PRUNING.

PLAN TAKES PRECEDENCE OVER PLANT SCHEDULE IF DISCREPANCIES IN QUANTITIES EXIST. SPECIFICATIONS TAKE PRECEDENCE OVER NOTES.

NO PLANT MATERIAL SUBSTITUTIONS WILL BE ACCEPTED UNLESS APPROVAL IS REQUESTED OF THE LANDSCAPE ARCHITECT BY THE LANDSCAPE CONTRACTOR PRIOR TO THE SUBMISSION OF A BID AND/OR QUOTATION.

ALL PROPOSED PLANTS SHALL BE LOCATED AND STAKED AS SHOWN ON PLAN. ADJUSTMENTS IN LOCATION OF PROPOSED PLANT MATERIALS MAY BE NEEDED IN FIELD. SHOULD AN ADJUSTMENT BE ADVISED, THE LANDSCAPE ARCHITECT MUST BE NOTIFIED.

ALL PLANT MATERIALS SHALL BE FERTILIZED UPON INSTALLATION WITH A 27-3-3 SLOW RELEASE FERTILIZER MIXED IN WITH THE PLANTING SOIL PER THE MANUFACTURER'S INSTRUCTIONS. PLANTS MAY BE TREATED FOR SUMMER AND FALL INSTALLATION WITH AN APPLICATION OF GRANULAR 27-3-3 AT 6 OZ PER 2.5" CALIPER PER TREE AND 3 OZ PER SHRUB WITH AN ADDITIONAL APPLICATION OF 27-3-3 THE FOLLOWING SPRING IN THE TREE SAUCER.

ALL PLANTING AREAS RECEIVING PERENNIALS, GROUND COVER, ANNUALS, AND/OR VINES SHALL RECEIVE A MINIMUM OF 12" DEPTH OF PLANTING SOIL CONSISTING OF 5 PARTS CLEAN TOPSOIL AND 1 PART PEAT. SHRUBS AND TREES TO BE BACKFILLED WITH SAME PLANTING SOIL.

ALL PLANTS TO BE INSTALLED AS PER PLANTING DETAILS. REMOVE ALL FLAGGING AND LABELS FROM PLANTS.

WRAPPING MATERIAL SHALL BE CORRUGATED PVC PIPING 1" GREATER IN CALIPER THAN THE TREE BEING PROTECTED OR QUALITY, HEAVY, WATERPROOF CREPE PAPER MANUFACTURED FOR THIS PURPOSE. WRAP ALL DECIDUOUS TREES PLANTED IN THE FALL PRIOR TO 12-1 AND REMOVE ALL WRAPPING AFTER 5-1.

BLACK METAL EDGER TO BE USED TO CONTAIN SHRUBS, PERENNIALS, AND ANNUALS WHERE BED MEETS SOD/SEED UNLESS NOTED OTHERWISE.

ALL ANNUAL AND PERENNIAL PLANTING BEDS TO RECEIVE 3" DEEP SHREDDED HARDWOOD MULCH WITH NO WEED BARRIER. ALL SHRUB BED MASSINGS TO RECEIVE 3" DEEP SHREDDED HARDWOOD MULCH AND FIBER MAT WEED BARRIER.

ALL TREES TO RECEIVE 4" DEEP SHREDDED HARDWOOD MULCH WITH NO MULCH IN DIRECT CONTACT WITH TREE TRUNK.

SPREAD GRANULAR PRE EMERGENT HERBICIDE (PREEN OR EQUAL) PER MANUFACTURER'S RECOMMENDATIONS UNDER ALL MULCHED AREAS.

MAINTENANCE STRIPS TO HAVE EDGER AND MULCH AS SPECIFIED/INDICATED ON DRAWING OR IN SPECIFICATION.

IF THE LANDSCAPE CONTRACTOR IS CONCERNED OR PERCEIVES ANY DEFICIENCIES IN THE PLANT SELECTIONS, SOIL CONDITIONS OR ANY OTHER SITE CONDITION WHICH MIGHT NEGATIVELY AFFECT PLANT ESTABLISHMENT, SURVIVAL OR GUARANTEE, HE MUST BRING THESE DEFICIENCIES TO THE ATTENTION OF THE LANDSCAPE ARCHITECT PRIOR TO PROCUREMENT AND/OR INSTALLATION.

CONTRACTOR SHALL SUBMIT A WRITTEN REQUEST FOR THE OWNER ACCEPTANCE INSPECTION OF ALL LANDSCAPE AND SITE IMPROVEMENTS.

CONTRACTOR IS RESPONSIBLE FOR ON-GOING MAINTENANCE OF ALL NEWLY INSTALLED MATERIALS UNTIL TIME OF OWNER ACCEPTANCE. ANY ACTS OF VANDALISM OR DAMAGE WHICH MAY OCCUR PRIOR TO OWNER ACCEPTANCE SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. CONTRACTOR SHALL PROVIDE THE OWNER WITH A MAINTENANCE PROGRAM INCLUDING, BUT NOT NECESSARILY LIMITED TO, PRUNING, FERTILIZATION AND DISEASE/PEST CONTROL.

CONTRACTOR SHALL GUARANTEE NEW PLANT MATERIAL THROUGH ONE CALENDAR YEAR FROM THE DATE OF OWNER ACCEPTANCE.

WARRANTY (ONE FULL GROWING SEASON) FOR LANDSCAPE MATERIALS SHALL BEGIN ON THE DATE OF ACCEPTANCE BY THE LANDSCAPE ARCHITECT AFTER THE COMPLETION OF PLANTING OF ALL LANDSCAPE MATERIALS. NO PARTIAL ACCEPTANCE WILL BE CONSIDERED.

UNLESS NOTED OTHERWISE THE APPROPRIATE DATES FOR SPRING PLANT MATERIAL INSTALLATION AND SEED/SOD PLACEMENT IS FROM THE TIME GROUND HAS THAWED TO JUNE 15.

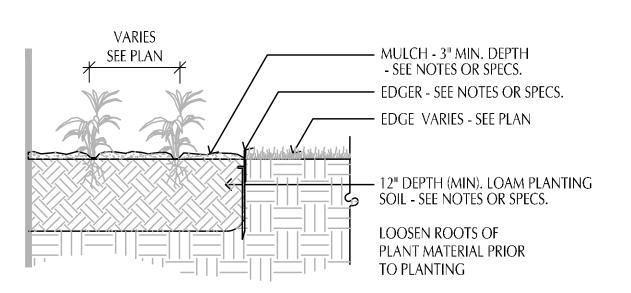
FALL SODDING IS GENERALLY ACCEPTABLE FROM AUGUST 15 - NOVEMBER 1. FALL SEEDING FROM AUGUST 15 - SEPTEMBER 15; DORMANT SEEDING IN THE FALL SHALL NOT OCCUR PRIOR TO NOVEMBER 1. FALL CONIFEROUS PLANTING MAY OCCUR FROM AUGUST 15 - OCTOBER 1 AND DECIDUOUS PLANTING FROM THE FIRST FROST UNTIL NOVEMBER 15. PLANTING OUTSIDE THESE DATES IS NOT RECOMMENDED. ANY ADJUSTMENT MUST BE APPROVED IN WRITING BY THE LANDSCAPE

PROTECT ALL EXISTING OAKS ON SITE SCHEDULED TO REMAIN. IF EXISTING OAKS ARE DAMAGED IN ANY MANNER, ABOVE OR BELOW GROUND IN THE ROOT SYSTEM, AN ASPHALTIC TREE PRUNING PAINT SHOULD BE APPLIED IMMEDIATELY AFTER WOUNDING. OAKS ARE NOT TO BE PRUNED, REMOVED OR TRANSPLANTED BETWEEN APRIL 15 AND JULY 1. NOTIFY LANDSCAPE ARCHITECT IF THESE DATES ARE UNAVOIDABLE.

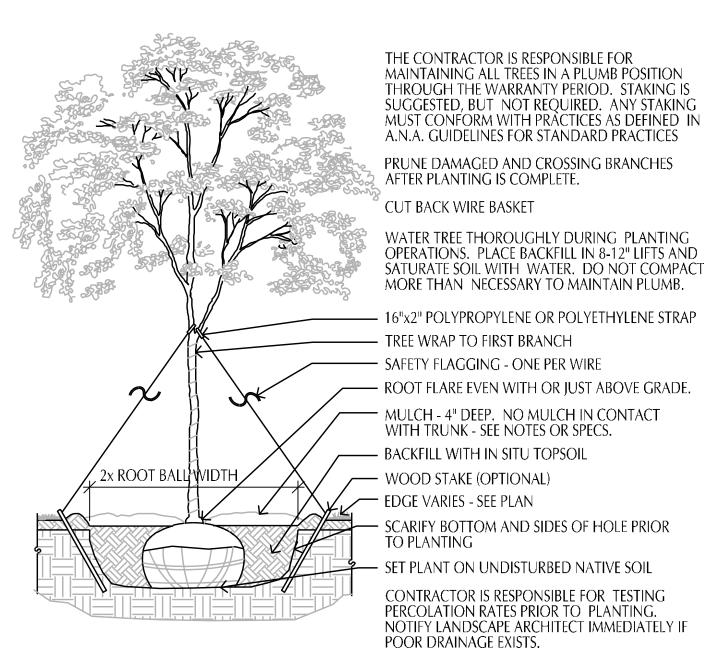
LANDSCAPE CONTRACTOR SHALL ESTABLISH TO HIS SATISFACTION THAT SOIL AND COMPACTION CONDITIONS ARE ADEQUATE TO ALLOW FOR PROPER DRAINAGE AT AND AROUND THE BUILDING SITE.

| REFER TO PLAN - SHRUBS TO BE PLACED SO THAT TOP OF CONTAINER SITS FLUSH 18" MIN. WITH PROPOSED GRADE. — PLANTING SOIL - SEE NOTES OR SPEC. ——— MULCH - 3" DEPTH - SEE NOTES OR SPEC. LANDSCAPE FABRIC - SEE NOTES OR SPEC. —— EDGING MATERIAL - SEE NOTES OR SPEC. — EDGE VARIES - REFER TO PLAN LOOSEN ROOTS OF ALL CONTAINERIZED PLANTS. SCARIFY BOTTOM AND SIDES OF HOLE PRIOR TO PLANTING — BUILDING WALL (TYP)

SHRUB PLANTING DETAIL SCALE: 3/4'' = 1'-0''







ECIDUOUS TREE PLANTING DETAIL

IRRIGATION NOTES:

SHOP DRAWINGS.

BUILDING FOUNDATION.

REQUIREMENTS.

ESTABLISHED.

IRRIGATION SYSTEM.

VERIFY EXISTING/PROPOSED IRRIGATION SYSTEM LAYOUT AND

PROVIDING AN IRRIGATION LAYOUT PLAN AND SPECIFICATION

AS A PART OF THE SCOPE OF WORK WHEN BIDDING. THESE

SHALL BE APPROVED BY THE LANDSCAPE ARCHITECT PRIOR TO

INCLUDING THOSE AREAS DIRECTLY AROUND AND ABUTTING

THE LANDSCAPE CONTRACTOR SHALL PROVIDE THE OWNER WITH AN IRRIGATION SCHEDULE APPROPRIATE TO THE PROJECT

IRRIGATION SYSTEM IS NOT TO SPRINKLE ACROSS PAVEMENT.

PLANTINGS OUTSIDE THE LIMITS OF IRRIGATION ARE TO BE

WATERED REGULARLY UNTIL PLANTING/SOD/SEED HAS BEEN

SITE CONDITIONS AND TO PLANT MATERIAL GROWTH

THE SYSTEM SHALL INCORPORATE A RAIN SENSOR INTO

SODDED/SEEDED AND PLANTED AREAS ARE IRRIGATED PROPERLY,

ORDER AND/OR INSTALLATION. IT SHALL BE THE LANDSCAPE

LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR

CONTRACTORS RESPONSIBILITY TO INSURE THAT ALL

CONFIRM COMPLETE LIMITS OF IRRIGATION PRIOR TO SUPPLYING

GENERAL NOTES:

CONTRACTOR SHALL VISIT SITE PRIOR TO SUBMITTING BID. HE SHALL INSPECT SITE AND BECOME FAMILIAR WITH EXISTING CONDITIONS RELATING TO THE NATURE AND SCOPE OF WORK.

ATTENTION OF THE LANDSCAPE ARCHITECT ANY DISCREPANCIES WHICH MAY COMPROMISE THE DESIGN AND/OR INTENT OF THE PROJECT'S LAYOUT.

VERIFY LAYOUT AND ANY DIMENSIONS SHOWN AND BRING TO THE

ASSURE COMPLIANCE WITH ALL APPLICABLE CODES AND REGULATIONS GOVERNING THE WORK OR MATERIALS SUPPLIED.

CONTRACTOR SHALL PROTECT ALL EXISTING ROADS, CURBS/GUTTERS, TRAILS, TREES, LAWNS AND SITE ELEMENTS DURING PLANTING OPERATIONS. ANY DAMAGE TO SAME SHALL BE REPAIRED AT NO COST TO THE OWNER.

CONTRACTOR SHALL VERIFY ALIGNMENT AND LOCATION OF ALL UNDERGROUND AND ABOVE GRADE UTILITIES AND PROVIDE THE NECESSARY PROTECTION FOR SAME BEFORE CONSTRUCTION / MATERIAL INSTALLATION BEGINS (MINIMUM 10' - 0" CLEARANCE).

ALL UNDERGROUND UTILITIES SHALL BE LAID SO THAT TRENCHES DO NOT CUT THROUGH ROOT SYSTEMS OF ANY EXISTING TREES TO REMAIN.

EXISTING CONTOURS, TRAILS, VEGETATION, CURB/GUTTER AND OTHER EXISTING ELEMENTS BASED UPON INFORMATION SUPPLIED TO LANDSCAPE ARCHITECT BY OTHERS. CONTRACTOR SHALL VERIFY ANY AND ALL DISCREPANCIES PRIOR TO CONSTRUCTION AND NOTIFY LANDSCAPE ARCHITECT OF SAME.

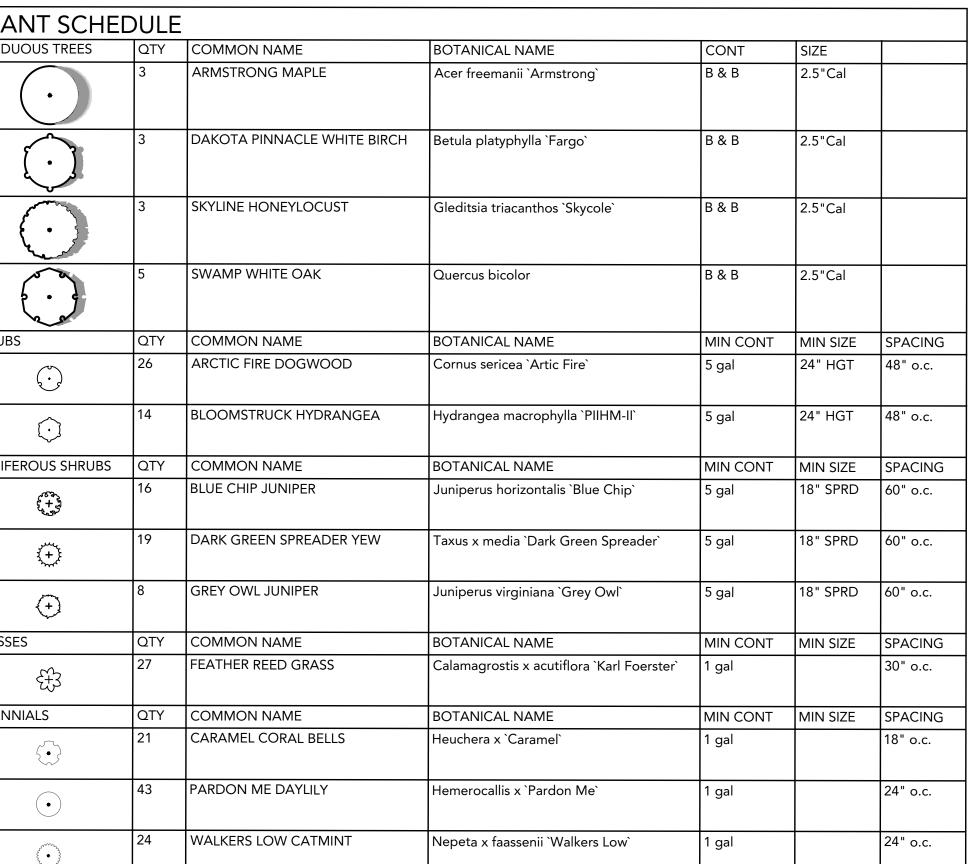
THE ALIGNMENT AND GRADES OF THE PROPOSED WALKS, TRAILS AND/OR ROADWAYS ARE SUBJECT TO FIELD ADJUSTMENT REQUIRED TO CONFORM TO LOCALIZED TOPOGRAPHIC CONDITIONS AND TO MINIMIZE TREE REMOVAL AND GRADING. ANY CHANGE IN ALIGNMENT MUST BE APPROVED BY LANDSCAPE ARCHITECT.

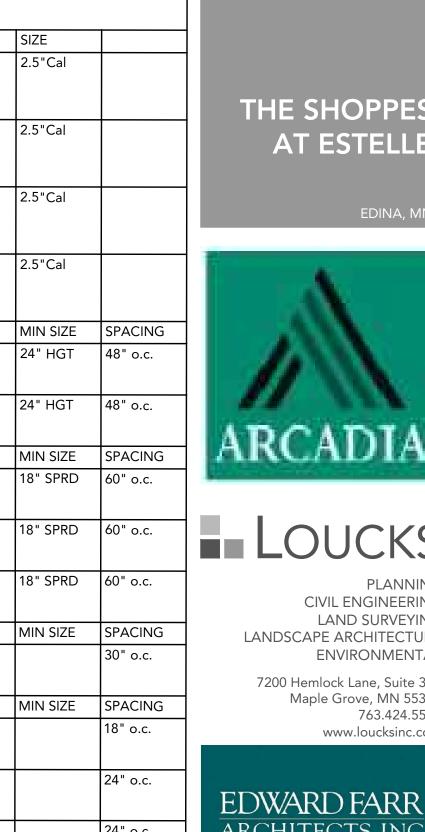
LANDSCAPE REQUIREMENTS: ONE TREE IS REQUIRED PER 40' OF SITE PERIMETER. SITE PERIMETER = 840'TREES REQUIRED = 21TREES PROPOSED = 21 (14 NEW AND 7 EXISTING)

THE MINIMUM SIZE OF THESE TREES SHALL BE AS FOLLOWS. CALIN PERCENT REQUIRED PROPOSED

TL. IIV.	LICLINI	KLQUIKLD	I KOI OSLD	
<2"	5%	1	0	
>2.5"	60%	13	14	
>3.5"	20%	4	4 (EXISTING)	
>4.5"	10%	2	2 (EXISTING)	
>5.5"	5%	1	1 (EXISTING)	

DECIDUOUS TREES	QTY	COMMON NAME	BOTANICAL NAME	CONT	SIZE	
$\overline{(\cdot)}$	3	ARMSTRONG MAPLE	Acer freemanii `Armstrong`	B & B	2.5"Cal	
$\overline{\bigcirc}$	3	DAKOTA PINNACLE WHITE BIRCH	Betula platyphylla `Fargo`	B & B	2.5"Cal	
	3	SKYLINE HONEYLOCUST	Gleditsia triacanthos `Skycole`	B & B	2.5"Cal	
	5	SWAMP WHITE OAK	Quercus bicolor	B & B	2.5"Cal	
SHRUBS	QTY	COMMON NAME	BOTANICAL NAME	MIN CONT	MIN SIZE	SPACING
\odot	26	ARCTIC FIRE DOGWOOD	Cornus sericea `Artic Fire`	5 gal	24" HGT	48" o.c.
\odot	14	BLOOMSTRUCK HYDRANGEA	Hydrangea macrophylla `PIIHM-II`	5 gal	24" HGT	48" o.c.
CONIFEROUS SHRUBS	QTY	COMMON NAME	BOTANICAL NAME	MIN CONT	MIN SIZE	SPACING
6 4 8 8 8 8 8 8 8 8 8 8 8 8 8 8 8 8 8 8	16	BLUE CHIP JUNIPER	Juniperus horizontalis `Blue Chip`	5 gal	18" SPRD	60" o.c.
(+)	19	DARK GREEN SPREADER YEW	Taxus x media `Dark Green Spreader`	5 gal	18" SPRD	60" o.c.
\oplus	8	GREY OWL JUNIPER	Juniperus virginiana `Grey Owl`	5 gal	18" SPRD	60" o.c.
GRASSES	QTY	COMMON NAME	BOTANICAL NAME	MIN CONT	MIN SIZE	SPACING
£	27	FEATHER REED GRASS	Calamagrostis x acutiflora `Karl Foerster`	1 gal		30" o.c.
PERENNIALS	QTY	COMMON NAME	BOTANICAL NAME	MIN CONT	MIN SIZE	SPACING
•	21	CARAMEL CORAL BELLS	Heuchera x `Caramel`	1 gal		18" o.c.
•	43	PARDON ME DAYLILY	Hemerocallis x `Pardon Me`	1 gal		24" o.c.
	24	WALKERS LOW CATMINT	Nepeta x faassenii `Walkers Low`	1 gal		24" o.c.







AT ESTELLE

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7200 Hemlock Lane, Suite 300 Maple Grove, MN 55369 763.424.5505 www.loucksinc.com



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PROFESSIONAL SIGNATURI

Loucks Project No Project Lead Drawn By Checked By Review Date

C1-2

05/31/19 SHEET INDEX **EXISTING CONDITIONS** DEMOLITION PLAN

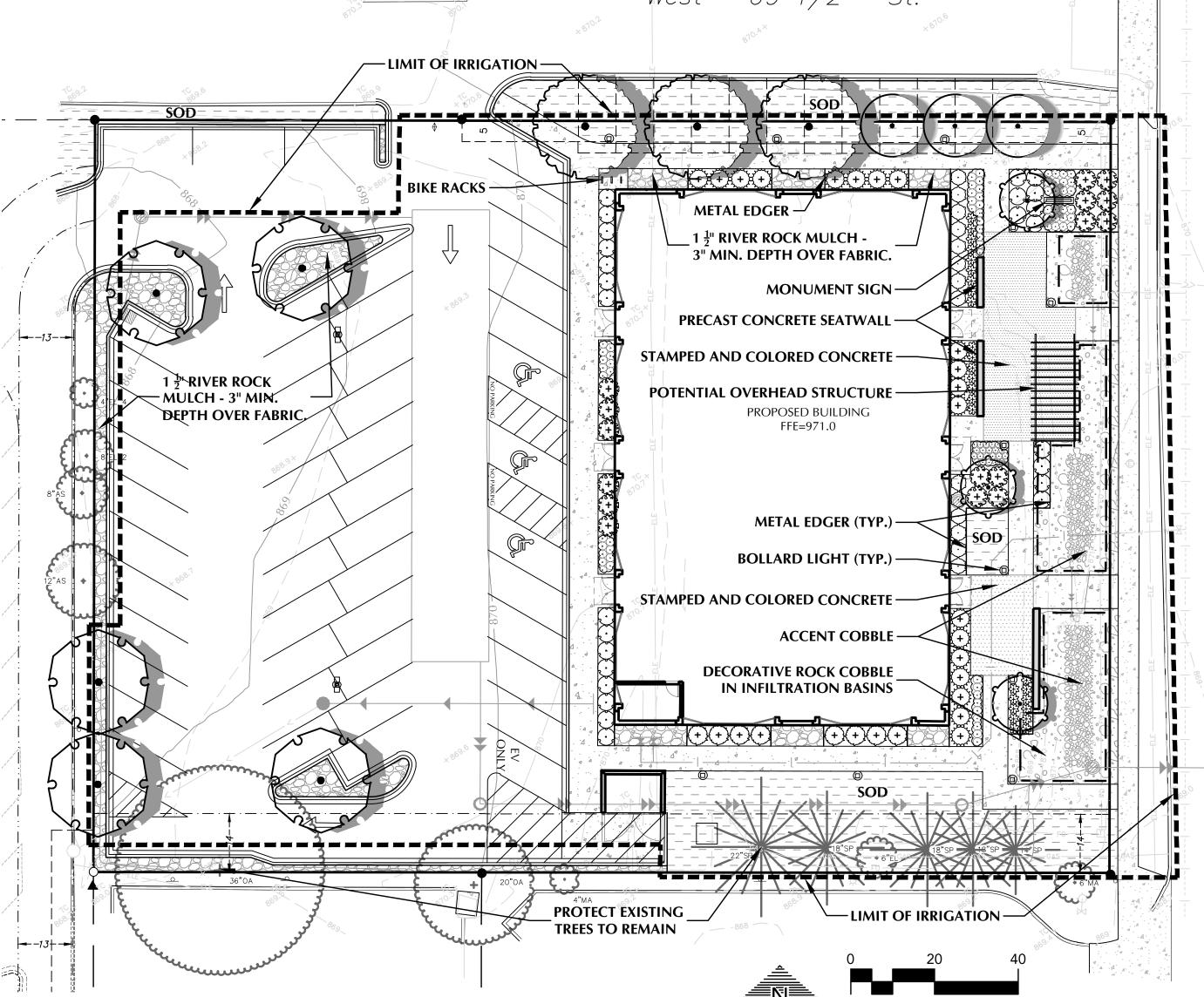
18697B

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TJF

C2-1 SITE PLAN C3-1 **GRADING PLAN** C3-2 SWPPP PLAN C3-3 SWPPP NOTES C4-1 UTILITY PLAN C8-1 DETAILS C8-2 DETAILS LANDSCAPE PLAN TREE INVENTORY PLAN

LANDSCAPE

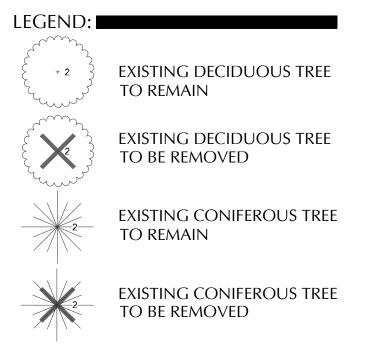


TREE MITIGATION REQUIREMENTS:

ALL PROTECTED TREES THAT ARE REMOVED MUST BE MITIGATED ON A TREE PER TREE BASIS EXCEPT FOR THE FOLLOWING EXEMPTIONS.

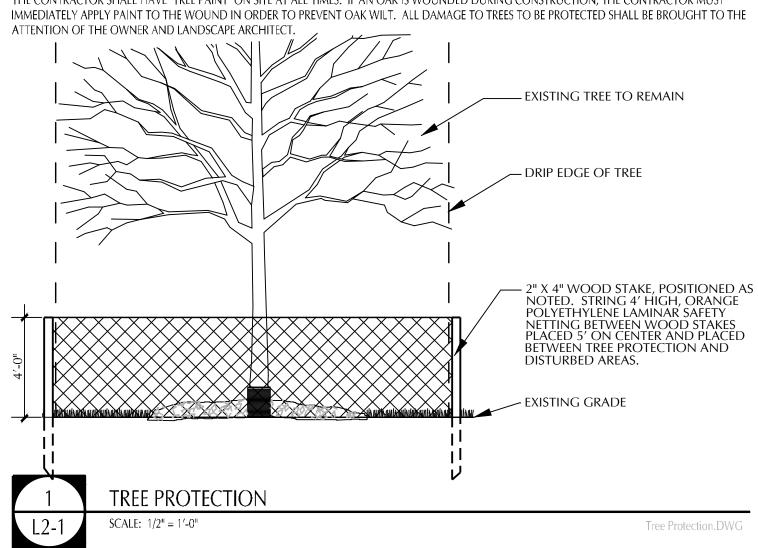
- TREES WITHIN 10' OF BUILDING PAD, DECK, OR PATIO
- TREES WITHIN 5' OF DRIVEWAYS AND PARKING AREAS
- TREES WITHIN 10' OF PUBLIC ROADS, UTILITIES, AND STORMWATER BASINS.

ZERO TREES ARE BEING REMOVED THAT ARE REQUIRED TO BE MITIGATED.

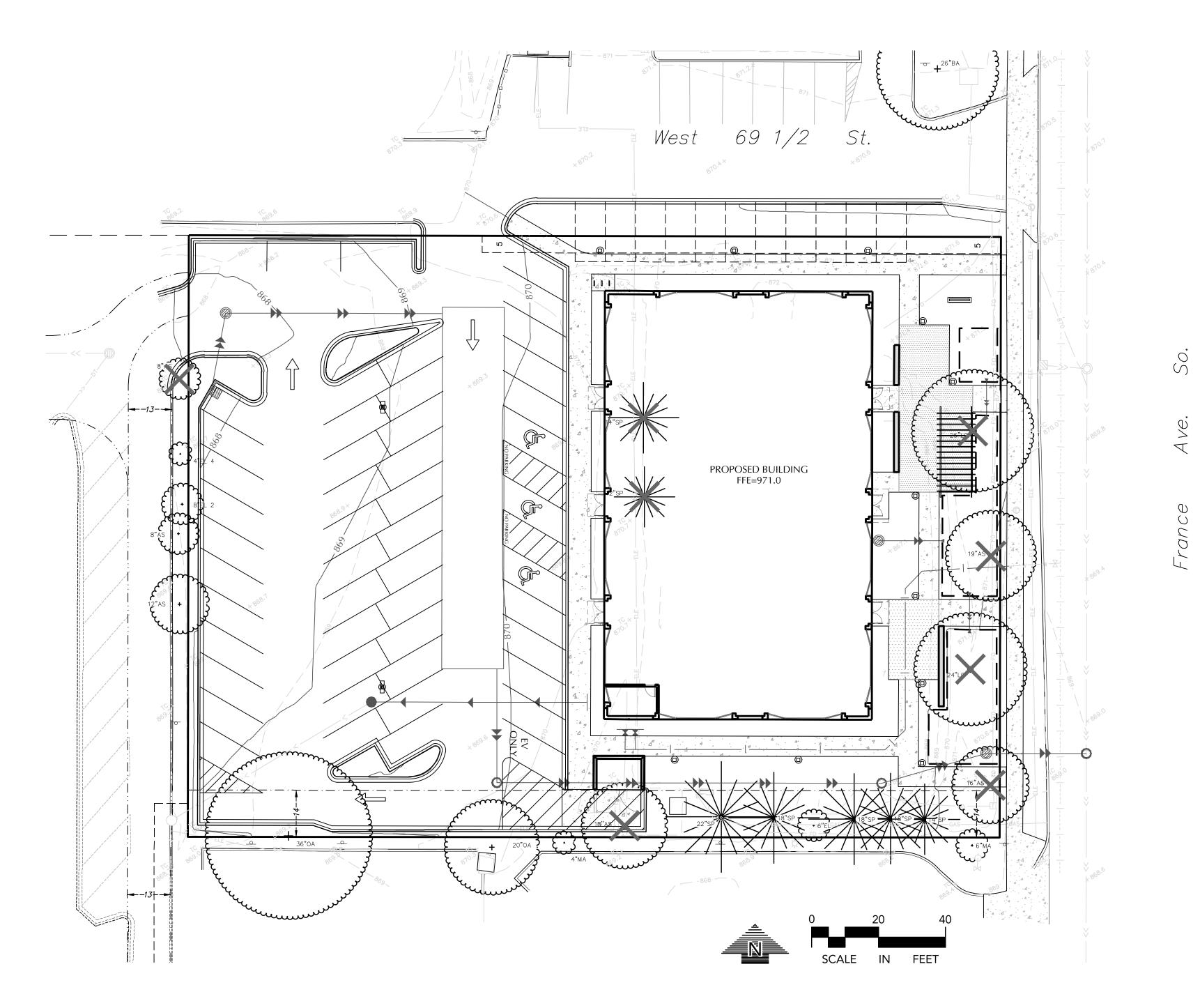


INSTALL SNOW FENCE AROUND EACH TREE TO BE PROTECTED PRIOR TO GRADING. FENCE SHALL BE PLACED AT THE DRIP EDGE OR CRITICAL ROOT ZONES OF THE TREES. FENCING SHALL BE NO CLOSER THAN 6' TO THE TRUNK OF ANY TREE TO BE PROTECTED. THE PERIMETERS FOR TREES BEING PROTECTED SHALL BE DESIGNATED AT ALL TIMES DURING CONSTRUCTION ACTIVITY AND SIGNAGE SHALL BE INSTALLED AT ALL TREE PROTECTION AREAS THAT INSTRUCTS WORKERS TO STAY OUT. CONTRACTOR SHALL AVOID ALL AREAS WITHIN TREE PROTECTION FENCE. SOIL SHOULD BE PROTECTED FROM EROSION AND CHANGES IN CHEMISTRY FROM CONCRETE OR TOXIC MATERIALS SUCH AS FUELS AND PAINTS.

THE CONTRACTOR SHALL HAVE "TREE PAINT" ON SITE AT ALL TIMES. IF AN OAK IS WOUNDED DURING CONSTRUCTION, THE CONTRACTOR MUST



TREE INVENTORY													
Tree Number	Species	Diameter (In.)	Health	Saved/Removed	Exempt	Reason for Exemption							
1	Honeylocust	26	Good	Removed	Yes	Located in stormwater basin							
2	Ash	19	Good	Removed	Yes	Located in stormwater basin							
3	Honeylocust	24	Good	Removed	Yes	Located in stormwater basin							
4	Ash	16	Good	Removed	Yes	Located near stormwater basin							
5	Spruce	14	Good	Saved	No								
6	Spruce	18	Good	Saved	No								
7	Spruce	18	Good	Saved	No								
8	Elm	6	Good	Saved	No								
9	Spruce	18	Good	Saved	No								
10	Spruce	22	Good	Saved	No								
11	Ash	18	Good	Removed	Yes	Located in drive aisle							
12	Oak	36	Good	Saved	No								
13	Ash	8	Good	Removed	Yes	Located near drive aisle							
14	Spruce	14	Good	Removed	Yes	Located in building pad							
15	Spruce	12	Good	Removed	Yes	Located in building pad							







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Consultant from any & all responsibilities, claims, and liabilities. SUBMITTAL/REVISIONS

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Landscape Architect under the laws of the State of Minnesota.

Impuly J. Fedie - LA

License No. 48303

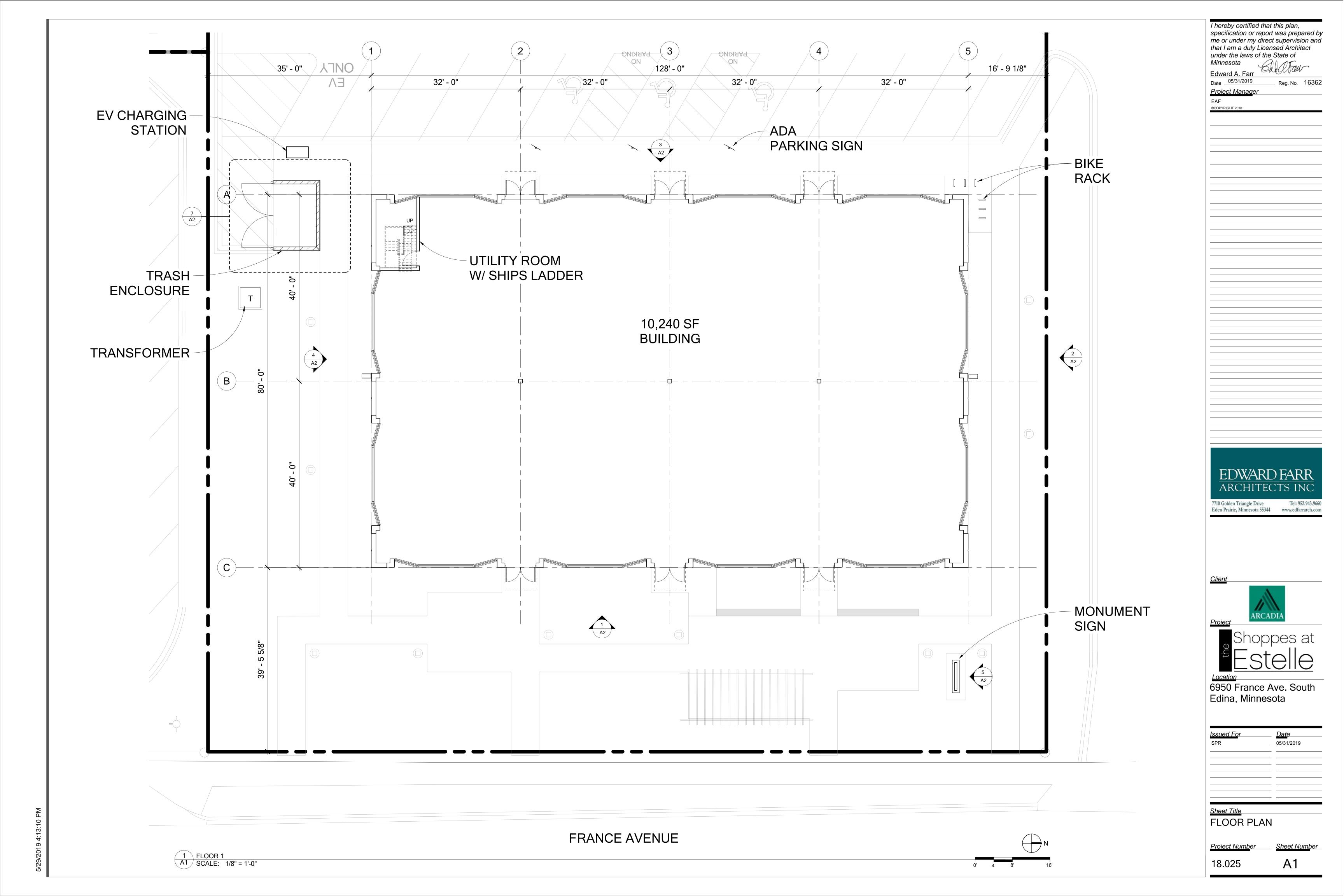
Date

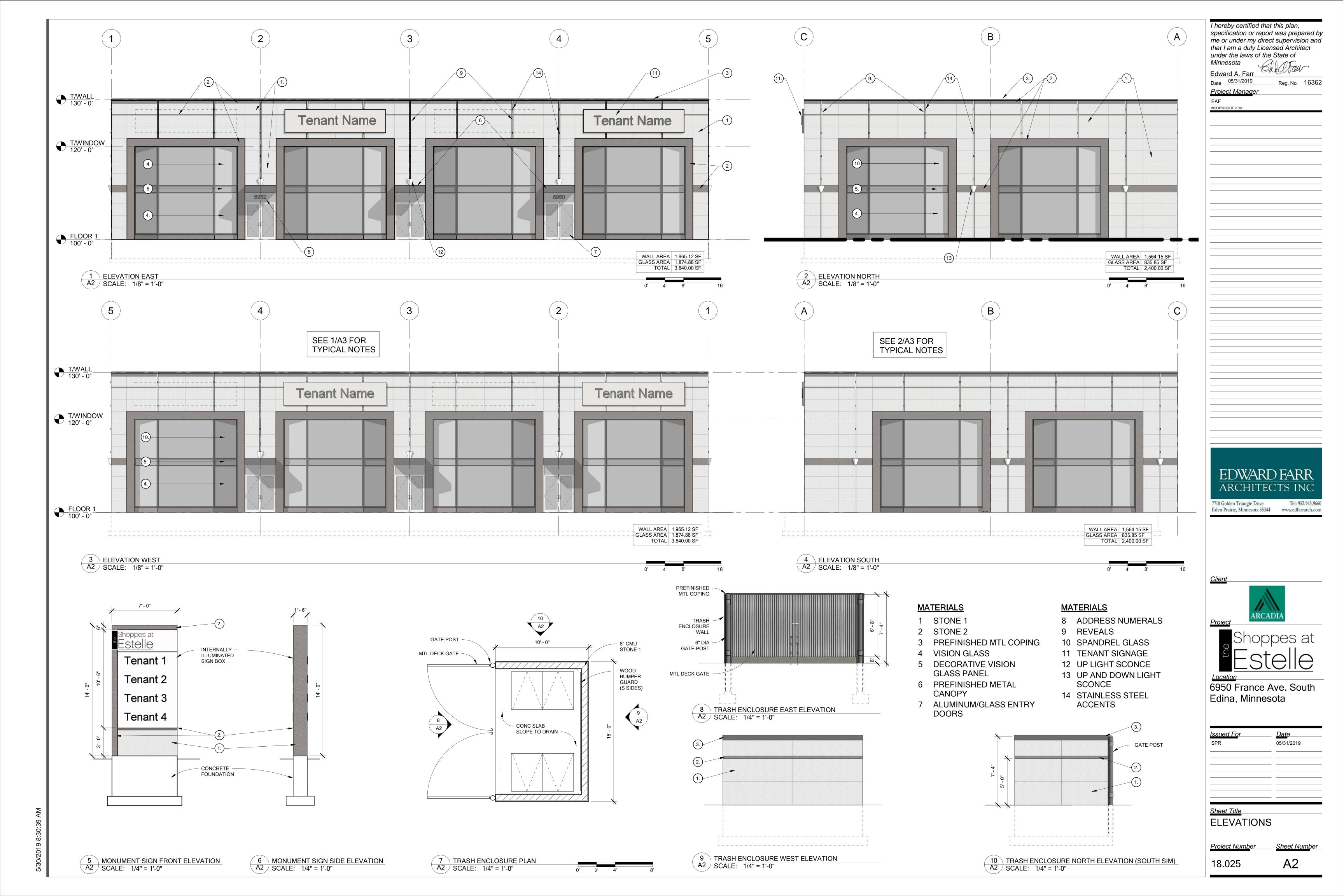
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	QUALITY CONTROL
Loucks Project	No. 18697
Project Lead	PAI
Drawn By	TJ
Checked By	TJ
Review Date	05/31/1
	SHEET INDEX

C1-2 DEMOLITION PLAN C2-1 SITE PLAN C3-1 GRADING PLAN C3-2 SWPPP PLAN C3-3 SWPPP NOTES UTILITY PLAN C8-1 DETAILS C8-2 DETAILS L1-1 LANDSCAPE PLAN TREE INVENTORY PLAN

EXISTING CONDITIONS

INVENTORY





Luminaire Sch	nedule
Cymbal	O±,

Laminano	Jiiodaio						
Symbol	Qty	Label	Arrangement	LLF	Description	Arr. Watts	Lum. Lumens
	2	AA	SINGLE	0.900	MCGRAW GLEON-AF-05-LED-E1-5WQ MOUNT ON 25FT POLE WITH 30IN BASE	279	32055
	10	BB	SINGLE	0.900	SELUX IBL-X-4Q-40-XX-120-DS 3FT	27.06	2088

Calculation Summary							
Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min
SITE GROUND	Illuminance	Fc	0.68	33.8	0.0	N.Ă.	N.A.
PARKING	Illuminance	Fc	2.01	3.1	0.9	2.23	3.44

Lumir	naire Location Summary					
LumN	lo Label	X	Υ	Z	Orient	Tilt
24	AA	511791.2	130786.2	27.5	90	0
25	AA	511792.1	130870.3	27.5	90	0
28	BB	511880.9	130921.2	3	0	0
29	BB	511909.5	130921.2	3	0	0
30	BB	511946.8	130911.8	3	0	0
31	BB	511947.4	130899	3	0	0
32	BB	511952.6	130846.5	3	0	0
33	BB	511952.7	130818.3	3	0	0
34	BB	511956.6	130790.3	3	0	0
35	BB	511956.5	130768.3	3	0	0
36	BB	511917.3	130767.4	3	0	0
37	BB	511885.5	130767.2	3	0	0





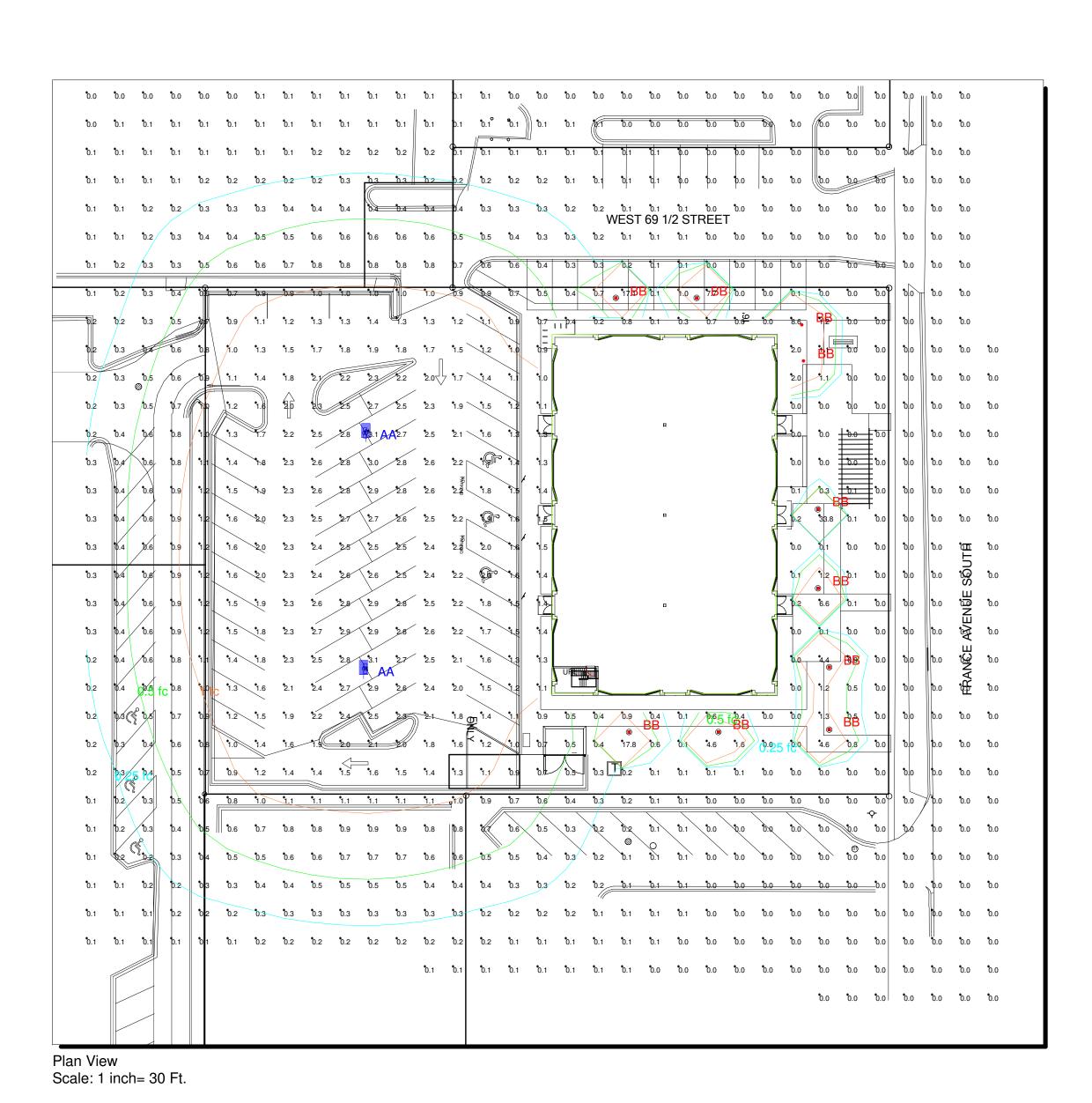
TYPE BB

GENERAL NOTES:

A. PULSE PRODUCTS DOES NOT ASSUME RESPONSIBILITY FOR THE INTERPRETATION OF THIS CALCULATION OR COMPLAINCE TO THE LOCAL, STATE, OR FEDERAL LIGHTNG CODES OR ORDINANCES.

B. LIGHTING LAYOUT IS NOT INTENDED FOR CONSTRUCTION DOCUMENTS BUT ONLY TO ILLUSTRATE THE PERFORMANCE OF THE PRODUCT.

C. ALL READINGS/CALCULATIONS SHOWN ARE SHOWN ON OBJECTS/SURFACES.



Revisions Drawn By: SANDY Checked By: TRENT Date:5/29/2019 Scale: AS NOTED S Ш Z S AT SHOPS EDINA

PAGE 1 OF 1



THE SHOPPES AT ESTELLE

EDINA, I





PLANNING
CIVIL ENGINEERING
LAND SURVEYING
NDSCAPE ARCHITECTURE

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intraments of the Committee perfections devices for use on with respect to the project. These CAOC lists will not be a with respect to the project. The CAOC lists will not be used in the second of this project, or be complied of this project, or be complied of this project, or be complied of the project project. The committee the project pr

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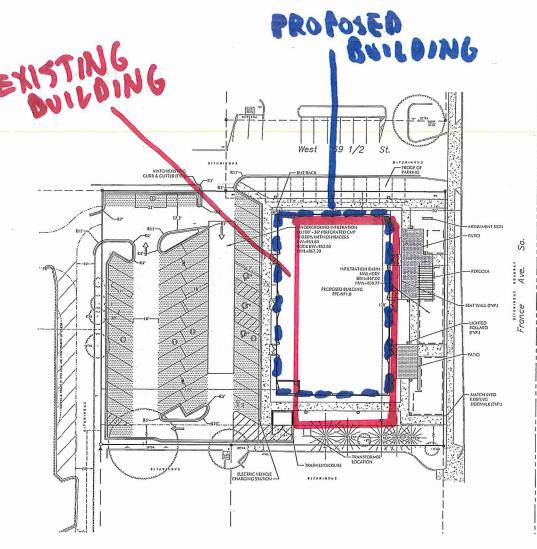
PROFESSIONAL SIGNATUR
hereby certify that this plan, specification considerepared by me or under my direct superior of the
area duly Excressed Landscape Armiting specifies the
fithe State of Minnesotta.

Crown) J. Fedie - LA 48

1	SHEET INDEX
Review Date	05/31/1
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Loucks Project No.	18697

1+1	EXISTING CONDITIONS
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2-1	SITE PLAN
3-1	GRADING PLAN
3-2	SWPPP PLAN
3-3	SWPPP NOTES
4-1	UTILITY PLAN
8-1	DETAILS
8-2	DETAILS
1-1	LANDSCAPE PLAN
2-1	TREE INVENTORY PLAN

LANDSCAPE PLAN L1-1





PROJECT BENCHMARK

SITE BENCHMARK: TOP NUT OF HYDRANT LOCATED NEAR SOUTHEAST CORNER OF SUBJECT PROPERTY, AS SHOWN HEREON, ELEVATION = 871,97 (NGVO 29)

SITE PLAN LEGEND



PARKING COUNTS



WARNING:

THE CONTRACTOR SHALL BE RESPONSITE FOR CALLING FOR LOCATIONS OF ALL DISTRICT UTBILLS. THEY SHALL COOPERATE WITH ALL UTBILLY CONTAINES BE MANIFLATERED THE RESPONSITE AND/OR RELOCATION OF DISES.

THE CONTEXCION SHALL CONTACT COPINGS STATE ONE CALL AT SELECTIONS IN ALL INDIDECOUND WATE. LAST 44 HOUSE BLADWING FOR THE COATHORS OF ALL INDIDECOUND WATE. CASEL CONDURS, FREE, ANDHOLES, VALVES OR OTHER BURED STRUCTURES REPORT DISCORD. THE CONTEXCION SHALL REPAIR OR REPORTED THE ABOVE WHEN DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.

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)	FLARED END SECTION	
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φ.	HYDRANT	
\$4	GATE VALVE	
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n	WATER MANHOLE/WELL	
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Ø	POWER POLE	
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· mm	TREE LINE	~~
	PARKING SETBACK LINE	
	BUILDING SETBACK LINE	_

GENERAL NOTES

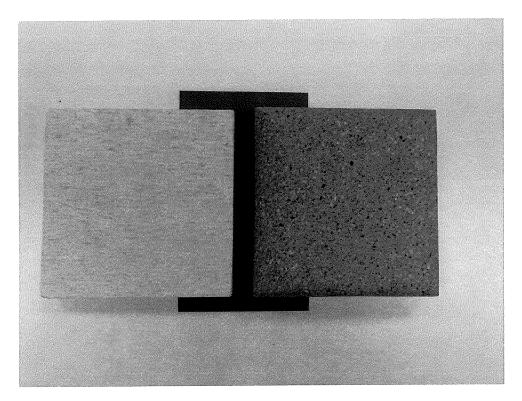
LEGEND

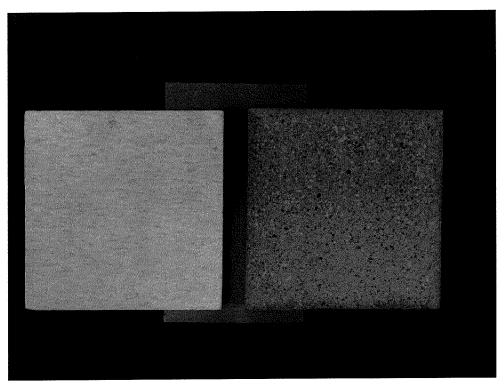
 MINNESOTA STATE STATUTE REQUIRES NOTIFICATION FER "COPHE CALL" FRIOR TO COMMENCING ANY CRADING, EXCAVATION OR L WORK.

- THE CONTRACTOR SHALL FIELD VERIFY LOCATIONS AND ELEVATION UTLITIES AND TOPOCRAPHIC FEATURES PRIOR TO COMMENCEMENT CONSTRUCTION ACTIVITY. THE CONTRACTOR SHALL NOTIFY THE E ANY DESCRIPANCES OR VARIATIONS FROM THE PLANS.
- THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO AN DAMAGE TO ADJECTIF PROPERTIES DURING THE CONSTRUCTION P FROJECT. THE CONTRACTOR WALL BE HELD RESPONSIBLE FOR ANY ADJACENT PROPERTIES OCCURRING DURING THE CONSTRUCTION P
- . THE CONTRACTOR WILL BE RESPONSIBLE FOR PROVIDING AND MAIN TRAFFIC CONTROL DEVICES SUCH AS BARRICADES, WARRING SIGNS, SIGNS, FLACKURE AND DICHTS TO CONTROL THE MOVEMENT OF TRA NECESSARY. PLACEMENT OF THESE DEVICES SHALL BE APPROVED BY PRIDE TO PLACEMENT, TRAFFIC CONTROL DEVICES SHALL CONFORM
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- THE DUTY OF THE ENGINEER OR THE DEVELOPER TO CONDUCT CO REVEW OF THE CONTRACTORS FERFORMANCE IS NOT INTENDED T REVEW OF THE ADEQUACY OF THE CONTRACTORS SAFETY MEASU. THE CONSTRUCTION SITE.
- AND SECURING CONSIDERING CONTRACTOR SHALL BY AND SEDUMENTATION CONTROL MEASURES IN ACCORDANCE WITH REQUIREMENTS, BEST MANAGEMENT FRACTICES, STATE AND LOCAL AND THE DETAILS SHOWN ON THE DETAIL SHEETIS) OF THE FROJEC
- B. ALL CONSTRUCTION PERMITS, APPLICATIONS AND FEES ARE THE RET THE CONTRACTOR.
- ALL ENTRANCES AND CONNECTIONS TO CITY STREETS SHALL BE CON THE REQUIREMENTS OF THE STATE AND LOCAL JURISDICTIONS. THE I SHALL BE RESPONSIBLE FOR ALL PERMITS AND NOTIFICATIONS AS REI
- 10.ALL STREET REPAIRS AND PATCHING SHALL BE PERFORMED FOR THE OF THE CITY. ALL TRAFFIC CONTROL SHALL BE PROVIDED BY THE CI AND SHALL BE ESTABLISHED FOR THE BEQUIREMENTS OF THE MIDNES OF LINFORM TRAFFIC CONTROL DEVICES (MUTICIDAND THE CITY, IT INCLIDE ALL SIGNACE, DRAFECADES, RASHESS AND PLACECTS AS IN BUBBLE CREETS, MANAGEMENT.
- 11. ADJUST ALL EXISTING STRUCTURES, BOTH PUBLIC AND PRIVATE TO T GRADES WHERE DISTURSED AND COMPLY WITH ALL REQUIREMENTS OWNERS, STRUCTURES BEING RESET TO PAVED AREAS MUST MEET ON REQUIREMENTS FOR TRAFFIC LOADING.
- 12.EXISTING TOPOGRAPHY PROVIDED BY LOUCKS ASSOCIATES, TOPOGL DATED 01;724/19.
- 13. THE CONTRACTOR MUST HAVE A CITY LICENSE.

14. A CITY RICHT-OF-WAY FERMIT IS REQUIRED PRIOR TO WORKING WIT







Stone 1 (left): Jerusalem Limestone, Ramon Gray Combed finish
Stone 2 (right): American Artstone Arch'l Precast, light grey mix #78-05 MAE
Accent metal trim / frames: Una-Clad Slate Gray



6950 France Av. So. Redevelopment Retail Building

May 31, 2019

Overview: The existing multi-story office building at 6950 France Av So. is at the end of its useful life. Our team is proposing redevelopment of this highly visible property with a new retail building that fits the city's vision for an energized France Avenue experience. No rezoning is needed.

Development Team:

Developer/Owner: Luigi Bernardi, dba Arcadia on France, LLC

Architect: Edward Farr Architects

Civil / Landscape: Loucks

At A Glance:

Site Area: 43,594 sq ft / 1.0 acre

Zoning: PCD-3

Proposed Redevelopment: New 10,000 sq ft single story retail building, 30 ft high.

Proposed Parking: 51 Stalls plus 11 Proof-of-Parking stalls.

Street Rooms and the Enhanced Pedestrian-Oriented Public Realm: Our small site fronts the France Av. spine on the east; and on the west it backs up to a future 'West Promenade', a north-south *woonerf* style Living Street conceived in the draft edition of the city's *Design Experience Guidelines*. Those guidelines call for buildings along France Av. to be setback from the curb 50 ft to allow for an engaging pedestrian-oriented experience. We have designed an energized urban landscape in this active zone to promote pedestrian engagement, including visible storm water management features, pedestrian plaza zones with bench seating, decorative lighting and select plantings to achieve an inviting and interesting face along France Av. We are encouraging our tenant prospects to participate in this front yard engagement as well, by possibly providing outdoor furniture display items that could integrate with our plaza space.

A future 'West Promenade' could offer an interesting shared circulation experience when enough adjacent redevelopment occurs to create such a public way.

Building Design: The building design is purposefully Modern Classic style, featuring extra-large showroom windows for the tenants on all four sides. We have followed the guidelines to have 'four-sided' architecture, maximizing glass on the facade and raising the roof height to 20 ft tall. The façade complies with the PCD-3 district requirements for exterior materials. It is clad in natural European limestone, with a slight textured finish and stainless steel accents. Energy efficient glass is in thermally broken aluminum frames. Door hardware will include distinctive door pulls. The parapet wall of the building has been extended up vertically to naturally screen the rooftop HVAC units.

Setbacks: The redevelopment complies with all but one setback requirement. Our variance request is for the north building setback from W. 69 ½ St. ROW; where we are approx 16 ft to the building. Note that we are improving the south building setback significantly (35 ft vs 14 ft now). We are also converting the northerly row of 11 parking stalls that are partially within the W. 69 ½ St ROW, to green space with landscaping; and designating those stalls as proof-of-parking.

Vehicular Access: This property is mid-block between W. 69th St and W. 70th St. The primary access and egress are from a short public way named W. 69 ½ St, which only serves <u>southbound</u> traffic along France Av. Northbound traffic is prohibited from turning into W. 69 ½ St due to the median separator. The property has been granted ingress and egress easements through the Kinderberry Hill Child Development Center, 3950 W. 70th St., located southwest of the subject property. Further, as part of this redevelopment project, we will establish a small ingress and egress easement through the BMO Harris Bank property, 3905 W. 69th St., located northwest of the subject property. This new ingress and egress easement will connect our parking lot entrance to the end of the W. 69 ½ St. ROW.

Parking: 51 parking stalls are proposed, plus another 11 proof-of-parking stalls, to meet the 62 stall total required for Retail buildings <25,000 sq ft. A bike rack will be provided to encourage ridership; and an EV charging station will be provided. One small format loading berth is also provided.

Site and Building Signage: We will have one freestanding monument sign along France Av, meeting the 100 sq ft size limit. Tenant wall signs will consist of individual backlit letters mounted onto a rectangular back panel that uniformly fits the wall module on the façade. Directional signage for internal circulation will be ground mounted signs on posts.

Traffic Study: A traffic study will be conducted for the new development. By comparison, the existing use is a 28,000 sq ft office building vs the new use of 10,000 sq ft retail space.

Utilities: The property is already served by water, sewer, gas and electric. Storm water management will be significantly improved by the addition of an underground storm water treatment system for water quality and rate control, in compliance with Nine Mile Creek watershed requirements. The front yard will feature a visible and educational storm water treatment rain garden.

Community Benefits from the New Project

- 1. Re-energize this tired site with greater pedestrian participation and building transparency.
- 2. The old office building is outdated / obsolete. The site is ready for a fresh, new development.
- 3. Embraces the *Street Room* concepts of transitional building scale on the west side of France Av. towards the Cornelia and building mass / height appropriate for the neighborhood adjacency.
- 4. Promote sustainable design strategies, such as visible / creative storm water collection, energy efficient construction and lighting, possible rooftop solar and an EV charging station.
- 5. New rear yard parking setback allows for future implementation of the West Promenade woonerf-type living street, per the draft *Design Experience Guidelines*.
- 6. Brings high-end retailers with engaging storefronts to the France Av. corridor.
- 7. Significant improvements to storm water management.
- 8. The redevelopment uses existing utilities and roads in the community. Urban planning considers this good stewardship to reuse existing sites with current infrastructure.

Development Questionnaire

What this is: A brief questionnaire to help community decision makers understand how this development aligns with key focus area of sustainability that ensures both developers and the community long-term value.

Why? Through the 2014 Visioning process, environmental stewardship is one of seven strategic focus areas. Vision Edina's Environmental Stewardship states:

"There is a growing awareness of the impact that the built environment has on the natural environment, and the individual and collective responsibility we all have towards good environmental stewardship. Community residents and stakeholders believe that Edina can take an active and ambitious internal and regional leadership role in promoting more comprehensive recycling, smart building, and energy efficiency practices. These themes couple well with the parallel benefits in smarter urban planning, increased transportation options, and application of technology."

Questions	Answers
Sustainable Design & Energy	
Have you utilized Xcel Energy's <u>Energy Design Assistance</u> and/or Centerpoint Energy's <u>Builder and Developer programs</u> for this development?	Too small to qualify
Will the buildings meet <u>SB2030</u> energy goals and/or will they be <u>Energy Star</u> certified? If not, please share the steps you are taking to support energy conservation.	Not SB2030; possibly EnergyStar*
Will you be optimizing the roof by installing a green roof?	No
Will there be any renewable energy generation on site?	No
Will there be purchase of renewable energy credits (RECs)?	No
Comments: *Our project is only the 'shell' building enclosure – tenants will be individually responsible for their own tenant improvements, including HVAC & Lighting. **We are trying to see if we can include rooftop solar instead of a green roof.	
Managing Water	
What percent of the property is pervious surface before the redevelopment? What is the percent post development?	77.66% 79.77%
What new services will be pervious? (i.e. Sidewalks, driveways, overflow parking)	None
How will the landscaping support the natural ecosystem? (i.e. Rain gardens, % native plants, % bee friendly pollinator plants)	See comments
Comments: Over 60% of the selected species are native to North America and many of them are pollinator friendly. Infiltration basins are used to allow water to filter through the ground and into the groundwater reservoirs.	
Managing Tree Canopy	
What percent of the property is covered by tree canopy before redevelopment? What is the percent post development?	16% 15.5%
Will you be replanting/replacing trees at least four to five inches in diameter to positively impact the tree canopy (ordinance requirement is only 2.5 inches in diameter)?	No*
Comments: *We are saving seven trees on the site which range from 6" to 36" in diameter.	

Development Questionnaire	
Managing Waste	
Will a recycling service be provided to all businesses on site?	Yes
Will an organic (i.e. food waste) recycling service be provided to all businesses on site?	Only if a food service tenan
Comments: We do not anticipate having tenants that prepare or serve food.	
Sustainable Transportation Will there be bike parking near main entrance for guests?	Yes
Do you have EV Charging Stations for owners or guests to use?	Yes
Will there be parking spaces provided for car-sharing vehicles to reduce the overall number of cars?	Possible, but not likely*
Comments: *Small retail buildings aren't good candidates for car-sharing trips, since customers drive individually, and there aren't enough store employees working same shifts.	