
Greater Southdale District **Design Experience Guidelines**

March 5, 2019

Table of Contents

A. Preface

What are Design Experience Guidelines?

1. Introduction

page 1

Placemaking Through the Lens of Experience

2. The Vision

pages 3-5

The Greater Southdale District Experience

3. The Framework

pages 7-11

The 200' x 200' Grid
Street Rooms and Seams

4. The Guidance

pages 13-35

Public Realm Experience
Transitions and Connections
Street Room Typologies

5. Implementing and Measuring Experience

page 37

Ten Things to Remember



Preface

What are Design Experience Guidelines?

A. Preface

What are Design Experience Guidelines?

For nearly four years, a Work Group comprised of Edina residents and business leaders contemplated the future evolution of the Greater Southdale District, using the notion of “experience” as the foundational element from which to shape the district over the next 50 years. The first phase of the Work Group’s efforts consisted of developing Working Principles that would guide the group’s efforts into subsequent stages as well as suggest a dialog for considering new introductions within the district. Phase Two translated these principles into an urban design framework plan that proposed enhanced connections across existing major streets, introduced a new block framework to encourage walkability, and posed design strategies to create an improved public realm and promote higher quality, pedestrian-oriented development.

The broader experience the urban design framework strives to achieve is the creation of a Greater Southdale District that connects seamlessly between the existing urban and residential fabric, maximizes the development potential of each site, enhances walkability and livability, and encourages interaction among current and future residents, businesses, and institutions. The urban design framework is the foundation to the Greater Southdale District Plan, a part of the City of Edina 2018 Comprehensive Plan. The Greater Southdale District Design Experience Guidelines is a companion to those documents, outlining high-level planning and design strategies for public realm, site, and building design supporting the desired experience outcomes.

The Design Experience Guidelines sets baseline guidance for developers, designers, City of Edina staff, Planning Commissioners and members of the City Council when proposing, designing or evaluating proposed introductions within the Greater Southdale District.

Use of the Design Experience Guidelines

The Design Experience Guidelines apply to proposed development within the Greater Southdale District and former Pentagon Park, and should be referred to when embarking on new development or redevelopment. The Design Experience Guidelines also offer direction for new public realm features and the reimagining of existing public infrastructure. The document does not stand alone; it must be considered as part of a larger set of district goals, urban design framework and patterns, and policy guidance outlined in the Greater Southdale District Plan and the 2018 City of Edina Comprehensive Plan. Together, these documents set forth

City expectations for the future form of the Greater Southdale District, and inform all potential projects within the district.

The Design Experience Guidelines are not a substitute for City of Edina zoning codes and ordinances, but instead provide substantial background for developers and design teams and a framework from which to approach proposed projects within the district. The Design Experience Guidelines clarify the dimensional characteristics of eight different Street Room typologies that together form the experience within the district. The physical qualities of each street room's height and width, and shaped by the architecture of the district, informs the design of the public realm on streets that connect and bind the human experience of the district. These physical characteristics also shape the experience of transition – transitions from existing single-family neighborhoods and transitions into and out of the district, outlining gradual changes in building scale within these critical transition areas to bridge between one- and two-story residences and the greater intensity of the Greater Southdale District.

In conjunction with the Urban Design chapter of the Greater Southdale District Plan, this document provides both a philosophical and practical framework to facilitate discussion among the City, development teams, and the community when considering proposals for change within the district. During the recommended Sketch Plan review process (described on page 70 of the Greater Southdale District Plan), the Design Experience Guidelines are intended to facilitate dialog about broader district goals, patterns and connections, building massing, programmatic opportunities and shared public realm connectivity. Dialog at this point in the review of a proposal requires imagination, looking beyond the immediate site to imagine the creation of a consistently positive human experience, requiring a proposal to recognize the ways in which it influences that experience on adjacent and nearby sites. Once a proposal progresses beyond the Sketch Plan, the document is used as a test of outcomes and touchstone to measure how every proposal meets the desired district experience.

Further specific design details related to landscaping, curb and intersection design, stormwater management and daylighting, lighting standards, street furniture, and a host of other factors related to the experience of the district will be described in future versions of this or a similar document as the need for direction related to those features becomes better understood.

1

Introduction

Placemaking Through the Lens of Experience

1. Introduction

Placemaking Through the Lens of Experience

Places link our past to the future. Within every community there exists a context of memory and identity. That context is made up of the choices/decisions of the past and is inseparable from the physical identity of place; how buildings and streets look, and what parks and open space feel like. These choices are integral to what a place will become.

By describing the evolving Greater Southdale District through experience criteria shaped by the physical context, a framework emerges for how streets will be structured, the relationship of open space to buildings, and how together the designed environment will support the desired experience outcome.

To expand this basic idea into contemporary design is to be both democratic and innovative. What it means to be democratic is to encourage everyone to contribute to the possibilities of a new experience within the notion that it comes from what is unique about living in Edina. To be innovative is to reach beyond current conditions to create an extraordinary place and experience.

Placemaking is not an act of invention... it is the study of how a unique place in world works, in combination with bringing contemporary design into alignment with the existing characteristics. The tools created via a collaborative process of engagement with the Greater Southdale Area Work Group takes this approach to the planning of the overall district, with the intent of providing the community, civic leaders, developers and designers the inspiration to think about the possibilities of this place in new and enterprising ways.

In the making of the Greater Southdale District we must:

- Support the public realm
- Create equity throughout
- Respect the larger context
- Express the collective nature of community
- Attend to all street rooms equally
- Represent the whole in all actions
- Put the collective before individual expression
- Act to support the next 200 years of the Edina community

— Architecture Field Office, 2018

2

The Vision

Aspirations for the Greater Southdale District

2. The Vision

The Greater Southdale District Experience

Background and Context

Collectively, for nearly four years, the Greater Southdale Area Work Group has focused on building upon our community's history of innovation, engagement and community commitment to create a vision for the possibilities of the Greater Southdale District. This work has culminated in the Greater Southdale District Plan (adopted December 2018) and the Greater Southdale District Design Experience Guidelines.

Change is inevitable within the district. In the past five years alone, over 1,500 new housing units have been constructed or are underway, along with myriad other commercial projects. This is a tremendous opportunity in time, and in the broader evolution of our community. We can transform the physical environment of the Greater Southdale District from a traditional car-oriented suburban commercial district with its sea of surface parking lots and 'superblocks,' into a vibrant place whose character is neither urban nor suburban, but blends the best characteristics of both to create a place that is uniquely of Edina. This new model will support all modes of transportation (cars, buses, bikes, scooters, and pedestrians) and serve future generations of Edina residents, employers, and visitors. It will complement, not compete with, the single family neighborhoods that have historically defined much of the community's fabric.

Within the broader 750+ acre Greater Southdale District, and the former Pentagon Park, there exist a remarkable variety of assets. This plan connects those assets with a new street grid that overlays a human scale and allows access via a variety of transportation options. The plan sets forth a strategy to bridge between adjacent single family neighborhoods and the more commercially focused areas of the district. And, it uses public realm infrastructure—including parks, plazas, green streets, woonerfs, and waterways that manage stormwater—as the connective tissue that gives the district its unique identity. Together, these attributes will set the stage for a remarkable daily experience for those who live, work and play within the Greater Southdale District.

Aspirations for the Future Greater Southdale District

The Greater Southdale Work Group summarized the overall districtwide vision and land use strategies as part of the District Plan. These aspirations, which follow, are at the foundation of design policy parameters and the overall experience we are striving to shape as the Greater Southdale Area evolves over the next 50 years.

1. Imagine Greater Southdale District evolution organized around human activity, with vibrant pedestrian-focused streets, beautiful parks and public spaces, and endearing and enduring buildings where:

- A sense of invention is expected from new introductions, both public and private, that build on the district's spirit of innovation.
- Its role as regional and local center for living, shopping, working, learning, entertainment, hospitality, and medical services is enhanced.
- Other Edina neighborhoods, near and distant, benefit from investment in the district and the evolution of each parcel.
- Investment in the public realm is reflected by a commensurate investment as private parcels evolve.
- Public and civic services accommodate a growing and diverse district and community population.
- Transitions at the district's edges recognize compatible use and scale and neighboring uses are perpetuated on their terms.

2. Make the Greater Southdale District the model of healthy urban living where:

- The district's form encourages healthy living habits, particularly through walking.
- The design of buildings and spaces, both public and private, attract the widest possible range of the district's population.
- Storm water is a valued resource by making it part of the experience of the district.
- Emissions and pollutants are mitigated through the introduction of significant tree canopy and limiting idling vehicles on streets, creating a more inviting walking experience along the district's streets.
- Sustainable solutions result in a stock of healthy buildings that compel healthy activity for their occupants.
- Public features mitigate impacts of non-local infrastructure, especially to contain the ill effects of adjacent highways.

3. Invent sustainable infrastructure matching the district's sense of innovation where:

- Mutually-supportive and forward-looking infrastructure is the norm, looking beyond baseline utilitarian functions of a single site to create a broadly supportive district infrastructure.
- Infrastructure aligns with the creation of public space in the district, sharing space and resources that result in compelling, attractive and high-functioning civic spaces.
- Care for and perpetuation of public infrastructure anticipates daily human activity in all seasons.

4. Create neighborhoods of activity within the broader mixed-use patterns of the district where:

- Logical boundaries based on reasonable walking distances are established, with major streets as seams binding the activity of each side into an inviting and accessible public space.
- Focal points of public activity are found within each neighborhood.
- Key community services and facilities are present and help define the fabric of the District.
- Core services are delivered within each neighborhood or in an adjacent neighborhood.
- Neighborhoods are linked along street and park corridors highlighted by visible human activity.

5. Offer a spectrum of living opportunities integrated through the district where:

- Housing orients to a variety of income levels and household types.
- Ownership options constitute a significant portion of the living opportunities in the area.
- "Missing Middle" living opportunities (duplexes, triplexes, side-by-side or stacked townhouses, rowhouses with multiple units, and small buildings with four to six apartments) allow a broader range of Edina residents, workforce members and others to consider relocating to the District.
- Buildings for living strongly orient to the public spaces of each neighborhood within the District.

6. Expand significantly the number and extent of parks and public spaces where:

- Opportunities for the introduction of another large signature public space complement the programming and activities available at Centennial Lakes.
- An extension of the Promenade to Strachauer Park links neighbors and activity to the district.
- New promenades on the East and West edges of the District create movement corridors for pedestrians and bicyclists and serve as vital places for a transition between neighborhoods and the District.
- Parks and publicly accessible spaces are clearly visible and directly accessible from the public realm.
- Spaces for visible human activity and occupation, either public or publicly accessible, occur on every block.

7. Encourage district evolution based on incremental change and the creation of a great pedestrian experience where:

- A basic framework of streets and blocks encourages pedestrian activity and well-considered buildings.
- A rich variety of public or publicly accessible spaces are woven into the experience of the district.
- Sites and buildings support a pedestrian experience first, with storage of cars not a focus.
- Development on each site links to adjacent streets and to neighboring sites to create continuous, safe, and inviting pedestrian experiences.

8. Build (or plan for) a street network encouraging pedestrian movement across and through the district where:

- Walkable block lengths (200 feet) are the baseline framework for development.
- Enhanced and more frequent street crossings facilitates pedestrian movement.
- Wide landscaped boulevards encourage pedestrian activity and create a distinct district signature.
- Community corridors within and extending well beyond the district enhance bicycle and pedestrian access while accommodating vehicle traffic on pedestrian terms.

9. Imagine transportation in the district where:

- Cars are not the focus and streets accommodate more than vehicles.
- Major streets balance access and mobility.
- Some streets serve as community corridors, linking to other community destinations with features that allow for movement in addition to cars.
- Transit is a baseline service, both within the district and to non-Edina locations.
- Transportation recognizes trends, including autonomous vehicles and a time when parking structures aren't needed for public parking

10. Expect the delivery of high quality, well-designed buildings and sites where:

- Spaces on sites are considered for people first, including connections between sites; then the ways structures are placed; and then places to store cars are found.
- Visible human activity is prominent and integrated at every site.
- People are brought to the streets via major building entries oriented to major streets.
- Storm water remains visible as an amenity, allowing it to become a central part of the experience of each site.

11. Frame development guidance for evolution where:

- Development review includes the desired experience, not solely quantitative thresholds.
- Accommodation of adjacent and near parcels are considered in the evolution of a single parcel.
- Early reviews focus on ideas, patterns and relationships, not specific and engineered plans, with that part of the review process based in dialog, not presentation and reaction.
- Demonstrations of quality and especially quality from a long-term perspective are baseline considerations.
- Collaboration leads to a superior result, with the community's expectations clearly framed as part of the deliberation.
- Flexibility is not a right, but rather the natural by-product of a fair exchange for benefits, collaboration, and quality in development.

3

The Framework

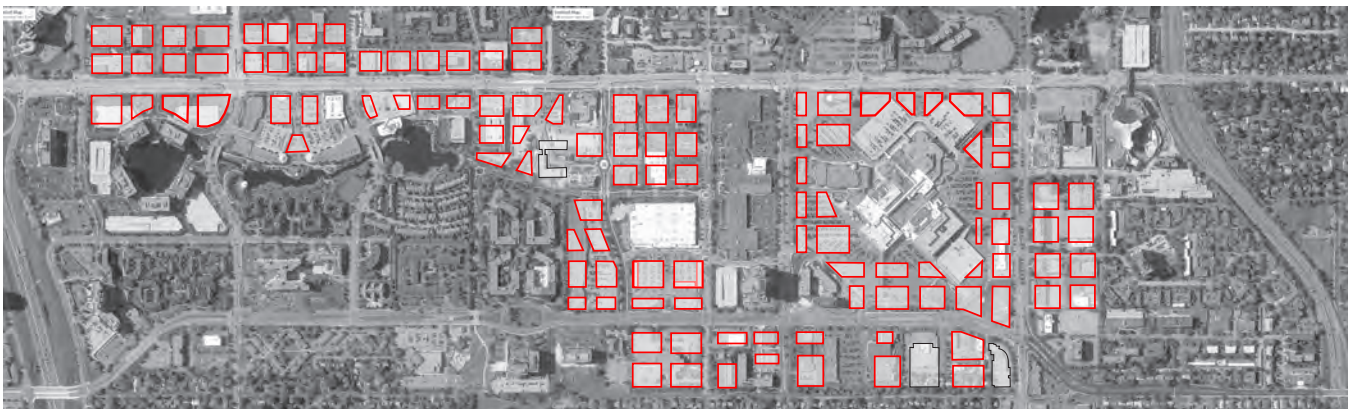
Building Blocks to Support the Vision

3. The Framework

Building Blocks to Support the Vision

THE 200'x200' GRID

There is an expectation of the street grid: it is democratic, it is uniform, it is connective. The uniform grid and the buildings that line the grid give the city its form. The space between buildings is used for access for pedestrians and vehicles; for entrance into both public and private buildings; and to provide light, air, and common green or social spaces.

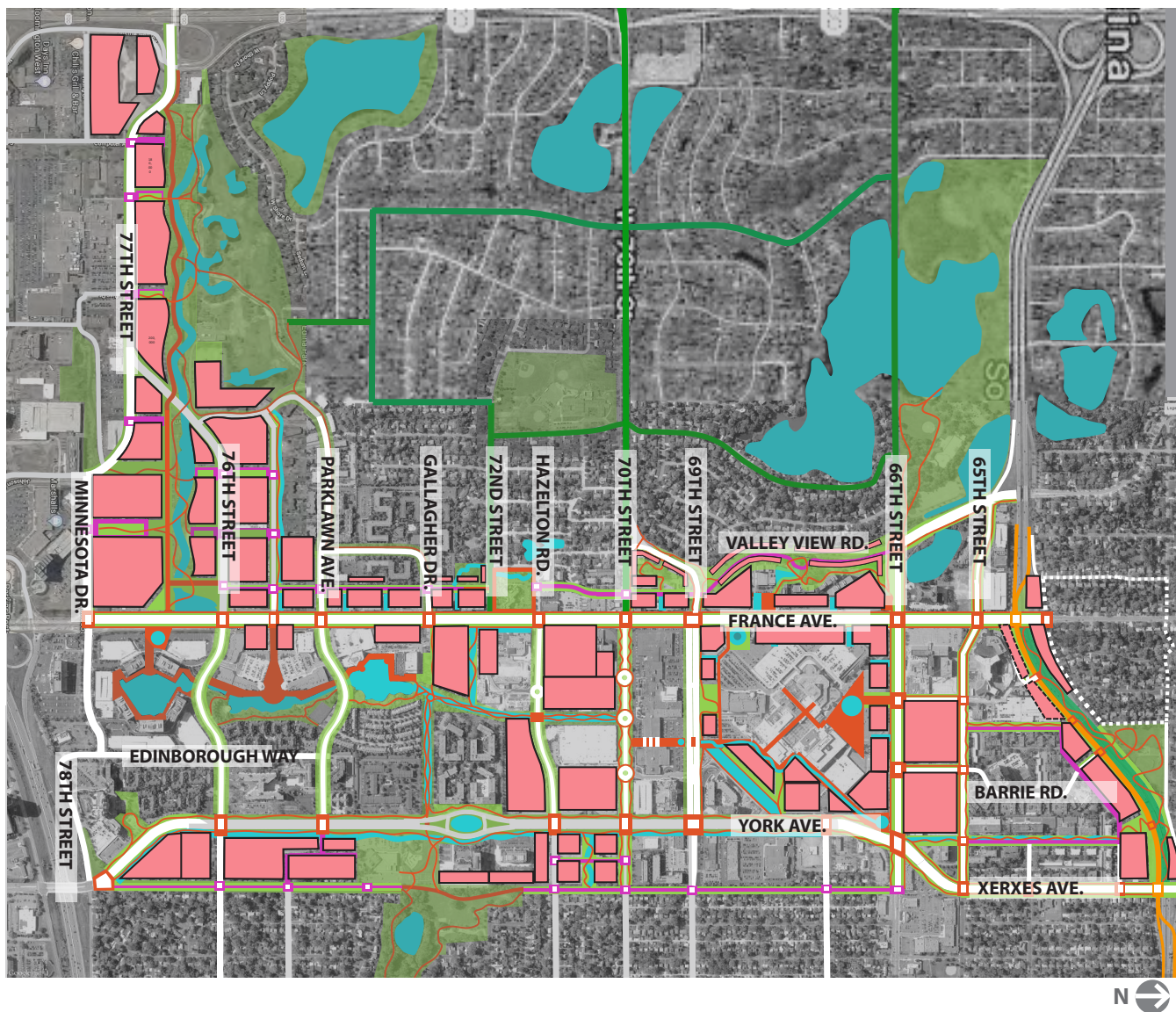


To break down the scale of the 'superblocks' that currently characterize the district's overall land use framework, three distinct street grid patterns were considered, to inform what how the Greater Southdale District might transform into a more uniform and connected community. Small Portland blocks (200' x 200') were compared to the long blocks of New York City (200' x 600') and the larger square blocks of Minneapolis (350' x 350'). The Work Group focused on Portland as a model because of its walkability, and the scale of its buildings resulting from the 200 foot restriction on the length of building elevations fronting the public realm. Further studies analyzed land ownership patterns, size of property and generally how connections could be made through the superblocks. It was concluded that the 200' x 200' foot system was more adaptable to variety of site conditions supporting a more engaging public realm and opportunities for a better community experience.



Considerations

- Width of street
- Sunlight in public spaces
- View corridors
- Building scale – height, length and footprint
- Transparency at street level
- Landscape, lighting and street furniture
- Streets and public realm paving
- Courtyards and pocket parks
- Cultural context – pride of place, historical framework



Potential Building Sites

This diagram illustrates those parcels within the Greater Southdale Area and adjacent Pentagon Park that are potential redevelopment opportunities. Criteria to measure the opportunity inherent in these sites include:

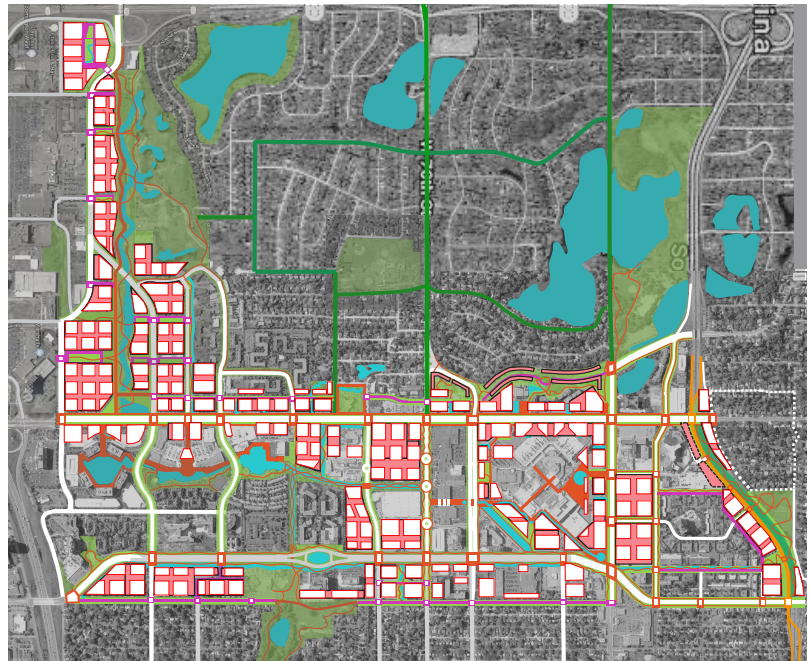
- Sites that currently have large, dominating surface parking lots.
- Sites that are critical to the overall success of development in the district.
- Important connections that would extend existing public realm assets such as the Promenade.
- Underutilized land that can be repurposed to serve the public realm goals of the district.

Potential Building Sites

The diagram at right illustrates how the basic principle of a 200' x 200' grid can be applied nominally on potential redevelopment sites throughout the district without consideration of property line. It is recognized that land ownership will influence the ultimate form of the grid.

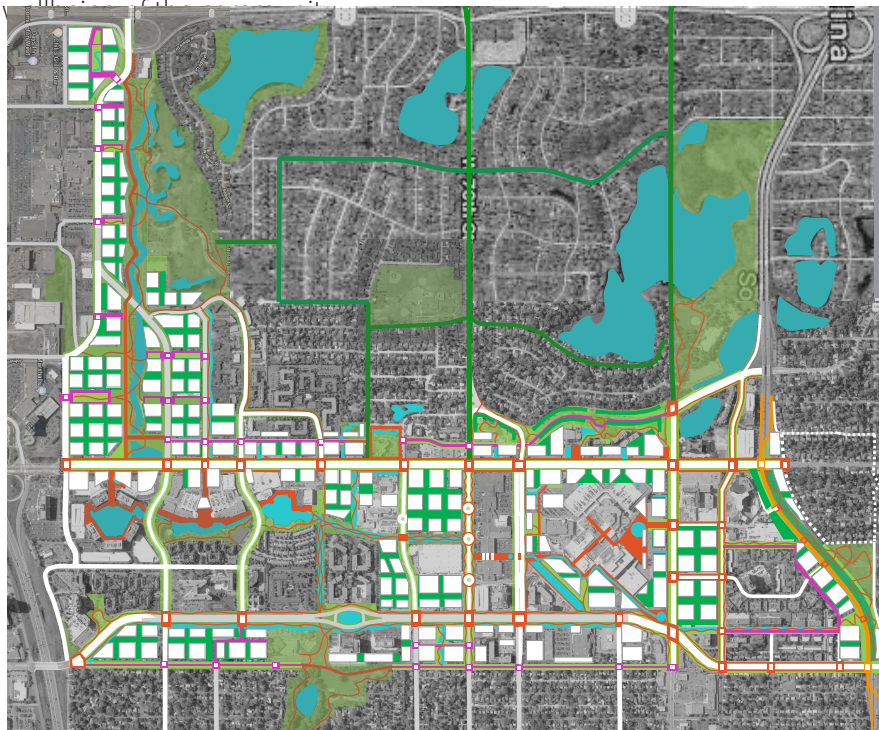
Other considerations influencing block pattern:

- Building scale
- Public realm connections
- Connections through blocks
- Pedestrian-oriented street intersections



The Space Between Buildings

As a result of the 200' x 200' block pattern, the space between buildings becomes an important asset in shaping the overall sense of landscape and continuity of public realm throughout the district. Because the superblocks have primary responsibility for serving the vehicular traffic needs of the district, access to the smaller blocks for drop-off, service and parking can be planned to stay at the perimeter of a block, allowing for the spaces between the remaining block to be used for a network of green spaces that support the health and



Opportunities for the “Space Between” buildings to become places and connections to larger community pathways:

- Parks
- Pocket Parks
- Recreation
- Play Areas
- Gardens
- Stormwater Management
- Wetlands
- Waterways

STREET ROOMS AND SEAMS

With changes in the way people are living in cities and suburban environments, it is important to seek new ways in which our communities can be experienced without sacrificing the spatial and architectural values of existing neighborhoods. Defining the way in which transitions are seen and developed, and the way each street can have a different character, use and form, puts the emphasis on the public qualities of the public realm rather than on the buildings themselves.

In this way buildings can develop based on highest and best use without compromising characteristics of existing community life. Respect of context can bring about a degree of commonality where every building is not required to mirror its context but rather, support a connected and meaningful community experience.

The intention in the design visioning process for the Greater Southdale District was to create a new paradigm for suburban mixed use districts, in which individual buildings respect their context and each other. The ensembles created as part of this process are **Street Rooms**. In this approach, the geometry of the city plays an important part in the definition of the street room, by width of street, length of block, solar orientation and axis as it relates to other grids defining other public rooms of the city like courtyard, plazas and parks. The real identity of the city is closely related to how the city is seen up close, making it possible to understand the true character of a place. It is these up close characteristics that make a city unique.

This holistic approach depends on an attitude of relational subjectivity in articulating the components of each street room. This means that each wall of a building is studied as a part of the street that it faces and in conjunction with the building directly across the street. Scale, color, material and construction details are considerations in the whole composition of the street room—allowing the street to communicate a sense of place and completeness. A district full of street rooms promotes activity and social discourse while signifying the possibility of new functions. The street room is a source of sense of community even when it is not in use, thereby stabilizing the overall fabric of the public realm.

Street Rooms within the Greater Southdale District

Throughout the Greater Southdale District, the public realm is defined by a series of street rooms. These rooms are further defined by edges, referred to as the “seam” between the volume of the street and the form of the building—which together, create the experience at a pedestrian scale. Seams dictate the basic height of the defining street room wall (i.e. the building podium) but not necessarily the rest of the building form, allowing buildings to respond to their context on all four sides of the building and creating a form that is appropriate for each street room surrounding it. This creates variety throughout the district, and supports the notion that there are transitions between characteristics of neighborhoods that are different in scale, program and building type. As an example, transitions from single family houses to 36-foot-tall or 60-foot-tall buildings will not have the same criteria as transitions from a podium base to taller structures sitting on the podium.

The following considerations all factor into the development of public realm and street rooms. These ideas should be utilized and considered by decision-making bodies when reviewing proposed development projects.

- Transparency of building walls in the public realm can be both private and public. When the ground plane is connected along and through the building wall, it creates a link to experience the richness of activities in the public realm
- Buildings could be sited perpendicular to streets, allowing vistas and pathways through the new street grid.
- Buildings can create a very consistent form and a clear sense of place within the public realm. Each building is part of the next – both internally and externally. Regardless of building age, the continuity of scale, rhythm and materials of each building that lines the street creates a sense of room.
- Continuity of building material quality is contained within each stone, brick, window pattern and cornice to define the edges of the public realm and the street room
- Continuity of architectural language in the edges of shapes and rhythm of openings define a street room into horizontal and vertical forms that frame the experience of being in room – whether interior or exterior. The architectural language of the street should not be replicated but rather understood and transformed in each new building within the context of its particular location within the Greater Southdale District.
- Buildings frame the public rooms of the district (parks, pocket parks, playgrounds, etc.) and should represent the scale required to meet the needs of the community. Moving through these spaces, one should easily see choices for other desired destinations.
- Good street rooms provoke a culture's spatial imagination, social discourse and creative energy. The street typologies and corresponding guidelines on building form define experience and spatial form that connects the district together.
- The structure of the street rooms is organized to support residents, defining a particular place within a neighborhood and the overall district. The memory of experiences in each of these rooms provides the experience of sense of place and connects to other places within the Greater Southdale District.
- Walking is a shared experience. An engaging walk can be short, long, slow, quick, or meandering – in weather that is wet, calm, windy, sunny, bright, cold, hot, or snowing – in places that appear open, closed, low, tall, long, narrow, wide or expansive. The experience of walking through the street rooms must be full of events connecting to a broader set of experiences.
- The materials of the walls that make up the street room define the characteristics of the public realm. A certain part of this will depend on the age of the street, the program of the building and the design style in which they were constructed. All of these factors, in whatever form, are read and experienced in different ways and need to be delineated clearly.

4

The Guidance

Shaping the Greater Southdale District
Experience

4. The Guidance

Shaping the Greater Southdale District Experience

Overlaying a new street network and street grid is a fundamental strategy in breaking down the scale of the existing massive ‘superblocks’ within the Greater Southdale District and improving the public realm experience. All new development should begin with the premise of the 200-foot-square block as the measure by which a building footprint is determined. The space between buildings are streets—which could be for cars, bikes, pedestrians only, or transformed into parks and open space. By connecting the design of streets with the concept of street rooms, the district will transform to one that is human-scaled, comfortable, green, and flexible for change and evolution over the next 50 years.

While each street room and neighborhood will provide a distinctive experience, there are certain characteristics that are common to streets throughout the Greater Southdale District, helping to creating a holistic experience for pedestrians and bicyclists, transit-riders, and drivers alike. Buildings lining the public realm/street room will incorporate a mix of uses, including housing, workplace/office, cultural, community, and commercial/retail space—setting the stage for a rich variety of experiences.

The overall public realm experience within the Greater Southdale District is supported by:

- Consistent building setbacks that create the opportunity for an expansive public realm within the district and sub-district.
- More frequent intersections to promote pedestrian connections within the district and to neighboring districts.
- Distinctive sidewalks that support the pedestrian experience, separate and dedicated bikeways, and appropriate number lanes of vehicular traffic.
- Wide sidewalks with places for gathering, play areas, gardens, outdoor cafes, etc.
- Publicly-accessible pocket parks and courtyards along the street and sidewalk extend the public realm of the sidewalk in between buildings.
- Separate and dedicated bikeways, and an appropriate number of lanes of vehicular traffic.
- Integrated signage and lighting systems that offer safety, interest and diversity to the pedestrian.
- Consistent signage that reflects sub-district identity to promote wayfinding within the larger Southdale District, identifying characteristics of the street and public amenities.
- Pedestrian and vehicular paving (permeable) that is unique to a sub-districts streets and sidewalks.
- Trees that vary in species, installed in rows or clumps, and spaced to create visual interest and promote a range of experiences such as shaded groves with benches, or a sun-dappled outdoor cafe, along the street.
- Pedestrian, bicycle and vehicular access routes through larger blocks.
- Safe, comfortable places where people can stop, view, socialize and rest. These may incorporate “landing zones” for ride sharing services such as Uber and Lyft — particularly near primary gathering places and public rooms along the Promenade, connector streets and future East and West Promenades. These places of respite should not conflict with other sidewalk uses.
- Different, and defined, zones on all sidewalk: building frontage zone, street furniture zone, movement zone, and the curb. Cafes and outdoor seating can be located in the building frontage zone, extending the activity of a building to include the sidewalk.

- Transit stops that are designed to provide districtwide continuity, reinforcing the qualities of the Greater Southdale District.
- Integrated plantings, water features and/or art to enhance public open space.
- Stormwater that is daylighted and used as a water feature or amenity, integrated into the overall experience of the street and the district.
- Street parking provided at the curb to support a mix of activities for both residential and commercial activities.
- Varied landscaping and street trees that create a canopy over the street. Consider the changing climate when selecting plant species with the understanding that indigenous plant materials may not always be the most appropriate choice.
- Building equipment, mechanical exhaust systems and/or service areas concealed in a manner that does not detract from the pedestrian environment.
- The public realm is for both summer and winter conditions and as such must be planned to be easily maintained in all conditions. Building owners and city stakeholders should plan for maintenance, operations and upkeep within the public realm. This includes prompt and thorough snow removal on every reach of the sidewalks, care and feeding of trees, landscape and decorative planters (which should be changed on a seasonal basis).

PRIMARY COMPONENTS OF THE PUBLIC REALM EXPERIENCE

The following guidance is provided to assist the community, civic leaders, designers and developers in understanding the vision and baseline parameters related to both building and public realm within the Greater Southdale District.

Connections

- The overall strategy is to connect intersections, incorporate street typologies, and incorporate green systems to add value to the experience of the district.
- The public realm is to be connected continuously north-south from Centennial Lakes, the Promenade, the Galleria, Southdale Center, Fairview Southdale, to Strachauer Park. All new development shall support that goal.
- The public realm should be connected east-west from Edinborough Park to Centennial Lakes, west of France to Pentagon Park and Fred Richards Park to Highway 100 on the west. This will set up future connections to districts to the west—such as 70th and Cahill—supporting an overall vision of a more connected and integrated Edina community.
- The district must be connected continuously east-west from the Cornelia neighborhood to Yorktown Park.
- New north-south promenades should be created on the west side of France and east side of Xerxes as part of the broader strategy to sensitively transition to single family residential neighborhoods.
- Expand Centennial Lakes Park to France Avenue... celebrate this important public amenity by making it more visible as a gateway into the district.
- Create a dynamic landscape that includes water, especially stormwater expressed as part of landscape, to create public amenity spaces.
- Streets within the superblocks, East and West Promenades, and extension of the Promenade north to Strachauer Park should be surfaced with pavers to promote a dominance of pedestrians and bikes over vehicles.
- New parks and plazas shall be either public or publicly-accessible, not private, in nature.
- Increase number of sidewalks, pathways, and smaller parks/gardens to better address mobility. Incorporate places to sit throughout the district.
- New trees should provide continuity of the street room experience with canopies that are consistent with the Street Room Typology to enhance the continuity of pedestrian experience.

Intersections

- Street Room Typologies overlay each other, unifying the overall district experience through the recognition of unique conditions that evoke unique design responses based upon location.
- Street Room Typologies connect intersections throughout the district, linking experiences together from one neighborhood to the next.
- Street Room Typologies with lower façade heights take precedent over those with higher façade heights at these points of intersection.
- The architecture of a façade of one block making up an intersection should be conceived as part of all corners of the intersection.
- Crosswalks at intersections need to be an integral part of the public realm and continue the overall street room experience from one block to another.
- The hierarchy of intersections will change based on an evolving context and investment in the intersection experience.

Street Room Form

- Building setbacks are to be considered as a part of the overall landscape and public amenities, and should be designed to create a continuous pedestrian experience along major corridors to support “pools of human activity.”
- Every new development should connect all publicly-accessible spaces such as pocket parks, courtyards and plazas to the street room typology.
- Along all major corridors, seventy five percent (75%) of face of building walls need to be at the setback line to support the creation of a ‘street room.’
- All new building façades in the district must have seventy five percent (75%) transparency at the ground level.
- All building façades are prime (including parking) and must be designed accordingly. There is no back side of a building.
- All facades on the first vertical 60 feet of a building (above grade) shall use natural materials facing the public realm.
- Above 50-60 feet, glass, precast panels with brick/tile are the preferred material palette. Metal panel can be used as a secondary part of a wall system.
- No building façade can be longer than 200’ without changing direction by a minimum of 90 degrees.

Building Form

- Ground floors should have a minimum ceiling height of 20’ for flexibility. This floor-to-floor height will allow the space to accommodate commercial, two floors of parking, or two-story townhouses.
- Above-grade parking structures should be designed with flat floorplates to allow for future conversion and lined with programmable public realm space to minimize the visual impact of car storage.
- Within 50-60 feet of the ground, it is preferred that rooftops be programmed to accommodate residential or public user activities (e.g. a restaurant or terrace).
- Rooftops facing the Promenades must be functional and programmed to provide interest and variety along these important pedestrian spines.
- All development services, including rooftop mechanical systems, should be located within buildings and should not be visible from the public realm, or semi-private and private areas of the development. The exception are rooftop-mounted solar panels, which should be located on the highest point of the buildings.
- Building footprints above 60 feet should be no greater than 12,000 SF for residential use and 24,000 SF for commercial space.
- Design buildings for flexibility and adaptability in the future, including use of structural systems that will allow a building’s function to fundamentally change.

TRANSITION AND CONNECTIONS

Within the Greater Southdale District, a new network of streets will provide both commercial and resident access to new mixed-use buildings along France and York Avenues, keeping traffic out of the adjacent single family neighborhoods. These new streets offer the opportunity for new development to more gradually transition from the scale of the existing single family neighborhoods and the commercial heart of the Southdale District. New development within transition zones is expected to balance scale and building use between these single family neighborhoods and the higher density, more commercially focused Greater Southdale District.



West and East Promenades

The character of the West and East Promenades, new north-south streets that run to the west of France Avenue and the east of York Avenue, to be envisioned as woonerfs—shared streets for pedestrians, bicycles and vehicles. This typology creates opportunities for multiple access to buildings for both below grade parking and service, as well as temporary/short-term parking for retail and building drop offs. Within the woonerf concept, pedestrians have priority over cars, and as a shared street, cars are forced to slow down and travel with caution.

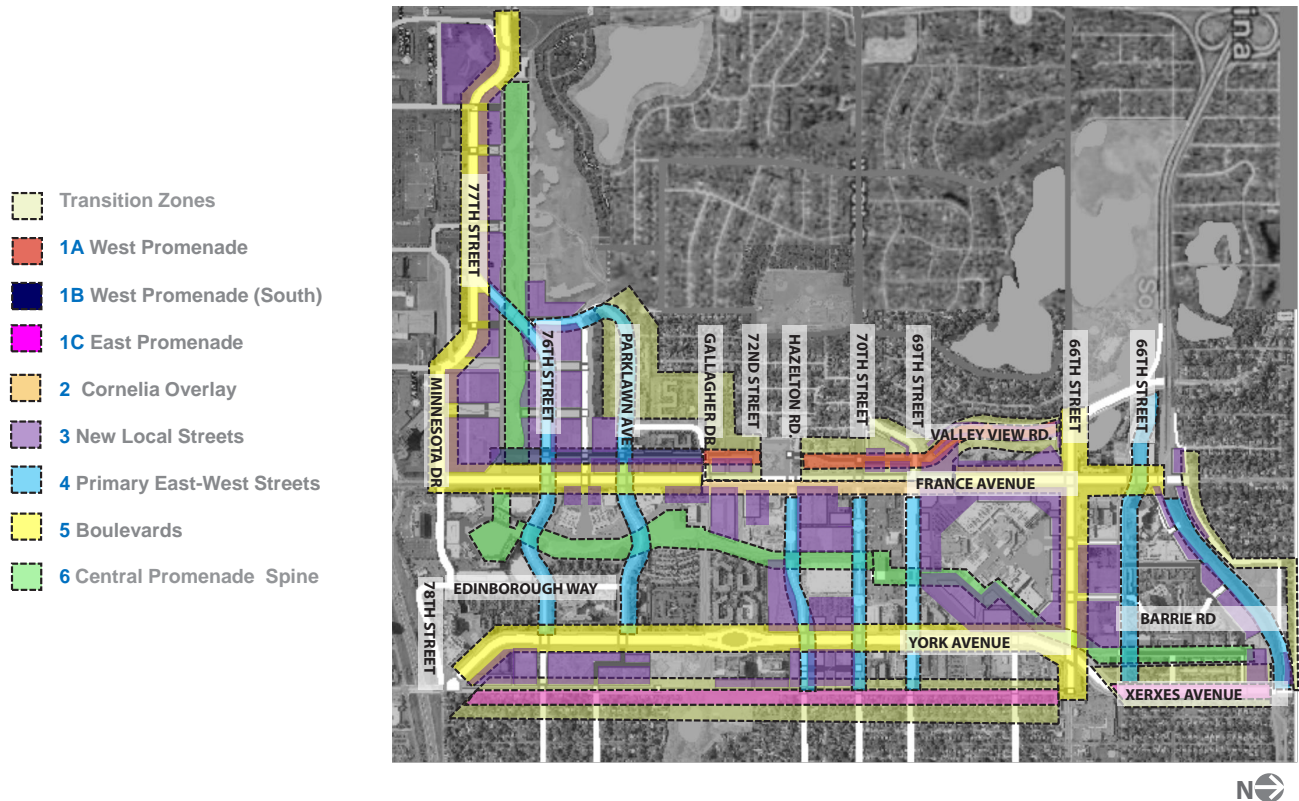
Because of their organization, these streets also can support a variety of uses, with building types catered to residential uses such as townhomes, with considerations for retail space that support less intensive commercial needs of adjacent neighborhoods. The sense of scale by way of width of street to height of building is maintained by creating a street form that is no less than 60 feet high at its edges, with developments potentially increasing in height as buildings reach the edge of the boulevards, and descending in height as they move to the single family neighborhoods, providing an edge to the east and west transition zones.

Primary East-West Streets

East-west streets through the Greater Southdale District connect existing single family neighborhoods to the heart of the district. The design of these streets is intended to respect the neighborhood scale and context in a meaningful way, with an ample tree canopy, extensive setbacks and consistently-scaled buildings at the face of the public realm. By employing these characteristics, the landscape experience serves as a bridge, knitting together the single family residential neighborhoods and the greater intensity of the district.

STREET ROOM TYPOLOGIES

A hierarchy of streets and pathways within the district is the framework for public realm development and related building form. Each street across the district has a role in how it serves pedestrians, bicycles and vehicles in connecting sub-districts, adjacent single family neighborhoods, and the overall Greater Southdale District and creating a unified sense of place. It is the intent that street typologies define the public realm experience: the space between buildings, dimensions of building setbacks from the street, heights of facades at the building face at the street and building step backs, where the façade of the building steps back from the volume of the street room.



Street Room Typology 1: Promenades and Transition Zones

Promenades are new woonerf-type streets on the west side of France Avenue and on the east side of York Avenue. Within this typology, there are several different variations for new building development in these important transition zones, responding to and respecting the context of adjacent single family neighborhoods.

Street Room Typology 2: Cornelia Overlay Zone

With the understanding that there is special sensitivity related to new building development near the Cornelia neighborhood, this is a special zone governing the design of the public realm/street room on the east side of France Avenue between the north side of 69th Street and Gallagher Drive. Buildings within this zone will be expected to maintain the east side of the France Avenue street room, but will be of a lower overall scale than new

buildings at the north or south ends of France.

Street Room Typology 3: *New Local Streets*

These are new 60' wide streets internal to existing superblocks. These streets will be constructed as new buildings are added to the district, and will help create the new network of streets and pedestrian pathways throughout the Greater Southdale District. Some of these spaces between buildings may become parks or plazas, extending the public realm. Others will become primary vehicular access for drop off and pick up, as well as access to parking and primary building services.

Street Room Typology 4: *Primary East-West Streets*

The existing 69th Street, 70th Street, Hazelton Road, Parklawn Avenue and West 76th Street are important connections through the district from east to west. This typology is intended to respect the neighborhood scale and context in a meaningful way, with an ample tree canopy, extensive setbacks and lower scale buildings at the face of the public realm. By employing these characteristics, the landscape experience of the single family residential neighborhoods is extended through the Greater Southdale District.

Street Room Typology 5: *The Boulevards*

France Avenue, York Avenue, West 66th Street and West 77th Street are the district's gateway streets. They carry the highest traffic volumes and are intended to have higher transit volumes than any other streets within the district. These streets will have the greatest impact in conveying the overall identity of the district: a consistent 50 foot setback with a double row of trees will extend the length of these streets, while consistency in building heights along the street edge will form the edge of the street room—bridging between the lower intensity and transitional areas and the higher intensity zones within the Greater Southdale District.

Street Room Typology 6: *Central Spine*

The Central Spine comprises the existing Promenade, its potential expansion northward, and future connections to the west to Fred Richards Park. This important pedestrian network is an attractive destination for both residents and visitors alike. New development along the spine must respond to and respect this important public amenity.

Street Room Typology 1A

West Promenade / Transition to Cornelia Neighborhood

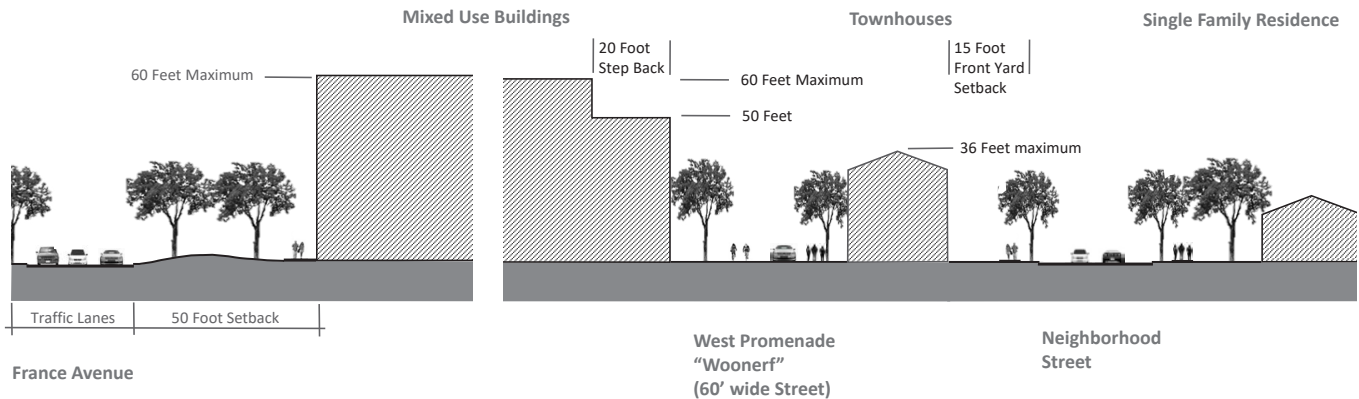


The sub-district to the west side of France between 69th and Parklawn is a unique transition zone within the Greater Southdale District. Any proposed developments within this zone should be approached with special sensitivity. This means that the street experience within that zone should be perceived as connected to the Cornelia neighborhood through landscaping and trees, and buildings that gradually transition in both height and function and use between lower intensity neighborhoods to the more commercially-focused district on the east side of France Avenue.

The West Promenade, a new north-south pedestrian, bike and vehicle street/woonerf that accommodates service access, is envisioned between France Avenue and the Cornelia neighborhood. This new shared street is intended to provide service access to new developments along France (keeping vehicular traffic out of single family neighborhoods), and providing a new framework to support the transition from townhouses and lower-scale residential buildings on the west side of the West Promenade, to slightly taller buildings on the east side of the West Promenade.

Dimensional Characteristics of Street Room Typology 1A

West Promenade / Transition to Cornelia Neighborhood



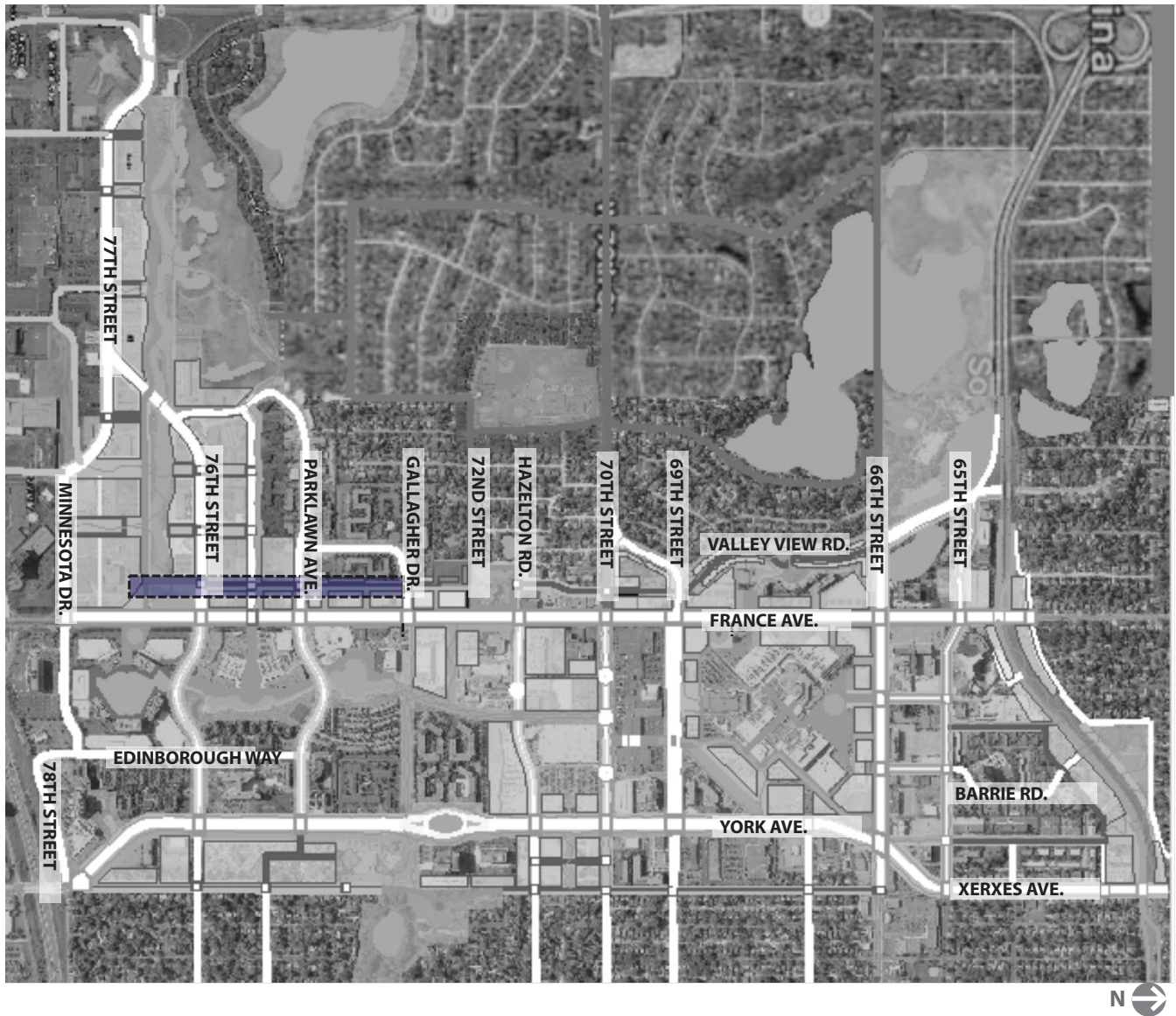
As illustrated in the section above, within this transition zone, building heights will step up incrementally, from those that are adjacent to single family homes to those that are facing France Avenue to provide a more gradual transition from the residential neighborhood to the more commercially-oriented Southdale District.

The street room experience within Typology 1A will be shaped by the following experience guidelines:

- New buildings that are adjacent to single family residential properties, on the west side of the West Promenade, should not exceed 36 feet in height. Townhomes are the preferred residential typology in this area of the transition zone.
- All ground level space east of the West Promenade should have 20-foot floor-to-floor height. This dimension allows for flexibility to accommodate one level of retail space along the street, or two-story townhomes facing the West Promenade.
- All parking, other than short-term retail or guest parking, and building services need to be located below grade or hidden within the building. If on ground level or above, parking and/or building services must be surrounded on all sides by program space such as commercial or housing.
- On the east side of the West Promenade, building faces should not exceed 50' in height. Any height above that limit should step back 20 feet from the facade of the building.
- On France Avenue, a 50 foot setback is required from curb to face of building with a maximum building height of 60 feet.
- On individual developments, should the City choose to permit height above the 60-foot height limit, it is recommended that additional height above 60 feet step back from the face of the building by a minimum of 10 feet in depth and 12 feet in height.

Street Room Typology 1B

West Promenade Between Parklawn and Minnesota Drive

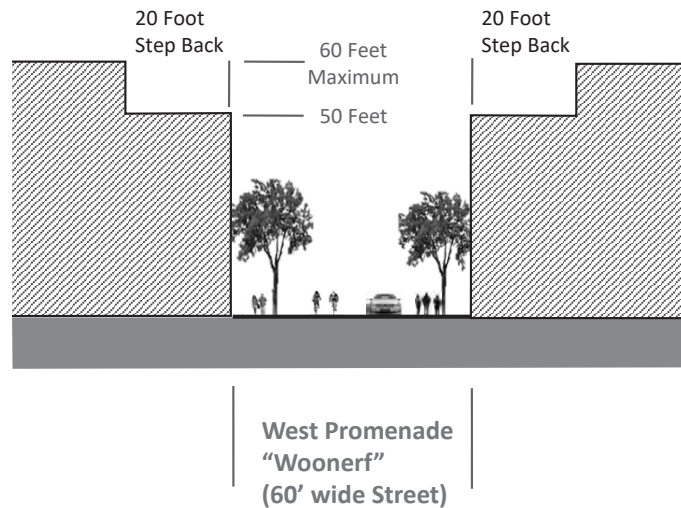


South of the zone designated as Street Room Typology 1A (where the Cornelia transition area immediately abuts single family neighborhoods), the Street Room experience shifts to respond to its changing context. It becomes one that is more commercial in nature and in concert with the expected higher intensity in the southwest quadrant the Greater Southdale District.

As the West Promenade extends south into Typology 1B, it continues to serve pedestrian, vehicular and service access. Uniform building heights on each side of the West Promenade are intended to support the transition from existing multi-family housing to taller buildings at the south end of France Avenue near the gateway from 494.

Dimensional Characteristics of Street Room Typology 1B

West Side of France Between Parklawn and Minnesota Drive



Within this zone, existing buildings are more commercial in nature and do not immediately abut single family neighborhoods. This unique condition lends itself to new development along the West Promenade that is still lower in scale, without the preference for townhomes or smaller scale buildings along one edge, as in Typology 1A.

The street room experience within Typology 1B will be shaped by the following experience guidelines:

- Building faces at the West Promenade within Typology 1B shall not exceed 50 feet in height. Any height above that limit should step back 20 feet from the facade of the building.
- All street level space shall be 20 feet, floor-to-floor in height. This dimension allows for flexibility for retail space (on France) and two-story townhomes facing the West Promenade.
- All parking, other than short-term retail or guest parking, and building services need to be located below grade or hidden within the building. If on ground level or above, parking and/or building services must be surrounded on all sides by program space such as commercial or housing.
- Building faces on the east side of the West Promenade are intended to provide continuity in scale and experience from Street Room Typology 1A and from one side of the street to another.

Street Room Typology 1C

East Promenade and Xerxes Avenue: Transition to Richfield



Similar to the transition strategy on the west side of France Avenue, on the east side of the district, the existing Xerxes Avenue South is recast as the East Promenade to transition between the single family Richfield neighborhood to the east and the more commercially focused Greater Southdale District on the west.

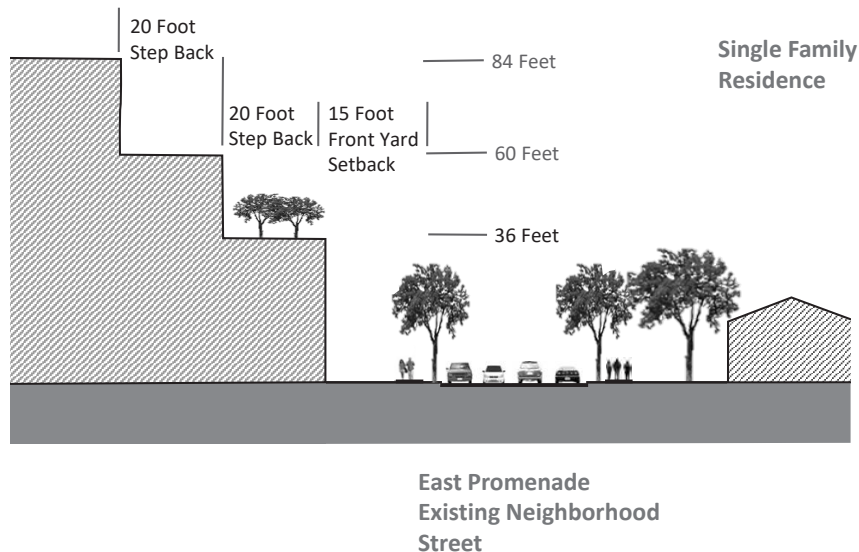
The general character of Xerxes is single family housing on the Richfield side and multi-family residential backed up to commercial on York Avenue. The intent of this typology is to have townhouses along the street,

set back to a taller building above, creating a scale appropriate to the existing character of the street. Xerxes is bisected by Yorktown Park and Adams Hill Park, near the Southdale YMCA. When Xerxes transitions through Yorktown Park and Adams Hill Park, the street becomes more woonerf-like, without vehicular traffic. This street-park hybrid is considered as public open space and needs to be programmed.

This typology extends along the north end of Xerxes between 65th and Highway 62 without the woonerf designation.

Dimensional Characteristics of Street Room Typology 1C

East Promenade and Xerxes Avenue: Transition to Richfield



The street room experience within Typology 1C will be shaped by the following experience guidelines:

- On both Xerxes Avenue north of 65th and on the East Promenade, a 15-foot "front yard" setback is required from curb to face of building.
- Building faces at the East Promenade across the street from single-family homes shall not exceed 36 feet in height. Height above that limit shall step back 20 feet from the facade of the building, and shall not exceed 60 feet in height. Any further height shall step back an additional 20 feet, to a maximum height of 84 feet.
- All parking, other than short-term retail or guest parking, and building services need to be located below grade or hidden within the building. If on ground level or above, parking and/or building services must be surrounded on all sides by program space such as commercial or housing.
- See Street Room Typology 5 for description of dimensional characteristics of new development facing York Avenue.

Street Room Typology 2

Cornelia Overlay at France Avenue

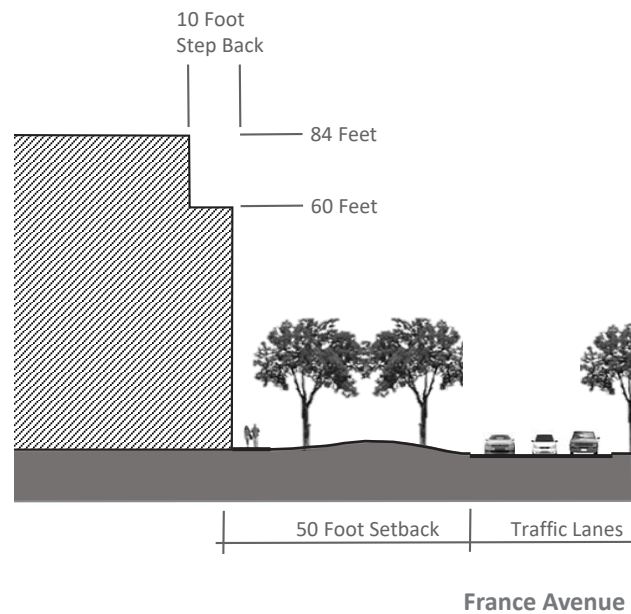


As a reflection of the scale of buildings east of the West Promenade in Street Room Typology 1A, Typology 2 extends the scale of this potential new development to the east side of France Avenue from Gallagher Drive to south of 68th Street. This strategy is intended to frame the street room experience along this corridor with similarly scaled buildings that are respectful of the nearby single family neighborhoods.

The goal is to establish this zone along France as a more commercially-focused corridor (rather than residential) while employing similar height buildings as on the west side of France within the Cornelia transition zone.

Dimensional Characteristics of Street Room Typology 2

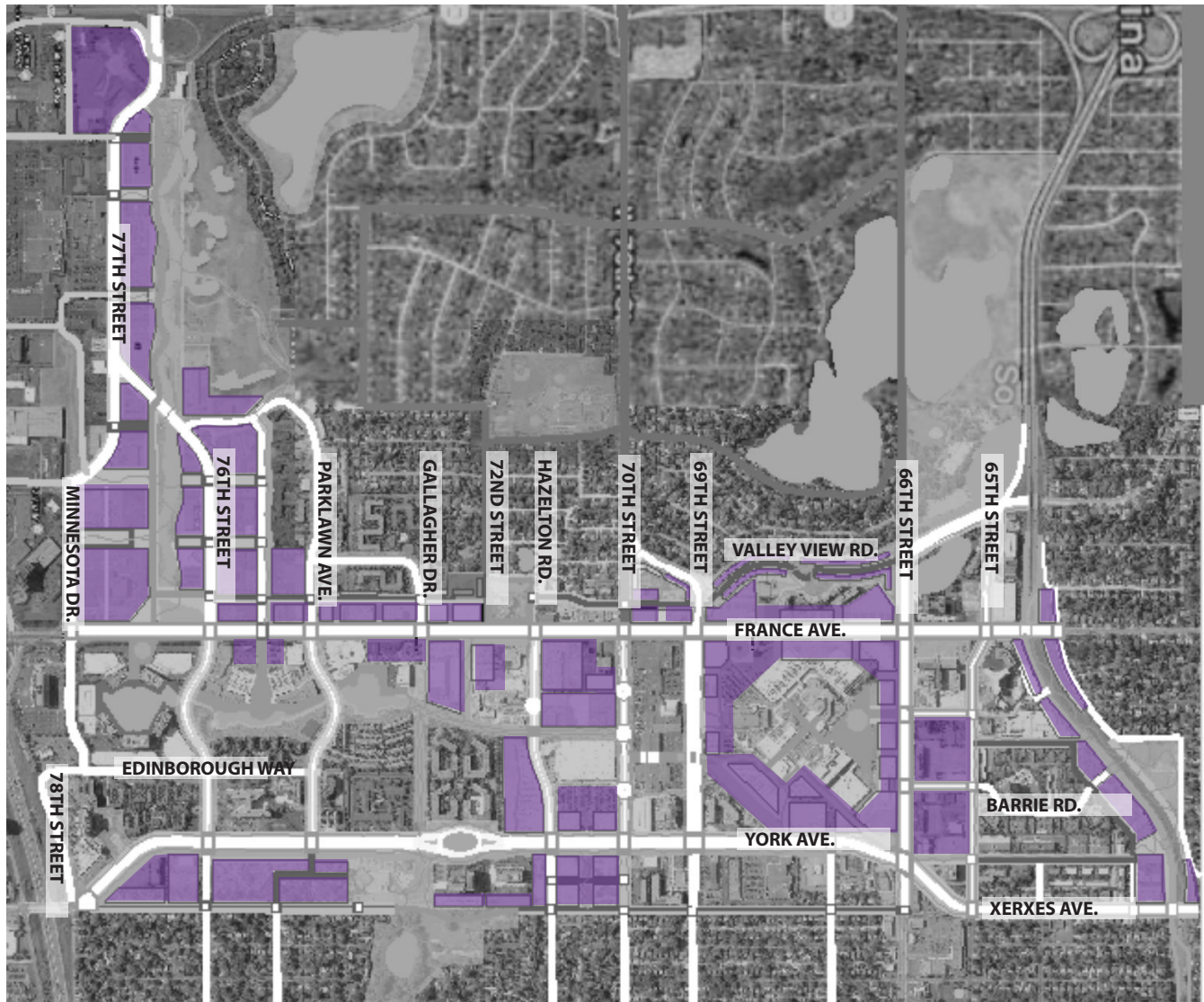
Cornelia Overlay at France Avenue



The street room experience within Typology 2 will be shaped by the following experience guidelines:

- On France Avenue, a 50-foot setback is required from curb to face of building with a building podium height of 60 feet. Above the 60-foot height limit, additional height should step back 10 feet from the face of the building, to a maximum height of 84 feet.
- All parking, other than short-term retail or guest parking, and building services need to be located below grade or hidden within the building. If on ground level or above, parking and/or building services must be surrounded on all sides by program space such as commercial or housing.

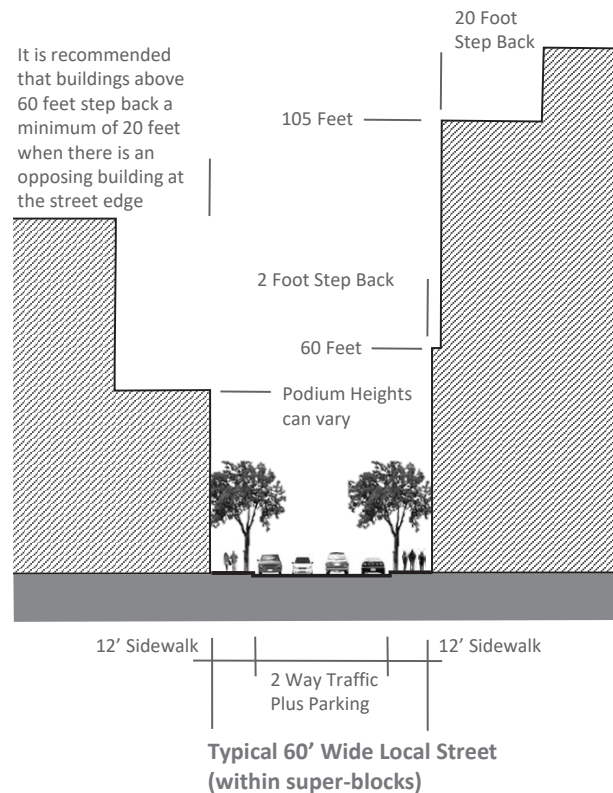
Street Room Typology 3 *New Local Streets*



These new local streets, created as part of the strategy of breaking down the scale of the existing superblocks, will augment the current street network, providing new circulation options that can connect residents and visitors across the district, and support community life. Creating internal pedestrian walkways, with accommodation for bicycles and potentially cars, combined with existing public and private infrastructure, supports connections within and outside the block.

The width of these new local streets, and the corresponding building form is based on the nature of the uses within the larger superblock structure. Streets can be lined with a mix of uses, including residential, commercial, or retail. They contain shady places to walk the dog or sit and have a coffee connecting to neighborhood parks, places of worship, and schools. Unique to the Greater Southdale District, some of these local streets may become linear parks between buildings, with vehicular access limited only to emergency responders.

Dimensional Characteristics of Street Room Typology 3 *New Local Streets*

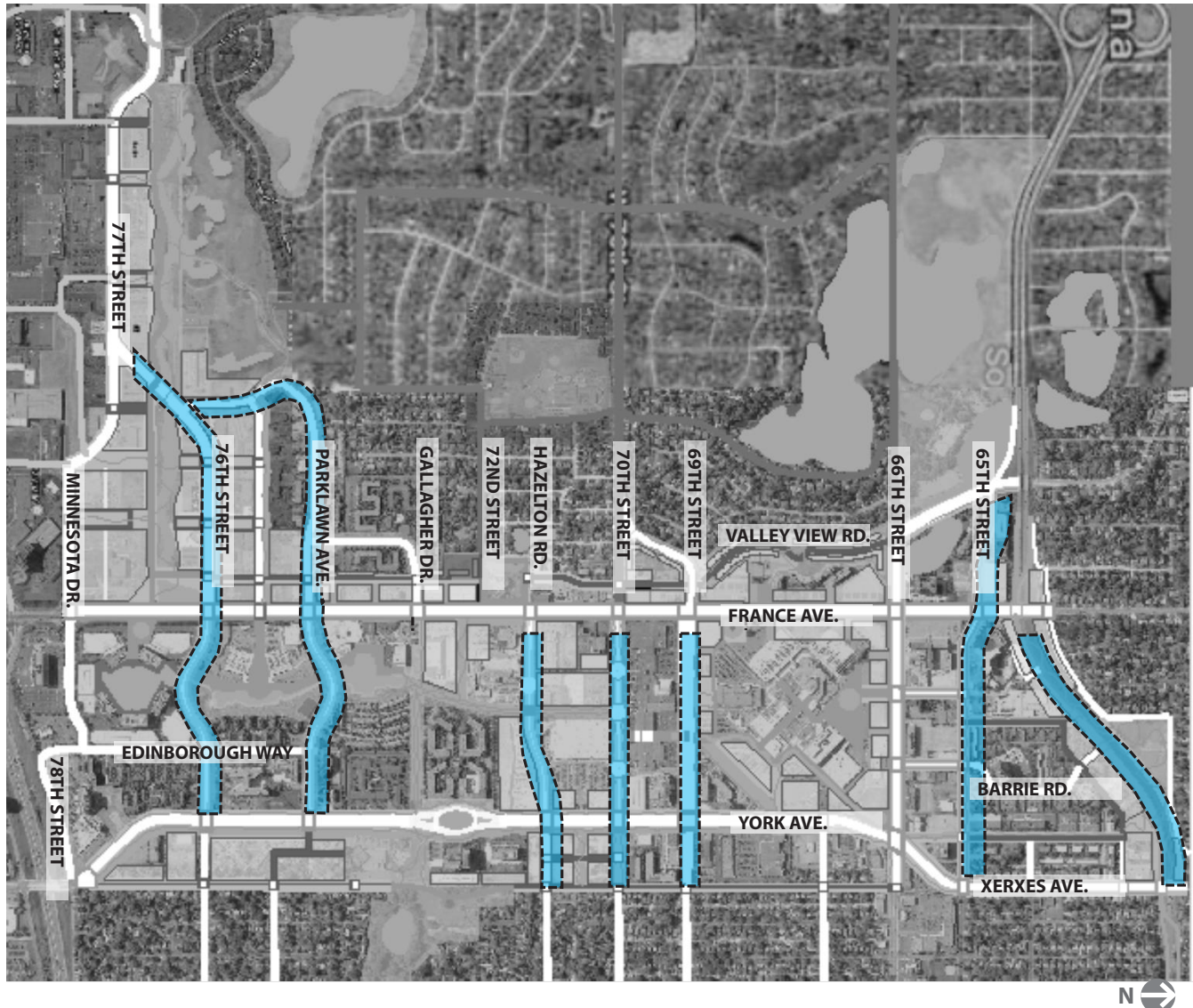


The street room experience within Typology 3 will be shaped by the following experience guidelines:

- New local streets should be 60 feet in width. Those streets which carry vehicular traffic should comprise two traffic lanes with two lanes of parking or pick-up/drop-off. Sidewalks should be located on each side of these vehicular streets as illustrated in the diagram above.
- Minimizing vehicular access to provide drop off, service and parking can be planned to share the vehicular needs of blocks allowing the remaining spaces between the remaining blocks to be used in a variety of ways for the benefit of the community. This “space between” buildings can be transformed into pocket parks, gardens, play areas, plazas, wetlands, and many other activities that support the health and wellbeing of the community.
- Building podium heights can vary, from 36 feet up to 60 feet.
- Above the 60 foot height limit, the long sides of a rectangular or “L” shaped building need to step back 20 feet from the street room facade (as illustrated in the building at left in the diagram above), and the narrow ends need to step back 2 feet from the street room facade (building at right in diagram above). This minimizes the impact of the taller building form on sunlight at the street, and provides a lower-scale building at the street, resulting in a more cohesive and comfortable pedestrian experience. The footprint on taller residential buildings should not exceed 12,000 SF, while taller commercial buildings are permitted larger footprints of up to 24,000 SF for efficient space utilization.
- All parking, other than short-term retail or guest parking, and building services need to be located below grade or hidden within the building. If on ground level or above, parking and/or building services must be surrounded on all sides by program space such as commercial or housing. Vehicular access to the buildings should be as close as possible to primary superblock streets (e.g. Typology 4 or 5).

Street Room Typology 4

Primary East-West Streets



These streets, including 69th Street, 70th Street, Hazelton Road, Parklawn Avenue, and West 76th Street, serve an important role within the district. These are unique streets in that they form the district's superblocks with France and York Avenues, keeping traffic out of adjacent single family neighborhoods while connecting to the neighborhoods through the 30 foot setbacks that are landscaped to provide a pedestrian focused experience.

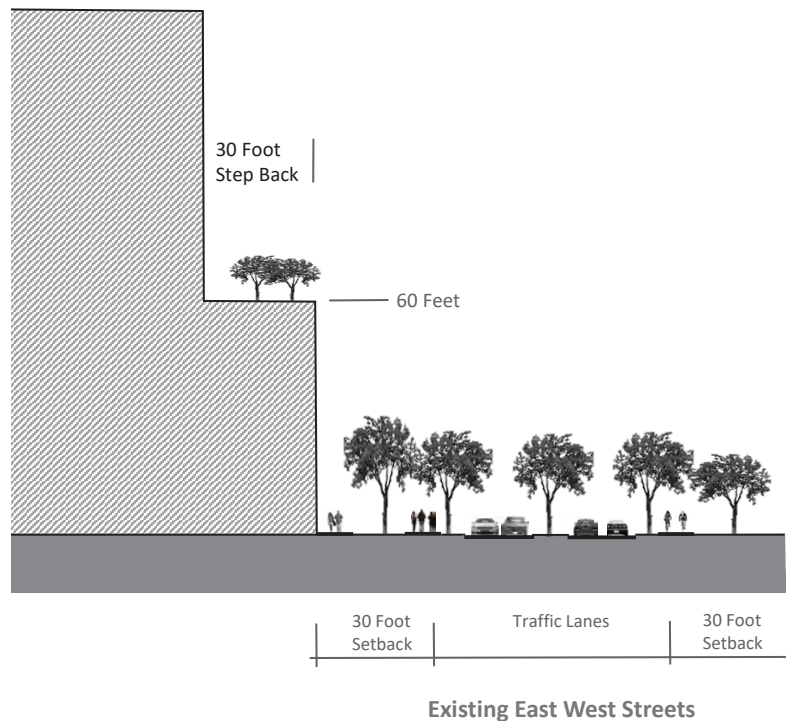
This typology is intended to respect the neighborhood scale and context in a meaningful way, with an ample tree canopy, extensive setbacks and consistently-scaled

buildings at the face of the public realm. By employing these characteristics, the landscape experience of the single family residential neighborhoods is extended through the Greater Southdale District.

Street Room Typology 4 consists of streets that are between 110 and 125 feet wide, with two lanes of traffic in each direction, a center median and no parking. Several of these existing streets feature roundabouts. These streets provide access to parking and building services for buildings in Typology 3, as described previously.

Dimensional Characteristics of Street Room Typology 4

Primary East-West Streets

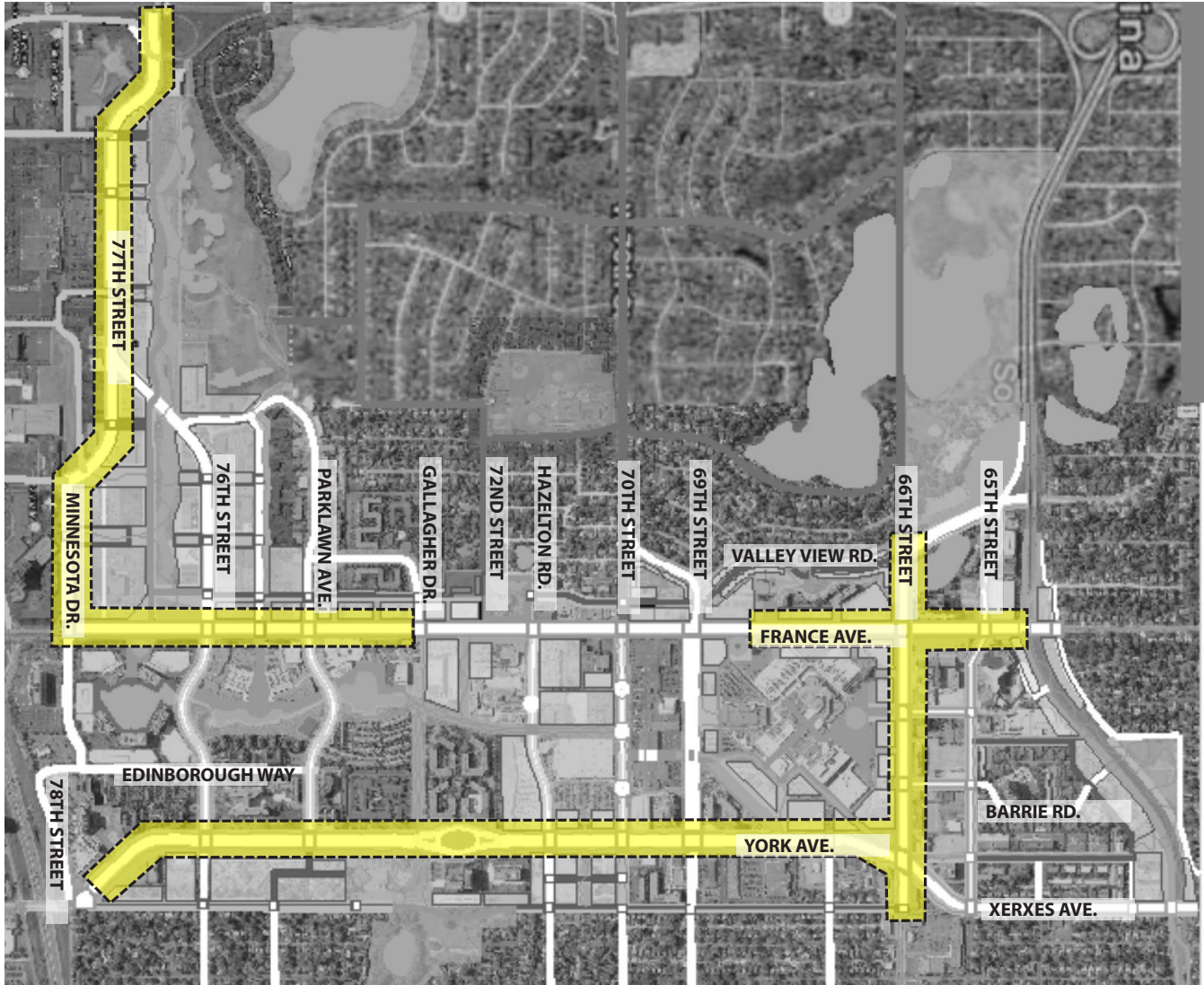


The street room experience within Typology 4 will be shaped by the following experience guidelines:

- Within this typology, a 30 foot setback is required from curb to face of building with a building podium height of 60 feet. Above the 60-foot height limit, additional height should step back 30 feet from the face of the building, to a maximum height of 105 feet. Any height about 105 feet should step back and additional 10 feet from the face of the building.
- Building podiums along these streets need to maintain as closely as possible the 60-foot height limit while still adhering to the guidance of 75% of building face at the setback line to create the fundamental experience of the street room.
- All parking, other than short-term retail or guest parking, and building services need to be located below grade or hidden within the building. If on ground level or above, parking and/or building services must be surrounded on all sides by program space such as commercial or housing.

Street Room Typology 5

The Boulevards



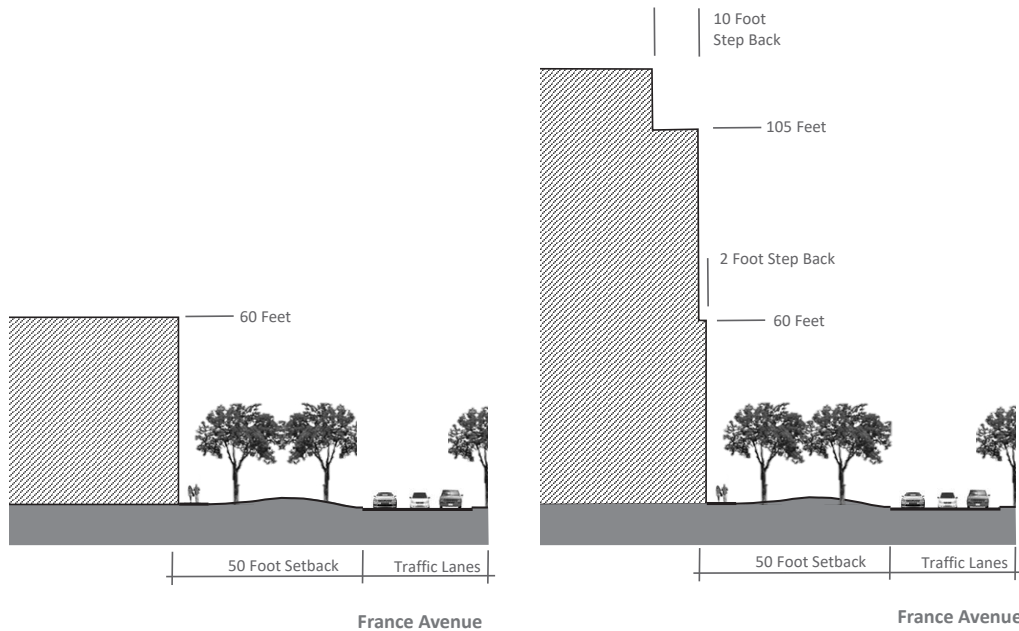
Streets that are included in this typology include the primary district boulevards such as France Avenue, York Avenue, W 66th Street and W 77th Street. In addition to being the widest streets in the district, they also currently carry a high volume of vehicular traffic. The intent of this typology is to create streets that connect the Greater Southdale District to the larger Edina community. These commercially-focused streets will reinforce the district's unique role in serving Edina's neighborhoods, while at the same time, recognizing that the district has a role in the broader metropolitan

region—providing employment, health, retail, entertainment, and a wide range of housing options.

The streets that fall into Typology 5 will have the greatest impact in conveying the overall identity of the district, with wide, multi-use streetscapes lined with a double row of trees within a consistent 50-foot setback. Medians may also be present in the boulevard streetscape to accommodate plantings and/or mass transit lines and stations. In many cases, boulevards will be adjacent to the tallest buildings in the district and will be the locations for transit stops.

Dimensional Characteristics of Street Room Typology 5

The Boulevards



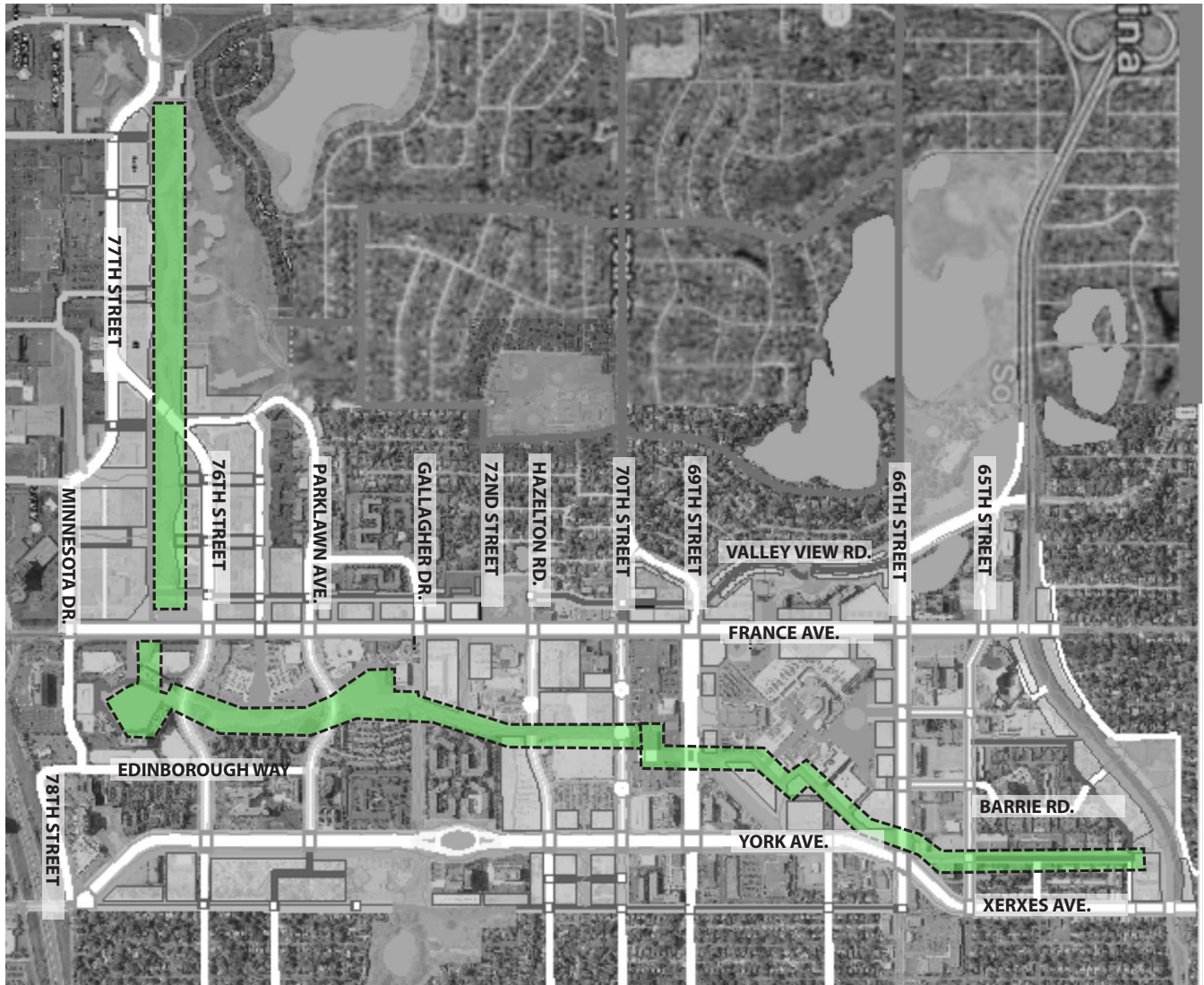
On these wide streets, a sense of scale is maintained by creating a uniform street wall of 60 feet, with taller structures stepping back from this 60-foot datum. This consistency in building heights along the street will form the edge of the street room—bridging between lower intensity and transitional areas, and the higher intensity zones within the Greater Southdale District.

The street room experience within Typology 5 will be shaped by the following experience guidelines:

- On France Avenue, a 50 foot setback is required from curb to face of building with a minimum building height of 60 feet (diagram at left). Above that 60 foot height, the building face should step back two feet to create a cornice line, and can then extend to 105 feet. Above 105 feet, building faces must step back an additional 10 feet (as illustrated in diagram at right, above.)
- Building podiums along these streets need to maintain as closely as possible the 60-foot height limit while still adhering to the guidance of 75% of building face at the setback line to create the fundamental experience of the street room.
- All parking, other than short-term retail or guest parking, and building services need to be located below grade or hidden within the building. If on ground level or above, parking and/or building services must be surrounded on all sides by program space such as commercial or housing.
- Parking and building services should not be accessed via these streets.
- Incorporate 10- to 12-foot wide sidewalks that create opportunities for gathering, outdoor cafes, pavilions, etc.
- Within the 50-foot setback, trees should be planted in a double row to add a strong canopy for pedestrian activity.

Street Room Typology 6

Central Promenade Spine



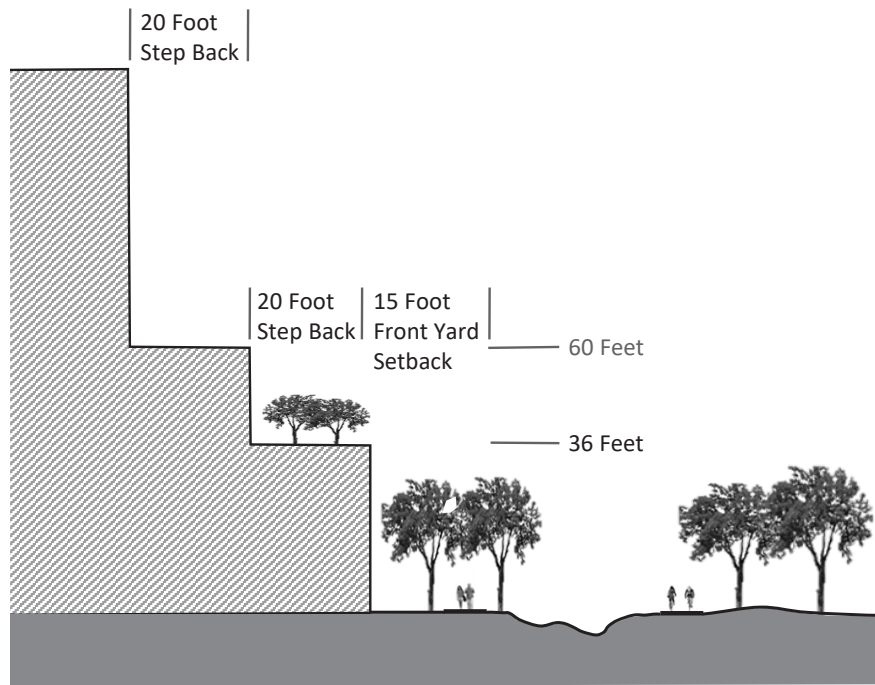
The Central Promenade Spine is intended to connect the Greater Southdale District from the west at Highway 100, extending east to the heart of Centennial Lakes and beyond to Edinborough Park. This Central Spine also extends the existing Promenade north through the Galleria and Southdale Center, and north across a future green lid over Highway 62 to Strachauer Park.

The Central Promenade Spine traverses through a variety of building types, ranging from townhouses to multi-family housing, to low scale commercial/retail

buildings, to mid-rise office buildings. As the physical form of buildings along this spine evolves, natural sunlight light and limited shadow will determine the experiential use of the space. Creating maximum height of 36 feet at its edges will support a mix of uses fronting the spine. Height above this 36 foot limit will step back from the building face, maximizing the program of new buildings rising along its edges without compromising the experience of walking and biking through a park-like environment

Dimensional Characteristics of Street Room Typology 6

Central Promenade Spine



Central Promenade Spine

The street room experience within Typology 6 will be shaped by the following experience guidelines:

- The 36 foot height along the Central Spine encourages a mix of uses focused on entertainment venues such as restaurants, gathering places or community-oriented facilities that provide destinations to come to and stay at. These lower-scale buildings that line the Promenade should reflect its stature as a special community amenity, with a rich variety of architectural experiences that front this park-like environment.
- Above 36 feet, buildings shall step back 20 feet to the 60 foot podium height. Above 60 feet, buildings shall step back an additional 20 feet.
- Buildings above the 60 foot height limit should be oriented to maximize the amount of sun on the Promenade.
- Locations where Typology 6 intersects Typology 4 and 5 are critical to reinforce the idea of the linear public spine that connects this entire district. These intersections are the gateway to the Spine and should have a unique architectural response.
- All parking, other than short-term retail or guest parking, and building services need to be located below grade or hidden within the building. If on ground level or above, parking and/or building services must be surrounded on all sides by program space such as commercial or housing.

5

Implementing and Measuring the Guidance

Ten Things to Remember

5. Implementing and Measuring the Guidance

Ten Things to Remember

1. Every new development begins with the 200' x 200' block, or some variation based on context.
2. Every block or building in a development will need streets to connect between buildings. Not all of these streets will need to accommodate vehicles, providing the opportunity for parks, plazas or courtyards—important parts of the public realm.
3. Buildings will not be greater than 200 feet in length, thereby minimizing the negative impact continuous walls can have on a comfortable pedestrian experience.
4. All streets are not equal. The plan outlines a hierarchy that is driven by the kind of experiences that are expected on these streets and how they facilitate an enlivened public realm.
5. Designated transition zones are about maintaining the quality of life in these areas without restricting growth in other parts of the district.
6. Promenades and East-West Streets are the bridge between single family neighborhoods, such as the Cornelia neighborhood of Edina and the west side of Richfield, to more intense parts of the district.
7. Street Rooms will intersect and overlap each other in many circumstances. At these intersections, lower building heights should prevail, giving the smaller scaled building precedence over larger scale buildings.
8. Building footprints above 60 feet in height are limited to 12,000 SF for residential uses, and 24,000 SF for commercial.
9. Within the first 60 vertical feet of a building, primary materials systems that are more traditional like brick, stone, glass wall systems are preferred. Above 60 feet, other materials such as metal wall systems within a larger curtainwall system, can be introduced. These baseline parameters should not be a deterrent to architectural innovation but rather are intended to serve as a measure of quality and continuity throughout the district
10. Transparency at the ground level facing the public realm is key to the individual experience and is a catalyst for how to activate and maintain a community-based approach to daily life and experience.

